






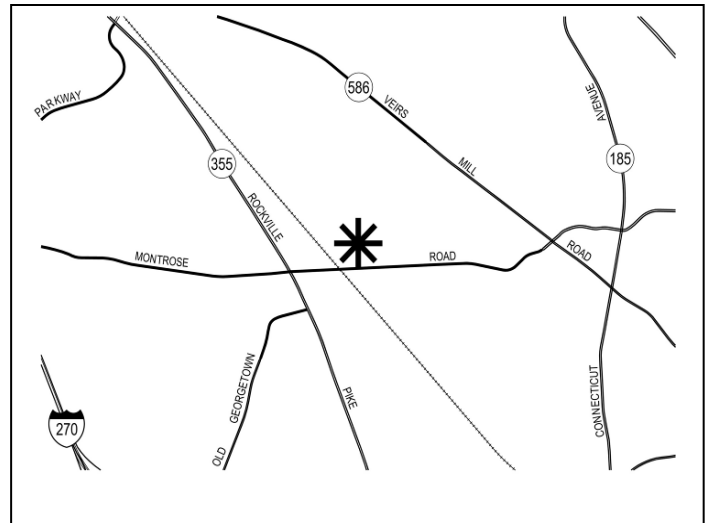
**Greencourt at Parklawn, Site Plan 820130180 (REVISED)**

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**Completed: 10/31/13**

**Description**

- Request to add a new floor (third-story) consisting of 24,727 square feet of Industrial / Research & Development office space to three existing buildings to bring the total square footage to 110,000 (0.99 FAR). An existing deli/restaurant in a fourth building will remain as-is.
- Located on Parklawn Drive, 225' south of Wilkins Avenue on Lots 6 & 17 and parts of 7 & 16;
- On 2.04 net acres in the I-4 Zone, in the Twinbrook Sector Plan;
- Applicant: Stephany Yu, Greencourt LLC;
- Submitted: 02-20-13.



**Summary**

- Staff recommends **approval** of the Site Plan, with conditions.
- Applicant proposes 110,000 square feet; and requests three waivers to the off-street parking standards. Applicant also requests a reduction in the required minimum green area.
- The Subject Property is currently developed with three warehouse buildings used for light-industrial and research and development office space and a free-standing deli/restaurant with surface parking areas on the perimeters of all four buildings. The 0.99 FAR is based on the site's gross tract area of 2.54 acres or 110,623 square feet before dedication to public use.
- The redevelopment proposal will allow for the three warehouse buildings to be fully integrated into one newly constructed building with a ground-level parking garage for 49 spaces at the southern section of the building. Of the existing 123 parking spaces, 74 spaces will remain as-built surface parking.
- Staff has not received any correspondence from noticed parties as of the date of this report.

## SITE PLAN RECOMMENDATION AND CONDITIONS

Staff recommends approval of Site Plan 820130180, Greencourt at Parklawn, for the addition of 24,727 square feet of Industrial/Research and Development space to an existing building, bringing the total square footage to 110,000 square feet (0.99 FAR) on 2.04 net acres in the I-4 Zone. All site development elements as shown on the site, landscape, lighting, and architectural plans received by the M-NCPPC on September 6, 2013, are required, subject to the following conditions:

### 1. Transportation – Pedestrian and Bicycle Facilities

- a. The Site Plan is limited to Research and Development (R&D) office use and the existing 1,305 square-foot restaurant totaling 110,000 square feet.
- b. The Applicant must satisfy the APF test for Transportation Policy Area Review (TPAR) by paying the transportation impact tax of \$205,566 to the County.
- c. The Applicant must enter into a Traffic Mitigation Agreement with the Montgomery County Department of Transportation (MCDOT) and the Planning Board in accordance with the *Twinbrook Sector Plan* requirements to participate in the North Bethesda Transportation Management Organization (TMO) and assist in achieving the *Twinbrook Sector Plan's* non-auto driver mode share. The Traffic Mitigation Agreement must be executed prior to Certified Site Plan.
- d. The Applicant must work with MCDOT to provide a pedestrian crosswalk on the north leg of Parklawn Drive at Wilkins Avenue North as recommended on page 36 of the *Twinbrook Sector Plan*.
- e. The Applicant must provide a total of 12 bicycle parking spaces with six inverted-U bike racks (that can store two bicycles each): one each near the main entrances on Parklawn Drive and Wilkins Avenue; and four in the parking garage for employees in a safe and convenient location.

### 2. Maintenance

The Applicant and subsequent owners must maintain all on-site landscaping, lighting, hardscape, and site elements.

### 3. Financial Security and Agreement

Prior to issuance of a building permit, the Applicant must provide financial surety in accordance with Section 59-D-3.5(d) of the Montgomery County Zoning Ordinance with the following provisions:

- a. The Applicant must provide a cost estimate of the materials and facilities, which upon Staff approval, will establish the initial surety amount.
- b. The amount of the bond or surety must include plant materials, on-site lighting, and green space and amenities.
- c. Prior to issuance of the building permit, the Applicant must enter into a Site Plan Surety Maintenance Agreement with the Planning Board in a form approved by the Office of General Counsel that outlines the responsibilities of the Applicant and incorporates the cost estimate.
- d. The Bond/Surety must be tied to the Development Program, and completion of plantings and installation of particular materials and green area/features covered by the surety for the development will be followed by inspection and reduction of the surety.

4. Architecture

The final exterior architectural character, proportion, materials, and articulation must be substantially similar to the schematic elevations shown on the submitted architectural drawings, as determined by Staff.

5. Development Program

The Applicant must construct the development in accordance with a development program that will be reviewed and approved by Planning Staff prior to the approval of the Certified Site Plan.

The development program must include the following items in the schedule:

- a. Demolition of existing buildings may commence prior to approval of the Certified Site Plan after M-NCPPC inspection and approval of all applicable environmental protection devices.
- b. Street lamps and sidewalks adjacent to the proposed building, and public open space areas, sitting areas and sidewalks must be installed prior to the release of the use-and-occupancy permit for the new building. Street tree plantings may wait until the next planting season.
- c. Community space must be installed before the release of the use-and-occupancy permit.
- d. Clearing and grading must correspond to the construction phasing to minimize soil erosion and must not occur prior to approval of the Sediment Control Plan, and M-NCPPC inspection and approval of all applicable environmental protection devices.
- e. The development program for installation of on-site landscaping and lighting.
- f. Phasing for dedications, stormwater management, sediment and erosion control, and other features, as applicable.

