



Boys Transit Station, Mandatory Referral, MR2020029

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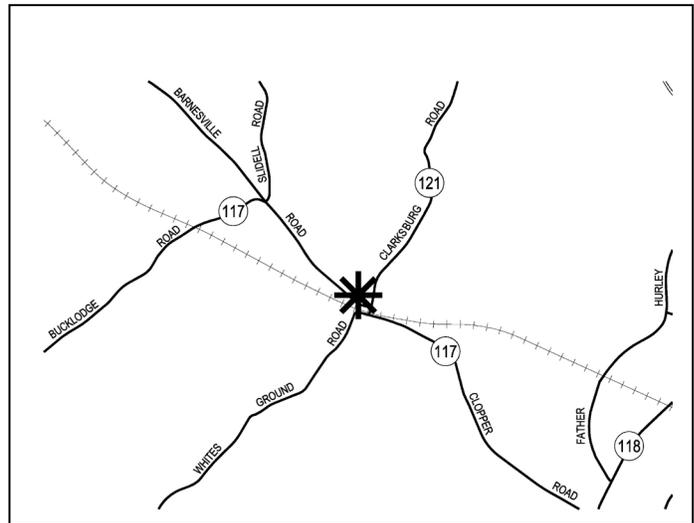
Completed: 10-01-20

Description

Construction of transit station improvements in Boyds, Maryland. The project elements are a 45-space parking lot, a bus stop to serve Ride On buses from Clarksburg, a 10-foot wide sidepath along the site frontage on Barnesville Road, and site access improvements on Barnesville Road.

- Applicant: Montgomery County Department of Transportation
- MARC Rail Communities Sector Plan (2019)

Staff Recommendation: Approval to Transmit Comments



Contents

Summary	2
Mandatory Referral Review	3
Recommendations	3
Proposal	5
Project Description.....	5
Project Background.....	6
Typical Cross Sections	8
Site Circulation/Parking and Bicycle Parking	11
Transportation Analysis	12
Design Elements - Transportation.....	12
Master Plan Conformance – Transportation	12
Master Plan – Major Recommendations	13
Environmental Analysis.....	15
Historic Resources Analysis.....	15
Community Outreach and Notification.....	16
Conclusion.....	16
Attachments.....	16

Summary

The Montgomery County Department of Transportation (MCDOT) is proposing transit station lot improvements on the site of the former Anderson property, located in Boyds, Maryland. The project includes the following improvements:

- Construction of a one-way parking loop with 39 parking spaces (2 of these are accessible spaces) on the former Anderson property,
- Construction of three parking spaces on the west side of the property,
- Construction of a concrete bus pad in front of the existing two buildings,
- Retention of the two existing larger buildings, the historic Hoyle's Mill building and the adjacent Poolesville Small Engine Repair Shop. This project includes the mothballing of the historic building to prevent further deterioration. The addition of a small restroom for bus drivers only is planned in the Poolesville Small Engine Repair Shop.
- Design and provision of 20 long-term bicycle parking spaces and 10 short-term bicycle parking spaces,
- Construction of a 5-foot-wide sidewalk on the west side of the property in front of the three parking spaces, running toward Barnesville Road (stops 35 feet short of Barnesville Road within the property),
- Construction of a sidewalk wrapping around the two buildings which will provide pedestrian access between the parking lot and the MARC rail station platforms,
- Construction of a 10-foot-wide sidepath with a 5-foot-wide grass buffer along the site frontage on Barnesville Road,
- Crosswalk improvements at the intersection of Barnesville Road with Clarksburg Road, and
- Construction of two site access locations along Barnesville Road, with the western drive providing full two-way access for all vehicles, while the eastern drive will provide bus egress only.

The project location is depicted in Figure 1. The current project, which includes full design and construction cost funding, is listed as CIP Project No. P501915. The current project cost estimate is \$5.65 million, which includes final design cost, land acquisition cost, as well as construction cost. The project is anticipated to be constructed in FY24. The 35 percent design plan presentation drawings are provided as Attachment A to this report. It should be noted that the two existing building structures will be maintained. The existing historic Hoyle's Mill building will be mothballed.



Figure 1: Project Limits and Site Vicinity

Mandatory Referral Review

This proposal for the construction of transit station improvements is required to undergo the Mandatory Referral review process under the Montgomery County Planning Department’s Uniform Standards for Mandatory Referral Review. State law requires all federal, state, and local governments and public utilities to submit proposed projects for a Mandatory Referral review by the Commission. The law requires the Planning Board to review and approve the proposed location, character, grade and extent of any road, park, public way or ground, public (including federal) building or structure, or public utility (whether publicly or privately owned) prior to the project being located, constructed or authorized.

Planning staff acknowledges that the implementation of master plan transportation recommendations is a challenge faced by the applicant in developing design plans to convert desired master plan recommendations into engineering design drawings. The design process up to 35 percent design typically brings clarity with considerably more detail than considered during a master plan, and issues such as environmental impacts, historical impacts, and construction costs may introduce new factors that need to be weighed in developing a final design solution. It is hoped that the Mandatory Referral process aids in this process to develop an optimal or at least an improved design solution.

Recommendations

Staff recommends **approval** to transmit the following comments to the Montgomery County Department of Transportation:

1. Prior to filing for any land disturbance permits, MCDOT should reengage with the community to proactively work on a plan for adaptive reuse of the Hoyle’s Mill Building.

2. Prior to filing for any building demolition permits, MCDOT should also consult with the Maryland Historic Trust on National Register evaluations and the need for further Section 106 review.
3. MCDOT should work directly with the HP Office and the Boyds Community on appropriate historical markers or signage to educate and inform visitors about the history of the area.
4. Design the grade, cross slope and material of the proposed sidepath to be level across both site driveways.
5. The applicant should work with the state to reduce the posted speed limit to 25 mph to match the Master Planned target speed of 25 mph. This will mitigate the sight distance deficiency at the egress drive.
6. The bus only egress is problematic and is likely to be used by cars, despite signage. This is a safety hazard because it has deficient sight distance for cars due to the existing vertical curvature. We recommend that additional measures, including a possible bus-activated gate, be considered to prevent autos from using this egress.
7. Consider alternatives to move the bus egress further west to avoid sight distance issues, which might require either shortening the bus stop primarily in front of the Poolesville Small Engine Repair Shop or by moving the bus stop further west as well (and this might impact the three parking spaces provided on the western side of the site).
8. The proposed shoulder and curb proposed along the eastbound direction along the site frontage is inconsistent with the MARC Rail Communities Sector Plan and should be modified.
9. A pedestrian warning signal for approaching trains and improving the at-grade crossing surface should be added at this station in coordination with CSX.
10. While we prefer a 6-foot buffer separation between a sidepath and the curb, the 5-foot buffer proposed is acceptable and consistent with Pedestrian Level of Comfort analyses now in use in the ongoing Montgomery County Pedestrian Master Plan. We do note that the Complete Streets Design Guidelines now under review by the Planning Board would likely recommend a minimum 6-foot-wide buffer on this type of street.
11. Signage should be added on Barnesville Road indicating the presence of bicycle parking consistent with requirements in Section 6.2.6 of the Montgomery County Zoning Code.
12. For the next design phase, more detail should be provided for the bicycle parking. This parking should be designed in conformance to Section 6.2.6 of the Montgomery County Zoning Code. It is important to note that each long-term bicycle parking space must be provided within a building, covered parking garage, or secure parking area located near the building or structure and the street or other bicycle right-of-way.
13. The two accessible parking spaces should be relocated to the western side of the site (near the proposed bike parking area) to be more proximate with the CSX platforms.
14. A crosswalk should be added to connect the sidewalk leading to the MARC northbound platform and the sidepath proposed along Barnesville Road.
15. The existing closed pedestrian underpass should be improved to be ADA accessible and reopened. The existing at-grade crossing is difficult for persons with mobility impairments especially in inclement weather.

16. MCDOT should work with CSX to explore the feasibility of adding a sidewalk on the south side of the two buildings, as this is a more direct way between the rail platforms and the parking lot. If this is not done, a fence will be needed to prevent this movement from occurring.
17. The channelized right-turn lane on Barnesville Road at Clarksburg Road should be modified to eliminate the channelized island (“hot right”). Crosswalk improvements would then be simpler to construct and safer. MCDOT should work with MDOT SHA to implement this improvement.

Proposal

Project Description

The Montgomery County Department of Transportation (MCDOT) is proposing transit station lot improvements to the former Anderson property site in Boyds, Maryland.

A project location map showing the regional context and other transportation design projects is provided in Figure 2.



Figure 2: Project Limits – Regional Context

The project site is surrounded by the MARC station and railroad tracks and residential and commercial-use properties. To the north is MD 117 (Barnesville Road) and further north is a wooded lot with several residential homes to the northwest. The current Boyds MARC station is adjacent to and south of the project site and consists of railroad tracks, pedestrian platforms and a patron parking area. Further to the south are residential homes along Clopper and White Ground Roads. Adjacent to the west of the project site is the active Boyds Country Store and further west along Barnesville Road is a small business complex.

A concrete bus loop will be provided along the western side of the site and additional parking loop will be provided on the eastern side of the site. The bus loop will consist of concrete pavement whereas the

parking area will be pervious pavement. A Filterra stormwater bioretention structure will be provided in the bus loop. Additional stormwater management facilities will not be provided. The Stormwater Management Concept/Site Development was approved on March 23, 2020. Landscaping and lighting will be provided throughout the site. Montgomery County Department of Transportation will maintain the facility.

A 10-foot-wide sidepath will be added along the frontage of MD 117 (Barnesville Road) as well as room for a future bikeable shoulder along the roadway. Minimal work will be done along existing MD 117 (Barnesville Road) and due to existing conditions, the road will not follow a standard typical section. The sidepath and future bikeable shoulder will allow a connection from the Ride On service to the existing Hoyle's Mill Trail that runs along MD 121 (Clarksburg Road). The addition of a bikeable shoulder follows the Montgomery County Bicycle Master Plan which envisions a bikeable shoulder along MD 121 (Clarksburg Road) and separated bikeways (sidepaths) along MD 121 (Clarksburg Road) and MD 117 (Clopper Road) going east towards Germantown. The Bicycle Master Plan intends to have safe and connected bicycle paths throughout Montgomery County. A sidewalk will be added internal to the site and along the bus stop and ultimately connect to the existing MARC station's northbound platform, the existing pedestrian tunnel, and the existing Boyds Country Store. Coordination with CSX Transportation will be required for the improvements within their right-of-way. A space for short-term and long-term bicycle parking has been provided near the MARC stop.

The existing historic Hoyle's Mill will remain. The adjacent existing hardware store may remain for a bus operator restroom and mechanical area for Ride On. Minimal work will be done to the existing hardware store to make it ADA compliant and therefore the building will not seek LEED Certification. A bus shelter will be provided in front of these buildings for users.

The existing site currently has two overhead utilities present which could interfere with construction. One existing septic field is located along the proposed bus loop and may require relocation. It is understood that there is an existing water well that may require relocation. These utilities will be test-pitted after preliminary engineering to determine their exact location and coordination with Montgomery County is underway.

