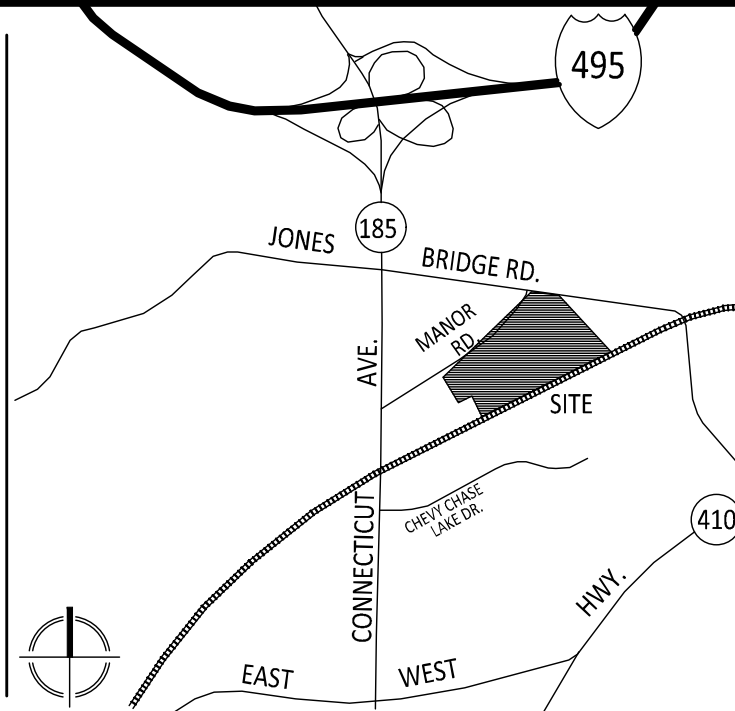


PRESTON PLACE & LAKESIDE APARTMENTS

MNCPPC SITE PLAN 820220180



VICINITY MAP
SCALE: 1" = 2000'



VIKA MARYLAND, LLC
20251 Century Blvd., Suite 400
Germantown, MD 20874
301.916.4100 | vika.com
Our Site Set on the Future.

PREPARED FOR:
TOLL MID-ATLANTIC LP
COMPANY, INC
6731 COLUMBIA GATEWAY DRIVE
SUITE 120
COLUMBIA, MD 21046
CONTACT: JEFFREY S. DRISCOLL
JDRISCOLL@TOLLBROTHERS.COM

DESIGN CONSULTANTS
PLANNER, CIVIL ENGINEER
VIKA MARYLAND, LLC
20251 CENTURY BOULEVARD
SUITE 400
GERMANTOWN MD, 20874
301.916.4100
CONTACT: JOSHUA SLOAN, RLA

ATTORNEY
LERCH, EARLY & BREWER,
CHTD
7600 WISCONSIN AVENUE
SUITE 700
BETHESDA MD, 20814
301.841.3832
CONTACT: PATRICIA A. HARRIS

REFERENCE NOTES

TAX ACCOUNT REFERENCE: 07-00419647
07-00419738
07-00419740
07-00419751
07-00419762

CURRENT ZONING: R-30
PROPOSED ZONING: R-30
APPROVED NRI NO: 420221260
WATERSHED: COQUELIN RUN TO LOWER ROCK CREEK
WATER SERVICE CATEGORY: W-1
SEWER SERVICE CATEGORY: S-1
SOIL DESIGNATION: 2UC AND 18
FLOODPLAIN ZONE: 'X'
FIRM COMMUNITY PANEL NO.: [24031C04 55D], FOR MONTGOMERY COUNTY, MARYLAND, DATED SEPTEMBER 29, 2006.

GENERAL NOTES

- THE HORIZONTAL DATUM IS MARYLAND STATE GRID NORTH (NAD83) AND THE VERTICAL DATUM IS NVGD29.
- THERE ARE NO WETLANDS, OR 100 YEAR FLOOD PLAINS LOCATED ON THE SUBJECT PROPERTY. HOWEVER, COQUELIN RUN OCCURS TO THE EAST OF THE SITE WITH A STREAM BUFFER THAT EXTENDS ONTO THE SUBJECT SITE. COQUELIN RUN IS A SOUTH-FLOWING, PARTIALLY INTERMITTENT, AND PARTIALLY PERENNIAL STREAM. THE WESTERN STREAM LIMITS WERE FLAGGED AND SURVEY LOCATED AS SHOWN ON THE PLAN. THE EASTERN SIDE OF THE STREAM WAS NOT DELINEATED AND THE STREAM BUFFER THAT EXTENDS TO THE EAST IS NOT SHOWN ON THE NRI/FSD PLAN. THE EPHEMERAL CHANNEL TO THE NORTH OF THE INTERMITTENT/PERENNIAL STREAM CONVEYS STORMFLOW FROM TWO CULVERTS. THE EPHEMERAL SECTION OF CHANNEL IS NOT REGULATED BY MDE OR M-NCPPC
- THERE ARE 1.21 ACRES OF FORESTED AREAS LOCATED ON THE NORTHEASTERN END OF THE SUBJECT PROPERTY AS DEFINED BY MONTGOMERY COUNTY FOREST LEGISLATION.
- IN A LETTER DATED MARCH 23, 2021, THE MARYLAND DEPARTMENT OF NATURAL RESOURCES HAS DETERMINED THAT THERE ARE NO RARE, THREATENED, OR ENDANGERED PLANTS, ANIMALS, OR CRITICAL HABITATS ON THIS PROPERTY.
- THE SUBJECT PROPERTY IS NOT IDENTIFIED IN THE MONTGOMERY COUNTY HISTORIC PRESERVATION ON-LINE MAPPER (HTTP://WWW.MONTGOMERYPLANNING.ORG/GIS/INTERACTIVE/HISTORIC.SHTM), ACCESSED ON NOVEMBER 11, 2021.
- BUILDING HEIGHT IS MEASURED FROM THE AVERAGE GRADE TO THE HIGHEST POINT OF THE FLAT ROOF SURFACE AND WILL BE FINALIZED WITH PERMITS.
- THIS SITE IS SUBJECT TO APPROVED NRI #420221260 DATED 02/03/22.
- AN ON-SITE PRE-CONSTRUCTION MEETING IS REQUIRED TO BE SET UP WITH THE DEPARTMENT OF PERMITTING SERVICES (DPS), ZONING & SITE PLAN ENFORCEMENT DIVISION BEFORE ANY BUILDING CONSTRUCTION ACTIVITY OCCURS ON-SITE. THE OWNER OR HIS DESIGNEE WHO HAS SIGNATURE AUTHORITY, AND GENERAL CONTRACTOR MUST ATTEND THE PRE-CONSTRUCTION MEETING WITH THE DPS SITE PLAN ENFORCEMENT INSPECTOR. A COPY OF THE CERTIFIED SITE PLAN IS REQUIRED TO BE ON-SITE AT ALL TIMES. TO SCHEDULE A SITE PLAN INSPECTION WITH DPS, ZONING & SITE PLAN ENFORCEMENT DIVISION, PLEASE CONTACT BRIAN KEELER, SUPERVISOR AT 240-581-4485.

