

long branch

sector plan ■ november 2013

Design Guidelines



montgomery county planning department

long branch sector plan

Design Guidelines

Abstract

This document contains design guidelines that should be used by property owners, community members, the City of Takoma Park, and planners to implement the vision and recommendations of the approved and adopted *Long Branch Sector Plan* (2013).

Source of Copies

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Online at montgomeryplanning.org/community/longbranch

long branch sector plan

Design Guidelines

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Approved by the Montgomery County Planning Board
Date TK

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introduction

The Long Branch Design Guidelines illustrate how Sector Plan recommendations could be achieved through design. They represent the County's, the City of Takoma Park's, and the community's design aspirations for Long Branch.

Urban design is concerned with the physical characteristics of an area, and these Guidelines consider the design implications of planning decisions in the public realm. An urban design strategy should serve as an integrating tool to coordinate how various development proposals will affect a community physically, with a principal focus on the public realm: the public faces of buildings, spaces for public use, and the streets, sidewalks, parks and plazas that provide the outdoor public venue for everyday activities.

The guidelines assist in implementing recommendations in approved and adopted master plans or sector plans by encouraging urban building attitudes on properties being considered for redevelopment, and by promoting the creation of safe pedestrian environments and attractive gathering places defined by buildings.

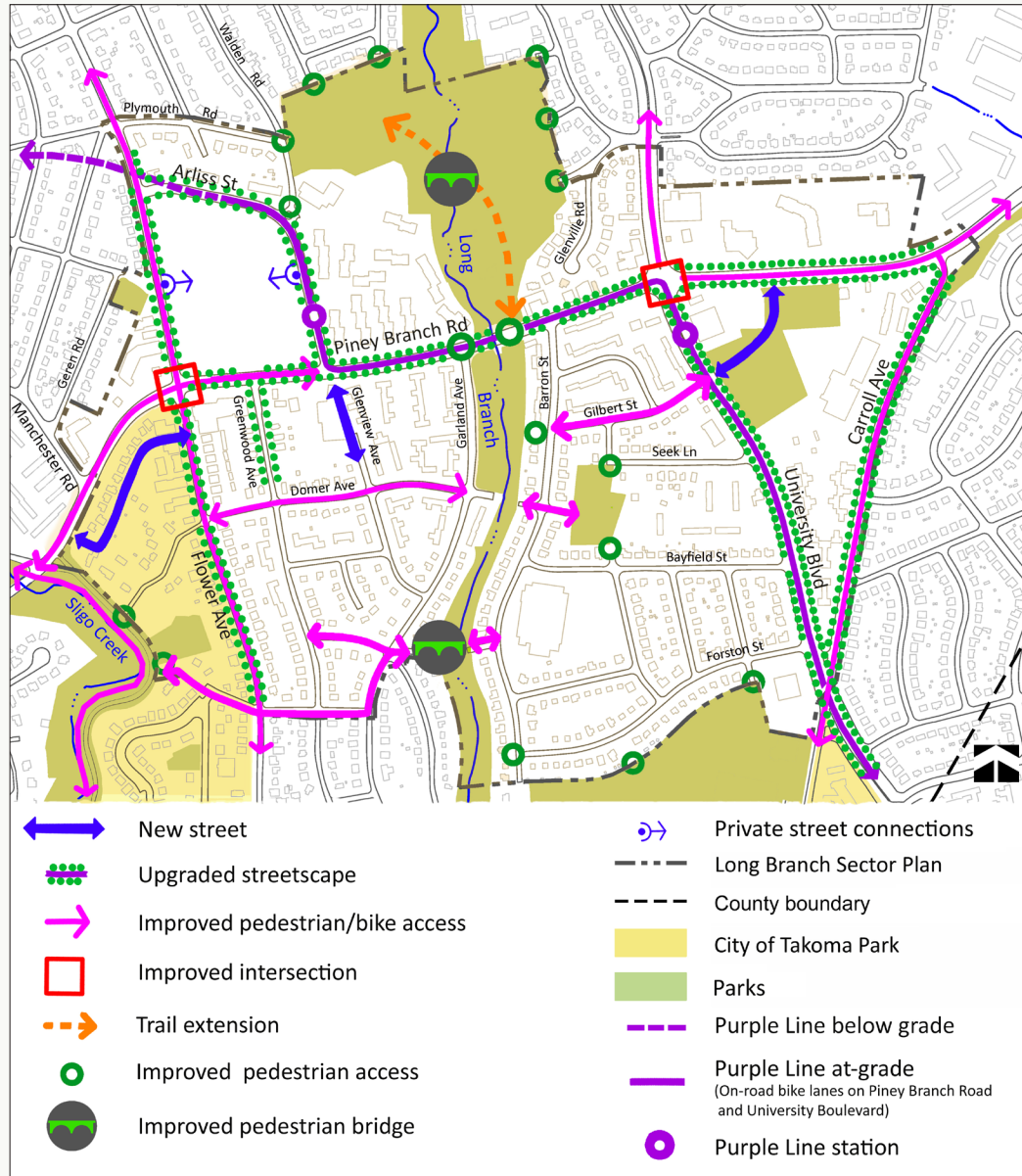
How to use this document

The Sector Plan clearly lays out land use and zoning changes for the Long Branch Plan area. The Design Guidelines describe how those changes should translate into urban design. The Design Guidelines are organized by five categories: Connections, Buildings, Parks and Open Space, the Purple Line, and Transitions. The first four categories describe design goals according to universal principles and also for specific conditions. Transitions explains in detail, using graphic sections, how new development should transition to adjacent residential neighborhoods.

An applicant, when exploring how land use changes to height and density affect a particular property, should review all five categories in the design guidelines. This will help provide a complete understanding of how the Sector Plan affects the built form of any future development on a property and of the public realm in general.

connections

Map 1: Mobility



Map 7 (page 14 in the Long Branch Sector Plan) illustrates the gaps and opportunities for connection in the current street and path system

The circulation pattern in Long Branch is disjointed and lacks alternatives. The major roads in the Plan area—Piney Branch Road and University Boulevard—serve regional as well as local traffic. Ninety percent of the traffic using these roads is commuter traffic passing through the Plan area. Without alternatives, local traffic, including pedestrians, are forced to use these roads for short trips within the Plan area.

The existing streets are characterized by narrow sidewalks (six feet wide in many cases) directly adjacent to the travel lanes. On Piney Branch Road and University Boulevard this arrangement is extremely pedestrian unfriendly. In addition, the sidewalks lack the shade provided by street trees. Existing crosswalks are few and inadequate—painted lines on asphalt.

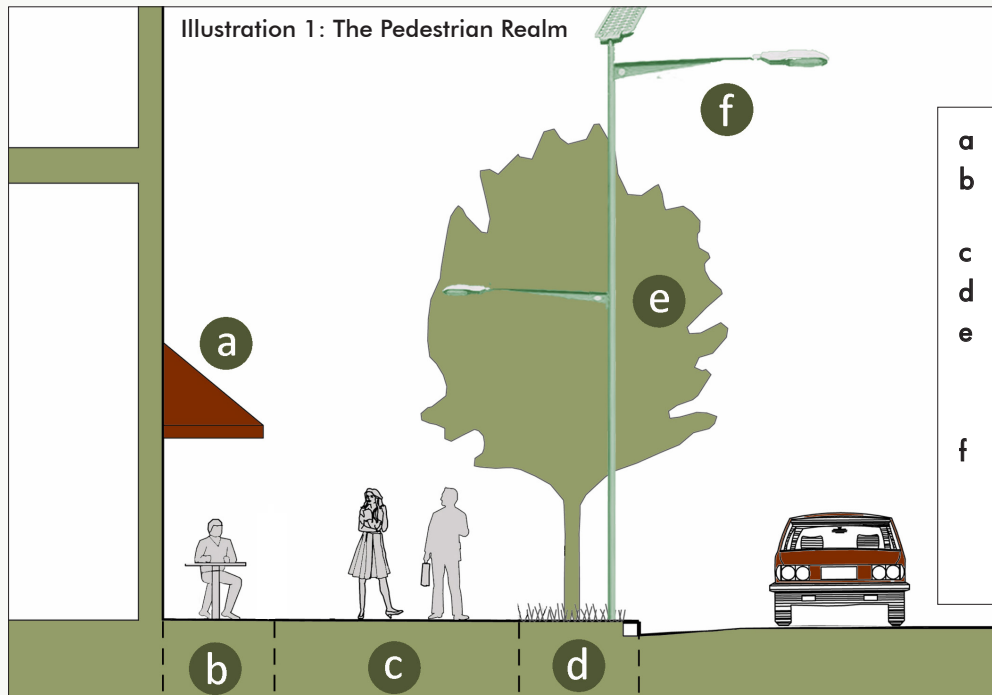
The Sector Plan proposes to enhance connectivity by providing a more balanced transportation system and these Guidelines make recommendations for improved connections, wider sidewalks, safer intersections, new or improved streets, and better access to transit (Plan, page 14).

Universal Principles

Wide Sidewalks

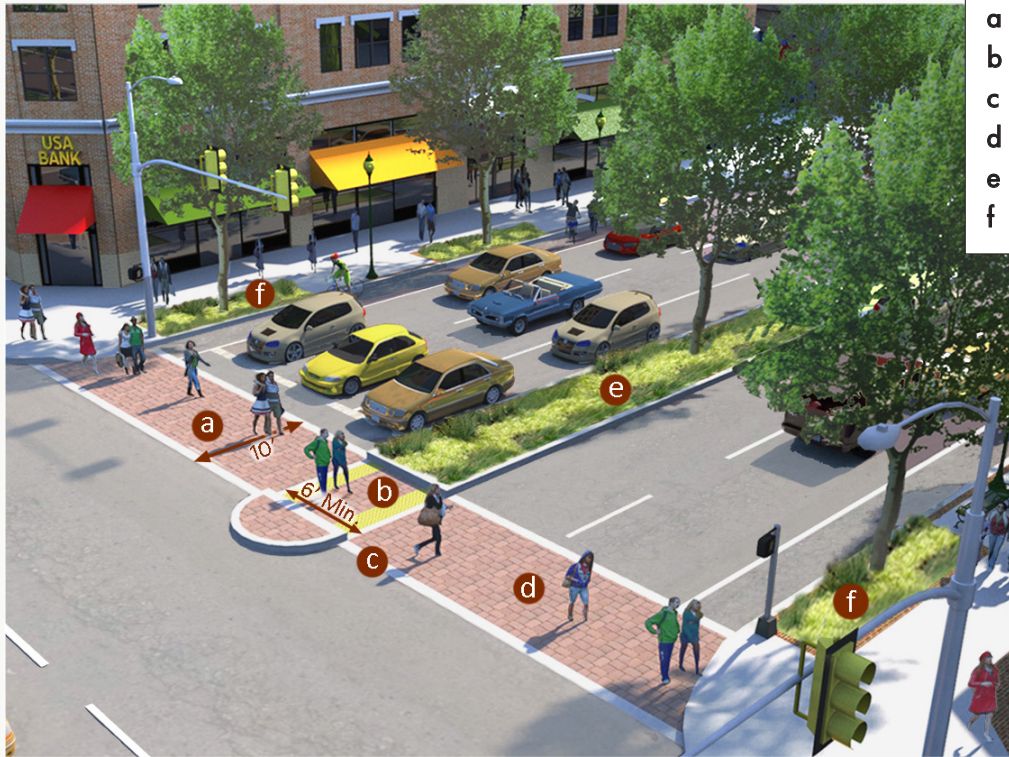
Sidewalks on Arliss Street, Piney Branch Road, University Boulevard, the proposed private street, and sections of other streets (see Map X) should ideally be 20 feet wide and no less than 15 feet wide, and should include:

- specialty paving such as brick, concrete pavers, or scored concrete with special banding
- Where existing conditions limit the sidewalk widths to 10 feet wide, tree grates flush with the sidewalk should be used to provide extra walking area
- shade trees planted 30 feet on center
- street furnishings that include:
 - pedestrian scale lighting either separate from or integrated with street lighting
 - benches at least four feet wide, though size can vary according to location. Benches should be located adjacent to building entrances and wherever adequate sidewalk space and an appropriate setting exist to increase the overall amount of seating
 - waste receptacles large enough to provide adequate storage and located at street intersections, in open spaces, and at building entrances.