Project Background

The Boyds MARC station is a stop along the Maryland Transit Administration's (MTA) MARC Brunswick line. It is currently a flag stop with four stops in the morning rush hours and six stops in the afternoon rush hours. Development to the north of the station along Barnesville Road (MD 117) consists primarily of commercial properties while development to the south of the station along Clopper Road is primarily comprised of residential properties. Clopper Road is currently an undivided two-lane two-way county roadway with a posted speed limit of 25 mph. While there are no pedestrian or bicycle facilities along the roadway, there is an existing pedestrian tunnel for access to the other side of the tracks located to the west of the station platform. The pedestrian tunnel has stairs and is not ADA compliant. The Boyds station and part of Clopper Road are located within the Boyds Historic District. Current ridership is in the high teens daily for the Boyds MARC train. The station's existing parking lot, which is owned by CSX Railroad and leased and maintained by MTA, provides fifteen (15) spaces.

In the early 2000s, the MTA considered closing the Boyds MARC station due to its low ridership and proximity to the Germantown station. However, over the years, the county received numerous citizen requests to keep the station open and add additional parking and Ride On routes to the Clarksburg and Germantown area. Given the current and projected growth in Clarksburg and Germantown, the county completed a feasibility study in 2015 with the purpose of adding parking and bus service to the Boyds station to keep the station viable and to potentially increase MARC service to the area.

Through extensive coordination with the Maryland-National Capital Park and Planning Commission (M-NCPPC) and the Boyds Civic Association's (BCA) Advisory Working Group, project purpose was defined and included adding a bus bay and turnaround for Montgomery County's Ride On system to access the Boyds MARC Station and adding parking with at least twenty-five (25) additional parking spaces in the same location. It was assumed implementation could be completed within five to ten years depending on funding and would require right-of-way acquisition and construction plans.

In order to achieve these goals, different sites in the area were researched and evaluated based upon numerous criteria developed by the study team for the feasibility study. Twelve (12) sites were selected to have potential in reaching at least one of the phased goals. The study team coordinated with M-NCPPC and the BCA's Advisory Working Group to select criteria and discuss these potential sites. The Anderson Property, located to the north of the tracks owned by CSX along Barnesville Road (Route 117), across from the existing MARC Station, and approximately 350 feet west of the intersection of Barnesville Road and Clarksburg Road, was selected as a preferred finalist. Combined, the two parcels total 1.16 acres of land. Currently, there are several buildings located on the properties with a gravel parking lot. One of those buildings, the Hoyle's Mill which is situated closest to the CSX tracks, is classified as historic and is located within the Boyds historic boundary. This determination was confirmed through coordination with the Historic Preservation Office.

A conceptual transit and parking option was developed utilizing the Anderson Property that combines sites 7 and 9 from the Boyds Feasibility Study. The goal is to provide a bus bay and turn around for Ride On to access the existing station and also provide parking spaces for commuters at the same location with easy access to the MARC platform. The option developed would satisfy both phases of the project goals identified in the Boyds Feasibility Study. In 2019, the county purchased this parcel for the expansion of the Boyds MARC station.

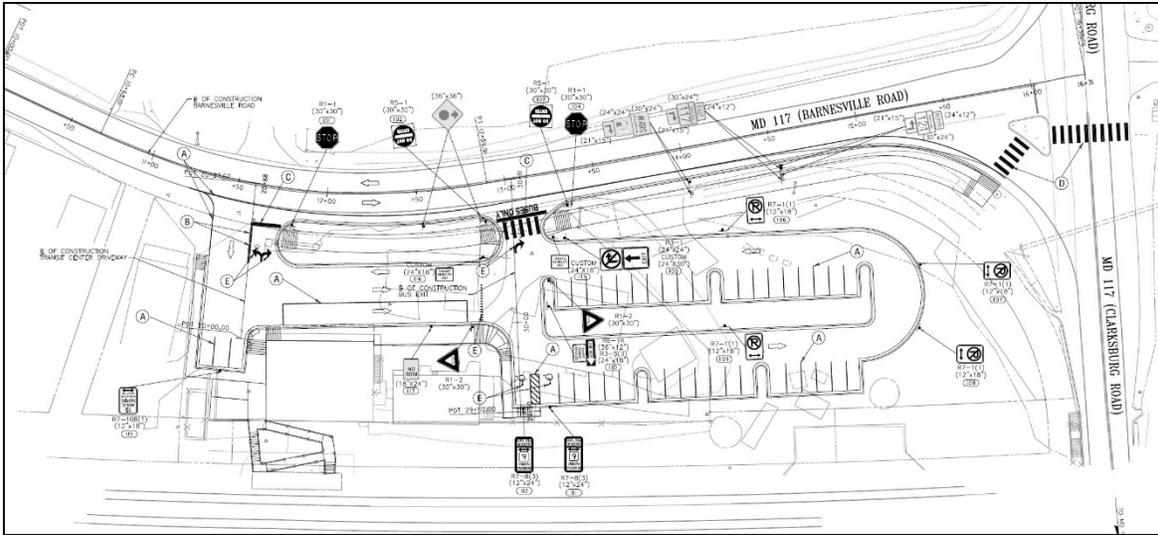


Figure 3 Site Plan – Pavement Marking Plan



Figure 4 Rendering – Proposed Boyds Transit Center looking North

Typical Cross Sections

Typical cross sections have been provided for the site at two locations, one on the western end of the site in the vicinity of the Bus Parking and adjacent to the two buildings (see Figure 5), and the second for the eastern end of the site in the vicinity of the parking loop (see Figure 6). In Figure 5, there will be a 10-foot-wide sidewalk in front of the existing buildings (7-foot minimum), a 12-foot-wide one-way bus

loading area and two 10-foot-wide travel lanes. Given the restriction of exiting traffic at the eastern driveway to bus traffic only, all car traffic will have to enter and exit the site via the western transit center driveway. This is an awkward circulation pattern that results in a large impervious surface area in front of the two buildings. The egress restriction is due to a sight distance deficiency at the eastern driveway for cars but not buses (due to higher driver elevation).

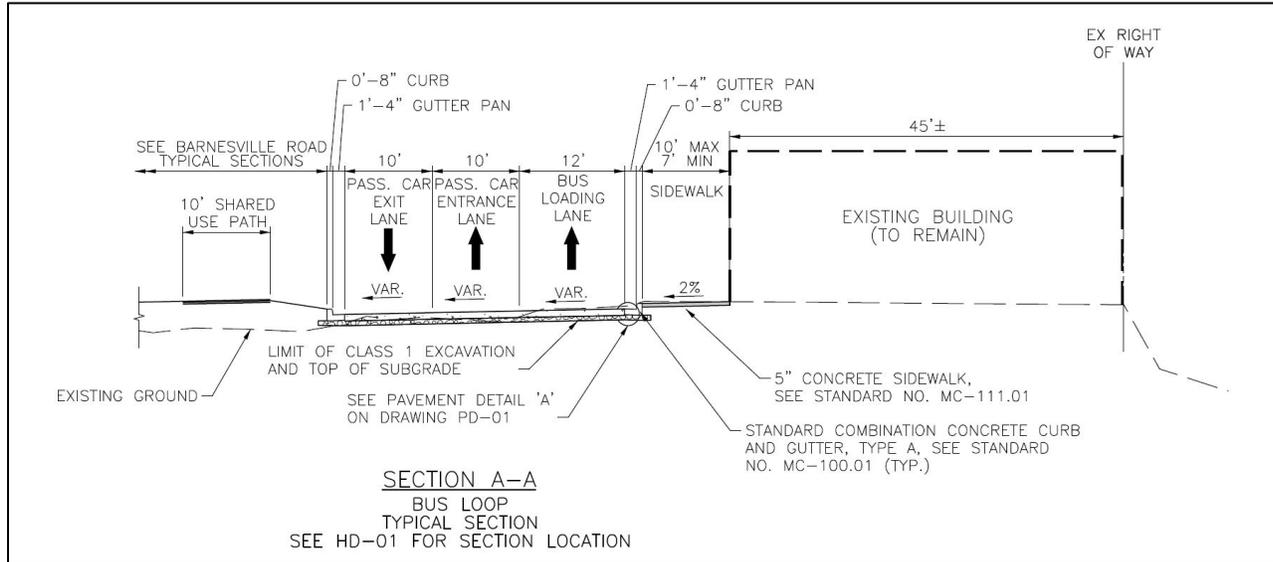


Figure 5 Cross Section – In Front of Existing Buildings

Figure 6 shows the one-way parking loop circulation which will be counterclockwise in direction with 17-foot-wide parking bays with 19-foot-wide travel lanes. No internal sidewalks will be provided in this portion of the site. Pedestrian circulation from parking spaces to the western portion of the side (where a sidewalk is planned) will require pedestrians to walk in the travel lane.

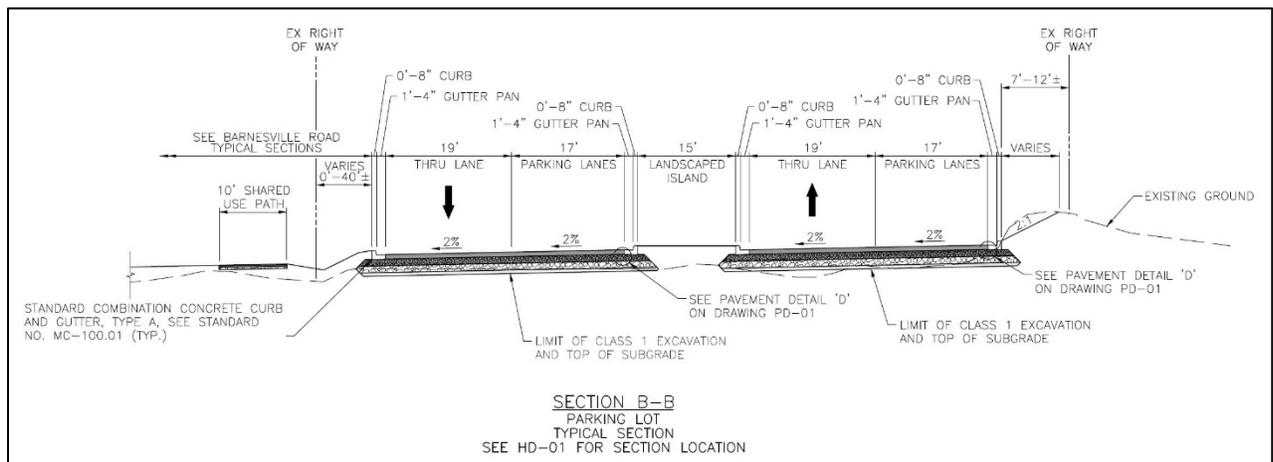


Figure 6 Cross Section – Parking Loop

Typical cross sections have been provided for Barnesville Road at two locations, one on the western end of the site in the vicinity of the bus parking and adjacent to the two buildings (see Figure 7), and the second

for the eastern end of the site in the vicinity of the parking loop (see Figure 8). For both, the proposed sidepath is shown as 10 feet wide with a 5-foot-wide grass buffer. Barnesville Road would be improved to provide a 4-foot-wide bikeable shoulder with curbing on the south or eastbound direction along the site frontage.