6. Certified Site Plan

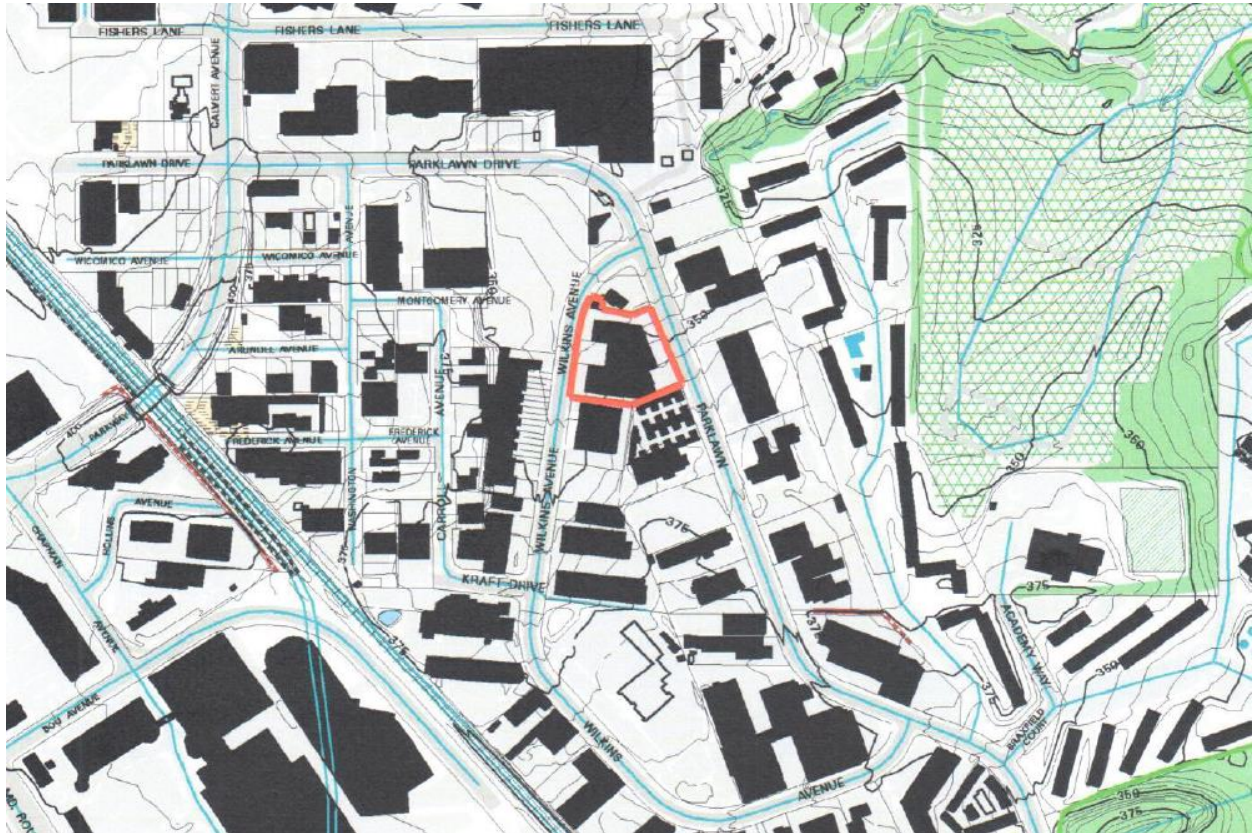
Before approval of the Certified Site Plan the following revisions must be made and/or information provided, subject to Staff review and approval:

- a. Development program, inspection schedule, and Resolution approving this Site Plan on the approval or cover sheet of the Certified Site Plan set.
- b. Remove unnecessary plan sheets, as required by Staff.
- c. Make corrections and clarifications to details, labeling, data tables, and schedules, as required by Staff.
- d. Ensure consistency of all details and layout for both the Site and Landscape/lighting plans.
- e. The Applicant must submit a LEED Check List for the proposed Green Roof with the Certified Site Plan.
- f. Address the comments in the Montgomery County Department of Transportation's (MCDOT's) letter dated October 24, 2013.