REVISIONS
REVISED PER DRC

DATE
09/02/22

PRESTON
PLACE AND LAKE
APARTMENTS
7TH ELECTION DISTRICT
MONTGOMERY COUNTY,
MARYLAND
WSSC GRID: 210NW04
TAX MAP: HN43

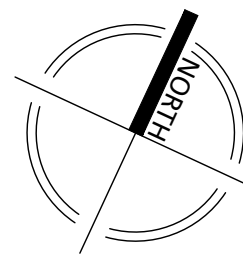
SUPPORTING DRAWINGS SHEET INDEX

APPROVED NRI/FSD
COMBINATION CONCEPT/SITE DEVELOPMENT STORMWATER
MANAGEMENT PLAN
FIRE ACCESS PLAN
COLOR UTILITY PLAN
HIGH VISIBILITY UNITS EXHIBIT
COMMON OPEN SPACE
TRUCK TURNING EXHIBIT

SUPPLEMENTAL DRAWINGS SHEET INDEX

EXISTING RECORD PLATS
CIRCULATION PLAN
EXISTING CANOPY EXHIBIT
PROPOSED CANOPY EXHIBIT
OFFSITE TRAIL

0 40' 80' 160'
SCALE: 1" = 80'



COVER SHEET

SITE PLAN
#820220180

PROFESSIONAL SEAL



I HEREBY CERTIFY THAT THESE DOCUMENTS WERE PREPARED OR APPROVED BY ME, AND THAT I AM A QUALIFIED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MARYLAND. ENGINEER'S NAME: DONALD NELSON, P.E. LICENSE NO.: 5289 EXPIRATION DATE: JULY 11, 2024

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DRAWN BY: _____
DESIGNED BY: _____
DATE ISSUED: 03/23/2022

VIKA PROJECT
DRAWING NO. SP-001

SHEET NO.

DEVELOPMENT TABULATIONS			
LAKE APARTMENTS & PRESTON PLACE			
TRACT AREA (GROSS)	549,504.46	SF	
LAKE APARTMENTS NORTH PROPERTY	212,058.32	SF	
PRESTON PLACE TOWNHOUSE PROPERTY	337,446.14	SF	
PREVIOUS DEDICATIONS	-	SF	
PROPOSED DEDICATIONS	5,883.67	SF	
R-30 DEDICATION TO MANOR ROAD	5,460.52	SF	
R-60 DEDICATION TO MANOR ROAD	423.15	SF	
SITE AREA (NET)	543,620.79	SF	
TRACT AREA BROKEN OUT BY ZONE	SF	ACRES	
R-30 ZONE	479,309.52	11.00	
R-60 ZONE	70,194.94	1.61	

MPDU Optional Method of Development Standards			
	Required/ Allowed	Proposed	Unit
1. SITE			
R-30 CALCULATIONS			
USABLE AREA (MIN)	11,700	479,309.52	SF (11.00 AC)
BASE DENSITY (MAX)	14.50	13.36	DU/AC
R-60 CALCULATIONS			
USABLE AREA (MIN) [2]	3.00	70,194.94	SF(1.61 AC)
BASE DENSITY (MAX) [2]	7.26	0.00	DU/AC
COMBINED CALCULATIONS [1]			
USABLE AREA (MIN)	142,380	549,504.46	SF (12.61 AC)
BASE DENSITY (MAX)	171	147	DUs
DENSITY (No MPDU Bonus Taken) [1, 2]			
Total Units	171	147	DUs
# MPDUs (15% of Proposed)	23	23	MPDUs
% MPDUs	15.00%	15.65%	MPDUs

	Required/ Allowed	Proposed	Unit
OPEN SPACE & COVERAGE			
COMMON OPEN SPACE (MIN) [1, 3]	133,866	164,911.00	SF (30%)
R-30 Zone Area (25% Min)	119,827		
R-60 Zone Area (20% Min)	14,039		
COVERAGE (MAX) [1]			
R-30 Zone Area (30% Max)	171,871	156,132.00	SF
R-60 Zone Area (40% Max)	143,793		
	28,078		