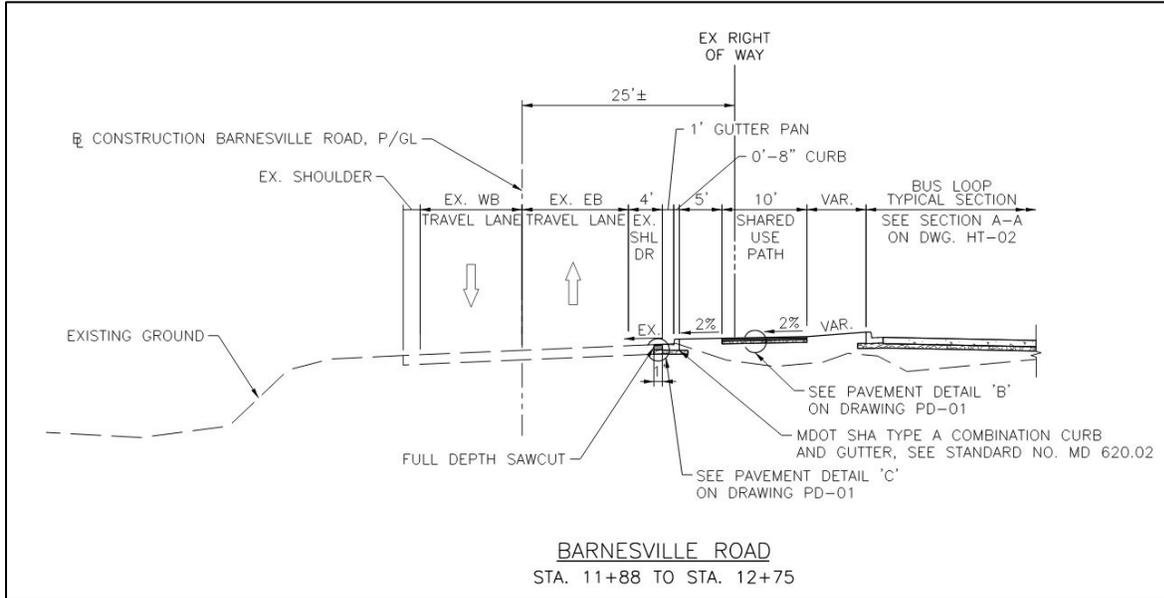


Figure 7 Cross Section – Barnesville Road – Western End of Site

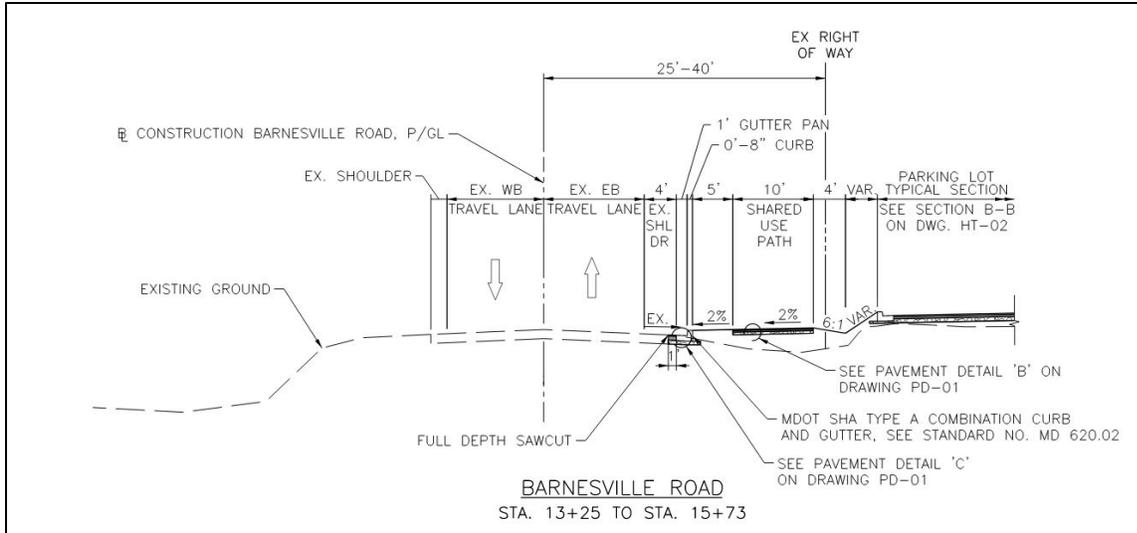


Figure 8 Cross Section – Barnesville Road – Eastern End

The internal site circulation is awkward for this site design, primarily due to a sight distance deficiency at the eastern driveway access. Due to a sight distance deficiency for cars but not buses, MCDOT has proposed that this driveway be restricted to car traffic, allowing this driveway to be used by buses for egress only (and typically a right turn onto Barnesville Road). The existing profile of Barnesville Road is shown in Figure 9. You will note that the proposed transit center drive is located at the crest of the hill, while the eastern drive is located downhill.

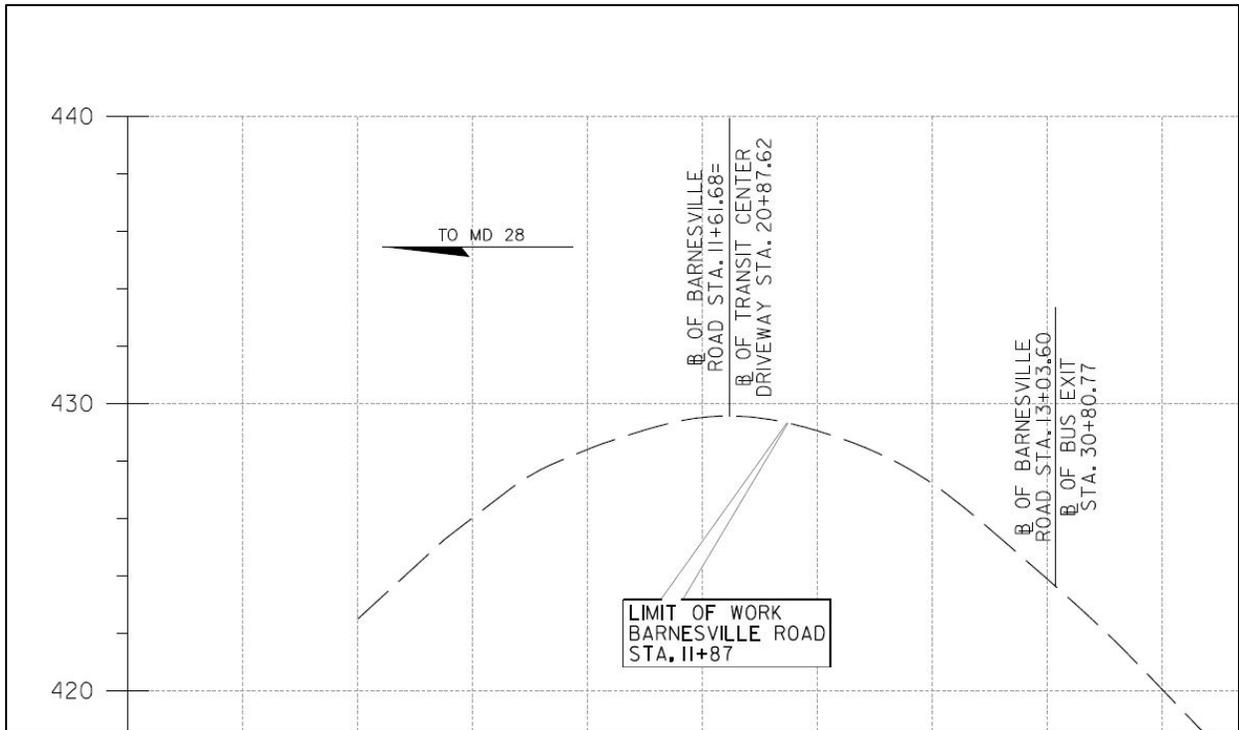


Figure 9: Profile – Barnesville Road (excerpt from plans) showing transit center driveway and bus egress locations

Site Circulation/Parking and Bicycle Parking

Figure 10 displays a site plan showing the different paving treatments proposed within the site and along the proposed sidepath, arrows highlight the proposed traffic circulation within the site, and the short and long-term bicycle parking locations. The dark areas on the figure indicate pervious pavement, which is proposed for the parking loop, the lighter shaded area is impervious concrete, and a small area located to the west of the two structures is paved with brick. The brick section has a surface area of 400 square feet, which is where the short and long-term bicycle parking spaces will be provided. No details on type and layout for bicycle parking were provided in this submission. Conformance with Section 6.2.6 of the Montgomery County Zoning Code is required. The site contains two accessible parking spaces which is consistent with Americans with Disabilities Act (ADA) requirements per the US Access Board.¹

¹ <https://www.access-board.gov/guidelines-and-standards/buildings-and-sites/about-the-ada-standards/ada-standards>

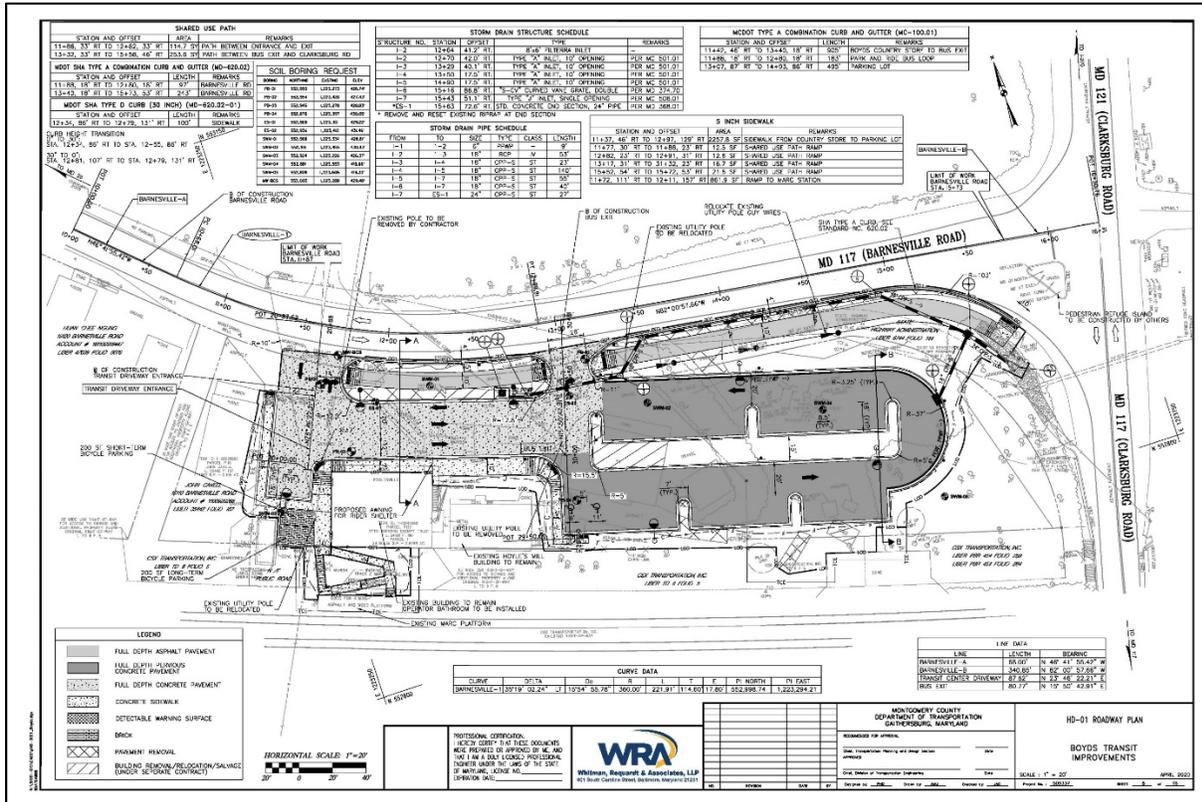


Figure 10: Pavement and Circulation Plan

Transportation Analysis

Design Elements - Transportation

General Comment: The proposed 10-foot-wide sidepath is consistent with the default sidepath width in the approved Bicycle Master Plan and the ongoing Complete Streets Design Guidelines. The 5-foot grass buffer is acceptable and consistent with Pedestrian Level of Comfort analyses now in use in the ongoing Montgomery County Pedestrian Master Plan. We do note that the Complete Streets Design Guidelines now under review by the Planning Board would likely recommend a minimum 6-foot-wide buffer on this type of street.