- a** awnings: add color and signage
- b** café seating zone: distinct from walking areas
- c** sidewalk: distinct from seating areas
- d** tree panel: pervious, shrubs and perennials
- e** shade trees:
 - elms, oaks, London Planes
 - one species per block
- f** pedestrian-scale lighting:
 - major streets: combination poles
 - business and private streets: pedestrian lights on a single pole

Illustration 2: Crosswalks and Medians



- a minimum 10-foot wide crosswalk
- b minimum 6-foot wide pedestrian refuge median
- c 12-inch wide concrete band
- d specialty paving in crosswalk
- e stormwater bioretention in median
- f bioretention in tree panels

Crosswalks and Medians

All crosswalks should include the following features:

- 10 to 12 feet wide
- specialty paving such as bricks, or concrete pavers, marking the crosswalk from the asphalt travel lane
- Consider special lighting to ensure pedestrian safety at night

On Piney Branch Road between Flower Avenue and Arliss Streets, proposed medians should include:

- a minimum six-foot wide pedestrian refuge
- on medians that are 12 feet wide, shade trees planted 30 feet on center between intersections
- on medians less than 12 feet wide, low plantings such as perennials and ornamental grasses
- stormwater management according to Best Management Practices, where practicable.



Special paving in handicap ramps provide visual and tactile cues to handicapped pedestrians



Brick crosswalks demarcate pedestrian areas within the intersection

Vehicle Lanes

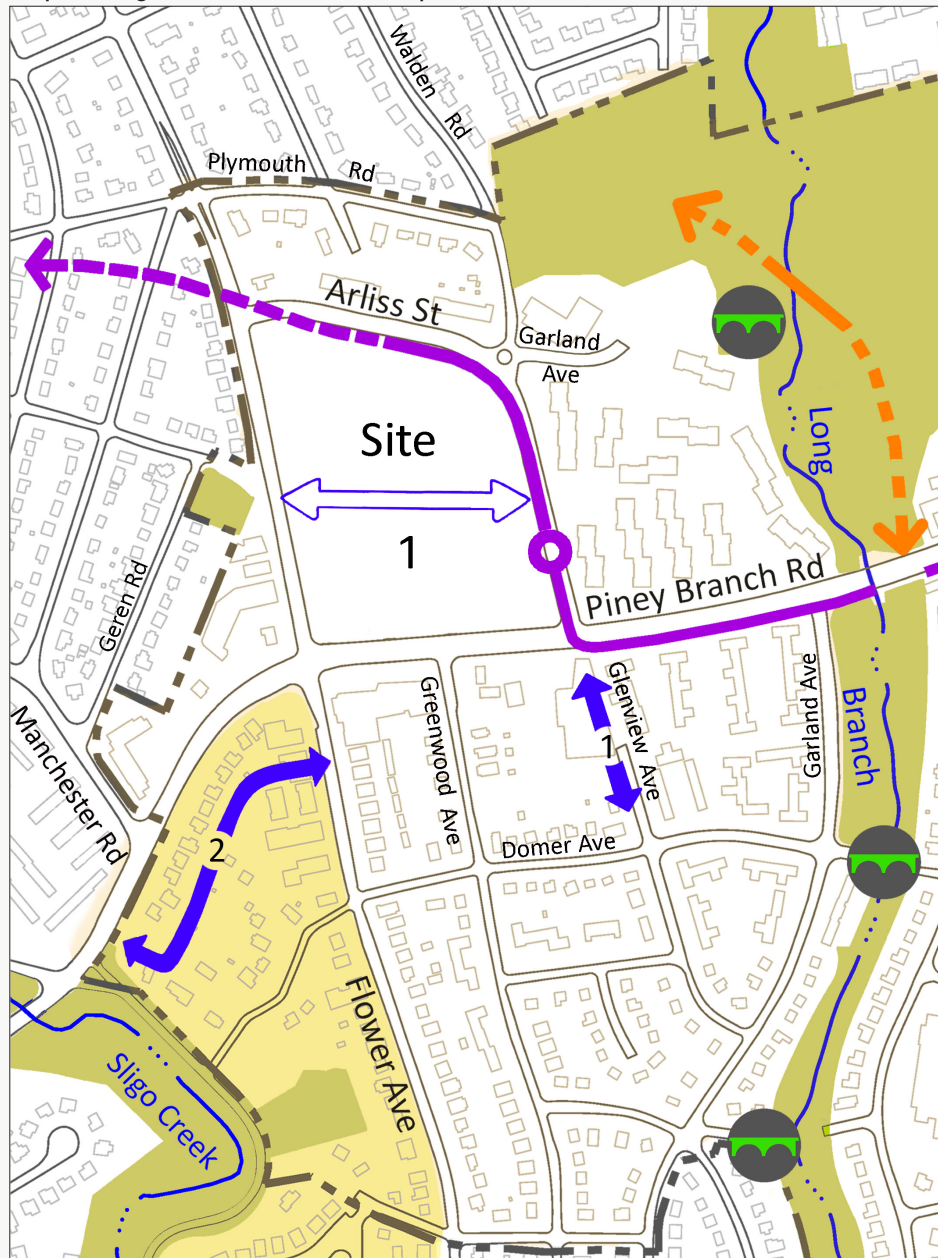
The Plan proposes bike lanes along Piney Branch Road, University Boulevard, and part of Flower Avenue (Plan, page 37).

- Vehicle lanes will include separate on-street bike lanes that are at least 5 feet wide.
- 70-foot rights-of-way on public streets and 60-foot rights-of-way on private streets, which can accommodate on-street bike routes and left turn lanes where appropriate.

Illustration 3: Vehicle Lanes



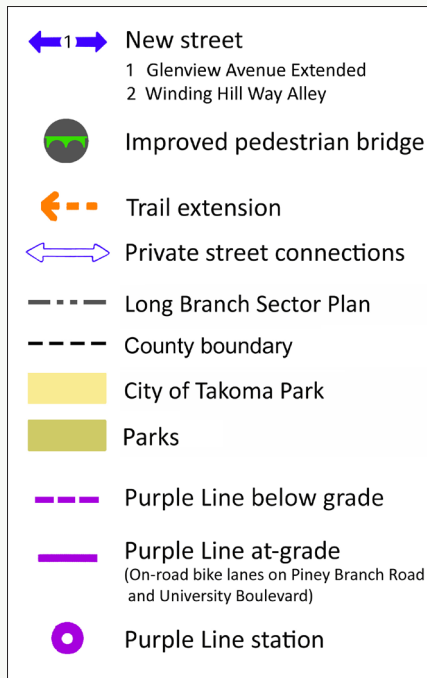
Map 2: Long Branch Town Center Proposed Streets



Application

Long Branch Town Center

In the Long Branch Town Center area the Plan proposes two new streets—a private street through Site 1 and a public alley, Winding Hill Way. It also proposes extending one public street—Glenview Avenue. The Plan also recommends a trail extension and three pedestrian bridge connections over the Long Branch stream (Plan, page 14). These guidelines address the private street, the public streets are addressed in the Sector Plan.



Private Street at Site 1

This private street, from Flower Avenue to Arliss Street, would create two smaller blocks, allowing more direct pedestrian and vehicular connections between Flower Avenue and the planned Arliss Street Purple Line station.

It should be a green street that can also serve as part of a larger public open space and should have a 60-foot right-of-way with 10-foot wide travel lanes instead of the standard 11 feet to help limit the speed of cars. The street should be flanked on both sides by 16-foot sidewalks and have an eight-foot wide parking lane on one side.

These standards will help the street function as public space, treating it as a wide pedestrian mews that can be closed to automobile traffic for special events.

The street and sidewalk should be designed as a single space and should include:

- travel lanes and sidewalks of the same material, such as brick or other special paving
- travel lanes flush with the sidewalks, and separated with bollards.

The private street's green features should include:

- pervious paving
- bioretention planters designed to avoid conflicts with pedestrian traffic
- trees planted 25 feet on-center instead of the standard 30 feet on-center
- street trees planted in continuous green panels
- buildings that define the street space and support climbing plants using structures integrated into the building design.

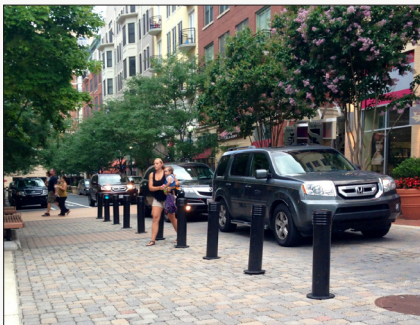
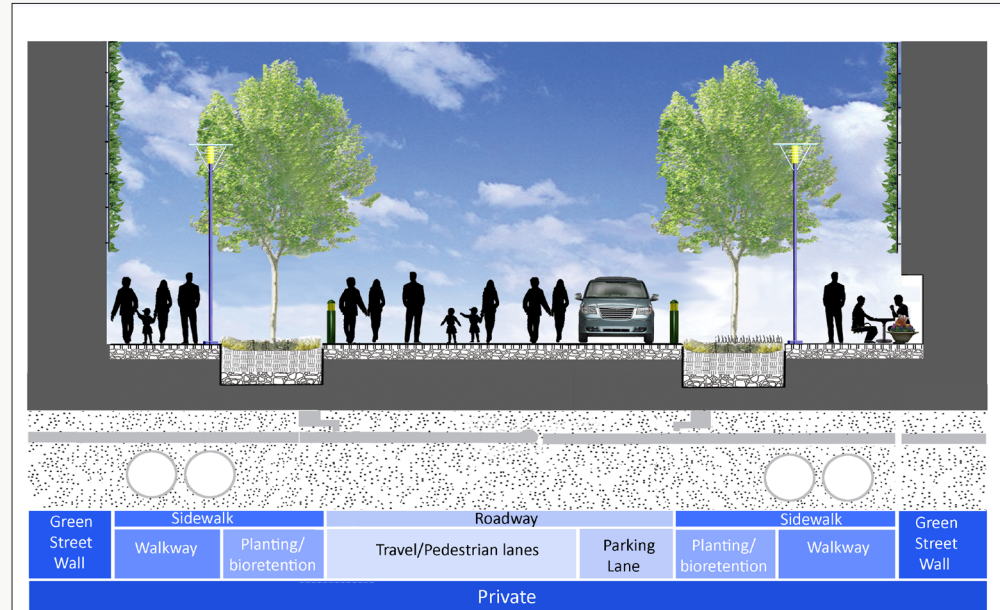


Illustration 4: Private Street Cross Section, Site 1

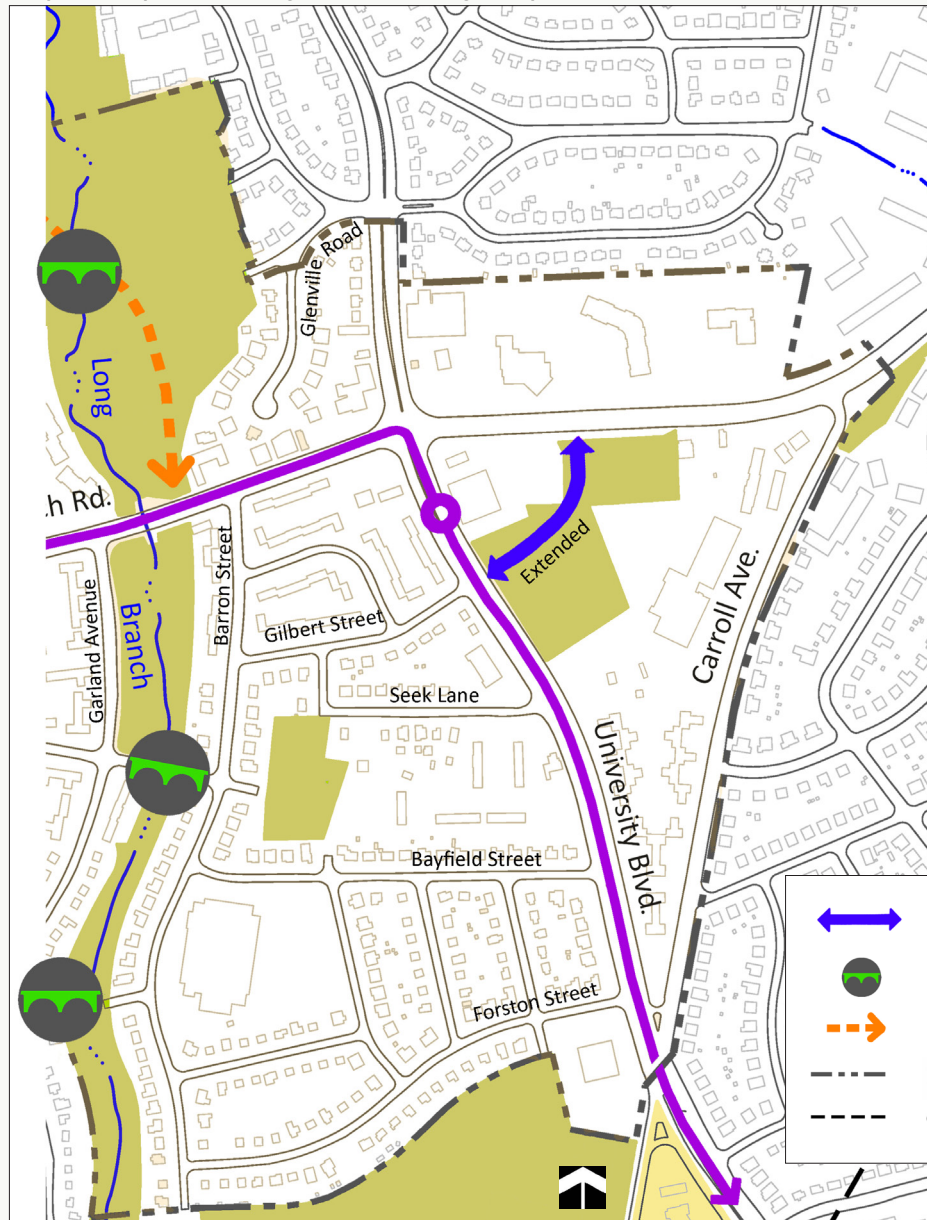


The private street will run through Site 1, creating a vehicular and pedestrian connection between Flower Avenue and Arliss Street. Streetscape improvements include wider sidewalks, at-grade travel lanes with special paving, on-street parking, street furnishings, and sustainable features that should include pervious paving, stormwater recharge, and green walls along street facades.