R-30 MPDU Optional Method of Development Lot, Placement, & Height Standards [2]			
	Required/ Allowed	Proposed	Unit
2. LOT			
LOT AREA (MIN.)	800	854	SF
LOT WIDTH AT FRONT BLDG. LINE (MIN.)	14	14	FT
FRONTAGE ON STREET OR OPEN SPACE	N/A	Provided	
LOT COVERAGE (MAX)	N/A	N/A	
3. PLACEMENT (MIN)			
PRINCIPAL BUILDING			
FRONT SETBACK FROM PUBLIC STREET	10	10	FT
FRONT SETBACK FROM PRIVATE STREET	4	4	FT
SIDE STREET SETBACK	5	5	FT
SIDE OR REAR SETBACK (Internal)	Determined at site plan	0	FT
SIDE OR REAR SETBACK (Along Purple Line ROW)	Determined at site plan	10	FT
SIDE OR REAR SETBACK ABUTTING PROPERTY NOT INCLUDED IN THE APPLICATION (Abutting Side Setback of Existing Towns)	4	10	FT
REAR SETBACK, ALLEY	4	4	FT
ACCESSORY STRUCTURE			
FRONT SETBACK FROM FRONT BUILDING LINE	5	5	FT
SIDE STREET SETBACK	5	5	FT
SIDE OR REAR SETBACK (Including Porches, Decks, & Steps)	Determined at site plan	0	FT
SIDE OR REAR SETBACK ABUTTING PROPERTY NOT INCLUDED IN THE APPLICATION	4	4	FT
REAR SETBACK, ALLEY [4]	4	4	FT
4. HEIGHT (MAX)			
PRINCIPAL BUILDING [5]	40	40	FT
ACCESSORY STRUCTURE	25	25	FT

[1] Under Section 4.4.2.A.2. MPDU Development Across Different Zones: Optional method MPDU Development may occur across different zones under the following limitations: a. The differently zoned areas must be contiguous; b. Uses and building types are governed by the zone; c. The site requirements in the optional method tables apply; density and open space must be calculated as if each area were developed individually; and d. The allowed number of units and required open space may be located without regard to the limits in the underlying zone.

[2] No units are proposed in the R-60-zoned portion of the site.

[3] Common open space means an outdoor area that is intended for recreational use by residents and their visitors. Common open space does not include private individual lots. (1) Common open space must be located in a central position or central positions in the neighborhood bordered by streets or building lots. It may be public or private. Common open space may also be placed in a location taking advantage of an important adjacent natural feature or open space. (2) The minimum width for any required common open space is 50 feet unless the deducing body grants an exception for items such as a trail easement, a mid-block crossing, or a linear park, by finding that its purpose meets the intent of Division 6.3. (3) A minimum of 50% of the required common open space must be in one contiguous area or only separated by a residential street. Any other areas must be a minimum of 2,000 square feet each and connected by sidewalks, paths, or trails.

[4] Excluding decks, which may project up to 9' into any rear setback under Sec. 4.1.7.8.5.a.i.

[5] Excluding encroachments allowed under Sec. 4.1.7.C.3, including elevator and stairwell overrun and roof structure up to 25% of roof area.

PLAN LEGEND

—CTV—CTV—	PROPERTY LINES
—E—E—E—E—E—E—	EXISTING CABLE TELEVISION CONDUIT
—X—X—X—X—X—X—	EXISTING ELECTRICAL CONDUIT
—CHW—CHW—CHW—CHW—	EXISTING EDGE OF PAVEMENT
—T—T—T—T—T—T—	EXISTING FENCE LINE
—SD—SD—SD—SD—SD—SD—	EXISTING NATURAL GAS CONDUIT
—W—W—W—W—W—W—	EXISTING OVERHEAD WIRES
—520—	EXISTING TELEPHONE CONDUIT
—524—	EXISTING PUBLIC UTILITIES EASEMENTS
—8" W—	EXISTING SANITARY SEWER CONDUIT
—8" S—	EXISTING STORM DRAIN CONDUIT
—8" S—	EXISTING WATER CONDUIT
—520—	EXISTING ZONE LIMITS
—524—	PROPOSED 1' CONTOUR
—8" W—	PROPOSED 2' CONTOUR
—8" S—	PROPOSED WATER LINE
—LOD—	PROPOSED SANITARY SEWER WITH STRUCTURE
—LOD—	PROPOSED STORM DRAIN
—LOD—	PROPOSED LIMITS OF DISTURBANCE
—LOD—	PROPOSED STORM WATER EASEMENT

"FOR LOCATION OF UTILITIES CALL 8-1-1 or 1-800-257-7777 OR LOG ON TO www.call811.com or http://www.missutility.net 48 HOURS IN ADVANCE OF ANY WORK IN THIS VICINITY"

The excavator must notify all public utility companies with underground facilities in the area of proposed excavation and have those facilities located by the utility companies prior to commencing excavation. The excavator is responsible for compliance with requirements of Chapter 56A of the Montgomery County Code.

