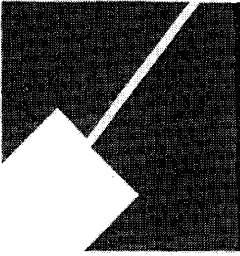


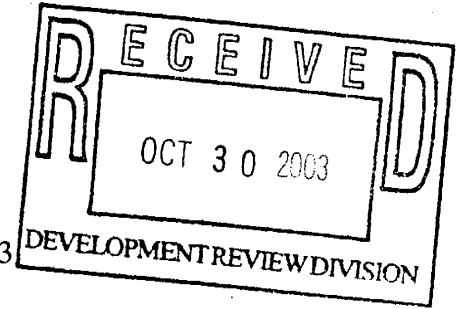
M-NCPPC



MONTGOMERY COUNTY DEPARTMENT OF PARK & PLANNING

THE MARYLAND-NATIONAL CAPITAL
PARK AND PLANNING COMMISSION


8787 Georgia Avenue
Silver Spring, Maryland 20910-3760




October 29, 2003

MEMORANDUM

TO: Malcolm Shaneman, Supervisor
Development Review Division

VIA: Ronald C. Welke, Supervisor
Transportation Planning Division 

FROM: Cherian Eapen, Planner/Coordinator
Transportation Planning Division 

SUBJECT: Preliminary Plan No. 1-01010-A, Burnt Mills Shopping Center
Columbia Pike (US 29), Fairland/White Oak Policy Area

This memorandum summarizes Transportation Planning staff's Adequate Public Facilities (APF) Ordinance review of the above Preliminary Plan to build a 32,392 square-foot retail center to the east of Columbia Pike (US 29) between Lockwood Drive and Southwood Avenue in the C-4 and R-90 Zones within the Fairland/White Oak Policy Area.

RECOMMENDATIONS

Transportation Planning staff recommends the following conditions as part of the APF test for transportation-related requirements to approve this Preliminary Plan:

1. Limit development on the site to a retail center of up to 32,392 square-feet.
2. Consistent with the 1997 Approved and Adopted White Oak Master Plan, dedicate adequate right-of-way along US 29 to provide 60 feet of right-of-way from centerline.
3. Obtain approval from the Maryland State Highway Administration (SHA) and design/install a fully operational, coordinated traffic signal at the access driveway to the proposed retail center on US 29 at the applicant's expense.
4. Provide pedestrian signals and a crosswalk across US 29 that will facilitate safe pedestrian crossing and connectivity between the Northwest Branch Stream Valley trails on either side of US 29.

5. Provide, as methods to reduce Local Area Transportation Review (LATR) impact per Section IV-B of the *Local Area Transportation Review Guidelines*, the following:
 - a. Two bus shelters and two real-time transit information signs on US 29 in the vicinity of the site. Coordinate with the Montgomery County Department of Public Works and Transportation (DPWT), Transit Services Division, to determine appropriate locations for the bus shelters and real-time transit information signs.
 - b. 1,170 linear feet of sidewalk improvements along US 29 in the vicinity of the site. Coordinate with DPWT's CIP Project No. 509997 – US 29/Lockwood Drive Sidewalks – to provide the above sidewalks. Staff suggests that the applicant give priority to the reconstruction/extension of sidewalks along both sides of US 29 that will connect to the Northwest Branch Stream Valley Park.
6. Provide necessary intersection operational/physical improvements at the US 29/Southwood Avenue intersection to accommodate an exclusive left-turn lane and a shared left-turn/right-turn lane on Southwood Avenue.
7. Provide a five-foot wide sidewalk and a tree panel on US 29 along the entire property frontage. The sidewalk should be offset two feet from the property line.
8. Coordinate with SHA, DPWT and M-NCPPC staff on the above recommendations, as appropriate.

DISCUSSION

Site Location and Access

The site, which formerly housed a 21,000 square-foot furniture store, is located on the east side of US 29 just within the southern boundary of the White Oak Master Plan area. US 29 is a northeast to southwest six-lane divided highway under SHA jurisdiction. Traffic signals currently exist along US 29 to the north of the site at its intersection with Lockwood Drive (approximately 1,100 feet from the proposed site driveway) and to the south of the site at its intersection with Southwood Avenue (approximately 2,700 feet from the proposed site driveway).

There is a mix of land uses in the area immediate to the site including retail, commercial, office, residential and recreational. A gas station and an auto repair facility are located immediately to the south of the site. Further south is the Northwest Branch Stream Valley, where the Master Plan area ends. An apartment complex is located to the rear of the site. Immediately to the north of the site, there is a convenience store, an auto repair facility, and several office uses. Further north and northeast of the site, land use is generally residential. The corporate headquarters for Choice Hotels International is located opposite the site on the west side of US 29.