Plan Recommendations

- Right-of-way:** 60 feet. Accommodate stormwater management within the right-of-way using Best Management Practices, where practicable
- Lanes:** Two lanes with an on-street parking lane to one side
- Pedestrian/Bike Access:** 16-foot wide sidewalks separated from travel lanes by decorative bollards
- Streetscape:** Street trees planted 25 feet on center within grates and/or green panels, pedestrian-scale street lighting, benches, bus shelters, bike racks, and trash cans

Map 3: Piney Branch Neighborhood Village Proposed Streets



Piney Branch Neighborhood Village

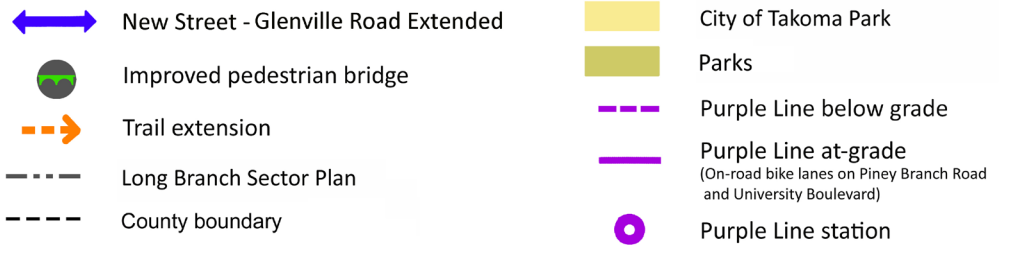
The circulation pattern at the intersection of Piney Branch Road and University Boulevard is disjointed with no alternative local routes. The major roads running through the Piney Branch Neighborhood Village Area are Piney Branch Road and University Boulevard. The intersection is a major bus transfer point in Montgomery County. As in the Long Branch Town Center area, 90 percent of the traffic using these roads is commuter traffic passing through the Plan area.

The Plan proposes a new public street that will provide alternative access for the residents and businesses in and around Piney Branch Neighborhood Village. It is an extension of Garland Road to connect the park and properties to the north (Plan, pages 14 and 74).

Gilbert Street Extended

This new road will provide:

- access for future development in the southeast quadrant of Piney Branch Neighborhood Village
- relief at the intersection of Piney Branch Road and University Boulevard
- on-street parallel parking for New Hampshire Estates Park
- buildings facing the park
- an additional crosswalk for pedestrians travelling to the future Gilbert Street Purple Line station.

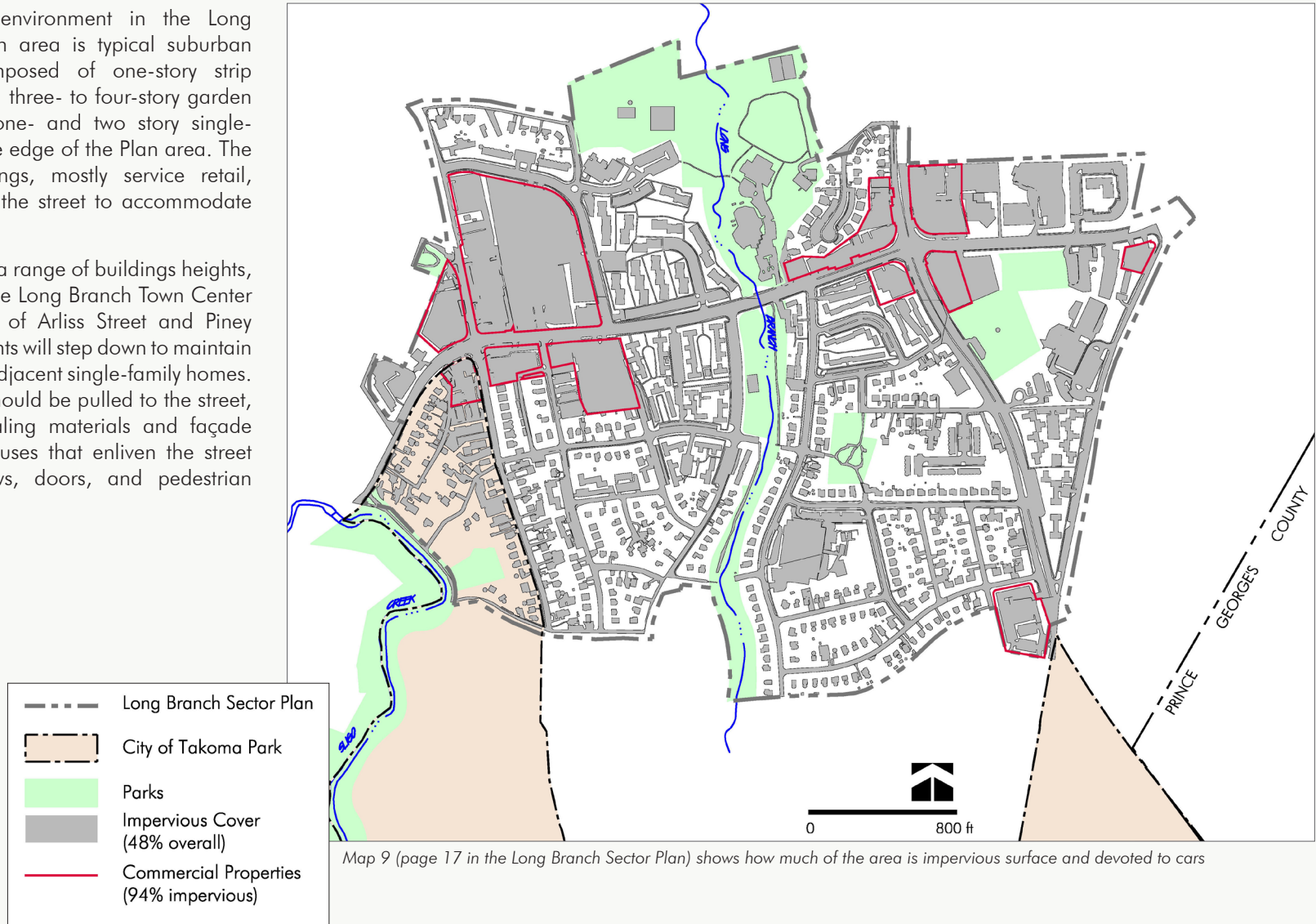


buildings

Today, the built environment in the Long Branch Sector Plan area is typical suburban development, composed of one-story strip retail buildings and three- to four-story garden apartments, with one- and two-story single-family homes at the edge of the Plan area. The commercial buildings, mostly service retail, are set back from the street to accommodate surface parking.

The Plan proposes a range of buildings heights, with the tallest in the Long Branch Town Center at the intersection of Arliss Street and Piney Branch Road. Heights will step down to maintain compatibility with adjacent single-family homes. Building facades should be pulled to the street, use visually appealing materials and façade design, and have uses that enliven the street with store windows, doors, and pedestrian activity.

Map 4: Long Branch Town Center and Piney Branch Neighborhood Village Impervious Surface



Universal Principles

Building Heights and Setbacks

- Locate structured parking on the site's interior and shielded from streets by liner buildings, with activating uses such as retail and sidewalk cafes.
- Use build-to lines to create a consistent building façade along the street.
- Vary from the build-to lines only in special circumstances, such as:
 - five-foot setbacks to accommodate café seating
 - public open spaces and pocket parks that serve the public.
- Do not pull building facades back from the street to create entry forecourts or front yards.
- Design buildings to create distinct top, middle, and ground floors.



Locate parking on site's interior



Use five-foot setbacks where necessary to accommodate cafe seating



Use a build-to line to create a consistent building facade

Façade Features and Street Activation

- Create eyes on the street with strategically located windows and doors.
- Use balconies where appropriate.
- Use signage to create character and set a tone. Signs should:
 - be artistic and distinctive
 - be integrated into the building façade
 - complement the architecture.
- Locate main entries to residential buildings on the primary street where practicable.
- Ground floor commercial facades should be designed with at least 60 percent glazing.
- Allow café seating in front of retail establishments.
- Use distinctive materials that will lend a unique character to the Plan area.
- Vary building heights to achieve visual interest.



Allow cafe seating in front of retail



Vary building heights to achieve visual interest



Distinctive materials lend character to the Sector Plan area



Use signage to create character

Transitions

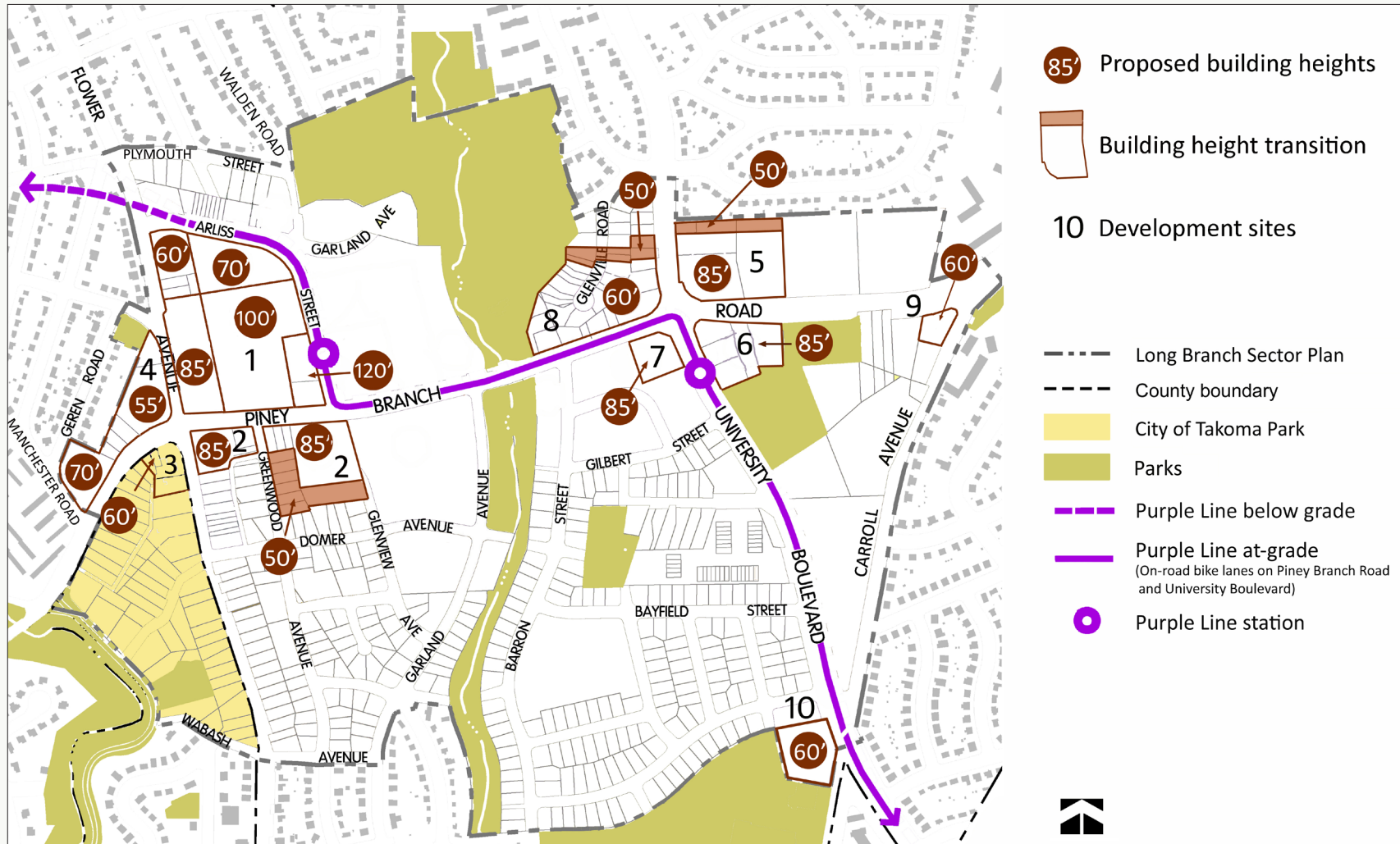
- Locate maximum building heights of 120 feet at the northwest quadrant of the intersection of Arliss Street and Piney Branch Road to focus the greatest densities at the planned Purple Line station.
- Step-down other Town Center building heights toward adjacent single-family neighborhoods.
- Step-down building heights to a 45-foot maximum where directly adjacent to single-family houses.
- Step buildings down to a maximum height of 45-feet at the edges of CR-zoned properties.
- Screen off-street parking located behind CR-zoned properties from adjacent single-family residential properties with fencing or evergreen hedges at least six feet tall.
- Avoid blank walls facing residential communities; use windows and balconies to create a human scale.
- Building walls facing residential neighborhoods should avoid bright colors or shiny finishes.