## SITE DESCRIPTION

### Site Vicinity

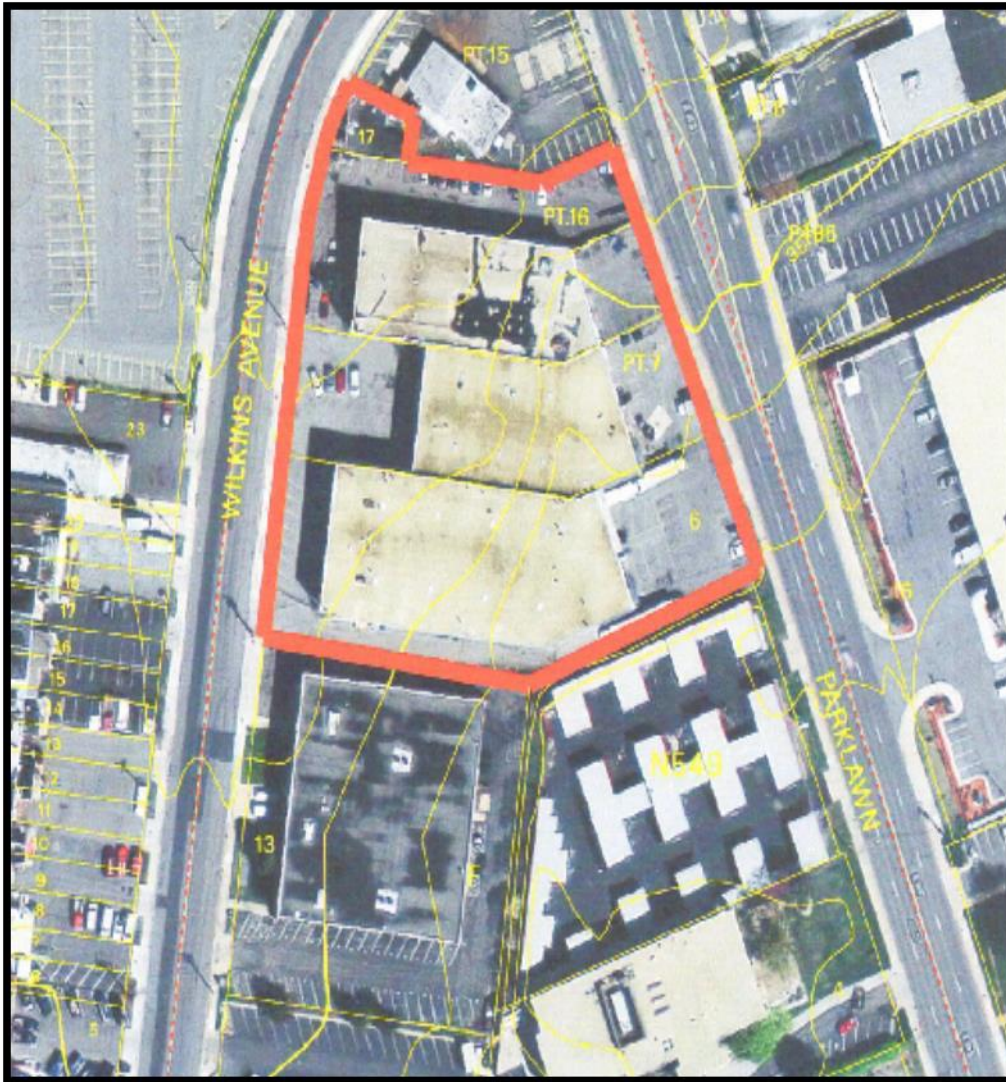
The Subject Property (Property) is located on Parklawn Drive, 225 feet south of Wilkins Avenue. All adjacent properties in the vicinity are developed with office, light industrial and mixed-uses, including surface parking lots. The site is within the Transit Station Development Area (TSDA) of the Twinbrook Sector Plan area. The Twinbrook Metrorail Station is located within one-half mile.



Site Vicinity Map

### Site Description

The Property is in the I-4 Zone and consists of two existing lots: Lot 6 and 17 and part of Lots 7 and 16 on a record plat titled Washington-Rockville Industrial Park.



**Site Location Aerial Photo**

The site contains 2.04 acres and is a 'through lot' with frontage on both Wilkins Avenue and Parklawn Drive. Formerly, the site contained 2.54 acres or 110,623 square feet; dedications to public use left the site with 2.04 acres or 88,961 square feet. Existing on-site improvements include three separate warehouse buildings and a stand-alone deli/restaurant totaling 85,273 square feet. Surface parking for customers and employees surrounds the buildings. Wilkins Avenue has existing metered, on-street parking. The Property has vehicular access onto Parklawn Drive and Wilkins Avenue.

## **PROJECT DESCRIPTION**

### **Previous Approvals**

#### Twinbrook Sector Plan

The 2009 *Twinbrook Sector Plan* identified the Property as within the Light Industrial Area (Area 9) of the Plan.

#### Natural Resources Inventory/Forest Stand Delineation

The Subject Property is exempt from the requirements of submitting a Forest Conservation Plan per an approved Forest Conservation Exemption, #42013093E, dated January 2, 2013. The 2.04-acre site contains no streams, wetlands, forests, or other environmental features. The property is located within the Lower Rock Creek watershed, which is designated as a Use Class I-P Waters by the State of Maryland.

#### Record Plat

The Applicant submitted minor subdivision Record Plat 22013091 to consolidate the two lots and two parts of lots into one lot of record, which is currently under review.

### **Proposal**

The Applicant seeks approval to convert the three existing warehouse buildings along with the surface parking lots into one fully integrated building that will contain approximately 108,695 square feet of light-industrial R&D space with amenities. The existing 1,305 square feet stand-alone restaurant will remain as-built.

A total of 123 off-street parking spaces are proposed; 49 of these spaces will be located in a covered garage incorporated in the ground-level of the southern section of the newly integrated building. The remaining 74 parking spaces will remain on the site's existing surface parking areas. A new third-story addition to the integrated structure will increase the height from the current two stories to the maximum allowable 42 feet. The proposal includes a new central atrium/lobby adjacent to a landscaped entry courtyard.

The proposal includes construction of two new entrances: a larger entrance located on the Wilkins Avenue side; and a smaller entrance located on the Parklawn Drive side. Both entrances will have atriums with decorative plantings, water features and stone benches for visitors and building occupants. The atrium entrances along with on-site planting islands/beds amount to 7,964 square feet of green area or 7.2% of the site's gross tract area. The remaining 2.8% green area requirement is being met by providing off-site streetscape improvements on Parklawn Drive and Wilkins Avenue, in the same TSDA. The proposed third floor addition will include two unenclosed pedestrian walkways. The larger of the two walkways is on the Wilkins Avenue side of the building and will connect the two sides of the building. The walkways' square footage is not included in the FAR calculation because these two features are not enclosed.

The Site Plan contains a proposed Green Roof plan, the design and extent of which is part of the Stormwater Management Concept Plan review by Montgomery County Department of Permitting Services (DPS).

The proposed Streetscape Plan will improve the existing street frontage along Parklawn Drive (Figure 1) with on-site planting beds adjacent to right-of-way where feasible. It will also improve the existing condition in both the Wilkins Avenue (Figure 2) and Parklawn Drive rights-of-way with the addition of green panels for shade/street trees.