Access to the site is proposed off US 29 via two driveways. The main site access driveway is proposed to be located across from the driveway to the Choice Centre, which the applicant proposes

to signalize. Staff has confirmed that an affirmative recommendation was forwarded from District 3 of the SHA to the Office of Operations and Traffic Safety, which has approved the request for the design and installation of a traffic signal at this location at the applicant's expense. In addition, the applicant will coordinate with the adjacent property owner to the south to eliminate one of the driveways to that property and to consolidate those movements into the applicant's property. The second site access driveway will be located to the north of the site and will be a right-turn in/right-turn out only driveway.

Several Metrobus Z routes and Ride-on route 22 serve this area and have bus stops along US 29, including in front of the site.

Master Plan Roadways and Pedestrian/Bikeway Facilities

The 1997 Approved and Adopted White Oak Master Plan describes Columbia Pike (US 29) along the front of the property as a Major Highway (M-10) between the Master Plan area boundary (Northwest Branch) to the southwest and New Hampshire Avenue (MD 650) to the northeast. A 120-foot minimum right-of-way (six-lane divided) is recommended for this section of US 29. The Master Plan recommends sidewalk improvements along US 29 between Burnt Mills Avenue and Northwest Branch. A Class III bikeway (PB-6) is also proposed in the Master Plan for US 29 between Lockwood Drive and Northwest Branch.

Nearby Transportation Improvement Projects

The Department of Public Works and Transportation's Capital Improvement Program (CIP) include the following nearby project:

No. 509997 – US 29/Lockwood Drive Sidewalks: for the engineering, right-of-way acquisition and construction of approximately 5,000 feet of sidewalk on the east side of US 29 to provide continuity of pedestrian facilities between University Boulevard (MD 193) to the south and Prelude Drive to the north as well as construction of approximately 700 feet of missing sidewalk links on the east side of Lockwood Drive from US 29 to MD 650. The scope also includes construction of approximately 3,300 feet of sidewalk on the west side of US 29 where feasible. The project includes the construction of retaining walls to reduce the impacts to properties adjacent to the public right-of-way. Construction of the US 29 sidewalks began in July 2002.

Local Area Transportation Review

A traffic study was required for the proposed Burnt Mills Shopping Center development per *Local Area Transportation Review Guidelines* since it generated more than 50 peak-hour trips during weekday morning (6:30 a.m. – 9:30 a.m.) and evening (4:00 p.m. – 7:00 p.m.) peak-periods.

The consultant for the applicant submitted a traffic study (dated October 2002), which was followed up with a warrant study for the proposed traffic signal at the main site access driveway intersection with US 29, and several supplementary analyses that discussed methods to reduce traffic impact related to the proposed development on study area roadways. The studies investigated the need for any off-site intersection improvements as well.

Using M-NCPPC trip-generation rates for a retail use, the proposed 32,392-square-foot retail center was estimated to generate 62 and 246 peak-hour trips during the respective weekday morning and evening peak-periods. Utilizing trip credit provided in the LATR for applicant-sponsored trip mitigation measures, the site was estimated to generate 13 total peak-hour trips during the weekday morning peak period and 197 total peak-hour trips during the weekday evening peak period. The trip reduction measures proposed by the applicant under Section IV-B of the *LATR Guidelines* (see Attachment 1) included providing two bus shelters (at ten trips per bus shelter for a credit of 20 trips), two real-time transit information signs (at ten trips per sign for a credit of 20 trips) and constructing 1,170 linear feet of sidewalks along US 29 (at one trip per 130 linear feet of sidewalk for a credit of nine trips) for a total credit of 49 peak-hour trips during both weekday morning and evening peak periods. Finally, with a staff approved “pass-by” trip rate of 45%, the site was estimated to generate seven “new” trips during the morning peak period and 108 “new” trips during the evening peak period. A summary of the above is provided in Table 1.

**TABLE 1
SUMMARY OF TRIP GENERATION CALCULATION
BURNT MILLS SHOPPING CENTER**

Trip Generation	Morning Peak Hour			Evening Peak Hour		
	In	Out	Total	In	Out	Total
Site Trips (32,392 SF Retail)	32	30	62	128	118	246
Trip Reduction Credit	-25	-24	-49	-25	-24	-49
Net Trips after Mitigation	7	6	13	103	94	197
Reduction for “Pass-by” Trips (45%)	-3	-3	-6	-46	-43	-89
Net “New” Site Trips	4	3	7	57	51	108

A summary of the Critical Lane Volume (CLV) analysis results for the study intersections for the weekday morning and evening peak hours during the respective peak periods as presented in an October 20, 2003 supplementary traffic analysis is presented in Table 2.