Illustration 5: Building Transitions



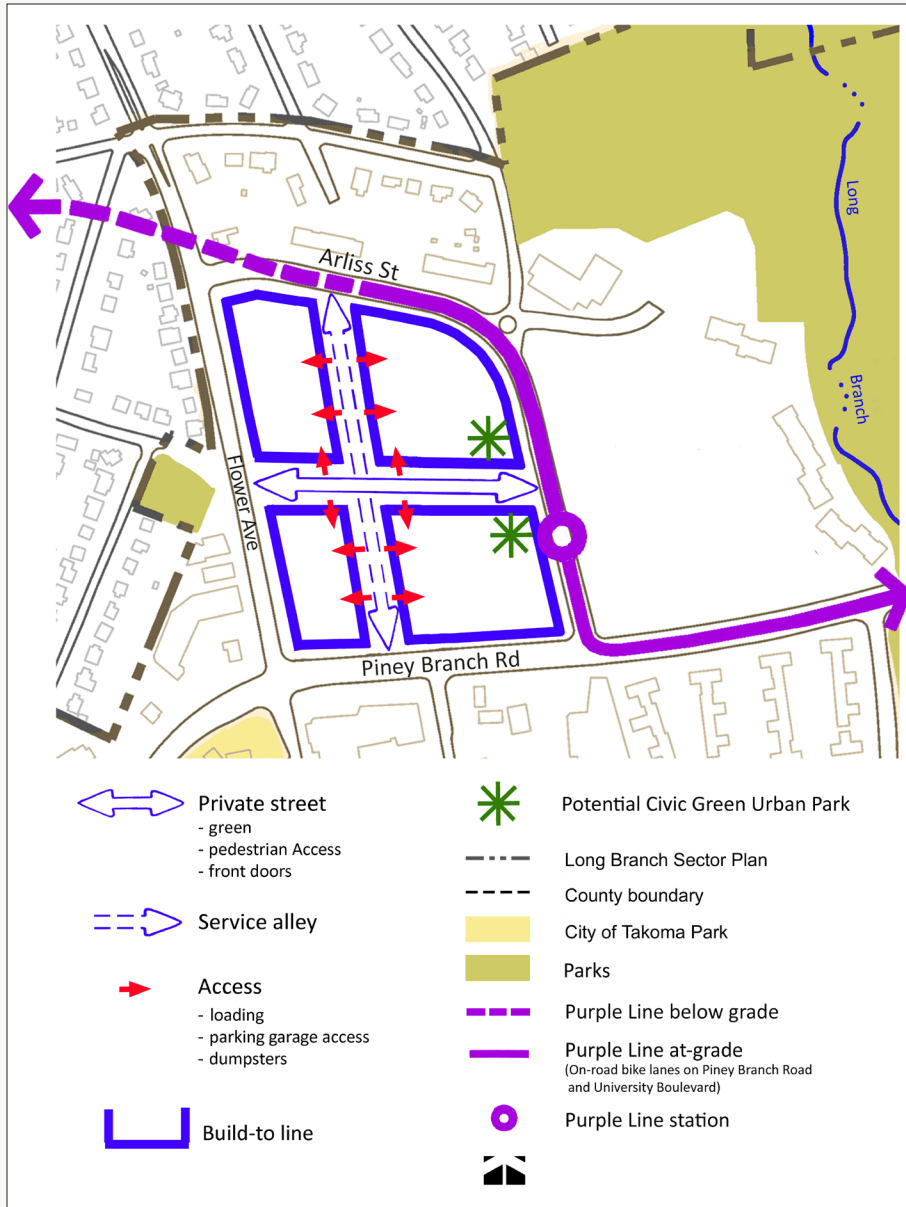
Step buildings down as they approach single-family residential

Map 5: Proposed Building Heights



The Plan proposes on Site 1: CRT 3.5 zoning, 60- to 120-foot building heights and on Site 9: CRT 2.5 zoning, 85- to 120-foot building heights

Map 6: Long Branch Town Center Proposed Development Pattern



Application

Long Branch Town Center

In the Long Branch Town Center Area, the Plan proposes extending a public street, a new private street, and connections to provide route options (Plan, pages 14 and 66-68), specifically, the Glenview Avenue extension. These Guidelines also cover the proposed crossing of Long Branch by a pedestrian bridge and Winding Hill Way becoming a public alley connected to Flower Avenue.

The Plan recommends that the Town Center be redeveloped as a “distinct node;” a place with a human scale, that is pedestrian-friendly, with a local design character (Plan, page 66). To that end, these guidelines address integrating new development, historic preservation, and placemaking in the Town Center.

Integrating New Development

Site 1 is the focal point of the Town Center. Currently, it is developed in an auto-oriented pattern, with two one-story grocery stores, a gas station facing Arliss Street, a one-story commercial building, a single-family home operating as a business, and the Flower Theatre and Shopping Center facing Flower Avenue.

Redevelopment on Site 1 should be coordinated and designed as a whole that incorporates connections, public parks and space, and mixed uses. The proposed private street between Flower Avenue and Arliss Street should act as a central spine along which new development can be organized, pulling activity into and through the center of the block. New development should also make appropriate transitions to surrounding residential neighborhoods, paying attention to building height and ensuring lively edge streets.

New development should:

- be focused on Flower Avenue, the Arliss Street Purple Line station, Piney Branch Road and on the proposed private street
- locate service, loading, and parking entrances off alleys, where feasible
- locate service entries internal to the site, not on Arliss Street, Flower Avenue, or Piney Branch Road
- intersect the private street with the proposed civic space.

Historic Preservation

The Plan recommends designating the Flower Theater (limited to the theater façade, two adjoining shoulders, and second wall plane to a depth of 40 feet from the theater facade) in the Master Plan for Historic Preservation and incorporating it into redevelopment (Plan Page 25). The Plan also recommends uses that serve the immediate neighborhood and meet community-wide planning and redevelopment goals, including housing near mass transit.

The theater façade should retain its prominence along the Flower Avenue frontage, with the original building fabric preserved with no substantial alteration. Along Flower Avenue, any new building must not be closer to the street than the current front facade of the Flower Theater. To preserve the theater façade as the dominant focal point on the block, new buildings should not be taller than the theater along the Flower Avenue facades for a depth of 40 feet. Additionally, to retain the theater element as the focal point along Flower Avenue, adjacent construction for a length of 20 feet and a depth of 40 feet should not be taller than one story. Beginning 40 feet behind the theater's front façade, new development may be as high as 85 feet.

Illustration 6: Flower Theater Historic Re-Use



If the proposed private street on Site 1 is located adjacent to the Flower Theater, new development behind the preserved entry and shoulders should face directly onto the private street to give the theater new prominence as a corner building. To make best use of this prominence and to support community goals, new development should:

- locate entries directly on the private street
- include retail uses
- accommodate café seating
- provide service and parking areas that can serve mixed uses and support market viability
- locate parking and loading entries on secondary streets.

Redevelopment of the theater should also ensure that historic features are preserved and enhanced.

Placemaking

The Flower Theatre is a focal point in the Long Branch Town Center and could be the basis for placemaking elements. For example, the theater's marquee could be lit or its poster boxes could contain illustrated interpretive panels on the area's past.

Additional placemaking efforts should include street furniture, wayfinding and interpretive signage, and public art incorporated into new development.



Public art can be incorporated into new development to help create a sense of place

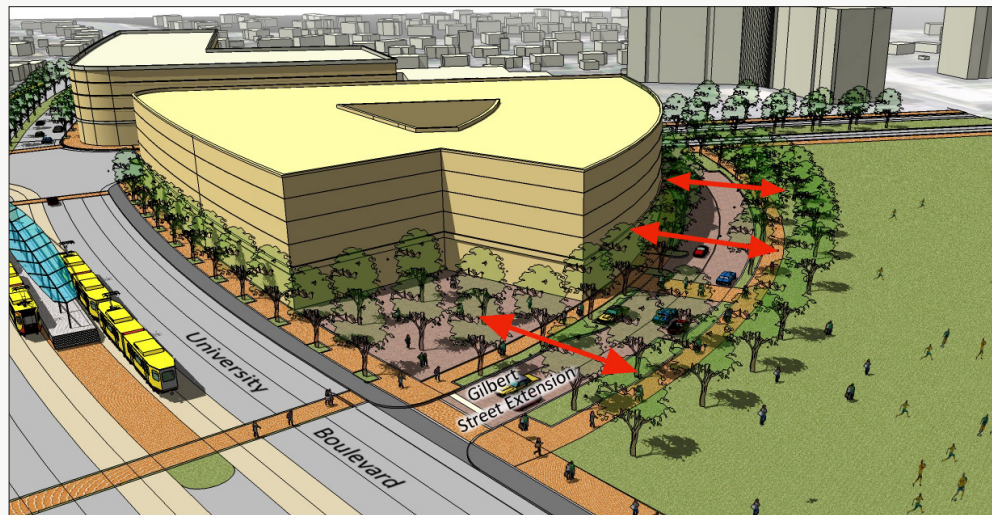
Piney Branch Neighborhood Village

Long Branch's smaller commercial area is a transit transfer point and should be redeveloped as a distinct community node, with a public space and pedestrian-friendly street connections to surrounding uses.

The New Hampshire Estates Neighborhood Park, which is to be renovated after the Purple Line Station is built, is presently cut off from existing commercial establishments to the north. The Plan proposes extending Gilbert Street to become the northern edge of the park and will provide parking that will be lost to Purple Line construction.

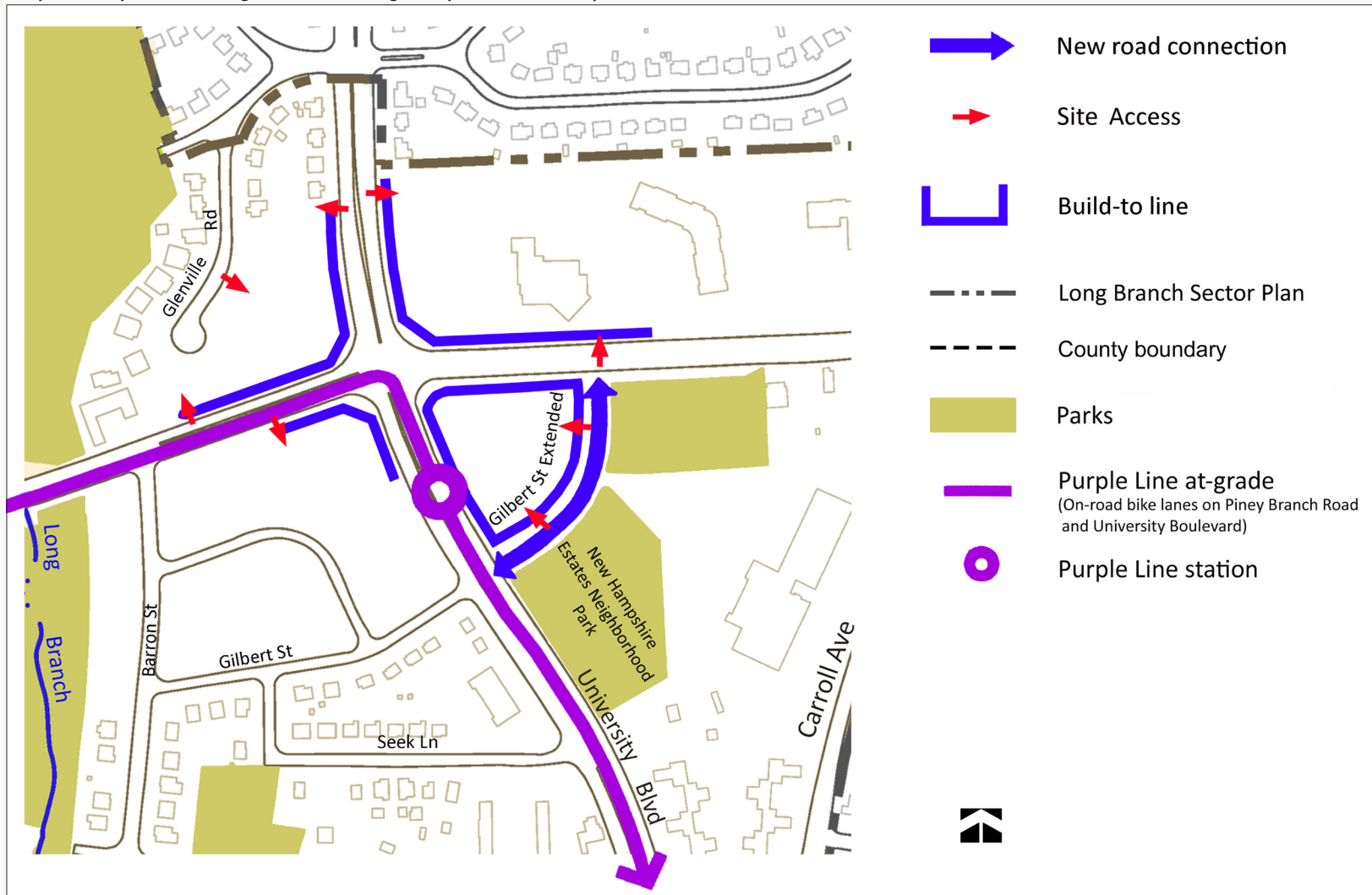
The renovated park should be connected to new development so that the two function together, supporting and adding life to each other with:

- buildings facing Gilbert Street Extended that create and define the street space, and relate to the park
- buildings on Gilbert Street Extended that have ground floor entries, and windows and balconies facing the park
- building façades that incorporate elements or themes related to the park
- at least 12-foot wide crosswalks from the park that cross Gilbert Street
- at least 12-foot wide crosswalks that cross University Boulevard to the Gilbert Street Purple Line station
- street and park furnishings of a shared vocabulary and that are distinct from street furnishings in the rest of Piney Branch Neighborhood Village
- parking, loading, and dumpsters located internal to the site.



Gilbert Street Extended should have ground-floor retail facing onto New Hampshire Estates Neighborhood Park

Map 7: Piney Branch Neighborhood Village Proposed Redevelopment Pattern



parks and open spaces

The Plan area is already well served by large public parks that include playgrounds, baseball and soccer fields. However, the Plan's recommended densities call for a series of smaller urban open spaces to serve residents and employees in the Town Center and Neighborhood Village.

Further, the 2012 Parks, Recreation and Open Space (PROS) Plan recommends a park hierarchy for all master and sector plans. The hierarchy for the Long Branch is detailed on pages of 32-34 of the Sector Plan.

In the Long Branch Plan area, public and private open spaces will include:

- a central civic green of at least ½ acre to serve the Sector Plan Area
- smaller neighborhood parks
- small urban parks and plazas at the block level.

These spaces will be a major part of the public realm in the Plan area. The open spaces that are intended to serve as focal points of community life should be located where a variety of activities and uses intersect. These places are typically at the highest concentrations of density and are near transit stations, libraries, community centers, or places of worship.

Universal Principles

Configuration and Location

- Locate parks and open space where two or more pedestrian paths converge.
- Locate parks near retail, office, and residential uses.
- An open space should open onto at least one street—it should feel like an extension of the sidewalk and not a separate space.
- There should be several easily accessible and visible points of entry into and out of the open space.
- Views into and out of the open space from the surrounding streets and sidewalks should be clear and unobstructed.
- The open space should be designed as a room with consideration of the space's floor, ceiling, and walls.
- Enliven the walls of the open space with building entries at ground floor level, and with windows and balconies that look directly onto the space.
- Design with CPTED principals to ensure a space that will feel safe for all users.

Site Details

- Locate building entries to open directly onto the open space.
- Include retail uses.
- Accommodate café seating.
- Provide service and parking areas that can serve mixed uses and support market viability.

Trail Connections

- Establish sidewalks and bikeways that connect to the Long Branch Trail, as well as other Long Branch area parks and open spaces.
- Design pavement markings, signage, bike racks, seating, landscaping, and art along the trail to reflect local character.
- Include mileage markers similar to heart-smart trail medallions.



Locate parks where two or more pedestrian paths converge



The open space should have a floor, walls and ceiling



There should be views into and out of the open space

Application

Long Branch Town Center

Civic Green

As Long Branch Town Center redevelops, the new design should provide a centrally located Civic Green Urban Park. It will serve existing and future employees, residents, and shoppers using the Town Center.

Civic Green Urban Park should be:

- at least ½ acre
- centrally located within Site 1
- mostly lawn with hardscape surfaces where appropriate
- in a visible location and integrated with the proposed east-west private street
- near the future Purple Line station on Arliss Street
- activated with commercial and retail uses on at least two sides
- enlivened with public amenities, such as a fountain
- shaded with trees
- fitted with electricity, lighting, Wi-Fi, tent structures, etc.
- designed with an event space.

Because of a short-term need for a central event space in Long Branch, the Department of Parks is encouraged to work with landowners to establish an interim Civic Green on any available land that is within or near Site 1. The interim park should be a simple design with seating and open areas for gathering.

Long Branch Local Park

- Redesign and relocate the playground to be more visible from the adjacent neighborhood, streets, and recreation center.

Flower Avenue Urban Park

Through redevelopment, consider a redesign that:

- activates the commercial edge with an outdoor plaza with seating and tables
- improves the playground area with a new natural play design element
- improves landscaping to add green elements but keep visibility unobstructed
- updates and adds artwork to the existing sculptures.

Illustration 7: Civic Green Configurations



New Hampshire Estates Neighborhood Park

New development in the Piney Branch Neighborhood Village should include renovation of New Hampshire Avenue Neighborhood Park. This public park and new development in the southeast quadrant of the Piney Branch/University Boulevard intersection should be viewed as two parts of a whole. As outlined under the Buildings section of these guidelines, the new development on the Gilbert Street extension should form a wall that faces the park. The park should also be visually and physically connected to the proposed Purple Line station to be located on University Boulevard, just south of Piney Branch Road. The Gilbert Street extension will provide parking and will have three wide crosswalks to encourage pedestrian movement between the redevelopment site at University Boulevard/Piney Branch Road and the park. The park should act as the “front yard” for new development at this corner.



The public open space should be:

- mostly green — today the existing lawn is the scene of pick up soccer games and it can continue to function this way in a re-configured park.
- activated with commercial and retail uses on Gilbert Street Extended
- integrated into the wide, tree-lined sidewalks on Gilbert Street Extended and University Boulevard
- connected to the future Purple Line station at University Boulevard and Piney Branch Road through a new crosswalk at Gilbert Street Extended and University Boulevard
- enlivened with a bandstand or other facility for public performances.

Specific uses and design features could include:

- spaces that meet urban residential recreational and social needs such as community gardens, picnic shelters, skateboarding, and grass volleyball courts
- a healing garden.

Seek Lane Neighborhood Park

Through redevelopment, consider a redesign that:

- creates a natural play and environmental learning area adjacent to the existing playground
- creates a safe crossing area on Bayfield Street from the school to the park
- improves park infrastructure with electrical service and amphitheater seating to create an outdoor classroom.
- potentially expands the park and links it with Rolling Terrace Elementary School and to improve views, if properties become available for acquisition.



Bandstands support public performances



Green lawns provide attractive play areas for children



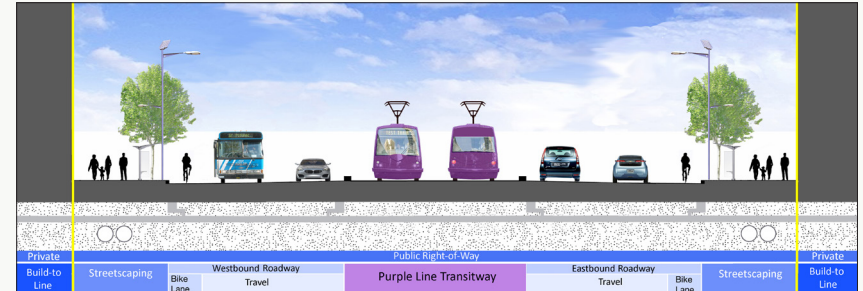
Provide play equipment and interactive fountains

purple line

Universal Principles

The 2010 Purple Line Functional Plan identifies two stations in the Long Branch Plan area, one at Arliss Street in the Long Branch Town Center and one at Gilbert Street in the Piney Branch Neighborhood Village. The Functional Plan calls for walk-up access and adjacent sidewalks at both stations.

The stations should be designed to integrate with the surrounding streets, sidewalks and buildings. They should create a sense of arrival that, while using a design vocabulary that is similar throughout the Purple Line, is distinct or unique to each station.



Application

Route

In the Long Branch Town Center, the Purple Line light rail line will run adjacent to Site 1, along Arliss Street. On Piney Branch Road and University Boulevard, the Purple Line will run in the median. MTA standards will expand the right-of-way and, in places, require a curb and fencing to keep cars and pedestrian out of the travel way.

The Purple Line will expand the current cross sections of Arliss Street, Piney Branch Road, and University Boulevard up to 30 feet to accommodate the transitway. This will result in long crosswalks and, where fencing is required, potentially visually and physically divide the street.

The following strategies could be employed to lessen these impacts.

- Limit fencing to block sections far from crosswalks.
- Consider using decorative fencing.
- Consider planting grass or other ground cover in between the tracks to lessen the impact of the widened cross section.

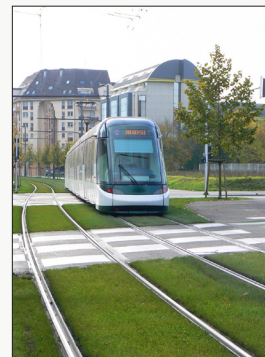
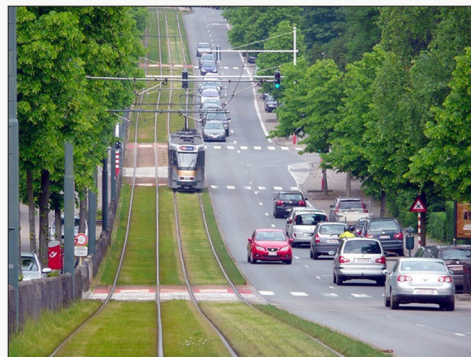


Illustration 8: Purple Line on Piney Branch Road



a 15-foot wide sidewalk

b 11-foot travel lanes

c grass between tracks

d 5-foot wide bike lane

Stations

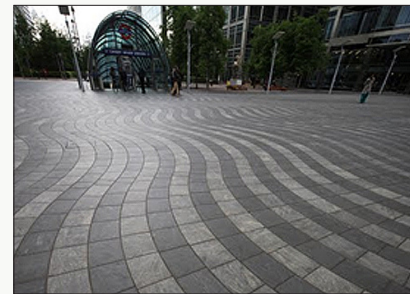
The two Purple Line stations in the Long Branch Sector Plan area should be designed to:

- integrate with the urban fabric by:
 - using *similar paving material in the station platforms* and adjacent sidewalks
 - creating a seamless flow from sidewalks to the station platform
 - designing station furnishings with materials similar to the *surrounding built environment*
- give priority to pedestrian access. Vehicular drop off areas or kiss-and-rides should be a secondary consideration, only after pedestrian access has been prioritized.
- create a unique sense of arrival at each station using:
 - embedded art, such as murals or decorative paving
 - canopies, railings, kiosks
 - iconic art that creates individuality related to place.

Tunnel

The Purple line will descend underground at a tunnel entrance on Arliss Street, which should be addressed using the following strategies.

- Use light colored concrete and appropriate illumination to reduce the impact of large, dark opening in the street
- Reduce tunnel opening to minimum required
- Avoid use of chain-link fencing around tunnel. Use fencing colors and material that recedes into background
- On concrete entry walls use designs that lighten the effect of large vertical surfaces of concrete.



Transitions

The Plan’s redevelopment sites are clustered in two areas—the **Long Branch Town Center** and the **Piney Branch Neighborhood Village**. New mixed-use development in the Town Center will be the most dense and needs to support the Purple Line. Redevelopment should create active pedestrian streets, connections to public spaces, and should incorporate the historic Flower Theater. The second cluster of sites at the intersection of Piney Branch Road and University Boulevard should create a distinct place and accommodate the Purple Line route and station at what will become a transit hub.

A key to successful development is creating proper transitions between new development and the adjacent, existing residential neighborhoods. The Sector Plan carefully delineates the step-down of the proposed heights for each of the new zones, ensuring appropriate relationships between land uses. The following sections illustrate how those transitions will work for specific sites in each area.