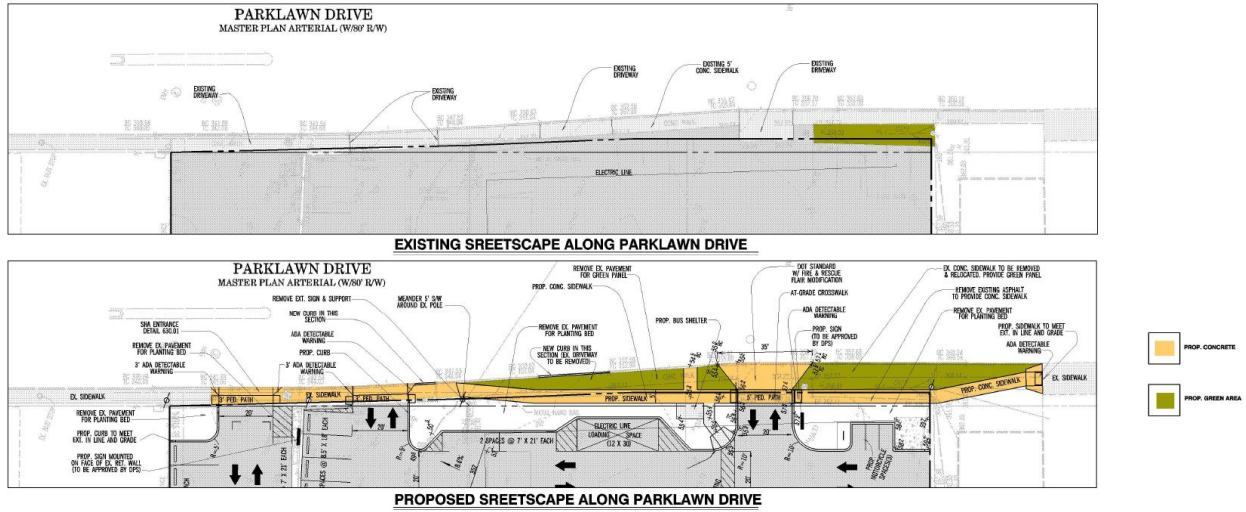


Figure 1

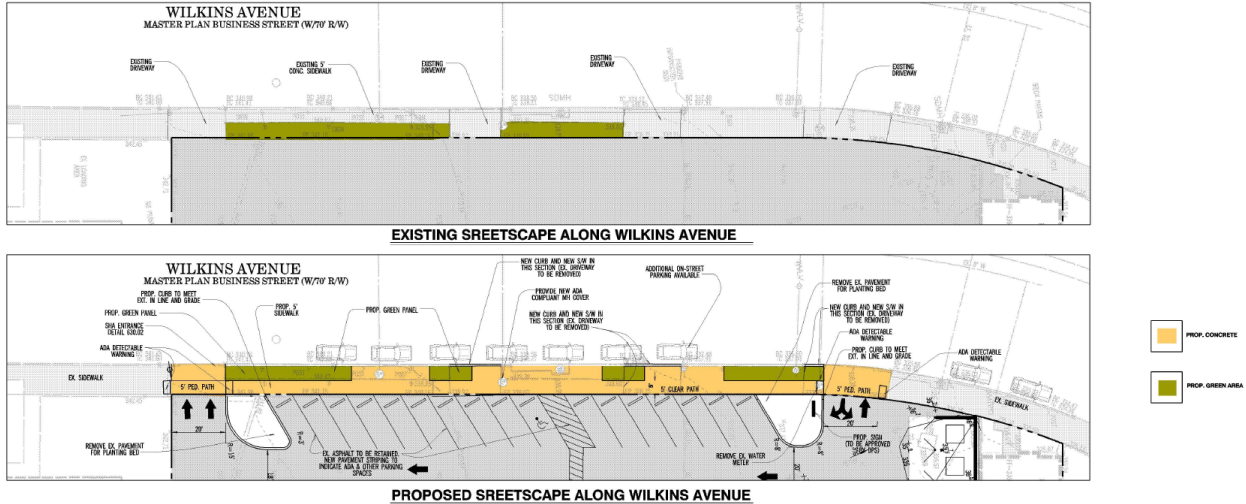


Figure 2 Wilkins Streetscape Plan 'Alternate B'



**View towards Wilkins Avenue**

**Figure 3: An example of one of the two proposed open air (unenclosed) pedestrian walkways.**





Proposed Site Plan (the green area shows the proposed green roof portion).

## COMMUNITY OUTREACH

The Applicant has complied with all submittal and noticing requirements.

## SITE PLAN ANALYSIS AND FINDINGS

### Development Standards

The following data table shows the application's conformance to the development standards applicable to an I-4 zoned property in a Transit Station Development Area.

*Table 1: Development Standards Data Table*

<b>PLAN DATA</b>	<b>Required/Permitted</b>	<b>Shown on proposed the Site Plan</b>	<b>Approved</b>
<b>Maximum Height</b> (59-C-5.31)	42 ft.	39 ft.	Up to 42 ft.
<b>Minimum Building Setbacks:</b>			
From adjoining industrial zone (59-C-5.35(b))	10 ft.	13.7 ft.	13.7 ft.
From mixed-use zones (from TMX-2 Zone across Wilkins Ave.) (59-C-5.44(f)(3))	50 ft.	108.4 ft.	108.4 ft. <sup>1</sup>
From streets (Wilkins Avenue) (59-C-5.44(f)(4)(B))	10 ft.	38.4 ft.	38.4 ft. <sup>2</sup>

<sup>1</sup> As measured from the closest corner of the building in relation to Wilkins Avenue (at the northwest corner).

<sup>2</sup> As measured from the western side of the building to the Wilkins Avenue right-of-way.

<b>PLAN DATA</b>	<b>Required/Permitted</b>	<b>Shown on proposed the Site Plan</b>	<b>Approved</b>
From streets (Parklawn Drive) (59-C-5.44(f)(4)(B))	10 ft.	60 ft.	60 ft. <sup>3</sup>
<b>Maximum Density</b> (59-C- 5.44(c))	1.0 FAR or 110,623sf (GFA)	up to 0.99 FAR or 110,000sf GFA	up to 0.99 FAR or 110,000sf GFA
<b>Minimum Green Area %</b> (59-C-5.32)	20% of GFA <sup>4</sup>	10%	10%
<b>Minimum Off-Street Parking Spaces</b> (59-E-3.7) 1.5 spaces/1,000sf GFA	179 <sup>5</sup>	123	123 <sup>6</sup>
<b>Minimum Off-Street Parking Setback</b> from the adjacent I-4 Zone along the north property line (59-C-5.44(d)(2))	10 ft. <sup>7</sup>	0 <sup>8</sup> (zero) ft.	0 (zero) ft.