**TABLE 2
SUMMARY OF CAPACITY CALCULATIONS
BURNT MILLS SHOPPING CENTER**

Intersection	Traffic Conditions			Total w/Applicant Funded Mitigation Imps
	Existing	Background	Total	
<i>Morning Peak Hour</i>				
US 29 and Southwood Avenue	1,929	2,044	2,048	2,044
US 29 and Lockwood Dr/Choice Dr	1,684	1,799	1,801	1,799
US 29 and Burnt Mills Avenue	1,620	1,712	1,714	1,713
<i>Evening Peak Hour</i>				
US 29 and Southwood Avenue	1,614	1,815	1,832	1,804
US 29 and Lockwood Dr/Choice Dr	1,395	1,617	1,585	1,584
US 29 and Burnt Mills Avenue	1,378	1,539	1,545	1,544

Note: Fairland/White Oak Congestion Standard: 1,550 CLV; Silver Spring/Takoma Park Congestion Standard: 1,650 CLV

As shown in Table 2, under total traffic conditions with applicant-funded trip-mitigation measures and intersection operational/physical improvements, CLV values at the US 29 intersections with Lockwood Drive/Choice Drive and with Southwood Avenue do not exceed the respective CLV under Background Traffic Conditions. Additionally, the CLV increase at US 29/Burnt Mills Avenue intersection is less than five which, under the Annual Growth Policy (AGP) Section TL1 (see Attachment 2), satisfies the LATR requirement or is under the 1,550 CLV standard for the Fairland/White Oak Policy Area.

Therefore, the proposed development satisfies the LATR test with the applicant-funded trip-mitigation measures as well as intersection operational/physical improvements.

Policy Area Transportation Review/Staging Ceiling Conditions

Under the FY 2004 AGP, as of September 30, 2003, the remaining transportation staging ceiling capacity for non-residential development is positive 3,251 jobs in the Fairland/White Oak Policy Area.

The vacated furniture store, with 21,000 square feet of space, was equivalent to 53 jobs as a retail land use. The proposed retail center, with approximately 32,392 square feet of general retail space will represent a net increase of 28 jobs (based on a net increase of 11,392 square feet) within

the Policy Area, which is well below the available staging ceiling capacity. Therefore, the proposed development satisfies the Policy Area Transportation Review test.

RCW:CE:kew

Attachments

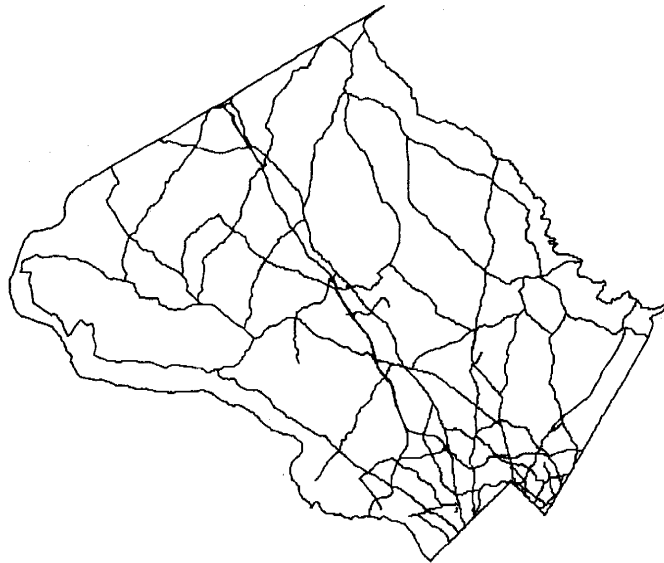
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William Kominers, Esq.
Greg Leck
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Approved
and
Adopted
July 2002

Local Area Transportation Review Guidelines

*Guidelines of the
Montgomery County Planning Board for the
Administration of the
Adequate Public Facilities Ordinance*



Published by



**THE MARYLAND-NATIONAL CAPITAL
PARK AND PLANNING COMMISSION**

Montgomery County Department of Park and Planning
8787 Georgia Avenue
Silver Spring, MD 20910-3760

B. Methods to Reduce Local Area Transportation Review Impact For Non-Residential Development

1. Construction of Sidewalks and Bike Paths

For non-residential *office* development, an applicant may choose to reduce LATR impact by constructing off-site sidewalks and/or bike paths that provide safe access from the proposed or an existing office development to any of the following uses:

- 1) Transit stations or stops (rail or bus)
- 2) Retail centers that employ 20 or more persons at any time
- 3) Housing projects
- 4) Other office centers
- 5) Existing sidewalks or bike paths

For non-residential *retail* development, an applicant may choose to reduce the LATR impact by constructing off-site sidewalks and/or bike paths that provide safe access from the proposed or an existing retail development to the following uses:

- 1) Transit stations or stops (rail or bus)
- 2) Office centers that employ 100 or more persons
- 3) Housing projects
- 4) Other retail development
- 5) Existing sidewalks or bike paths

These uses must be within a one-quarter mile of the edge of the proposed or an existing development. For transit stations or stops, the frequency of the transit service must be at intervals of 20 minutes or less during the morning and evening peak periods.

2. Provision of Bus Shelters

An applicant may also choose to reduce LATR impact by constructing a bus shelter, including a concrete pad, to encourage bus use which reduces weekday peak-hour vehicle trips by diverting some person-trips to buses. The bus shelter must be within one-quarter mile of the edge of the proposed or an existing development and the frequency of the transit service must be at intervals of 20 minutes or less during the weekday morning and evening peak periods.

For any off-site improvement shown in Table 4, pedestrians and bicyclists should be able to safely cross any roadway to reach their destination. The applicant may provide improvements that Transportation Planning staff agrees would increase the safety of the crossing.

3. Provision of Bike Lockers

An applicant may also choose to reduce LATR impact by providing bike lockers for a minimum of eight bikes at an activity center located within a one-mile radius of the edge of the development.

4. Provision of Real-Time Transit Information

An applicant may also choose to reduce LATR impact by providing electronic signs at bus shelters, large office buildings, retail outlets, transit centers, or residential complexes that indicate real-time transit information, e.g., the scheduled or estimated arrival of the next bus on a given route.

5. Maximum Reduction

The maximum reduction for any development is related to the congestion standard for that policy area. In policy areas with higher congestion standards, the maximum reduction in trips is higher, in recognition of the desire to encourage transit use in these areas. (See Table 4.)

The size of the development is a factor in determining the reduction in the number of trips that will be allowed for the construction of a sidewalk or bike path. The applicant may get a credit of one trip for each 130-foot section of sidewalk or bike path for 100 new employees within one-eighth mile of the off-site sidewalk or bike path being constructed. In other words, if there are 100 new employees within one-eighth mile of an off-site sidewalk or bike path being constructed, and the length of the off-site sidewalk or bike path is 1,300 feet, then the applicant may get credit for 10 trips. For bus shelter construction or real-time transit information sign installation, a non-residential applicant may get a credit of one trip reduction for every 25 new employees within one-quarter mile of the new shelter or sign, with a maximum of 10 trips per bus shelter or sign.

Table 4 identifies trip reduction options for non-residential development. Any or all of the options may be used for a given application.

Table 4: Trip Reduction for Non-Residential Development

Construction of:	Off-Site Sidewalks And Bike Paths	Bus Shelters	Bike Lockers (eight-locker facility)	Real-Time Transit Information Signs
Reduction in Trips during the Peak Hour	1 trip per 130 linear feet, with a minimum of 100 employees within 1/8 mile either side of the new sidewalk	1 per 25 employees within 1/4 mile of the shelter	1 trip per locker set	1 per 25 employees within 1/4 mile of the sign
Maximum Reduction	10 trips per sidewalk or bike path link	10 trips per shelter	1 trip per locker set	10 trips per sign
Maximum Trip Reduction per Development				
Congestion Standard				
1450-1600	20 (2600 feet)	20 (2 shelters)	2 (2 sets)	20 (2600 feet) 2 signs
1650-1800	30 (3900 feet)	30 (3 shelters)	3 (3 sets)	30 (3900 feet) 3 signs

Adopted Fiscal Year 2003
Annual Growth Policy
for Montgomery County, Maryland

Guidelines for the Administration of the
Adequate Public Facilities Ordinance
&
Growth Capacity Ceilings for FY2003

Adopted by the Montgomery County Council
Effective July 15, 2002

TP7.5 Special Ceiling Allocation for Affordable Housing

If an application for a preliminary plan approval that uses the special ceiling allocation for affordable housing is denied by the Planning Board after July 1, 1992, the applicant retains its original queue date and is subject to all other applicable provisions of TP7.