Master Plan Conformance – Transportation

The project is in conformance with the 2018 Bicycle Master Plan and the 2018 Master Plan of Highways and Transitways (MPOHT). The 2018 Bicycle Master Plan recommends a sidepath on the south side of Barnesville Road between the Boyds MARC rail station and Clarksburg Road (MD 121). On Clarksburg Road, the Bicycle Master Plan recommends a sidepath (east side) and bikeable shoulders between Ten-Mile Creek and Clopper Road.

The 2018 Master Plan of Highways and Transitways classifies Barnesville Road between Boyds Post Office and Clarksburg Road as a two-lane Country Road with a master plan right-of-way of 62 feet. The Master Planned Target Speed is 25 mph on Barnesville Road. Clarksburg Road is classified as a two-lane Arterial with a master plan right-of-way of 80 feet and master planned target speed of 30 mph.

The Boyds MARC rail station has been master planned in the 2018 Bicycle Master Plan to provide 20 long-term bicycle spaces and 10 short-term bicycle parking spaces. The project provides space for both long and short-term bicycle parking; however, no layout and related details have been provided at this stage of design.

Master Plan – Major Recommendations

The mandatory Referral for the Boyds MARC Station generally complies with the [MARC Rail Communities Sector Plan](#); however, there are some inconsistencies.

1. On page 16 of the Plan under Priorities, the Plan states “create a commuter-serving facility at the historic Hoyle’s Mill site, integrating the mill as part of the facility.” **The design calls for the existing facility to be mothballed so that it does not undergo further deterioration. Hopefully, in the future the historic structure will be renovated and better incorporated into the station area.**
2. On page 39, Connect Goal 1: Support the future viability of MARC rail through measures to expand ridership and service, while improving connections to the station. The Boyds-related recommendations include the following:
 - a. Retain the existing Boyds MARC stop and its function at its current location, and the existing MARC parking lot on Clopper Road south of the railroad tracks (Site BE).
 - b. Expand the supply of commuter parking at the Boyds station by utilizing the Anderson properties on the north side of the MARC stop near the intersection of Barnesville and Clarksburg Roads as a commuter-serving parking lot. **The project provides 42 parking spaces (Two of these spaces will be accessible).**
 - d. Establish regular Ride On bus service to the Boyds MARC Station, including appropriate bus-related facilities at the station, to accommodate additional MARC riders. **A Ride On bus loop is being provided at the station. It is a little wider than what we anticipated, but understandable given further sight distance analysis of Barnesville Road.**
 - e. Provide enhanced bicycle and pedestrian connections along and across the tracks near the Boyds MARC Station.
 - Connect the expanded commuter parking lot and new bus facilities to the MARC platform with ADA-compliant walkways and facilities. **Because there will not be a grade-separated ADA-accessible facility to connect the site with the southbound tracks, this goal is only partially met.**
 - Upgrade the existing pedestrian underpass with ADA-compliant ramps, wheelchair lifts, improved lighting on the ramps and entrance canopies to allow use during inclement weather. **The project does not touch the existing pedestrian underpass. It is hoped that in the future this pedestrian underpass will be upgraded to provide a safer crossing alternative to the current at-grade crossing.**
 - Improve the safety of the at-grade crossing over the railroad tracks for pedestrians, such as by installing a pedestrian warning signal for approaching trains and improving the at-grade crossing surface. **These improvements were**

not included with the applicant's submission, and in coordination with CSX, these project elements should be added.

3. On page 46, the Plan provides guidance on better pedestrian and bicycle connectivity along Barnesville Road. "c. Preserve the rural character of Barnesville Road in the historic district by minimizing changes to the road, its width, alignment and elements along the edges, such as trees and slopes created by the roadway." **The project includes a shoulder and curbing, which is inconsistent with the classification of Barnesville Road as a Country Road.**
 - Install a sidewalk along the south side of Barnesville Road, from the Boyds Post Office at the western plan boundary to the MARC station. Design a sidewalk with an informal, meandering alignment to limit the loss of trees and the need for retaining walls. **The design upgrades the sidewalk to a 10-foot wide shared use path.**
4. On page 70 in the Renew Chapter, the Plan provides additional guidance on the enhanced MARC station.
 - Provide a MARC and Ride On shelter possibly with a restroom. Existing or sensitively-designed new structures may be used for this purpose. These structures provide a co-location opportunity for community uses. **The restrooms in the Poolesville Small Engine Repair Shop will be available for the bus drivers only, not the general public. This is inconsistent with the Sector Plan recommendation.**
 - Create a plaza area and green space within the bus loop. Preserve the green area to protect the existing septic field. **The project eliminates the green area in order to mitigate a sight distance deficiency. This is inconsistent with this recommendation.**
 - Design the new walkways connecting the parking area and bus loop to the MARC platform for compatibility with the historic Hoyle's Mill and other resources. Sensitively design canopies over the pedestrian underpass with guidance from Historic Preservation staff to assure visual compatibility. **The Poolesville Small Engine Repair Shop will have an awning in front for commuters. The walkways from the parking lot to the MARC platform will be made of concrete. All of these should be reviewed by the Historic Preservation staff to ensure that they are consistent with the historic resource, Hoyle's Mill, and its setting. Site lighting would be designed to be pedestrian -scaled and consistent with the Boyds Historic District.**

The project attempted to limit impervious surfaces to some extent with the use of pervious pavement in the parking loop; however, the wide paved area within the site is also inconsistent with Recommendation 2b on page 42 as follows: "Minimize roadway and pavement widths to minimize impervious surfaces that adversely impact the water quality of Little Seneca Lake and its watershed."

The project is inconsistent with the MARC Rail Communities Sector Plan in the provision of curbing and shoulders as identified in Recommendation 3c (Boyds) on page 46, which specifically states "Classify Barnesville Road as a country road. Shoulders [and curbing] should not be added to the roadway to maintain the rural character of the area."

Environmental Analysis

Natural Resources Inventory/ Forest Stand Delineation

The Application meets the requirements of Chapter 22A of the Montgomery County Forest Conservation Law. A Natural Resources Inventory and Forest Stand Delineation (NRI/FSD) #420200900 for the Property was approved by staff on February 20, 2020. The Property is located in the Little Seneca Creek watershed Use Class I-P waters.

There are no streams, wetlands, or environmental buffers on or affecting the Property.

The Application meets all applicable requirements of the Environmental Guidelines. (see attached Forest Conservation report – Item 5A).

Forest Conservation

The Application meets the requirements of Chapter 22A of the Montgomery County Forest Conservation Law. A Forest Conservation Plan with a Tree Variance Request has been submitted for review and is recommended for approval as a separate action by the Planning Board as part of this Mandatory Referral review (see attached Forest Conservation report – Item 5A).

Note: For a complete analysis of conformance with Chapter 22A, please refer to the Final Forest Conservation Plan Staff report being considered concurrently with this Mandatory Referral review.

Historic Resources Analysis

The project is immediately adjacent to the Boyds Master Plan Historic District. The project appears to be partially located within a portion of the National Register-eligible historic district. The latter is governed by the Maryland Historic Trust and all project improvements must be reviewed by MHT through the section 106 review process for a finding of no adverse effect. For the Master Plan Historic District, the project improvements are occurring just at the District boundaries. The recently adopted MARC Rail Communities Plan noted the following goals for the property and adjacent historic mill:

- Plan Goal: Coordinate the design of improvements, such as sidewalk materials and retaining walls, within the historic district with Montgomery Planning’s Historic Preservation Office staff. Historic Preservation staff have reviewed and will continue to review the design.
- Prior to any changes, additions and/or removals, assess all the structures on the site for National Register of Historic Places significance. This site falls within the National Register Historic District. Review all potential changes to the mill with the county’s Historic Preservation Commission, remove the large, Dutch-gabled building and the small block building adjacent to Barnesville Road to create the room for a Ride On bus loop and drop off, provide a MARC and Ride On shelter possibly with a restroom. Existing or sensitively designed new structures may be used for this purpose. These structures provide a co-location opportunity for community uses. Create a plaza area and green space within the bus loop. Preserve the green area to protect the existing septic field. Design the expanded MARC parking area with an extensive tree canopy and stormwater management treatments to protect the water quality of Little Seneca Lake. Screen all parking with

landscaping and low walls to protect the historic resources and community character. Design the new walkways connecting the parking area and bus loop to the MARC platform for compatibility with the historic Hoyle's Mill and other resources. Sensitively design canopies over the pedestrian underpass with guidance from Historic Preservation staff to assure visual compatibility.

- The first recommendations must be coordinated with the Maryland Historical Trust. The screening and landscape design are adequate. The pedestrian underpass is not being altered at this time.
- Adaptively reuse the historic Hoyle's Mill within the Boyds Historic District and explore reuse of the nearby barn building outside of the historic district to establish a focal point for the Boyds commercial area (Site BE) (See Appendix A for MCDOT's Concept Study): Assess all of the structures on the site with the historic Hoyle's Mill, Parcels 155 and 157, for National Register eligibility prior to any changes, additions and/or removals. Commemorate historical uses on the parcel, including, among others, the location of the former station house, which is no longer standing. Redesign the site to support the reuse of the mill building. Coordinate all changes to the mill with the Montgomery County Historic Preservation Commission. Design alterations to the repurposed mill and any other buildings—including landscaping, lighting and walkways—in a manner that is sensitive to the surrounding historic district. Design landscaping, lighting, signage, paths and structures that support and are sensitive to the character of the historic district. Explore relocation and reuse of the Quonset hut and the barn building, if possible.
- Adaptive Reuse of the Mill is a Plan Goal, but this is not being addressed directly in this application. The adjacent warehouse/office building is also being retained; this will help to further the adaptive reuse of the mill site by providing a potential adjacent location that has water and electrical hookups that the mill lacks. There is no interpretation proposed in the application. MCDOT should work directly with the HP Office and the Boyd's Community on appropriate historical markers or signage to educate and inform visitors about the history of the area. Relocation and Reuse of the Quonset Hut and Barn were not explored.

Community Outreach and Notification

This application was noticed in accordance with the Uniform Standards for Mandatory Referral Review. Presentations to the Boyds Civic Association occurred on July 18, 2019 and October 18, 2019. A public meeting was held on October 22, 2019 at Ronald McNair Elementary School in Germantown to discuss this project and design consideration. Additionally, numerous meetings with the Boyds Civic Association and a public meeting were held for the feasibility study prior to this project.

Conclusion

Based on information provided by the applicant and the analysis contained in this report, staff concludes that the proposed Boyds Transit Station project can be designed with some modifications to meet transportation standards as specified on pages 3 through 5 of this staff report.

Attachments

- A. Proposed Project Plans



Mark Elich
County Executive

DEPARTMENT OF PERMITTING SERVICES

Hadi Mansour
Acting Director

Mr. Jason Cosler, PE
March 23, 2020
Page 2 of 2

March 23, 2020

Mr. Jason Cosler, PE
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Baltimore, MD 21231
Via email: jcosler@wralp.com

Re: **COMBINED STORMWATER MANAGEMENT CONCEPT/SITE DEVELOPMENT STORMWATER MANAGEMENT PLAN** for
Boys Transit Center
Address: 15100 Barnesville RD
SM File #: 285472
Tract Size: 1.21 ac
Total Concept Area: 1.21 ac
Watershed: Seneca Creek

This letter must appear on the sediment control/stormwater management plan at its initial submittal. The concept approval is based on all stormwater management structures being located outside of the Public Utility Easement, the Public Improvement Easement, and the Public Right of Way unless specifically approved on the concept plan. Any divergence from the information provided to this office, or additional information received during the development process, or a change in an applicable Executive Regulation may constitute grounds to rescind or amend any approval actions taken, and to reevaluate the site for additional or amended stormwater management requirements. If there are subsequent additions or modifications to the development, a separate concept request shall be required.