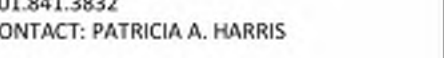
LAYOUT: SP-1, Plotted By: ran

SHEET INDEX

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SP-002 PLAN APPROVALS
SP-003 DETAILED SITE PLAN
SP-004 DETAILED SITE PLAN
LL-001 COVER
LL-001.1 RECREATIONAL FACILITY PLAN
LL-002 30 SCALE LANDSCAPE PLAN
LL-002.1 30 SCALE LANDSCAPE PLAN
LL-003 10 SCALE LANDSCAPE PLAN ENLARGEMENTS
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LL-003.2 10 SCALE LANDSCAPE PLAN ENLARGEMENTS
LL-003.3 10 SCALE LANDSCAPE PLAN ENLARGEMENTS
LL-004 PLANTING SCHEDULE AND DETAILS
LL-004.1 SITE DETAILS
LL-004.2 SITE DETAILS
LL-004.3 LIGHTING DETAILS
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LL-005 PHOTOMETRIC PLAN
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ARCH-100 24X46 INTERIOR
ARCH-100.1 27X50 INTERIOR
ARCH-200 FRONT LOAD - FRONT ELEVATION
ARCH-300 FRONT LOAD - REAR ELEVATION
ARCH-400 FRONT LOAD - SIDE ELEVATION
ARCH-500 FRONT LOAD - SIDE ENTRY ELEVATION

ARCH-600 REAR LOAD - FRONT ELEVATION
ARCH-700 REAR LOAD - MANOR ROAD FRONT ELEVATION
ARCH-800 REAR LOAD - MANOR ROAD FRONT ELEVATION
ARCH-900 REAR LOAD - MANOR ROAD HIGH VIS SIDE ELEVATION
ARCH-900.1 SIDE ENTRY ELEVATION

LAYOUT: SP-1, Plotted By: ran



Signature: _____ Date: _____

~~03.3.22~~

~~Revised 09.08.22~~

~~Revised 12.22.22~~

Revised 01.30.23

M-NCPPC
Marco Fuster & Tsaiquan Gatling
Planner II, Down County Planning
Montgomery County Planning Department
8787 Georgia Avenue
Silver Spring, Maryland 20910

RE: Preston Place and Lake Apartments, 120220130
Variance Request for Specimen Tree Removal/Impact

Dear Mr. Fuster & Mr. Gatling:

Introduction

The project associated with this variance request is Applicant's redevelopment of Preston Place and Lake Apartments located at 3728, 3722, 3712, 3710, and 3700 Manor Road in Chevy Chase (the "Property"), which consists of approximately 12.61 acres of currently improved 67 townhomes and 66 garden apartment residential rental properties. The site is located along the southern side of Manor Road, southwest of the intersection with Jones Bridge Road. The project proposes redevelopment of the Property with 147 transit-oriented townhomes to include recreation areas, outdoor space, and improved stormwater management. The stream valley buffer on the east of the site will include a Forest Conservation area with a master-planned ADA-compliant shared-use trail that will connect existing community sidewalks and paths to the nearby Capital Crescent Trail, proposed Purple Line Station, local restaurants and shops, and other amenities.

As part of the proposed Project, Applicant seeks a variance for impacts to the Critical Root Zone ("CRZ") of specimen trees on the Property as described below. This variance request complies with M-NCPPC and Maryland state law, which require Applicant to file for a variance from these laws to remove or impact any specimen tree (tree 30" or greater in Diameter at Breast Height [DBH] or tree with a DBH equal to or greater than 75% of the current State Champion of its species as designated by MDNR); trees that are part of an historic site or associated with an historic structure; any tree designated by the State or County as a national, State, or County champion tree; or any tree, shrub or plant identified on the Rare,



Threatened, or Endangered (RTE) species list provided by the U.S. Fish and Wildlife Service or the Maryland Department of Natural Resources.

The following tables list the specimen trees for which Applicant seeks a variance to remove or impact the CRZs. These are broken down into two categories:

1. Significant tree impacts that are minimal and which will be saved
2. Significant tree removals, including those within cleared forest and those counted as removed, with intent to save.

Trees Impacted to be Saved					
TREE NO.	BOTANICAL NAME	COMMON NAME	D.B.H. (in.)*	CONDITION	RECOMMENDATION
53	<i>Liriodendron tulipifera</i>	Tuliptree	34	Fair	SAVE: 28% CRZ IMPACT
55	<i>Liriodendron tulipifera</i>	Tuliptree	31	Good	SAVE: 2% CRZ IMPACT
60	<i>Liriodendron tulipifera</i>	Tuliptree	36	Good	SAVE: 28% IMPACT
62	<i>Liriodendron tulipifera</i>	Tuliptree	34	Fair	SAVE: 40% CRZ IMPACT
65	<i>Liriodendron tulipifera</i>	Tuliptree	54	Good	SAVE: 46% CRZ IMPACT
66	<i>Liriodendron tulipifera</i>	Tuliptree	44	Good	SAVE: 6% CRZ IMPACT
72	<i>Quercus palustris</i>	Pin oak	35.5	Fair	SAVE 7% CRZ IMPACT
73	<i>Quercus palustris</i>	Pin oak	32.5	Fair	SAVE: 15% CRZ IMPACT
74	<i>Quercus palustris</i>	Pin oak	30.5	Fair	SAVE: 11% CRZ IMPACT
75	<i>Quercus rubra</i>	Northern red oak	38	Poor	SAVE: 16% CRZ IMPACT
76	<i>Quercus palustris</i>	Pin oak	33.6	Fair	SAVE: 12% CRZ IMPACT