<sup>3</sup> As measured from the northeast corner of the building in relation to Parklawn Drive.

<sup>4</sup> Pursuant to Section 59-C-5.44(f)(6), the Planning Board may approve a reduction of the green area requirement to no less than 10% of the gross tract area. A maximum of 50% of the required green area may be located off-site in the same Transit Station Development Area.

<sup>5</sup> In addition to the 165 parking spaces required for the approved building, 14 parking spaces are required for the existing deli/restaurant based on the amount of square footage of the patron area.

<sup>6</sup> Waiver required for the Board to approve fewer than the minimum number of parking spaces required per 59-E-4.5.

<sup>7</sup> Pursuant to 59-C-5.44(f), the Planning Board may waive the minimum parking setback requirement of 59-E to achieve a better development design.

<sup>8</sup> Waiver required for the Board to approve less than the minimum required parking setback per footnote 7.

<b>PLAN DATA</b>	<b>Required/Permitted</b>	<b>Shown on proposed the Site Plan</b>	<b>Approved</b>
<b>Minimum Off-Street Parking Setback (from street)</b> (59-C-5.44(d)(3))	25 ft. <sup>9</sup>	0 <sup>10</sup> (zero) ft.	0 (zero) ft.
<b>Minimum Bicycle Parking Spaces</b> (59-E-2.3)	9	12	12

In addition to the above development standards, the Subject Property, at over 2 acres in size, exceeds the recommended minimum lot size of 5,000 square feet expressed in Section 59-C-5.44(f)(2).

Requested Waivers

Section 59-C-5.44(f), Special Standards -Transit Station Development Area (TSDA) authorizes the Planning Board to apply six special review standards for a lot in an I-4 Zone in a TSDA through the site plan approval procedures of Division 59-D-3. This section also gives the Planning Board authority to grant waivers to special standards. For example, the Planning Board may approve a lesser building setback from a mixed-use zone if it finds that a lesser setback would not have an adverse impact on adjoining mixed-use zoned property. These special review standards include:

- 1) area requirements,
- 2) setbacks from mixed-use and residential zones,
- 3) setbacks from a street,
- 4) off-street parking,
- 5) green area and
- 6) accessory residential units.

Special review standards 2 through 5 are applicable to this proposal. The proposal meets the following standards from the above list:

- 2) setbacks from mixed-use and residential zones; and
- 3) setbacks from a street.

However, the proposal does not meet the off-street parking and the green area standards due to site constraints and the Applicant’s intentions to redevelop the site by retaining most of the three warehouse buildings’ existing footprints. Therefore, three waivers to these development standards are necessary. Requested waivers are described in the Applicant’s Amended Statement of Justification (Attachment A) and include: one for the minimum required off-street parking spaces; and two for the minimum required parking setbacks. Additionally, Applicant requests that the Board reduce the required green area percentage to 10%, pursuant to Section 59-C-5.44(f)(6). Applicant believes that these waivers and the reduction to the minimum green space area will help achieve the type of

<sup>9</sup> Pursuant to 59-C-5.44(f), the Planning Board may waive the minimum parking setback requirement of 59-E to achieve a better development design.

<sup>10</sup> Waiver required for the Board to approve less than the minimum required parking set back per footnote 9.

redevelopment envisioned in the *Twinbrook Sector Plan*. Staff agrees, and supports the Planning Board's waiver of off-street parking standards and the reduction in required green area (discussed in more detail below), because strict compliance to these requirements will not allow this proposal, which is consistent with the type of project envisioned by the Sector Plan.

The Site Plan requests an off-street parking reduction of 31% of the required 179 parking spaces. The basis of the waiver is: 1) the site's proximity and accessibility to the Twinbrook Metro Station; 2) the provision of transportation-related improvements both on and off-site to facilitate increased transit use by occupants of the Property and others in the surrounding area; and, 3) the existence of on-street parking along Wilkins Avenue (Attachment A, page 6). Staff concurs with the basis for this waiver.

In addition to the proposed reduction of off-street parking spaces from 179 to 123, the Applicant also requests two parking setback waivers: one in relation to an adjacent zone and, one in relation to the streets. The required parking setback to an adjacent zoned property is a 10-foot minimum. The waiver request is for no provision of a setback (or zero). The required setback in relation to the streets is a minimum of 25 feet, again, no (or zero) setback is proposed. Staff concurs with the Applicant's justification for all three of these waiver requests because, providing the required parking setbacks is not possible without losing more off-street parking due to the existing building(s) layout. Pursuant to 59-C-5.44(f)(5), waivers to the parking setbacks will allow Applicant to "achieve a better development design."

#### Green Area Reduction

The Applicant submitted a Streetscape Plan 'Alternate B' to mitigate the visual impact of surface parking that will remain as-is. The Plan includes a request to reduce the required 20% green area to 10%. The Applicant's request is based on the fact that there is insufficient land area for the Plan to meet the 20% green area requirement and maintain adequate vehicular circulation and surface parking (Attachment A, see page 8). The Applicant has obtained agency approvals from the respective utility companies acknowledging utility easements are not needed along the site's roadway frontage on both Parklawn Drive and Wilkins Avenue (see Attachment D).

Staff supports the requested reduction to green area and agrees that, in order to achieve better site design as envisioned by the Sector Plan, the requested reduction to 10% green area is appropriate. It should be noted the streetscape proposed for both Parklawn Drive and Wilkins Avenue will provide green area in the public right-of-way, which is considered to be off-site. However, this area will be located in the same TSDA. In addition, better site design will be achieved with this proposal because retention of the existing warehouse buildings (footprints) is a recommendation in the Sector Plan. The renovation of these three buildings also means two other Sector Plan recommendations will be achieved: 1) a street-oriented building and 2) a new streetscape.