If you have any questions regarding these actions, please feel free to contact Mary Fertig at 240-777-6340 or at may.fertig@montgomerycountymd.gov.

Sincerely,

Mark C. Etheridge, Manager
Water Resources Section
Division of Land Development Services

MCE:MMF

cc: N. Braunstein
SM File # 285472

ESD Required/Provided: 0.945 cf / 3.120 cf
PE Target/Achieved: 2.97 / 1.0
STRUCTURAL: N/A
WAIVED: 3.120 cf

Dear Mr. Cosler:

Based on a review by the Department of Permitting Services Review Staff, the stormwater management concept for the above mentioned site is acceptable. The stormwater management concept proposes to meet required stormwater management goals via permeable paving. This approval includes the granting of a partial waiver from the requirements to provide on-site SWM.

The following items will need to be addressed during the detailed sediment control/stormwater management plan stage:

1. A detailed review of the stormwater management computations will occur at the time of detailed plan review.
2. An engineered sediment control plan must be submitted for this development.
3. All facilities must be designed using latest available MCDPS guidance documents.

This list may not be all-inclusive and may change based on available information at the time.

Payment of a stormwater management contribution in accordance with Section 2 of the Stormwater Management Regulation 4.90 is required.



DEVELOPER'S/BUILDER'S CERTIFICATION

I HEREBY CERTIFY THAT ALL CLEARING, GRADING, CONSTRUCTION AND/OR DEVELOPMENT WILL BE DONE PURSUANT TO THIS PLAN AND THAT ANY RESPONSIBLE PERSONNEL INVOLVED IN THE CONSTRUCTION PROJECT WILL HAVE A CERTIFICATE OF ATTENDANCE OF A DEPARTMENT OF NATURAL RESOURCES APPROVED TRAINING PROGRAM FOR THE CONTROL OF SEDIMENT AND EROSION BEFORE BEGINNING THE PROJECT.

DATE

TIMOTHY H. CUPPLES, P.E., CHIEF
DIVISION OF TRANSPORTATION ENGINEERING

DESIGN CERTIFICATION

I HEREBY CERTIFY THAT THIS PLAN HAS BEEN PREPARED IN ACCORDANCE WITH THE "2011 MARYLAND STANDARDS AND SPECIFICATIONS FOR SOIL EROSION AND SEDIMENT CONTROL", MONTGOMERY COUNTY DEPARTMENT OF PERMITTING SERVICES EXECUTIVE REGULATIONS 5-90, 7-02AM AND 36-90, AND MONTGOMERY COUNTY DEPARTMENT OF PUBLIC WORKS AND TRANSPORTATION "STORM DRAIN DESIGN CRITERIA" DATED JUNE, 2014.

DATE

JASON D. COSLER, P.E.
MD REGISTRATION NO. 28467

CERTIFICATION OF QUANTITIES

I FURTHER CERTIFY THAT THE TOTAL AMOUNTS OF EXCAVATION AND FILL AS SHOWN ON THESE PLANS HAVE BEEN COMPUTED TO BE 3,960 CUBIC YARDS OF EXCAVATION AND 475 CUBIC YARDS OF FILL AND THAT THE TOTAL AREA TO BE DISTURBED AS SHOWN ON THESE PLANS HAS BEEN DETERMINED TO BE A MAXIMUM OF 51,898 SQUARE FEET OR 1.19 ACRES.

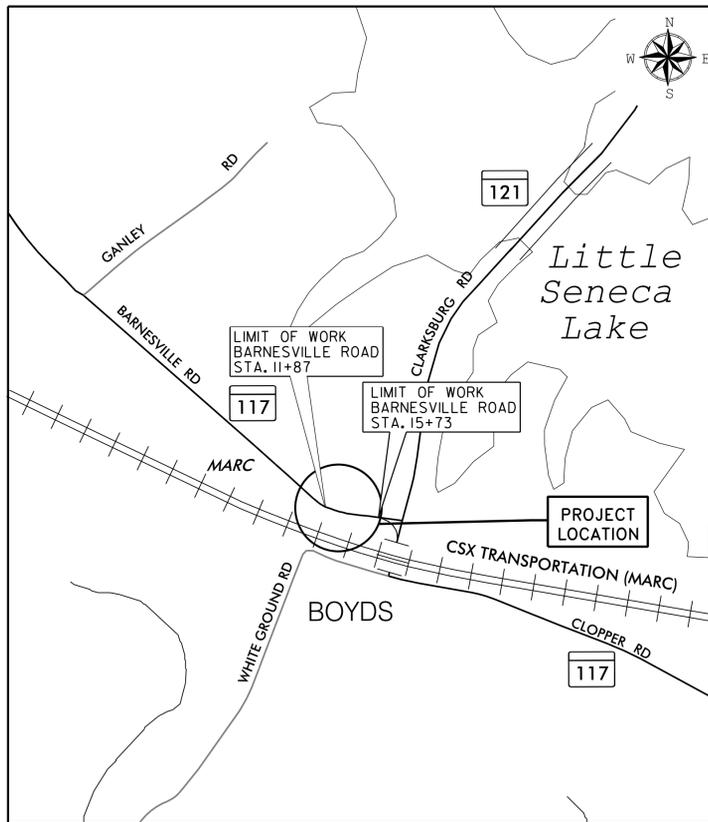
PAMELA H. DESTINO, P.E.
MD REGISTRATION NO. 42708

DATE

MONTGOMERY COUNTY DEPARTMENT OF TRANSPORTATION

BOYDS TRANSIT IMPROVEMENTS – 35% DESIGN

C. I. P. PROJECT 509337



PROJECT LENGTH = 0.07 MILES MONTGOMERY COUNTY

VICINITY MAP
SCALE: 1" = 500'

OWNER/ADDRESS:
MONTGOMERY COUNTY
DEPARTMENT OF TRANSPORTATION
100 EDISON PARK DRIVE, 4TH FLOOR
GAITHERSBURG, MD 20878

CONTACT:
REBECCA PARK, P.E.
240-777-7263

SWM SUMMARY TABLE								
measure number	type of measure	drainage area to measure	impervious area to measure	% Impervious	Rv for ESD calculations	Min ESDv	Max ESDv	Credited volume
PC-1	Permeable	0.35 ac.	0.35 ac.	100	0.95	-	3120 cf	3120 cf
Total		0.35 ac.	0.35 ac.					3120 cf
Structural								
Total	N/A	N/A						N/A
Does not drain to a SWM measure								
QN Waiver acres and % impervious								2,605 cf
QL Waiver acres and % impervious								N/A
Project								
Total		1.19 ac.	0.81 ac.	68	0.70	2862 cf	5725 cf	5,725 cf
ESDv Required		5,725 (ESD Target)/ 2,862 (ESD Min)						
ESDv Provided		5,725 cf						
Structural Required		N/A						
Structural Provided		N/A						

IT IS THE RESPONSIBILITY OF PERMITTEE/OWNER OF THIS SITE TO OBTAIN ALL REQUIRED PERMITS PRIOR TO ISSUANCE OF APPROVED SEDIMENT CONTROL PERMIT.

TYPE OF PERMIT	REQD	NOT REQD	PERMIT NO.	EXPIRATION DATE	WORK RESTRICTION DATES
M.C.D.E.P. Floodplain District		X			
WATERWAY/WETLANDS					
a. Corps of Engineers		X			
b. M.D.E.		X			
c. M.D.E. Water Quality Certification		X			
M.D.E. Dam Safety		X			
DNR Roadside Tree Care Permit	X				
DPS Roadside Tree Protection Plan	X				
N.P.D.E.S. NOTICE OF INTENT	X				
M.C.D.P.S. STORMWATER MANAGEMENT	X				
M.C.D.P.S. SEDIMENT CONTROL	X				

GENERAL NOTES

1. SITE CLEANUP, DEMOLITION AND GROUNDWATER REMEDIATION SHALL BE DONE UNDER A SEPARATE CONTRACT.
2. THE SPECIFICATIONS FOR THIS CONTRACT WILL BE THOSE OF THE MARYLAND DEPARTMENT OF TRANSPORTATION STATE HIGHWAY ADMINISTRATION DATED JULY 2019, ALL ERRATA AND ADDENDA THERETO, THE MARYLAND DEPARTMENT OF TRANSPORTATION STATE HIGHWAY ADMINISTRATION BOOK OF STANDARDS FOR HIGHWAY AND INCIDENTAL STRUCTURES, WASHINGTON SUBURBAN SANITARY COMMISSION (W.S.S.C.) STANDARDS, MONTGOMERY COUNTY DEPARTMENT OF TRANSPORTATION STANDARDS, AND SOIL CONSERVATION SERVICE POND CONSTRUCTION SPECIFICATIONS FOR MARYLAND.
3. HORIZONTAL DATUM: NAD 83(1991) VERTICAL DATUM: NAVD 88.
4. TYPES OF STORM DRAIN STRUCTURES REFER TO THE "DESIGN STANDARDS" OF MONTGOMERY COUNTY DEPARTMENT OF TRANSPORTATION, UNLESS OTHERWISE NOTED.
5. WHEN THE DROP ON THE MAIN LINE THROUGH A STORM DRAIN STRUCTURE CAN BE ACCOMMODATED BY AN INVERT SLOPE OF 1.5:1 OR FLATTER, A ROUNDED CHANNEL LINED WITH SEWER BRICK ON EDGE SHALL BE BUILT TO THE CROWN OF THE PIPES. WHEN THE INVERT SLOPES WOULD BE GREATER THAN 1.5:1 A SPECIAL INVERT SHALL BE CONSTRUCTED AS NOTED.
6. ALL STORM DRAIN PIPE SHALL BE INSTALLED WITH CLASS "C" BEDDING UNLESS OTHERWISE SPECIFIED.
7. THE CONTRACTOR SHALL MAKE FIELD ADJUSTMENTS TO STORM DRAIN STRUCTURES, WHEN NECESSARY, TO MEET EXISTING CONDITIONS, AS APPROVED BY MONTGOMERY COUNTY DEPARTMENT OF TRANSPORTATION'S PROJECT INSPECTOR.
8. INFORMATION CONCERNING UNDERGROUND UTILITIES WAS OBTAINED FROM AVAILABLE RECORDS, BUT THE CONTRACTOR MUST DETERMINE THE EXACT LOCATIONS AND ELEVATIONS OF THE LINES BY DIGGING TEST PITS BY HAND AT ALL UTILITY CROSSINGS, WELL IN ADVANCE OF TRENCHING. IF CLEARANCES ARE LESS THAN SHOWN OR SIX (6) INCHES, WHICHEVER IS LESS, CONTACT MONTGOMERY COUNTY DEPARTMENT OF TRANSPORTATION'S PROJECT INSPECTOR AND THE APPROPRIATE UTILITY OWNER BEFORE PROCEEDING WITH CONSTRUCTION.
9. REPAIRS TO UTILITIES OR PROPERTY DAMAGE AS A RESULT OF THE CONTRACTOR'S NEGLIGENCE OR METHOD OF OPERATION MUST BE MADE AT THE CONTRACTOR'S EXPENSE BEFORE PROCEEDING WITH CONSTRUCTION.
10. CLEARING IS TO BE LIMITED TO THE "LIMIT OF GRADING" AS SHOWN ON THE PLANS.
11. ALL GRADING SHALL BE DONE IN SUCH A MANNER AS TO PROVIDE POSITIVE DRAINAGE.
12. DISTURBED AREAS ADJACENT TO ESTABLISHED LAWNS SHALL BE SODDED. OTHER DISTURBED AREAS SHALL BE SEEDED AND MULCHED.
13. THE CONTRACTOR SHALL OBTAIN A ROADSIDE TREE PERMIT FOR ANY MAINTENANCE, TREATMENT, PLANTING, REMOVAL, OR ROOT CUTTING ON TREES WITHIN THE PUBLIC RIGHT OF WAY. PERMIT REQUIREMENTS MAY BE OBTAINED FROM THE DEPARTMENT OF NATURAL RESOURCES, MARYLAND FOREST, PARK AND WILDLIFE SERVICE, TELEPHONE 301-854-6060.
14. CONTACT THE WASHINGTON SUBURBAN SANITARY COMMISSION SYSTEM MAINTENANCE ENGINEER BEFORE EXCAVATING BENEATH OR IN THE VICINITY OF EXISTING WATER OR SEWER LINES. BACKFILL TO BE DONE UNDER SUPERVISION OF CALL WSSC 301-206-9772.
15. ALL UTILITY POLES NOTED FOR RELOCATION SHALL BE PERFORMED BY OTHERS.
16. PRIOR TO VEGETATIVE STABILIZATION, ALL DISTURBED AREAS MUST BE TOPSOILED PER THE MONTGOMERY COUNTY "STANDARDS AND SPECIFICATIONS FOR TOPSOIL".
17. CALL "MISS UTILITY" AT 1-800-257-7777 48 HOURS PRIOR TO THE START OF WORK. THE EXCAVATOR MUST NOTIFY ALL PUBLIC UTILITY COMPANIES WITH UNDERGROUND FACILITIES IN THE AREA OF PROPOSED EXCAVATION AND HAVE THOSE FACILITIES LOCATED BY UTILITY COMPANIES PRIOR TO COMMENCING EXCAVATION. THE EXCAVATOR IS RESPONSIBLE FOR COMPLIANCE WITH REQUIREMENTS OF CHAPTER 36A OF THE MONTGOMERY COUNTY CODE.