TL Local Area Transportation Review (LATR)

TL1 Establishment of Local Area Transportation Review Standards

The transportation planning model used for Policy Area Transportation Review addresses the average level of traffic in the policy area. If this were the only test, an area with acceptable average level of service could have one or more intersections, or roadway links, with unacceptably poor levels of service. It is necessary, therefore, that a local area test be applied to assure that new development is not allowed to cause such congestion.

To achieve an approximately equivalent transportation level of service in all areas of the County, greater congestion is permitted in policy areas with greater transit accessibility and usage. Table 7 shows the intersection level of service standards by policy area. Local Area Transportation Review must at all times be consistent with the standards and staging mechanisms of adopted master plans and sector plans.

Local Area Transportation Review must be undertaken for subdivisions which would generate 50 or more peak hour automobile trips in either of the following circumstances:

- for the policy area, total approved development is within 5 percent of the policy area ceiling; or
- for the local area, the proposed development is located near a congested area.

In administering the Local Area Transportation Review (LATR), the Planning Board must not approve a subdivision if it finds that an unacceptable peak hour level of service will result after taking into account existing roads, programmed roads, available or programmed mass transportation, and improvements to be provided by the applicant. If the subdivision will affect an intersection, or roadway link for which congestion is already unacceptable, then the subdivision may only be approved if it does not make the situation worse.

The nature of the LATR test is such that a traffic study is necessary if local congestion is likely to occur. The Planning Board and staff must examine the applicant's traffic study to determine whether adjustments are necessary to assure that the traffic study is a reasonable and appropriate reflection of the traffic impact of the proposed subdivision after taking into account all approved development and programmed transportation projects.

For Local Area Transportation Review purposes, the programmed transportation projects to be considered are those fully funded for construction in the first 5 years of the current approved Capital Improvements Program, the state's Consolidated Transportation Program, or any municipal capital improvements program.

For these purposes, any road required under Section 302 of the County Charter to be authorized by law is not programmed until the time for petition to referendum has expired without a valid petition, or the authorizing law has been approved by referendum.

If an applicant is participating in a traffic mitigation program or one or more intersection improvements to meet Local Area Transportation Review requirements, that applicant will be considered to have met Local Area Transportation Review for any other intersection where the volume of trips generated is less than five Critical Lane Movements.

The Planning Board has adopted guidelines for the administration of Local Area Transportation Review. To the extent that they are consistent with this Policy, the Planning Board guidelines may continue to apply or to be amended as the Planning Board deems it necessary to do so.

After consultation with the Council, the Planning Board may adopt administrative guidelines that allow use of a "delay" or queuing analysis, different critical lane volume standards, or other methodologies, to determine the level of congestion in appropriate geographic locations such as in urbanized areas, around Metrorail stations, or in specific confined areas planned for concentrated development related to other forms of transit.

In its administration of Local Area Transportation Review, the Planning Board must carefully consider the recommendations of the County Executive concerning the applicant's traffic study and proposed improvements or any other aspect of the review.

To achieve safe and convenient pedestrian travel, the Planning Board may adopt administrative guidelines requiring construction of off-site sidewalk improvements consistent with Section 50-25 of the County Code. To maintain an approximately equivalent transportation level of service at the local level considering both auto and non-auto modes of travel, the Planning Board may permit a reduction in the amount of roadway construction or traffic mitigation needed to satisfy the conditions of Local Area Transportation Review in exchange for the construction of non-automobile transportation amenities, such as sidewalks and bus shelters.

TL1.1 Metro Station Policy Area LATR Standards

In each Metro Station Policy Area, the Planning Board, in consultation with the Department of Public Works and Transportation, must prepare performance evaluation criteria for its Local Area Transportation Review. These criteria must be used to accomplish: (a) safety for pedestrians and vehicles; (b) access to buildings and sites; and (c) traffic flow within the vicinity, at levels which are tolerable in an urban situation. The County Executive also must publish a Silver Spring Traffic Management Program after receiving public comment and a recommendation from the Planning Board. This program must list those actions to be taken by government to maintain traffic flow at tolerable levels in the Silver Spring CBD and protect the surrounding residential area.

TL1.2 Potomac LATR Standards

In the Potomac Policy Area, only the areas contributing traffic to the following intersections must be subject to Local Area Transportation Review: (a) Montrose Road at Seven Locks Road; (b) Democracy Boulevard at Seven Locks Road; (c) Tuckerman Lane at Seven Locks Road; (d) Democracy Boulevard at Westlake Drive; (e) Westlake Drive at Westlake Terrace; (f) Westlake Drive at Tuckerman Lane; and (g) Bradley Boulevard at Seven Locks Road.