#### Landscape and Lighting

The Landscape Plan includes three deciduous tree species along the sidewalks on Parklawn Drive and Wilkins Avenue, along with ornamental grasses and evergreen shrubs. The two proposed atrium areas will have bamboo in planters and vine plantings. The proposed landscaping in the atrium areas will contribute to the site's green area.

A Lighting Plan shows that the proposal will meet the industry standards and provide for safe, efficient and adequate lighting, the latter during evening hours.

## General Requirements

### Sector Plan and Design Guidelines Conformance

The Approved and Adopted (2009) *Twinbrook Sector Plan* included the Property within the Light Industrial Area (Area 9) of the Plan. The Sector Plan notes that the area surrounding “Carroll Avenue, Wilkins Avenue, and Parklawn Drive includes a mix of light industrial uses, some that serve technology businesses, others traditional light industrial and service commercial uses. It is also an area that can meet the needs of start-up businesses” (p.42). The Sector Plan supports the preservation of light-industrial zone properties, such as the subject Site Plan, and recommends modifications to the Light Industrial, Low Intensity (I-4) Zone to address “development standards suitable for industrial uses on small sites” (p.43).

The Sector Plan made no specific recommendations for the Property. However, the following Sector Plan recommendations are applicable:

- Allow parking waivers in this area, recognizing its proximity to transit.
- Encourage redevelopment projects to contribute to the area’s environmental and urban design goals, including accommodating stormwater management (p.43).

The proposed project represents the type of industrial redevelopment envisioned in the Sector Plan. It will implement some of the Plan recommendations, including reusing industrial buildings, providing street-oriented development, and new streetscape.

### Light Industrial, Low Intensity (I-4) Zone

The Sector Plan recommended an amendment to the I-4 Zone “to increase development flexibility and provide an accessory residential use option in this area of urban-oriented, light industrial uses” (p.46). *Section 59-G-5.44 (F) Special Standards-Transit Station Development Area* in the I-4 Zone was created to allow for development, such as the proposed project. Two non-site specific recommendations for the I-4 Zone are as noted above in the **Sector Plan and Design Guidelines Conformance** part of this report and these further reinforce the flexibility that was intended for redevelopment proposals.

### Building Heights and Density

The proposed building height at 39 feet is consistent with the Sector Plan’s recommendations for the Light Industrial Area 9. As proposed, the total development at 0.99 FAR is consistent with the Plan’s development limits for the light industrial area.

### Street, Pedestrian and Bikeway Network

Wilkins Avenue and Parklawn Drive provide western and eastern access to the site, respectively. Parklawn Drive is classified as an arterial roadway (A-64) with a minimum right-of-way of 80 feet and Wilkins Avenue is classified as a commercial business street (B-15) with a minimum 70-foot right-of-way. The proposed Site Plan shows 35 feet of r-o-w from the centerline of Wilkins Avenue for a 70 feet right-of-way and 35 feet from the centerline of Parklawn Drive with an additional five feet of r-o-w as sidewalk. Both of the Property’s roadway frontages will be enhanced with new streetscape, including new street trees and planting area.

The Sector Plan makes several recommendations for the pedestrian network, including:

- Construct streetscape sidewalks throughout the Plan area. They should be eight to 10 feet wide, from the curb, and buffered with street trees.

- Redevelopment projects should devise a streetscape plan that includes bicycle facilities, and coordinates all aspects of the street and building relationship to create pedestrian safety, comfort, and convenience.
- Buildings should be sited and designed to create clear street front entrances and active street facades (p.15).

The Proposal will improve both Parklawn Drive and Wilkins Avenue frontages with new sidewalks, new planting beds, street trees and the provision of a new bus shelter on Parklawn Drive relocated from the property to the north. Along Parklawn Drive, the Sector Plan recommends a bike lane (BL-27) as well as the Countywide Bikeways Functional Master Plan (2005). The Countywide Bikeways Master Plan states that the implementation of the bike lane would require “reduced lane widths or wider road to accommodate the bike lanes” (p.57). There is no recommendation in either plan for any bikeways on Wilkins Avenue.

### Environment

The Sector Plan’s environmental goal is “to move beyond the benefits of location through green building and site design” (p.28). Specifically, the Plan recommends:

- Increase the amount of pervious surface to improve the quality and reduce the quantity of stormwater run-off, mitigate heat island effects, and contribute to a green and pedestrian-friendly environment.
- A significant portion of paved area should be shaded through trees within five years of occupancy or paved with reflective materials.
- Improve air quality through development decisions that increase tree canopy and pervious surfaces, and the use of reflective materials.
- Street trees should be spaced and sized to create 30 percent canopy coverage on redeveloped areas, and open space should be planted and substantially pervious (p.28).

A partial green roof, street trees and a tree bed along Parklawn Drive will further the Plan’s environmental recommendations. Staff recommends that the Applicant submit a LEED checklist at Certified Site Plan.

### Urban Design Compliance

The proposed development is consistent with the Planning Board approved *Twinbrook Urban Design Guidelines* (2010), which provide design guidance for streets, open space and buildings. For the light industrial area, the Guidelines support adaptive reuse of light industrial uses and the creation of an appropriate pedestrian-oriented environment. The Guidelines note that “several streets are built to standards that pre-date the Road Code” but streetscape improvements should be made along Parklawn Drive (p.33).

The proposed green area with a water feature implements the Guidelines recommendation that open spaces benefit the occupants of a building (p.33). Further, the proposed green roof is consistent with the Guidelines’ environmental recommendation.

Regarding parking, the Guidelines promote limiting “parking supply through the Zoning Ordinance based on Metro Station proximity and allow parking waivers in the Light Industrial Area. It encourages shared and structured parking rather than surface lots” (p.22). This Site Plan is seeking parking waivers, which is supported by the Guidelines.

The Guidelines also state that surface parking should be located at the “rear or side of building, away from primary streets and sidewalks. Surface parking should not be visible from primary streets. Considerations should also include: measures to reduce heat island warming and tree canopy and permeable areas to treat stormwater” (p.23). The proposed Site Plan moves 49 of the 123 existing surface parking spaces into a parking garage that will be integrated into the building renovation and, additional canopy trees will be provided in the remaining surface parking areas, thus achieving these Guidelines.