TREE CANOPY REQUIREMENTS TABLE	
To be completed by the consultant and placed on the first sheet of the Sediment Control / Stormwater Management plan set for all projects.	
Exempt: Yes <input type="checkbox"/> No <input type="checkbox"/> If exempt under Section 55-5 of the Code, please check the applicable exemption category below.	
Total Property Area	Total Disturbed Area
square feet	square feet
Shade Trees Required	Shade Trees Proposed to be Planted
Fee in Lieu (Trees Required - Trees Planted) x \$250 = \$	
Required Number of Shade Trees	
Area (sq. ft.) of the Limits of Disturbance	Number of Shade Trees Required
FROM TO	
1 6,000	3
6,001 8,000	6
8,001 12,000	9
12,001 14,000	12
14,001 40,000	15
If the square footage of the limits of disturbance is more than 40,000, then the number of shade trees required must be calculated using the following formula: (Number of Square Feet in Limits of Disturbance ÷ 40,000) × 15	
EXEMPTION CATEGORIES:	
<input type="checkbox"/> 55-5(a) any activity that is subject to Article II of Chapter 22A;	<input type="checkbox"/> maintenance has obtained all required permits;
<input type="checkbox"/> 55-5(b) any commercial logging or timber harvesting operation with an approved exemption from Article II of Chapter 22A;	<input type="checkbox"/> 55-5(h) any stream restoration project if the person performing the work has obtained all necessary permits;
<input type="checkbox"/> 55-5(i) any activity conducted by the County Parks Department;	<input type="checkbox"/> 55-5(i) cutting or clearing any tree to comply with applicable provisions of any federal, state, or local law governing safety of dams;
<input type="checkbox"/> 55-5(j) routine or emergency maintenance of an existing stormwater management facility, including an existing access road, if the person performing the	<input type="checkbox"/> OTHER: Specify per Section 55-5 of the Code.

MCDPS-SC/SWM SHEET NO. 1 OF 10

MONTGOMERY COUNTY DEPARTMENT OF PERMITTING SERVICES APPROVED FOR:		NOTE: MCDPS APPROVAL DOES NOT NEGATE THE NEED FOR A MCDPS ACCESS PERMIT!
STORMWATER MANAGEMENT	SEDIMENT CONTROL TECHNICAL REQUIREMENTS	ADMINISTRATIVE REQUIREMENTS
Reviewed _____ Date _____	Reviewed _____ Date _____	Reviewed _____ Date _____
Approved _____ Date _____	Approved _____ Date _____	Approved _____ Date _____
S.M. FILE NO. _____		MCDPS APPROVAL OF THIS PLAN WILL EXPIRE ONE YEAR FROM THE DATE OF APPROVAL IF THE PROJECT HAS NOT STARTED UNLESS THE PERMIT HAS BEEN EXTENDED.

MONTGOMERY COUNTY DEPARTMENT OF TRANSPORTATION GAITHERSBURG, MARYLAND		TI-01 TITLE SHEET
RECOMMENDED FOR APPROVAL		BOYDS TRANSIT IMPROVEMENTS
Chief, Transportation Planning and Design Section _____ Date _____		SCALE : NTS APRIL 2020
Chief, Division of Transportation Engineering _____ Date _____		Project No. : 509337 SHEET 1 of 18
Designed by: PHD Drawn by: AMU Checked by: JAG		

PROFESSIONAL CERTIFICATION.
I HEREBY CERTIFY THAT THESE DOCUMENTS WERE PREPARED OR APPROVED BY ME, AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MARYLAND, LICENSE NO. _____
EXPIRATION DATE: _____



SHARED USE PATH		
STATION AND OFFSET	AREA	REMARKS
11+86, 33' RT TO 12+82, 33' RT	114.7 SY	PATH BETWEEN ENTRANCE AND EXIT
13+32, 33' RT TO 15+58, 46' RT	253.6 SY	PATH BETWEEN BUS EXIT AND CLARKSBURG RD

MDOT SHA TYPE A COMBINATION CURB AND GUTTER (MD-620.02)		
STATION AND OFFSET	LENGTH	REMARKS
11+88, 18' RT TO 12+80, 18' RT	97'	BARNESVILLE RD
13+40, 18' RT TO 15+73, 53' RT	243'	BARNESVILLE RD

MDOT SHA TYPE D CURB (30 INCH) (MD-620.02-01)		
STATION AND OFFSET	LENGTH	REMARKS
12+34, 86' RT TO 12+79, 131' RT	100'	SIDEWALK

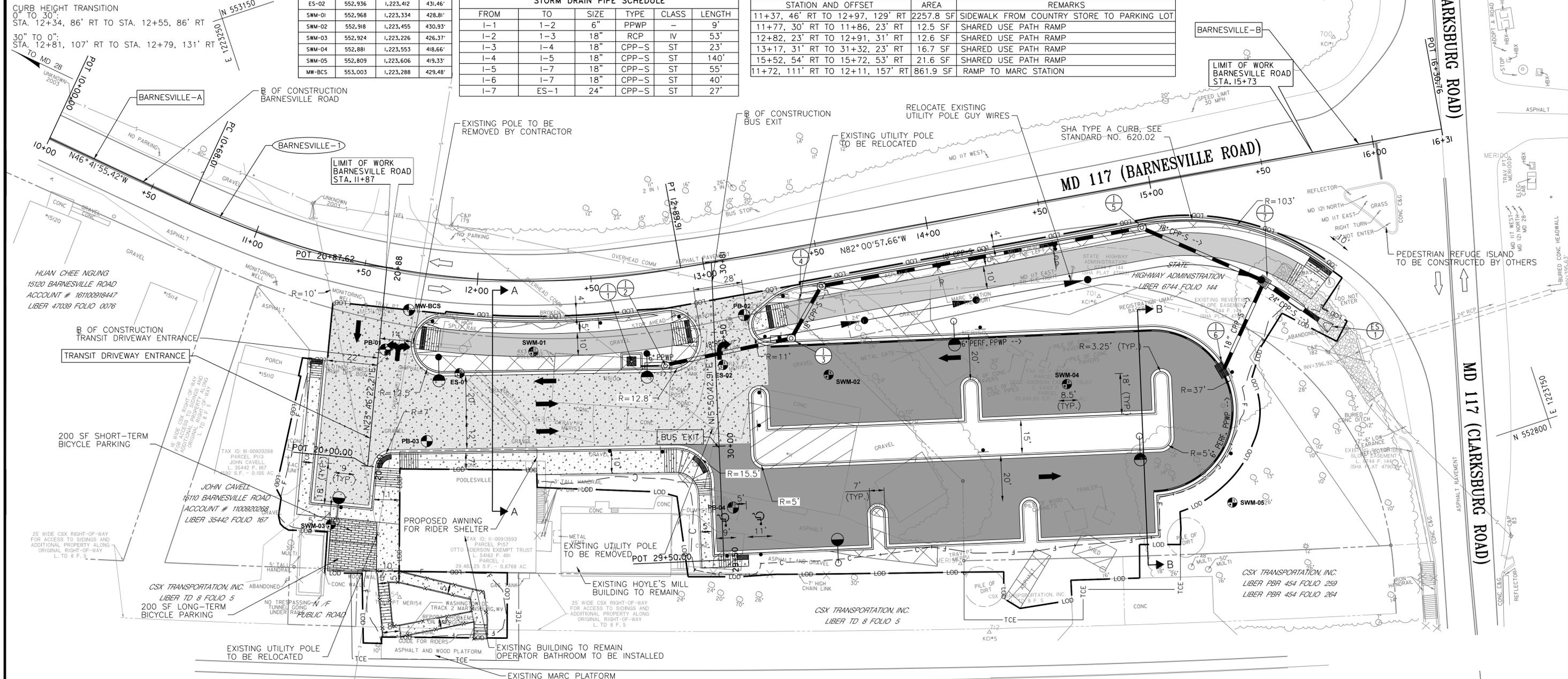
STORM DRAIN STRUCTURE SCHEDULE				
STRUCTURE NO.	STATION	OFFSET	TYPE	REMARKS
I-2	12+64	41.2' RT.	8'x6' FILTERRA INLET	-
I-2	12+70	42.0' RT.	TYPE "A" INLET, 10' OPENING	PER MC 501.01
I-3	13+29	40.1' RT.	TYPE "A" INLET, 10' OPENING	PER MC 501.01
I-4	13+50	17.5' RT.	TYPE "A" INLET, 10' OPENING	PER MC 501.01
I-5	14+90	17.5' RT.	TYPE "A" INLET, 10' OPENING	PER MC 501.01
I-6	15+16	86.8' RT.	"S-CV" CURVED VANE GRATE, DOUBLE	PER MD 374.70
I-7	15+43	51.1' RT.	TYPE "J" INLET, SINGLE OPENING	PER MC 506.01
*ES-1	15+63	72.6' RT.	STD. CONCRETE END SECTION, 24" PIPE	PER MD 368.01

MCDOT TYPE A COMBINATION CURB AND GUTTER (MC-100.01)			
STATION AND OFFSET	LENGTH	REMARKS	
11+42, 46' RT TO 13+40, 18' RT	905'	BOYDS COUNTRY STORE TO BUS EXIT	
11+88, 18' RT TO 12+80, 18' RT	183'	PARK AND RIDE BUS LOOP	
13+07, 87' RT TO 14+93, 86' RT	495'	PARKING LOT	