Long Branch Town Center

Site 1: Flower Theater and Shopping Center, 8805-8809 Flower Avenue, 8750 Arliss Street, 8528 Piney Branch Road, and 8850 Piney Branch Road in Long Branch Town Center (does not include Flower Theater and Shopping Center Property)

- Locate the tallest buildings at the southeast corner of Site 1 at the intersection of Arliss Street and Piney Branch Road with a maximum height of 120 feet, stepping down to a maximum height of 60 feet at the southeast corner of Arliss Street and Flower Avenue.
- Along Piney Branch Road, step down building heights from 120 feet at the intersection of Arliss Street and Piney Branch Road to 100 feet (at the Bestway property), and to 85 feet at the intersection of Piney Branch Road and Flower Avenue.
- Create a private street connecting Flower Avenue to Arliss Street that:
 - links communities to the west and north along Flower Avenue to Arliss Street
 - provides a visual connection to the Purple Line station.
- Create a private service alley that provides for loading, parking garage access, and dumpster locations.

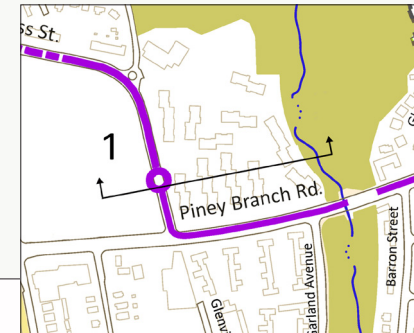
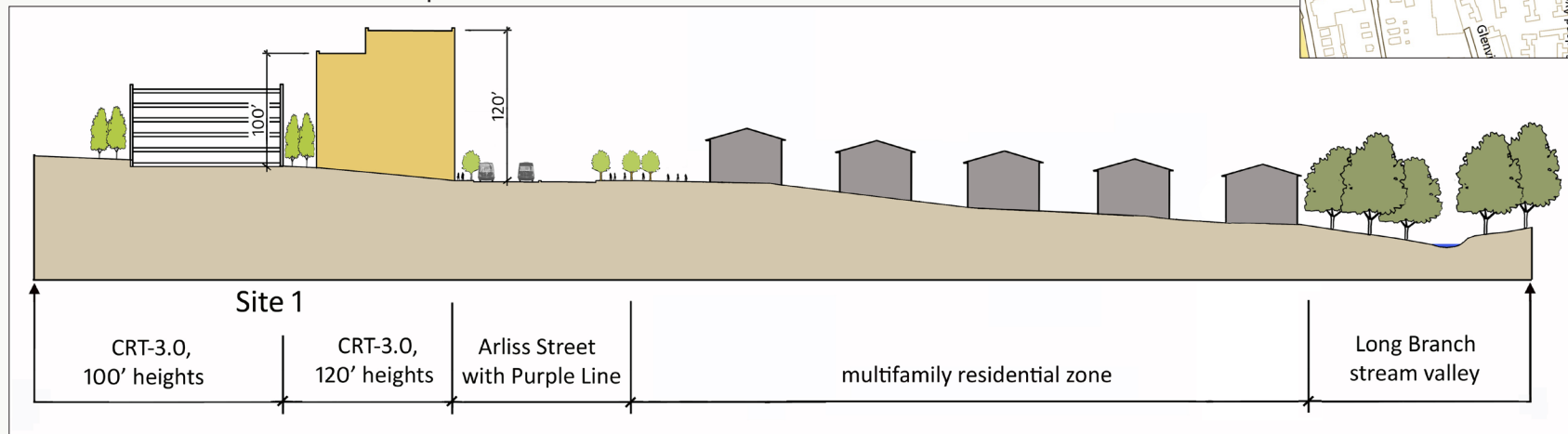


Illustration 9: Site 1 and Flower Branch Apartments



Site 1 and Arliss Street

- Step buildings down to maximum 70-foot heights at Arliss Street.
- Sidewalks along Arliss Street to be a minimum 5 feet, wide an with additional 5' for a tree panel adjacent to the curb.
- Activate street wall with entryways and windows.

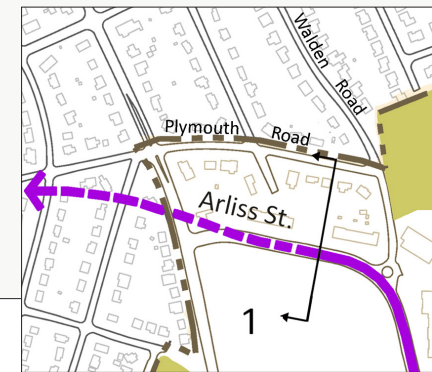
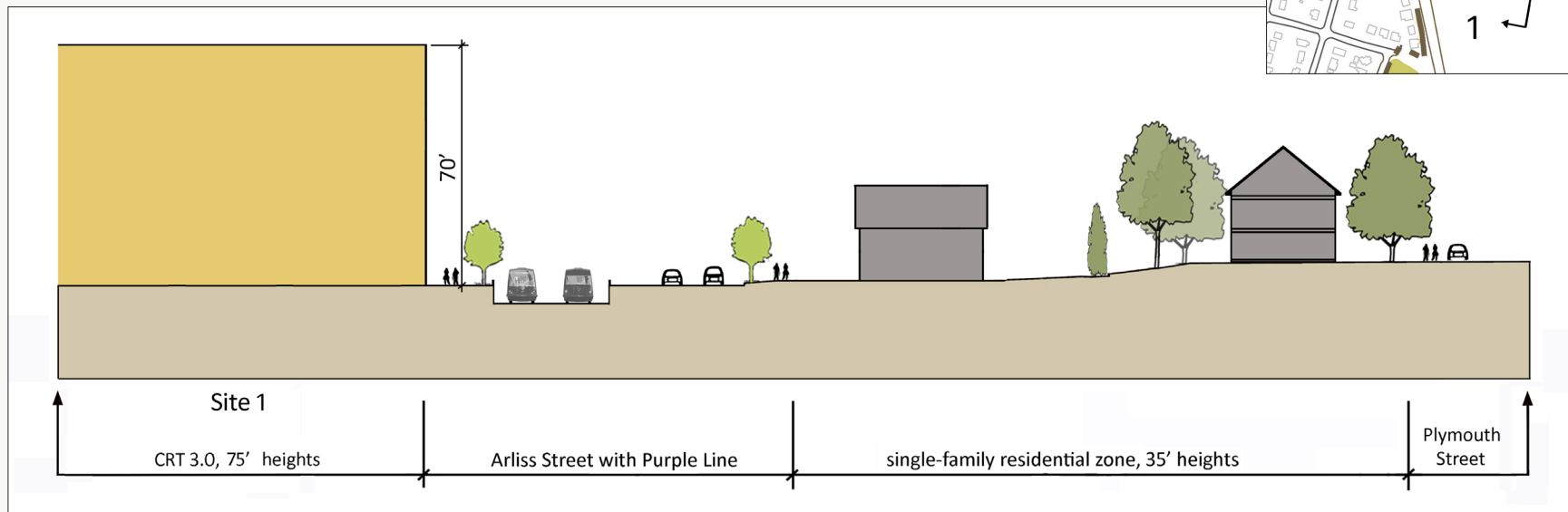


Illustration 10: Site 1 and Arliss Street



Site 2 Piney Branch Road and Domer Avenue

- Maximum 85-foot building heights on Piney Branch Road, stepping down to maximum 45-foot building heights adjacent to single-family zoned properties on Domer Avenue.
- Incorporate evergreen screening trees adjacent to single-family neighborhoods.

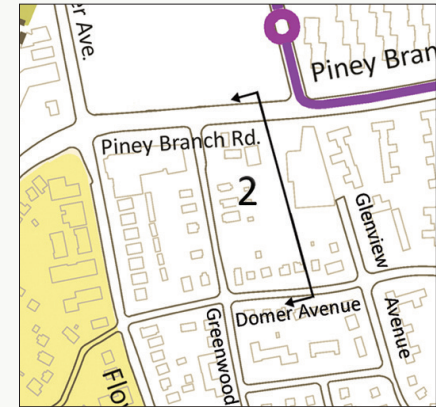
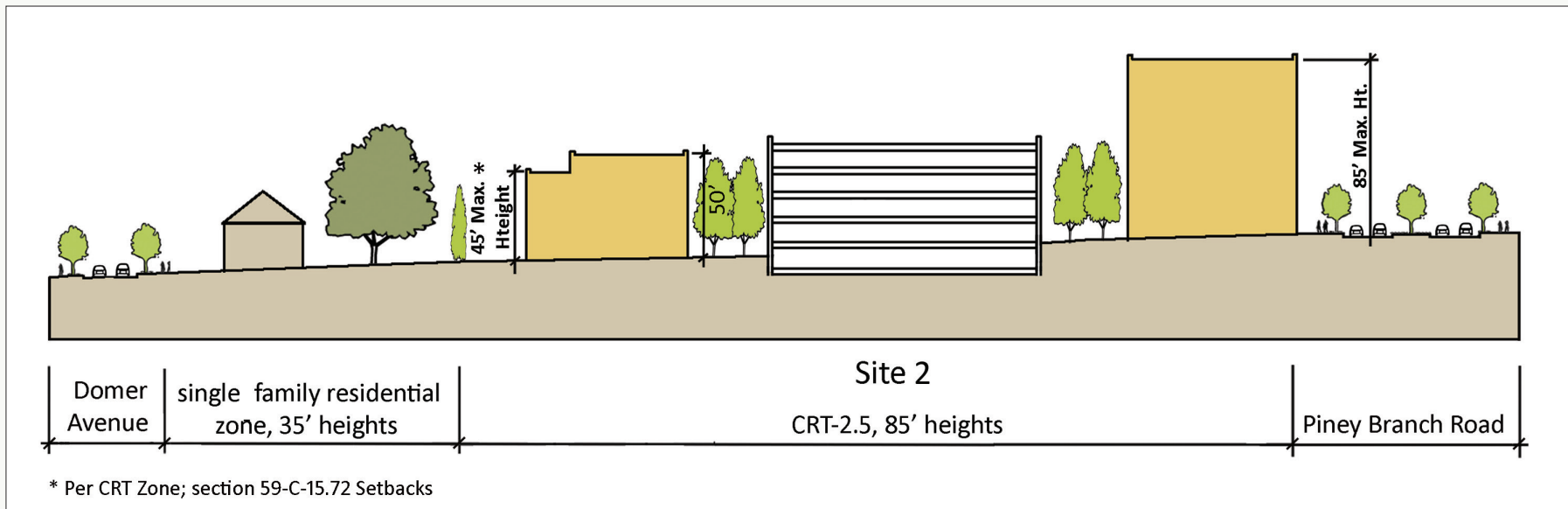


Illustration 11: Site 2 Piney Branch Road and Domer Avenue



Site 2 Piney Branch Avenue

- Maximum 85-foot building height, stepping down to a 45-foot maximum adjacent to single-family homes.
- Incorporate a gateway feature at the corner of Piney Branch Road and Flower Avenue.

Site 3 Piney Branch Road/Flower Avenue (southwest)

- Maximum 60-foot building height, stepping down to a 45-foot maximum adjacent to single-family homes.
- Incorporate a gateway feature at the corner of Piney Branch Road and Flower Avenue.