The proposed architecture shows how the parking garage will reduce approximately one-third of the existing surface parking, so it will no longer be visible from the street. These architectural elevations demonstrate the Applicant’s intention to construct a modern building that is mostly transparent to allow for natural light inside and to display the building’s interior framework.



**View from Wilkins Avenue**



**View from Parklawn Drive**



## **Transportation**

### Transportation Demand Management

The Property is within the boundary of the North Bethesda TMD. As new development, the Applicant will be required to enter into a Traffic Mitigation Agreement with the Planning Board and MCDOT and assist in achieving and maintaining the *Twinbrook Sector Plan's* 39% non-auto driver mode share goal for employees.

### Public Transit Service

Transit service is available along Parklawn Drive with four bus routes:

1. Ride-On route 10 operates with 30-minute headways between the Twinbrook Metrorail Station and Hillandale (at New Hampshire Avenue/Powder Mill Road intersection).
2. Ride-On route 44 operates with 30-minute headways between the Twinbrook Metrorail Station and Rockville Metrorail Station.
3. Metrobus route C-4 operates with 20-minute headways between the Twinbrook Metrorail Station and Greenbelt Metrorail Station.
4. Metrobus route J-5 operates with 30-minute headways between the Twinbrook Metrorail Station and Silver Spring Metrorail Station.

Transit service is not available along Wilkins Avenue.

### Pedestrian and Bicycle Facilities

The Applicant is upgrading the existing sidewalks along its Parklawn Drive and Wilkins Avenue frontages. On-site pedestrian circulation will be as follows:

1. Green panels are proposed to provide a physical separation between the sidewalks and curbs.
2. The sidewalk crossings on the site's driveways are proposed to be ADA-compliant/at-grade crossings.
3. An ADA-compliant lead-in sidewalk is provided from the Wilkins Avenue sidewalk to the west entrance.
4. Handicap ramps are provided to be ADA-compliant for the lead-in sidewalk from Parklawn Drive.

The Applicant is providing 12 bicycle parking spaces using six inverted-U bike racks. One bike rack is provided for each of the two main entrances facing Parklawn Drive and Wilkins Avenue. The other four bike racks are located in the parking garage near the entrance in a well-lit area.

### Local Area Transportation Review (LATR)

The proposed redevelopment would generate a maximum of 143 peak-hour trips during the weekday morning peak period (6:30 to 9:30 a.m.) and 119 peak-hour trips during the weekday evening peak period (4:00 p.m. to 7:00 p.m.):

Land Use	Square Feet	Weekday Peak-Hour	
		Morning	Evening
<b>Proposed Land Uses</b>			
Smokey's Restaurant	1,305	10	3
R&D Office Space	108,695	133	116
<b>Subtotal</b>	<b>110,000</b>	<b>143</b>	<b>119</b>
<b>Existing Land Uses</b>			
Smokey's Restaurant	1,305	10*	3*
General Light Industrial Use	83,968	49*	39*
<b>Subtotal</b>	<b>85,273</b>	<b>59</b>	<b>42</b>
<b>Net Increase</b>	<b>24,727</b>	<b>84</b>	<b>77</b>

As indicated by an asterisk\* in the table above, the existing number of site-generated peak-hour trips was derived from traffic counts at the existing driveways from Parklawn Drive and Wilkins Avenue, and adjusted to reflect the highest previous tenant occupancy level rather than the lower current occupancy level.

The Applicant submitted a traffic study to satisfy the LATR test because the proposed development generates 30 or more total peak-hour trips within the weekday morning and evening peak hours. The table below shows the calculated Critical Lane Volume (CLV) values at the analyzed intersections for the following traffic conditions. The future Parklawn Drive/Montrose Parkway intersection was not analyzed because the Consolidated Transportation Plan project by the State Highway Administration has no construction funding at this time.

1. Existing: Existing traffic conditions as they exist now.
2. Background: The existing condition plus the trips generated from approved but un-built nearby developments.
3. Total: The background condition plus the site-generated trips.

Analyzed Intersection	Weekday Peak Hour	Congestion CLV Standard	Traffic Condition		
			Existing	Background	Total
Parklawn Drive & Wilkins Avenue (North)	Morning	1,800	289	404	426
	Evening	White Flint	337	518	531
Parklawn Drive & Randolph Road	Morning	1,550	1,038	1,266	1,293
	Evening	North Bethesda	1,157	1,432	1,445

As shown on the table above, the CLV values at the two intersections are less than the applicable congestion standard for these intersections. Thus, the LATR test is satisfied.

Transportation Policy Area Review (TPAR)

The Applicant must satisfy the TPAR test by paying the transportation impact tax at 25% of the total development impact applicable in the North Bethesda Policy Area. With the existing restaurant of 1,305 square feet proposed to be retained, the impact tax would be as follows:

Non-Residential Use	2013 Rate per Square Feet	Proposed Square Feet	Transportation Impact Tax
Proposed (R&D) Office	\$12.30	108,695	\$1,336,949
Existing (Light) Industrial	\$6.15	-83,968	-\$ 514,687
		24,727	\$ 822,262
25% of the Net Increase			\$ 205,566

## Environment

### Environmental Guidelines

The 2.04-acre site contains no streams, wetlands, forests, or other environmental features. The Property is located within the Lower Rock Creek watershed, which is designated within a Use Class I-P Waters<sup>11</sup> by the State of Maryland.

### Forest Conservation

The Property is exempt from the requirements of submitting a Forest Conservation Plan per the approved Forest Conservation Exemption, #42013093E, dated January 2, 2013, because per Chapter 22A, Section 22A-5(t) of the Forest Conservation Law, the proposed project is a modification to an existing developed property and (1) the modification would not remove more than 5,000 square feet of forest, (2) the modification does not affect any forest in a stream buffer or forest located on property in a special protection area, which would require a water quality plan, and (3) the modification does not require approval of a new Subdivision Plan.