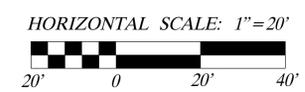
SOIL BORING REQUEST			
BORING	NORTHING	EASTING	ELEV
PB-01	552,990	1,223,273	428.74'
PB-02	552,954	1,223,428	427.43'
PB-03	552,946	1,223,278	428.83'
PB-04	552,876	1,223,397	430.05'
ES-01	552,969	1,223,301	429.22'
ES-02	552,936	1,223,412	431.46'
SWM-01	552,968	1,223,334	428.81'
SWM-02	552,918	1,223,455	430.93'
SWM-03	552,924	1,223,226	426.37'
SWM-04	552,881	1,223,553	418.66'
SWM-05	552,809	1,223,606	419.33'
MW-BCS	553,003	1,223,288	429.48'

STORM DRAIN PIPE SCHEDULE					
FROM	TO	SIZE	TYPE	CLASS	LENGTH
I-1	I-2	6"	PPWP	-	9'
I-2	I-3	18"	RCP	IV	53'
I-3	I-4	18"	CPP-S	ST	23'
I-4	I-5	18"	CPP-S	ST	140'
I-5	I-7	18"	CPP-S	ST	55'
I-6	I-7	18"	CPP-S	ST	40'
I-7	ES-1	24"	CPP-S	ST	27'

5 INCH SIDEWALK			
STATION AND OFFSET	AREA	REMARKS	
11+37, 46' RT TO 12+97, 129' RT	2257.8 SF	SIDEWALK FROM COUNTRY STORE TO PARKING LOT	
11+77, 30' RT TO 11+86, 23' RT	12.5 SF	SHARED USE PATH RAMP	
12+82, 23' RT TO 12+91, 31' RT	12.6 SF	SHARED USE PATH RAMP	
13+17, 31' RT TO 31+32, 23' RT	16.7 SF	SHARED USE PATH RAMP	
15+52, 54' RT TO 15+72, 53' RT	21.6 SF	SHARED USE PATH RAMP	
11+72, 111' RT TO 12+11, 157' RT	861.9 SF	RAMP TO MARC STATION	



LEGEND	
	FULL DEPTH ASPHALT PAVEMENT
	FULL DEPTH PERVIOUS CONCRETE PAVEMENT
	FULL DEPTH CONCRETE PAVEMENT
	CONCRETE SIDEWALK
	DETECTABLE WARNING SURFACE
	BRICK
	PAVEMENT REMOVAL
	BUILDING REMOVAL/RELOCATION/SALVAGE (UNDER SEPERATE CONTRACT)



CURVE DATA								
CURVE	DELTA	Dc	R	L	T	E	PI NORTH	PI EAST
BARNESVILLE-1	35°19' 02.24" LT	15°54' 55.78"	360.00'	221.91'	114.60'	17.80'	552,998.74	1,223,294.21

LINE DATA		
LINE	LENGTH	BEARING
BARNESVILLE-A	68.00'	N 46° 41' 55.42" W
BARNESVILLE-B	340.85'	N 82° 00' 57.66" W
TRANSIT CENTER DRIVEWAY	87.62'	N 23° 46' 22.21" E
BUS EXIT	80.77'	N 15° 50' 42.91" E

PROFESSIONAL CERTIFICATION.
I HEREBY CERTIFY THAT THESE DOCUMENTS WERE PREPARED OR APPROVED BY ME, AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MARYLAND, LICENSE NO. _____ EXPIRATION DATE: _____

WRA
Whitman, Reardon & Associates, LLP
801 South Caroline Street, Baltimore, Maryland 21231

NO.	REVISION	DATE	BY

MONTGOMERY COUNTY
DEPARTMENT OF TRANSPORTATION
GAITHERSBURG, MARYLAND

RECOMMENDED FOR APPROVAL

Chief, Transportation Planning and Design Section _____ Date _____

Chief, Division of Transportation Engineering _____ Date _____

Designed by: PHD Drawn by: AMU Checked by: JAG

HD-01 ROADWAY PLAN

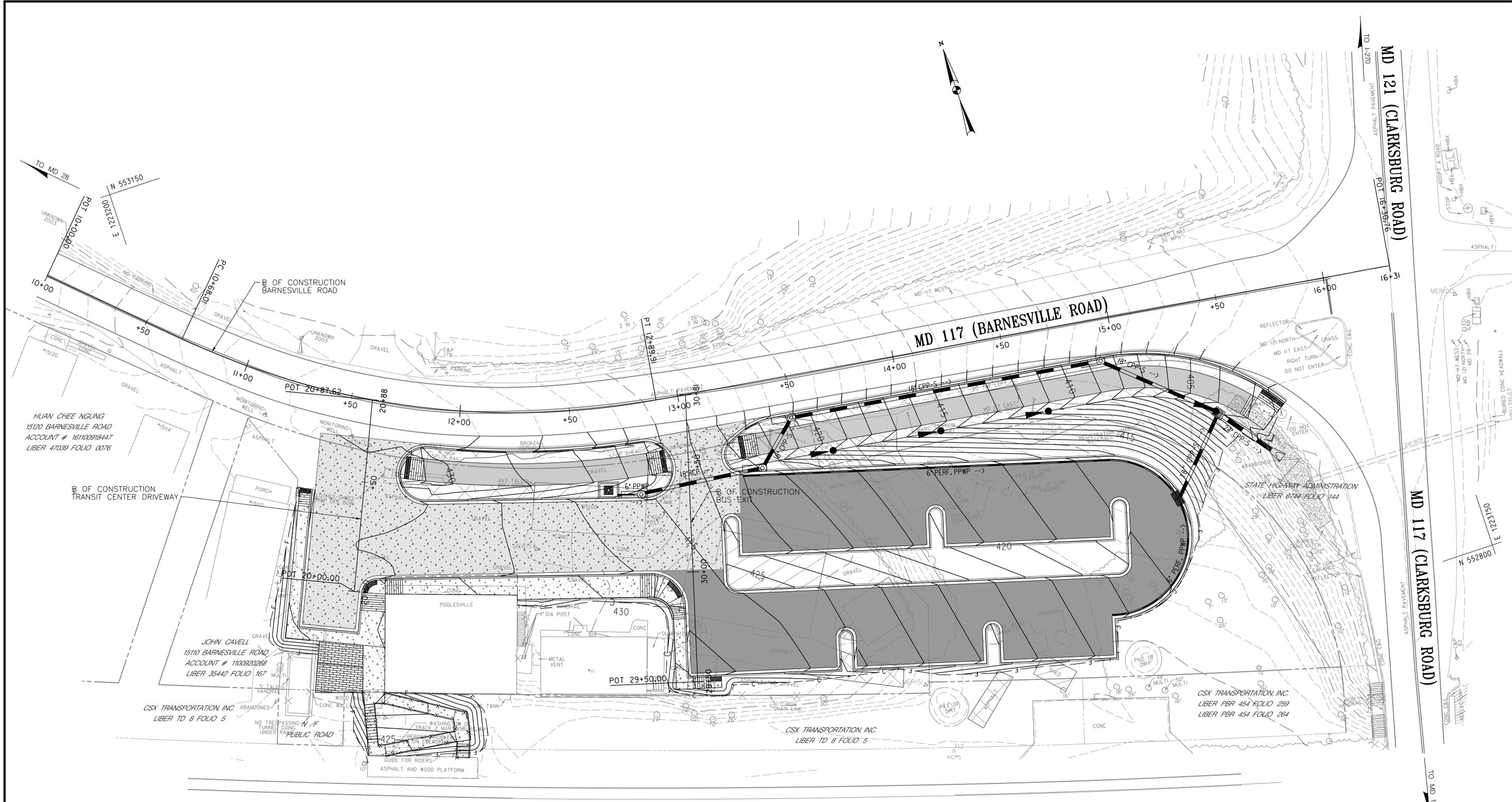
BOYDS TRANSIT IMPROVEMENTS

SCALE: 1" = 20'

APRIL 2020

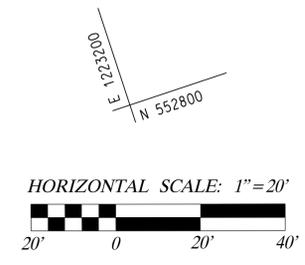
Project No.: 509337 SHEET 8 of 18

N:\31951-019\CADD\MD-0001_Boyd.dgn DATE: 4/8/20



LEGEND

	FULL DEPTH ASPHALT PAVEMENT
	FULL DEPTH CONCRETE PAVEMENT
	CONCRETE SIDEWALK
	DETECTABLE WARNING SURFACE
	BRICK PAVERS
	PAVEMENT REMOVAL
	BUILDING REMOVAL/RELOCATION/SALVAGE



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 EXPIRATION DATE: _____

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 Whitman, Requardt & Associates, LLP
 801 South Caroline Street, Baltimore, Maryland 21231

NO.	REVISION	DATE	BY

MONTGOMERY COUNTY DEPARTMENT OF TRANSPORTATION GAITHERSBURG, MARYLAND	
RECOMMENDED FOR APPROVAL	
Chief, Transportation Planning and Design Section	Date
APPROVED	
Chief, Division of Transportation Engineering	Date
Designed by: PHD	Drawn by: AMU
Checked by: JAG	