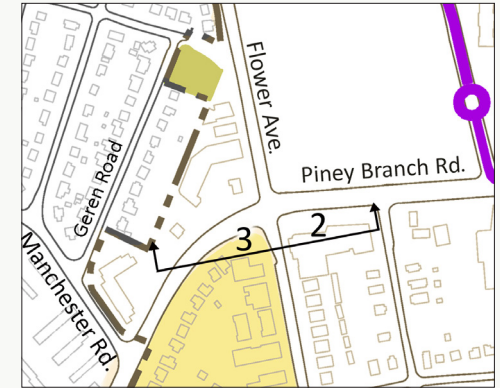
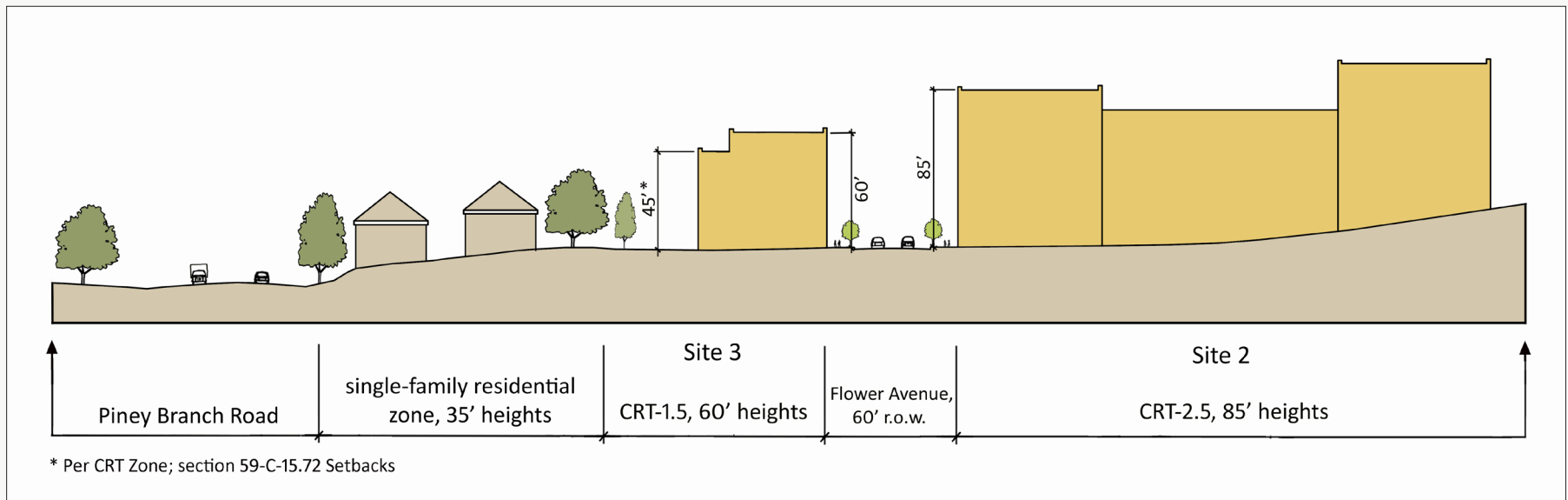


Illustration 12: Sites 2 and 3 Cross Section

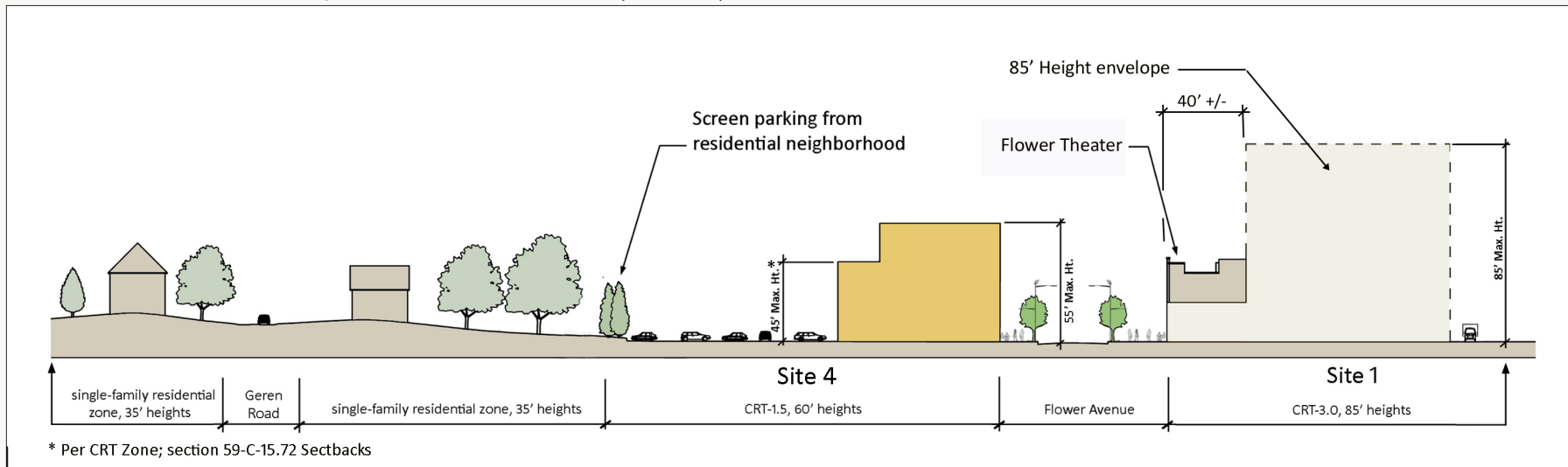


Site 4 and 1 Piney Branch Road/Flower Avenue (northwest)

- Maximum 55-foot building heights at the northwest quadrant of the intersection of Piney Branch Road and Flower Avenue.
- Maximum 85-foot building heights at the northeast quadrant of the intersection of Flower Avenue and Piney Branch Road.
- Maximum 70-foot building heights at corner of Manchester Road and Piney Branch Road, stepping down to a 45-foot maximum adjacent to single-family homes on Geren Road.
- Orient commercial uses toward Piney Branch Road and Flower Avenue.
- Incorporate a gateway feature at the corner of Piney Branch Road and Flower Avenue that marks the gateway with an architecturally significant building or iconic art.
- At Piney Branch Road and Manchester Road, building setbacks should allow a landscape strip that emphasizes the residential character of this section of Piney Branch Road.



Illustration 13: Sites 4 and 1 Piney Branch Road/Flower Avenue (northwest)



Piney Branch Neighborhood Village

Site 5 Piney Branch Road/University Boulevard (northeast)

- Maximum 85-foot building heights, stepping down to a 45-foot maximum next to the single-family homes to the north on Heron Drive.
- Incorporate evergreen screening trees adjacent to single-family neighborhood.

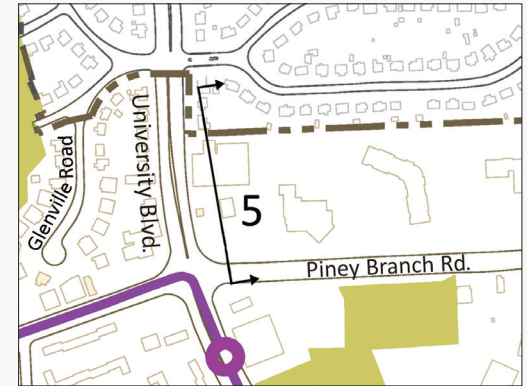
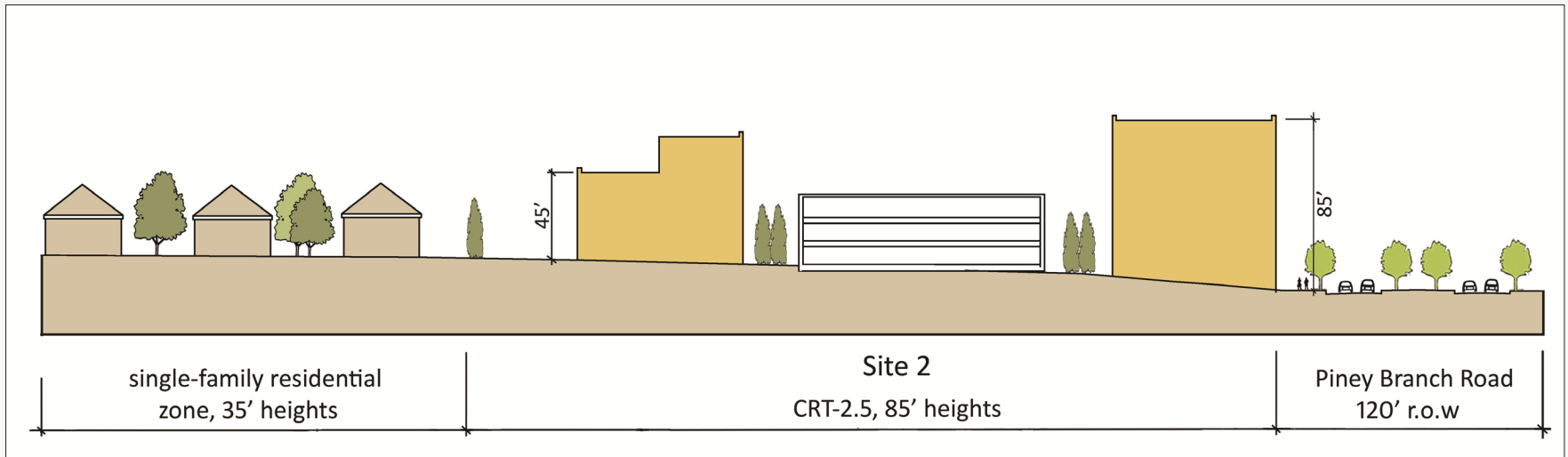


Illustration 14: Site 5 Piney Branch Road/University Boulevard (northeast)



Site 6 and New Hampshire Estates Local Park Section (southeast)

- Maximum 85-foot building heights.
- Incorporate the CRT Zone-required 10 percent public use space either at the intersection of Piney Branch Road and University Boulevard or at the corner of Garland Street extended and University Boulevard.

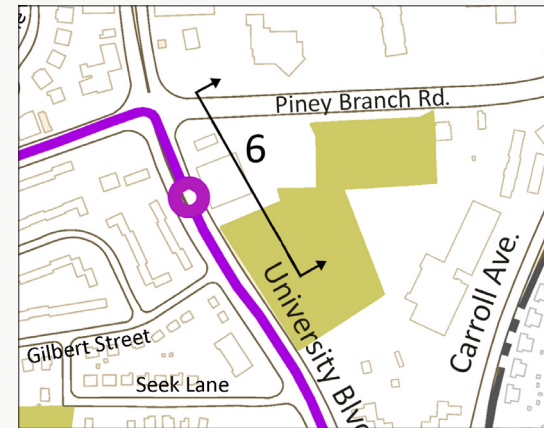
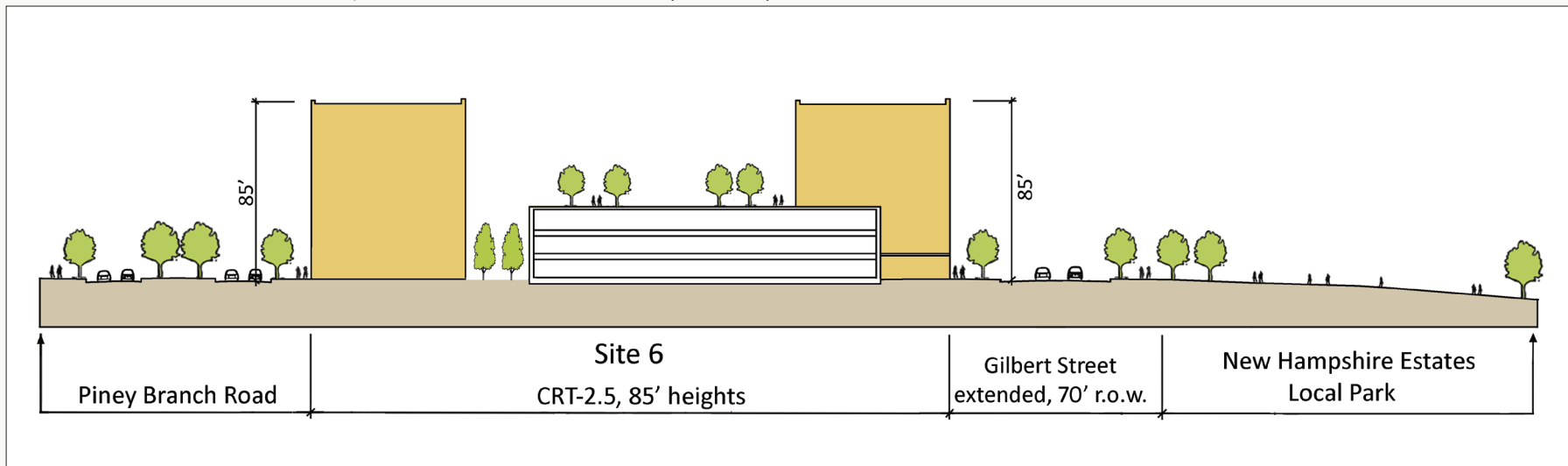


Illustration 15: Site 6 and New Hampshire Estates Local Park Section (southeast)