Trees Removed or Counted as Removed with Intent to Save					
TREE NO.	BOTANICAL NAME	COMMON NAME	D.B.H. (in.)*	CONDITION	RECOMMENDATION
2	<i>Acer rubrum</i>	Red maple	34	Fair	REMOVE: 100% CRZ IMPACT
4	<i>Liquidambar styraciflua</i>	Sweetgum	32	Good	REMOVE: 100% CRZ IMPACT
7	<i>Liriodendron tulipifera</i>	Tuliptree	44	Good	REMOVE: 100% CRZ IMPACT
8	<i>Acer rubrum</i>	Red maple	30	Good	REMOVE: 100% CRZ IMPACT
9	<i>Acer rubrum</i>	Red maple	34	Good	REMOVE: 100% CRZ IMPACT
13	<i>Acer rubrum</i>	Red maple	33	Fair	REMOVE: 100% CRZ IMPACT
18	<i>Quercus rubra</i>	Northern red oak	43	Good	REMOVE: 100% CRZ IMPACT
19	<i>Liquidambar styraciflua</i>	Sweetgum	31	Good	REMOVE: 100% CRZ IMPACT
20	<i>Quercus palustris</i>	Pin oak	32	Fair	REMOVE: 100% CRZ IMPACT
21	<i>Quercus palustris</i>	Pin oak	36	Fair/Poor	REMOVE: 100% CRZ IMPACT
22	<i>Quercus palustris</i>	Pin oak	35	Fair	REMOVE: 97% IMPACT*



TREE NO.	BOTANICAL NAME	COMMON NAME	D.B.H. (in.)*	CONDITION	RECOMMENDATION
23	<i>Quercus rubra</i>	Northern red oak	30	Fair/Poor	REMOVE: 100% CRZ IMPACT
24	<i>Quercus rubra</i>	Northern red oak	45	Fair	REMOVE: 98% IMPACT*
26	<i>Quercus rubra</i>	Northern red oak	40	Fair	REMOVE: 98% IMPACT*
27	<i>Acer rubrum</i>	Red maple	40	Fair/Good	REMOVE: 100% CRZ IMPACT
29	<i>Acer rubrum</i>	Red maple	42	Poor	REMOVE: 100% CRZ IMPACT
32	<i>Quercus palustris</i>	Pin oak	50	Good	REMOVE: 100% CRZ IMPACT
35	<i>Acer rubrum</i>	Red maple	39	Good	REMOVE: 100% CRZ IMPACT
37	<i>Quercus rubra</i>	Northern red oak	43	Fair	REMOVE: 100% CRZ IMPACT
38	<i>Quercus palustris</i>	Pin oak	33	Fair/Good	REMOVE: 97% IMPACT*
39	<i>Quercus palustris</i>	Pin oak	46	Fair	REMOVE: 100% CRZ IMPACT
40	<i>Quercus montana</i>	Chestnut oak	41	Fair	REMOVE: 98% CRZ IMPACT*
41	<i>Quercus palustris</i>	Pin oak	41	Fair/Poor	REMOVE: 100% CRZ IMPACT
42	<i>Platanus occidentalis</i>	American sycamore	36	Fair	REMOVE: 100% CRZ IMPACT
44	<i>Liquidambar styraciflua</i>	Sweetgum	35	Fair/Good	REMOVE: 44% CRZ IMPACT*
45	<i>Pinus strobus</i>	Eastern white pine	42	Fair	REMOVE: 39% IMPACT*
46	<i>Acer rubrum</i>	Red maple	32	Fair/Poor	REMOVE: 100% CRZ IMPACT**
48	<i>Pinus strobus</i>	Eastern white pine	34	Fair	REMOVE: 100% CRZ IMPACT**
50	<i>Pinus strobus</i>	Eastern white pine	46	Fair	REMOVE: 52% CRZ IMPACT
54	<i>Quercus rubra</i>	Northern red oak	32	Fair	REMOVE: 51% IMPACT*
68	<i>Liriodendron tulipifera</i>	Tuliptree	41	Good	REMOVE 36% CRZ IMPACT*
69	<i>Liriodendron tulipifera</i>	Tuliptree	37	Fair	REMOVE 41% CRZ IMPACT*
70	<i>Liriodendron tulipifera</i>	Tuliptree	37	Good	REMOVE 59% CRZ IMPACT*
77	<i>Liriodendron tulipefera</i>	Tuliptree	40	Good/Fair	REMOVE: 100% IMPACT

*Removed with Intent to Save

** Removed from Cleared Forest

Justification of Variance

Section 22A-21 of the County Ordinance authorizes the County to approve variances to the Forest Conservation Law allowing disturbances to certain trees, including specimen trees. An applicant seeking a variance must present a request in writing and the applicable approving authority must make certain findings and descriptions prior to approval. Applicant's variance request satisfies the required findings as follows:

- (1) Describe the special conditions peculiar to the property which would cause the unwarranted hardship;



The proposed construction is for the purpose of redeveloping a currently improved property occupied by buildings constructed in the 1940s and 1950s. The buildings are becoming obsolete under contemporary market trends and do not meet current building or environmental codes. With the pending Purple Line and significant development on adjacent properties, these buildings have declining value and do not provide as much transit-oriented density as desired.

Because of its age, much of the site is currently inaccessible under ADA standards and a master-planned bikeway cannot be built without significant impacts to a stream valley buffer and interior area of a forest stand. There are also no stormwater management facilities on site to clean, cool, and slow runoff to the Coquelin Run. Further, there are no restricted affordable units on site, which redevelopment will allow. Any economically feasible redevelopment of the site requires the tear-down of the existing buildings and resulting regrading and infrastructure reconstruction to current standards. Given these constraints, the Project has been intentionally designed to minimize stream buffer impacts, preserve forest, and limit redevelopment to the previously improved areas. But removal of existing improvements alone would cause impacts to the specimen trees. Once the buildings are removed, the creation of a viable redevelopment project and implementation of master plan recommendations, stormwater regulations, and regulatory standards entails the removal or impact to the specimen trees as described herein.