### Stormwater Management

The Department of Permitting Services issued a letter accepting the Stormwater Management Concept for the Property on June 7, 2013 (Attachment C). The stormwater management concept proposes to meet required stormwater management goals via a Green Roof, micro biofilters and a stormfilter with volume storage.

## FINDINGS

1. *The site plan conforms to all non-illustrative elements of a development plan, diagrammatic plan, and all binding elements of a schematic development plan, certified by the Hearing Examiner under Section 59-D-1.64, or is consistent with an approved project plan for the optional method of development, if required, unless the Planning Board expressly modifies any element of the project plan.*

This finding is not applicable: the site is not subject to a development plan, diagrammatic plan, schematic development plan, or project plan.

<sup>11</sup> Use I-P - Waters that are suitable for: water contact sports; play and leisure time activities where the human body may come in contact with the surface water; fishing; the growth and propagation of fish (other than trout); other aquatic life, and wildlife; agricultural water supply, industrial water supply, and public water supply.

2. *The site plan meets all of the requirements of the zone in which it is located, and where applicable conforms to an urban renewal plan approval under Chapter 56.*

An urban renewal plan is not applicable to the subject property. Section 59-C-5.44(f) in the I-4 Zone, Special Standards -Transit Station Development Area (TSDA) authorizes the Planning Board to apply six special review standards for a lot in a TSDA through the site plan approval procedures of Division 59-D-3. These special review standards include:

- 1) area requirements,
- 2) setbacks from mixed-use and residential zones,
- 3) setbacks from a street,
- 4) off-street parking,
- 5) green area and
- 6) accessory residential units.

Special review standards 2 through 5 are applicable to this proposal. Due to site constraints and the Applicant's intention to redevelop the site by retaining most of the three warehouse buildings' existing footprints, waivers to three off-street parking standards are necessary. These waivers include: one for minimum required off-street parking; and two for minimum required parking setbacks. In addition, a reduction to the green area requirement is necessary to achieve the type of redevelopment envisioned in the *Twinbrook Sector Plan*. Staff supports the Planning Board's application of the TSDA special standards, in addition to the necessary waivers and reduction in minimum green area, because strict compliance to these requirements will not allow the type of project envisioned by the Sector Plan. With the three requested waivers, green area reduction and conditions of approval, the proposed Site Plan No. 820130180 meets the requirements of the I-4 Zone, using the Special Standards in a TSDA as shown on the Data Table.

3. *The location of buildings and structures, open spaces, landscaping, and pedestrian and vehicular circulation systems are adequate, safe and efficient.*

- a. Building Location

The proposed renovated building is located in an adequate, safe, and efficient manner. The renovated building and existing restaurant building will be located close to the existing and proposed parking and have direct access to Parklawn Drive and Wilkins Avenue. The Applicant proposes to retain much of the existing three warehouse building footprints in redevelopment of the site as a result, the proposal will provide for green areas, landscaping and improved pedestrian and vehicular circulation.

- b. Green Area Reduction

The proposal includes a reduction to the required amount of green area from 20% (of the GFA [or 22,125 square feet] to 10% (or 11,063 square feet). The proposal also includes a new streetscape along the frontages of both Parklawn Drive and Wilkins Avenue, where no such green area currently exists. This green area will provide a pleasing environment to mitigate the visual impacts of the existing surface parking condition. The green area will mitigate the amount of impervious surface and add landscaping where none is currently planted (i.e., the latter will be in two atriums at the two entrances). As with the three requested waivers to off-street parking standards, based on the constrained land area, the proposal to redevelop the site would not be achievable if this green area reduction was not granted.

c. Landscape and Lighting Plans

The proposed landscaping and lighting provides an adequate, safe, and efficient environment complementing the site design and amenities. Significant numbers of trees will provide canopy for comfort and shade along Parklawn Drive and Wilkins Avenue where none currently exists. Lighting will ensure a safe environment throughout the existing and proposed parking areas.

d. Pedestrian and Bicycle Facilities

An adequate, safe, and efficient sidewalk system will be provided throughout the Property connecting the renovated building, green space, and parking areas to the adjacent streets and the greater pedestrian network. Bicycle facilities will be provided at the proposed renovated building where no such facilities were previously provided.

e. Vehicular Circulation

The proposed project provides adequate, safe, and efficient vehicular facilities. Vehicular access points remain unchanged along Wilkins Avenue and from Parklawn Drive. These have been slightly modified to improve circulation; and were reviewed by MCDOT and the Fire Marshal. Parking is provided in a newly constructed parking garage structure for 49 spaces, 74 spaces in the existing surface parking area, and additional existing metered on-street parking on Wilkins Avenue. Handicap parking spaces are provided near the renovated building's entry points.

The proposal includes a waiver to the required off-street parking and parking setbacks in relation to adjacent properties in other zones. The Site Plan demonstrates there is insufficient land area to provide all of the required off-street parking and the parking setbacks. Staff concurs with the Applicant's justification of these waiver requests, because providing the required off-street parking and the two parking setbacks is not possible without losing more off-street parking due to the existing building layout. If the Planning Board does not grant these three parking waivers and allow for flexibility in the redevelopment of the site, this proposal would not be achievable.

4. *Each structure and use is compatible with other uses and other site plans and existing and proposed adjacent development.*

There are no pending applications for adjacent development. The proposed light-industrial/R&D office uses are compatible with the adjacent light-industrial/ R&D developments, and the renovated building is designed and sited with appropriate setbacks and buffering to be compatible with the size and types of buildings surrounding the Property.

5. *The site plan meets all applicable requirements of Chapter 22A regarding forest conservation, Chapter 19 regarding water resources protection, and any other applicable law.*

The Property is exempt from the requirements of submitting a Forest Conservation Plan per the approved Forest Conservation Exemption, #42013093E, dated January 2, 2013.

On February 11, 2013, the Department of Permitting Services accepted the Storm water Management Concept for the Subject property. The stormwater management concept proposes to meet required stormwater management goals via a green roof, micro biofilters and a stormfilter with volume storage.

## **ATTACHMENTS**

- A. Amended Justification Statement 9/6/13
- B. Twinbrook Sector Plan pages
- C. Department of Permitting Services Stormwater Approval
- D. Agency Approval Letters