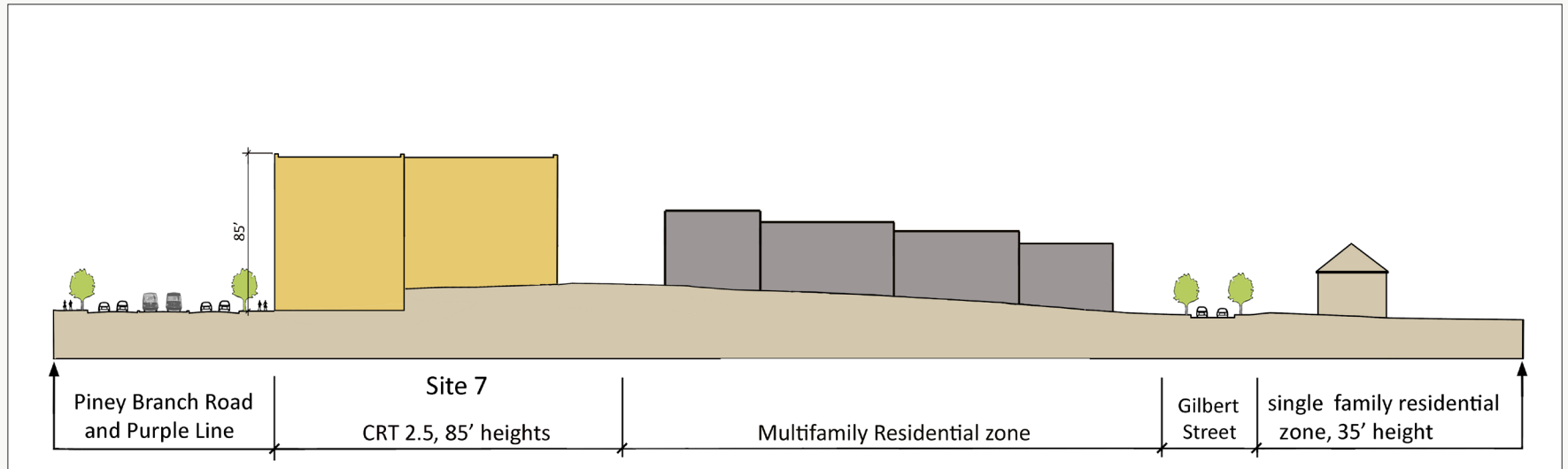


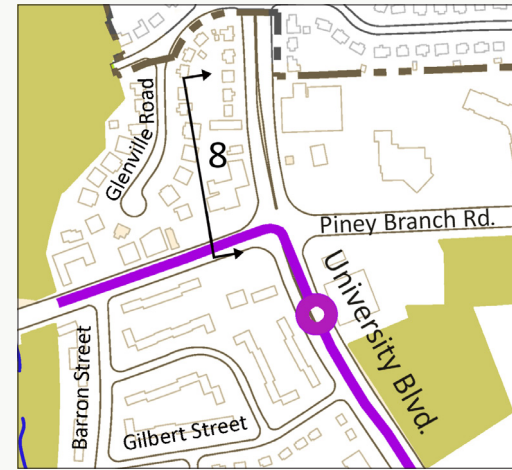
Site 7 Piney Branch/University Boulevard (southwest)

- Maximum 85-foot building heights.
- Preserve neighborhood retail services in ground floor retail at Piney Branch and University Boulevard.



Illustration 16: Site 7 Piney Branch/University Boulevard (southwest)

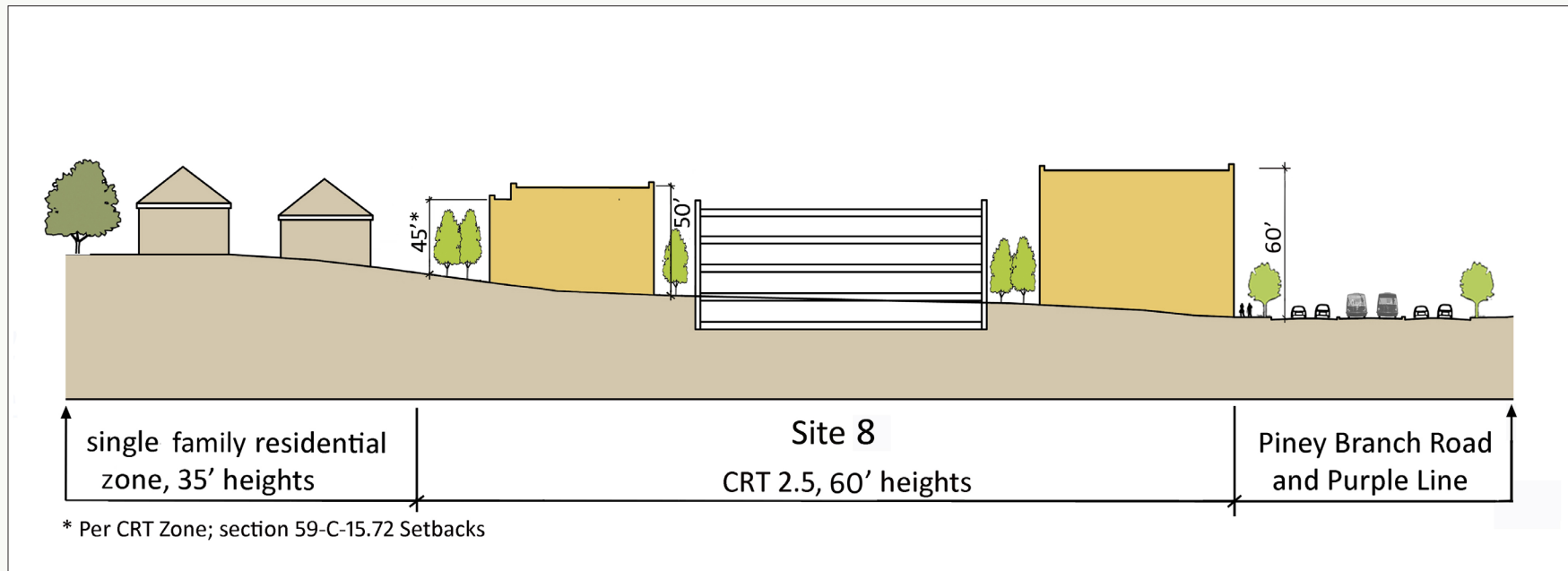




Site 8 Piney Branch Road/University Boulevard (northwest)

- Maximum 60-foot building heights, stepping down to a 45-foot maximum next to the single-family houses.
- Provide access from Piney Branch Road as part of redeveloping this quadrant.
- Incorporate evergreen screening trees adjacent to single-family neighborhoods.

Illustration 17: Site 8 Piney Branch Road/University Boulevard (northwest)



Site 9 Carroll Avenue and Piney Branch Road

- Maximum 60-foot building heights at the intersection of Piney Branch Road and Carroll Avenue, stepping down to a 45-foot maximum next to townhouses on Carroll Avenue.

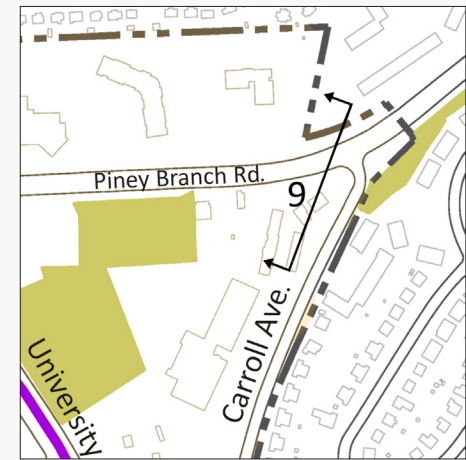
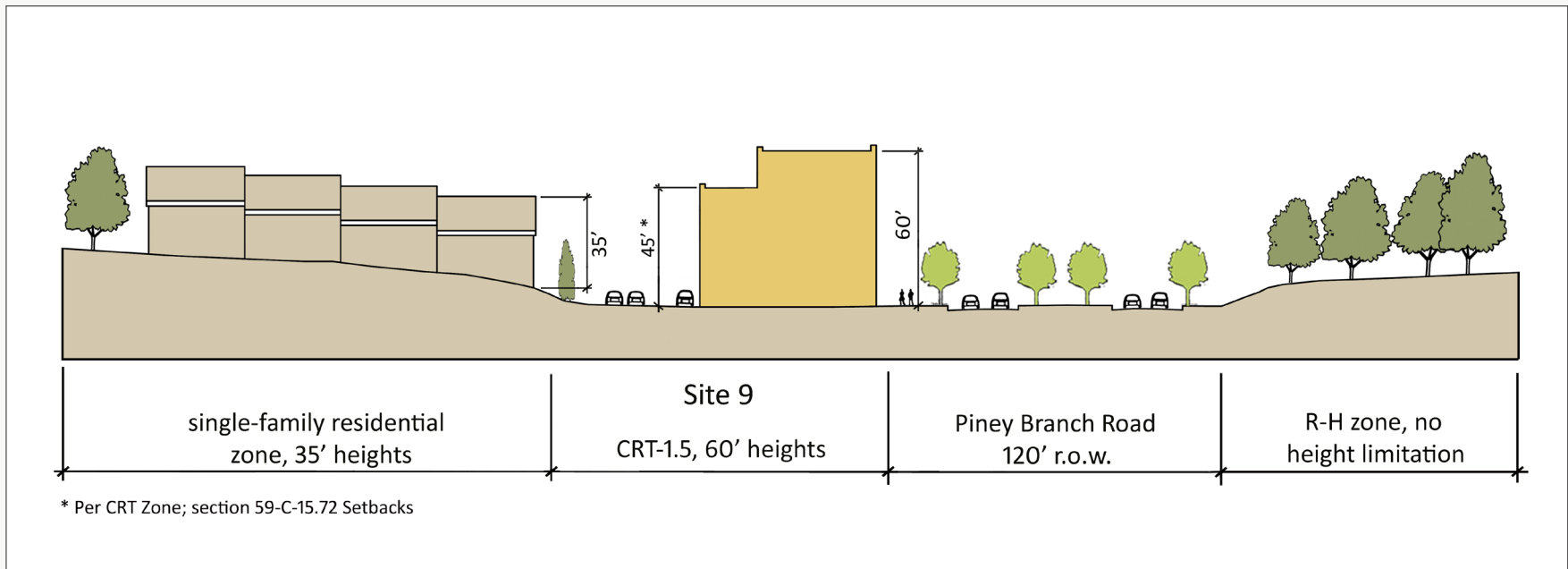


Illustration 18: Site 9 Carroll Avenue and Piney Branch Road



Site 10 Forston Street and University Boulevard East

- Maximum 60-foot building heights at the intersection of Forston Street and University Boulevard East, stepping down to a 45-foot maximum next to single-family houses on Forston Street.

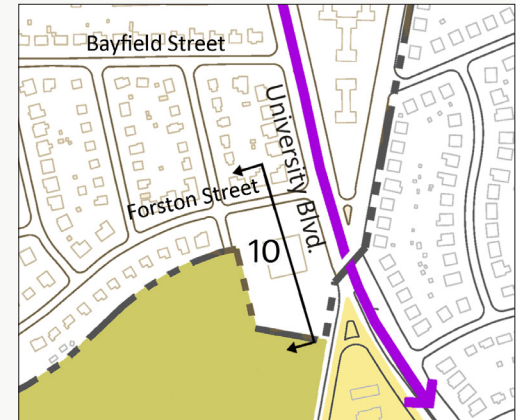
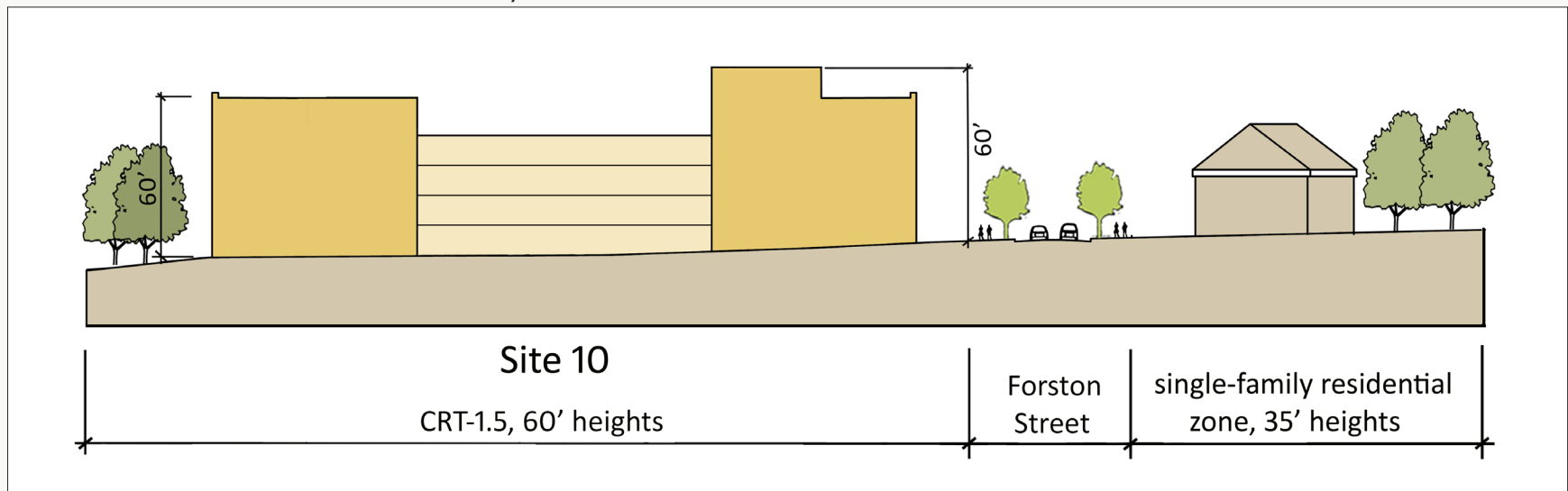


Illustration 19: Site 10 Forston Street and University Boulevard East



long branch sector plan

Design Guidelines

November 2013



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Silver Spring, MD 20910

MontgomeryPlanning.org