Four specific goals for redevelopment:

- The Sector Plan and Bicycle Master Plan both recommend a shared-use path through the Coquelin Parkway right-of-way. This right-of-way is within a stream buffer and currently forested; thus, to implement the master-planned connection, significant impacts are required. To minimize impacts, the Applicant proposes to locate the path on the subject property rather than within the right-of-way to decrease – to the extent possible – stream and forest impacts. This master-planned element alone is responsible to impacts and removals of several specimen trees.
- Road standards along Manor Road require reconstruction of sidewalks to current 6-foot standards, provision of a new lawn panel, and reconstruction of the curb and infrastructure. These required improvements result in impacts or removals to many specimen trees.
- Contemporary stormwater facilities and utility easements and connections are much different than when the existing development was built. In fact, there no existing stormwater management facilities on site, and several outfalls spew untreated runoff from parking lots and drives directly into the stream valley buffer area, which is causing erosion and siltation and detrimental impacts to water quality. Provision of the necessary infrastructure to serve the development and stormwater facilities to improve water quality impact results in impacts to many specimen trees.
- Grading to current ADA standards to ensure that site elements and community facilities are accessible requires significant earthwork across the site. Currently the Lake Apartments (western) portion of the site has an elevation of change of 32 feet



from corner to corner and the Preston Place (eastern) portion of the site has an elevation change of 24 feet from the northern center to the lower corner. Because of the recommended connections to the Capital Crescent Trail, grading the site for pedestrian and bicyclist connectivity results in impacts to many specimen trees.

There are few properties with such transit proximity and infrastructure connectivity in the area zoned for the proposed use. Given several factors, including the current zoning and subdivision regulations, equitable connectivity standards, Sector Plan recommendations, functional plans, environmental policies, and affordable housing standards, denial of the redevelopment proposal by leaving the specimen trees in an undisturbed state is an unwarranted hardship because it would not allow reasonable and significant use of the property.

Specific details and justification for each impact or removal are listed below.

- Tree 2
Proposed CRZ Impact: Severe at 100%
Condition: Fair
Waiver Request: Approve for removal
Tree 2 is proposed for removal due to its location inside the proposed Limits of Disturbance (LOD). This tree is within the redevelopment area described in the general discussion and removal is necessary to implement the project to current standards. An effort to save this tree would require a significant reduction in usable building space to clear the CRZ and would not be allow reasonable and significant use of the property. Specifically, this tree must be removed to remove existing buildings, grade the site, install utility and stormwater infrastructure, build sidewalks and roads to complete streets standards, and implement a compact, transit-oriented design with new open spaces, a Sector-Planned shared-use path, and forest conservation areas.
- Tree 4
Proposed CRZ Impact: Severe at 100%
Condition: Good
Waiver Request: Approve for removal
Tree 4 is proposed for removal due to its location inside the proposed Limits of Disturbance (LOD). This tree is within the redevelopment area described in the general discussion and removal is necessary to implement the project to current standards. An effort to save this tree would require a significant reduction in usable building space to clear the CRZ and would not be allow reasonable and significant use of the property. Specifically, this tree must be removed to remove existing buildings, grade the site, install utility and stormwater infrastructure, build sidewalks and roads to complete streets standards, and implement a compact, transit-oriented design with new open spaces, a Sector-Planned shared-use path, and forest conservation areas.
- Tree 7
Proposed CRZ Impact: Severe at 100%



Condition: Good

Waiver Request: Approve for removal

Tree 7 is proposed for removal due to its location inside the proposed Limits of Disturbance (LOD). This tree is within the redevelopment area described in the general discussion and removal is necessary to implement the project to current standards. An effort to save this tree would require a significant reduction in usable building space to clear the CRZ and would not be allow reasonable and significant use of the property. Specifically, this tree must be removed to remove existing buildings, grade the site, install utility and stormwater infrastructure, build sidewalks and roads to complete streets standards, and implement a compact, transit-oriented design with new open spaces, a Sector-Planned shared-use path, and forest conservation areas.

- Tree 8

Proposed CRZ Impact: Severe at 100%

Condition: Good

Waiver Request: Approve for removal

Tree 8 is proposed for removal due to its location inside the proposed Limits of Disturbance (LOD). This tree is within the redevelopment area described in the general discussion and removal is necessary to implement the project to current standards. An effort to save this tree would require a significant reduction in usable building space to clear the CRZ and would not be allow reasonable and significant use of the property. Specifically, this tree must be removed to remove existing buildings, grade the site, install utility and stormwater infrastructure, build sidewalks and roads to complete streets standards, and implement a compact, transit-oriented design with new open spaces, a Sector-Planned shared-use path, and forest conservation areas.

- Tree 9

Proposed CRZ Impact: Severe at 100%

Condition: Good

Waiver Request: Approve for removal

Tree 9 is proposed for removal due to its location inside the proposed Limits of Disturbance (LOD). This tree is within the redevelopment area described in the general discussion and removal is necessary to implement the project to current standards. An effort to save this tree would require a significant reduction in usable building space to clear the CRZ and would not be allow reasonable and significant use of the property. Specifically, this tree must be removed to remove existing buildings, grade the site, install utility and stormwater infrastructure, build sidewalks and roads to complete streets standards, and implement a compact, transit-oriented design with new open spaces, a Sector-Planned shared-use path, and forest conservation areas.