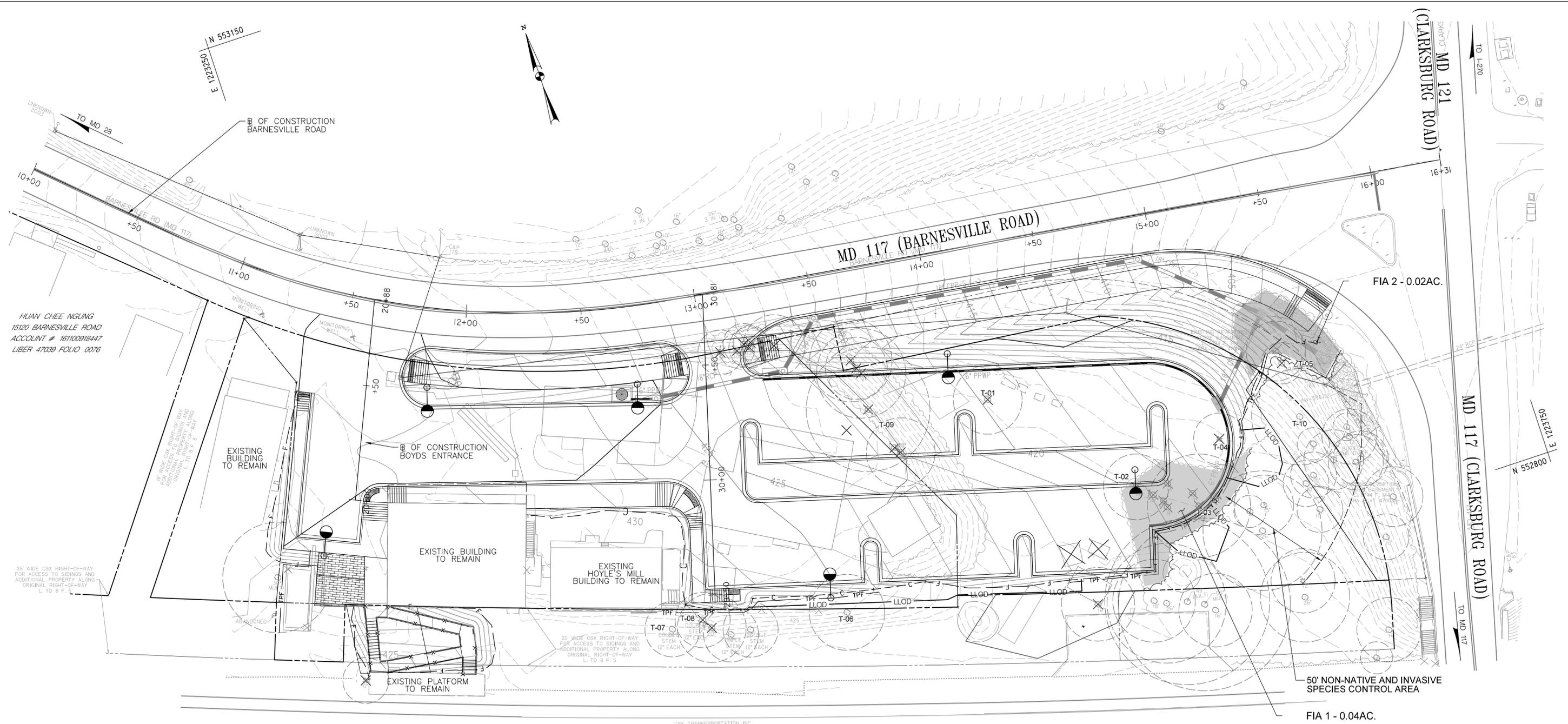
GR-01 SITE GRADING PLAN

**BOYDS TRANSIT
IMPROVEMENTS**

SCALE : 1" = 20' APRIL 2020

Project No. : 509337 SHEET 12 of 18

N:\31951-019\CADD\UGR-P001_Boyds.dgn 5/24/20



HUAN CHEE NGUNG
15120 BARNESVILLE ROAD
ACCOUNT # 161100918447
LIBER 47039 FOLIO 0076

25' WIDE CSX RIGHT-OF-WAY
FOR ACCESS TO SIDINGS AND
ADDITIONAL PROPERTY ALONG
ORIGINAL RIGHT-OF-WAY
L. TO 8' F. 5

EXISTING BUILDING
TO REMAIN

OF CONSTRUCTION
BOYDS ENTRANCE

EXISTING BUILDING
TO REMAIN

EXISTING
HOYLE'S MILL
BUILDING TO REMAIN

EXISTING PLATFORM
TO REMAIN

FIA 2 - 0.02AC.

50' NON-NATIVE AND INVASIVE
SPECIES CONTROL AREA

FIA 1 - 0.04AC.

TREE SUMMARY TABLE

ID	SIZE	BOTANICAL/COMMON	CONDITION	CANOPY	REMARKS
T-01	20"	ULMUS SP./ELM	DEAD	26.5'	IVY GROWING ON TRUNK
T-02	19"	ACER NEGUNDO/BOXELDER	FAIR/POOR	26'	ONE SIDED
T-03	30"	CATALPA SPECIOSA/CATALPA	FAIR	28'	
T-04	15"	ACER NEGUNDO/BOXELDER	POOR	22'	TRIPLE STEM
T-05	12"	ACER NEGUNDO/BOXELDER	POOR	18'	ONE SIDED, TRIPLE STEM
T-06	22"	ACER NEGUNDO/BOXELDER	POOR	20'	
T-07	26"	CATALPA SPECIOSA/CATALPA	FAIR/GOOD	22'	DOUBLE 12" STEMS
T-08	22"	CATALPA SPECIOSA/CATALPA	POOR	18'	SEVERE LEAN AND VINE GROWTH DOUBLE 12" STEMS
T-09	36"	CATALPA SPECIOSA/CATAPLA	POOR	20'	
T-10	28"	AILANTHUS ALTISSIMA/ TREE OF HEAVEN	POOR	35'	DOUBLE, 36" & 24" STEMS

- NOTES:
1. INITIATE NON-NATIVE INVASIVE(NNI) REMOVAL PRIOR TO INSTALLATION OF PROPOSED PLANTS.
 2. INVASIVE REMOVAL SHALL FOLLOW THE STANDARDS OF M-NCPPC INVASIVE REMOVAL SPECS WITH THE EXCEPTION OF NO SOIL DISTURBBANCE IS PERMITTED BEYOND THE LIMITS OF DISTURBBANCE FOR THE PROJECT.
 3. A TWO-YEAR MAINTENANCE CONTRACT SHALL BE INCORPORATED INTO THE CONTRACT



LEGEND

- EXISTING TREE WITH CRITICAL ROOT ZONE
- EXISTING TREE TO BE REMOVED
- SPECIMEN / SIGNIFICANT TREE
- FOREST IMPACT AREA (FIA) - 0.13AC. TOTAL
- PROPOSED WOODED EDGE
- TREE PROTECTION FENCE/ ORANGE CONSTRUCTION FENCE/ TREE ROOT PRUNING

PROFESSIONAL CERTIFICATION.
I HEREBY CERTIFY THAT THESE DOCUMENTS
WERE PREPARED OR APPROVED BY ME, AND
THAT I AM A DULY LICENSED PROFESSIONAL
LANDSCAPE ARCHITECT UNDER THE LAWS OF
THE STATE OF MARYLAND, LICENSE NO. 935
EXPIRATION DATE: 03/28/2022

MAHAN RYKIEL
LANDSCAPE ARCHITECTURE
URBAN DESIGN & PLANNING

NO.	REVISION	DATE	BY

MONTGOMERY COUNTY
DEPARTMENT OF TRANSPORTATION
GAITHERSBURG, MARYLAND

RECOMMENDED FOR APPROVAL

Chief, Transportation Planning and Design Section _____ Date _____

APPROVED

Chief, Division of Transportation Engineering _____ Date _____

Designed by: M.F. Drawn by: M.A.B. Checked by: J.K.S.

TREE SAVE PLAN

BOYDS TRANSIT IMPROVEMENTS

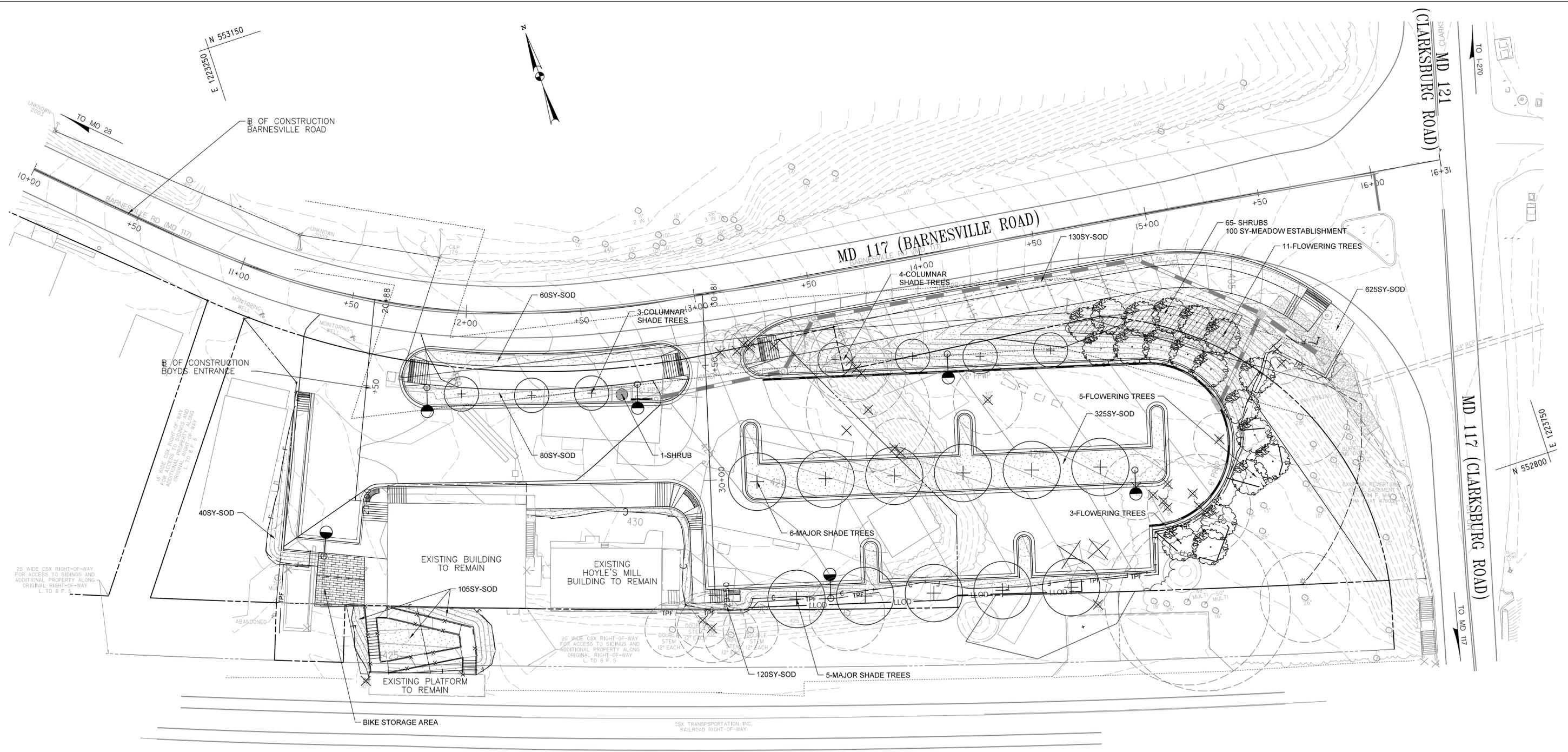
SCALE : 1" = 20'

APRIL 2020

Project No. : 509337 SHEET 16 of 18

FI-01

\$\$\$DONORPAGES\$\$\$
DATE: 03/28/2022



LEGEND

	MAJOR SHADE TREE - 11 TOTAL		EXISTING TREE TO REMAIN WITH CRZ
	COLUMNARR SHADE TREE - 7 TOTAL		EXISTING TREE TO BE REMOVED
	FLOWERING TREE - 19 TOTAL		TURFGRASS SOD ESTABLISHMENT-1,485 SY TOTAL
	PROPOSED WOODED EDGE		SHRUBS - 66 TOTAL MEADOW ESTABLISHMENT - 100 SY
	TPF - PROPOSED TREE PROTECTION - 351 LF		
	LLOD - LANDSCAPE LIMIT OF DISTURBANCE		

NOTE
TREE ROOT PRUNING WILL OCCUR ALONG TPF. SEE DETAIL FOR LOCATION.



LD-01

PROFESSIONAL CERTIFICATION.
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LANDSCAPE ARCHITECT UNDER THE LAWS OF
THE STATE OF MARYLAND, LICENSE NO. 935
EXPIRATION DATE: 03/28/2022.

MAHAN RYKIEL
LANDSCAPE ARCHITECTURE
URBAN DESIGN & PLANNING

NO.	REVISION	DATE	BY

MONTGOMERY COUNTY DEPARTMENT OF TRANSPORTATION GAITHERSBURG, MARYLAND	
RECOMMENDED FOR APPROVAL	
Chief, Transportation Planning and Design Section	Date
APPROVED	
Chief, Division of Transportation Engineering	Date
Designed by: M.F.	Drawn by: M.A.B.
Checked by: J.K.S.	

LANDSCAPE PLAN	
BOYDS TRANSIT IMPROVEMENTS	
SCALE : 1" = 20'	APRIL 2020
Project No. : 509337	SHEET 17 of 18

DATE PLOTTED: 04/15/2020 10:58:58 AM