- Tree 13

Proposed CRZ Impact: Severe at 100%

Condition: Fair

Waiver Request: Approve for removal

Tree 13 is proposed for removal due to its location inside the proposed Limits of Disturbance



(LOD). This tree is within the redevelopment area described in the general discussion and removal is necessary to implement the project to current standards. An effort to save this tree would require a significant reduction in usable building space to clear the CRZ and would not be allow reasonable and significant use of the property. Specifically, this tree must be removed to remove existing buildings, grade the site, install utility and stormwater infrastructure, build sidewalks and roads to complete streets standards, and implement a compact, transit-oriented design with new open spaces, a Sector-Planned shared-use path, and forest conservation areas.

- Tree 18

Proposed CRZ Impact: Severe at 100%

Condition: Good

Waiver Request: Approve for removal

Tree 18 is proposed for removal due to its location inside the proposed Limits of Disturbance (LOD). This tree is within the redevelopment area described in the general discussion and removal is necessary to implement the project to current standards. An effort to save this tree would require a significant reduction in usable building space to clear the CRZ and would not be allow reasonable and significant use of the property. Specifically, this tree must be removed to remove existing buildings, grade the site, install utility and stormwater infrastructure, build sidewalks and roads to complete streets standards, and implement a compact, transit-oriented design with new open spaces, a Sector-Planned shared-use path, and forest conservation areas.

- Tree 19

Proposed CRZ Impact: Severe at 100%

Condition: Good

Waiver Request: Approve for removal

Tree 19 is proposed for removal due to its location on the proposed Shared-Use Trail recommended by the Sector Plan and the Bicycle Master Plan. While envisioned in MNCPPC planning documents to run through the Coquelin Parkway, the Applicant has proposed a shift onto the subject property to minimize disturbance close to the stream and through an interior forest condition. By placing the trail at this location, near the existing edge of the forest, it will have less impact providing a greater buffer to the stream. It is also more visible and, therefore, safer for users who will now be able to connect from points north to the Capital Crescent Trail and the Purple Line. Removal is necessary to implement County policy under area and functional plan goals.

- Tree 20

Proposed CRZ Impact: Severe at 100%

Condition: Fair

Waiver Request: Approve for removal

Tree 20 is proposed for removal due to its location along the proposed right-of-way. This tree is within the redevelopment area described in the general discussion and removal is necessary to implement the project to current standards. An effort to save this tree would require a significant



reduction in usable building space to clear the CRZ and would not be allow reasonable and significant use of the property. Specifically, this tree must be removed to remove existing buildings, grade the site, install utility and stormwater infrastructure, build sidewalks and roads to complete streets standards, and implement a compact, transit-oriented design with new open spaces, a Sector-Planned shared-use path, and forest conservation areas. This tree is also in failing condition with a poor crown and long-term canopy will be better safeguarded by removing and replanting the street tree now.

- Tree 21

Proposed CRZ Impact: Severe at 100%

Condition: Fair/Poor

Waiver Request: Approve for removal

Tree 21 is proposed for removal due to its location along the proposed right-of-way. This tree is within the redevelopment area described in the general discussion and removal is necessary to implement the project to current standards. An effort to save this tree would require a significant reduction in usable building space to clear the CRZ and would not be allow reasonable and significant use of the property. Specifically, this tree must be removed to remove existing buildings, grade the site, install utility and stormwater infrastructure, build sidewalks and roads to complete streets standards, and implement a compact, transit-oriented design with new open spaces, a Sector-Planned shared-use path, and forest conservation areas. This tree is also in failing condition with a poor crown and long-term canopy will be better safeguarded by removing and replanting the street tree now.

- Tree 22

Proposed CRZ Impact: Severe at 97%

Condition: Fair

Waiver Request: Approve for removal with intent to save

Tree 22 is a street tree targeted for retention. Because of required streetscape, including expansion of the sidewalk width, this tree has a significant impact. To retain the tree, however, numerous steps will be implemented in coordination with an arborist to minimize damage to the root structure beyond the tree protection fencing established outside the root flare. While many of the details will be determined during the construction document and design phase in spring when more information can be gathered on tree health, we intend to use several techniques to minimize and mitigate impacts to this tree:

- Tree protection fencing
- Aeration and root pruning
- Reduced soil compaction
- Maintaining post-construction grade elevation
- Hand demolition of hardscape elements
- Selective canopy reduction
- Fertilization and irrigation



- Tree 23

Proposed CRZ Impact: Severe at 100%

Condition: Fair/Poor

Waiver Request: Approve for removal

Tree 23 is proposed for removal due to its location along the proposed right-of-way. This tree is within the redevelopment area described in the general discussion and removal is necessary to implement the project to current standards. An effort to save this tree would require a significant reduction in usable building space to clear the CRZ and would not be allow reasonable and significant use of the property. Specifically, this tree must be removed to remove existing buildings, grade the site, install utility and stormwater infrastructure, build sidewalks and roads to complete streets standards, and implement a compact, transit-oriented design with new open spaces, a Sector-Planned shared-use path, and forest conservation areas. This tree is also in failing condition with a poor crown and long-term canopy will be better safeguarded by removing and replanting the street tree now.

- Tree 24

Proposed CRZ Impact: Severe at 98%

Condition: Fair

Waiver Request: Approve for removal with intent to save

Tree 22 is a street tree targeted for retention. Because of required streetscape, including expansion of the sidewalk width, this tree has a significant impact. To retain the tree, however, numerous steps will be implemented in coordination with an arborist to minimize damage to the root structure beyond the tree protection fencing established outside the root flare. While many of the details will be determined during the construction document and design phase in spring when more information can be gathered on tree health, we intend to use several techniques to minimize and mitigate impacts to this tree:

- Tree protection fencing
- Aeration and root pruning
- Reduced soil compaction
- Maintaining post-construction grade elevation
- Hand demolition of hardscape elements
- Selective canopy reduction
- Fertilization and irrigation

- Tree 26

Proposed CRZ Impact: Severe at 98%

Condition: Fair

Waiver Request: Approve for removal with intent to save

Tree 26 is a street tree targeted for retention. Because of required streetscape, including expansion of the sidewalk width, this tree has a significant impact. To retain the tree, however, numerous steps will be implemented in coordination with an arborist to minimize damage to the root structure beyond the tree protection fencing established outside the root flare. While many



of the details will be determined during the construction document and design phase in spring when more information can be gathered on tree health, we intend to use several techniques to minimize and mitigate impacts to this tree:

- Tree protection fencing
 - Aeration and root pruning
 - Reduced soil compaction
 - Maintaining post-construction grade elevation
 - Hand demolition of hardscape elements
 - Selective canopy reduction
 - Fertilization and irrigation
-
- Tree 27
Proposed CRZ Impact: Severe at 100%
Condition: Fair/Good
Waiver Request: Approve for removal
Tree 27 is proposed for removal due to its location inside the proposed Limits of Disturbance (LOD). This tree is within the redevelopment area described in the general discussion and removal is necessary to implement the project to current standards. An effort to save this tree would require a significant reduction in usable building space to clear the CRZ and would not be allow reasonable and significant use of the property. Specifically, this tree must be removed to remove existing buildings, grade the site, install utility and stormwater infrastructure, build sidewalks and roads to complete streets standards, and implement a compact, transit-oriented design with new open spaces, and an additional recommended pedestrian connection between Manor Road and the Purple Line right-of-way.

 - Tree 29
Proposed CRZ Impact: Severe at 100%
Conditions: Poor
Waiver Request: Approve for removal
Tree 29 is proposed for removal due to its location inside the proposed Limits of Disturbance (LOD). This tree is within the redevelopment area described in the general discussion and removal is necessary to implement the project to current standards. An effort to save this tree would require a significant reduction in usable building space to clear the CRZ and would not be allow reasonable and significant use of the property. Specifically, this tree must be removed to remove existing buildings, grade the site, install utility and stormwater infrastructure, build sidewalks and roads to complete streets standards, and implement a compact, transit-oriented design with new open spaces, and an additional recommended pedestrian connection between Manor Road and the Purple Line right-of-way.

 - Tree 32
Proposed CRZ Impact: Severe at 100%
Condition: Good



Waiver Request: Approve for removal

Tree 32 is proposed for removal due to its location inside the proposed Limits of Disturbance (LOD). This tree is within the redevelopment area described in the general discussion and removal is necessary to implement the project to current standards. An effort to save this tree would require a significant reduction in usable building space to clear the CRZ and would not be allow reasonable and significant use of the property. Specifically, this tree must be removed to remove existing buildings, grade the site, install utility and stormwater infrastructure, build sidewalks and roads to complete streets standards, and implement a compact, transit-oriented design with new open spaces, and an additional recommended pedestrian connection between Manor Road and the Purple Line right-of-way.

- Tree 35

Proposed CRZ Impact: Severe at 100%

Condition: Good

Waiver Request: Approve for removal

Tree 35 is proposed for removal due to its location inside the proposed Limits of Disturbance (LOD). This tree is within the redevelopment area described in the general discussion and removal is necessary to implement the project to current standards. An effort to save this tree would require a significant reduction in usable building space to clear the CRZ and would not be allow reasonable and significant use of the property. Specifically, this tree must be removed to remove existing buildings, grade the site, install utility and stormwater infrastructure, build sidewalks and roads to complete streets standards, and implement a compact, transit-oriented design with new open spaces, and an additional recommended pedestrian connection between Manor Road and the Purple Line right-of-way.

- Tree 37

Proposed CRZ Impact: Severe at 100%

Condition: Fair

Waiver Request: Approve for removal

Tree 37 is proposed for removal due to its location inside the proposed Limits of Disturbance (LOD). This tree is within the redevelopment area described in the general discussion and removal is necessary to implement the project to current standards. An effort to save this tree would require a significant reduction in usable building space to clear the CRZ and would not be allow reasonable and significant use of the property. Specifically, this tree must be removed to remove existing buildings, grade the site, install utility and stormwater infrastructure, build sidewalks and roads to complete streets standards, and implement a compact, transit-oriented design with new open spaces, and an additional recommended pedestrian connection between Manor Road and the Purple Line right-of-way.

- Tree 38

Proposed CRZ Impact: Severe at 97%

Condition: Fair/Good



Waiver Request: Approve for removal with intent to save

Tree 38 is a street tree targeted for retention. Because of required streetscape, including expansion of the sidewalk width, this tree has a significant impact. To retain the tree, however, numerous steps will be implemented in coordination with an arborist to minimize damage to the root structure beyond the tree protection fencing established outside the root flare. While many of the details will be determined during the construction document and design phase in spring when more information can be gathered on tree health, we intend to use several techniques to minimize and mitigate impacts to this tree:

- Tree protection fencing
- Aeration and root pruning
- Reduced soil compaction
- Maintaining post-construction grade elevation
- Hand demolition of hardscape elements
- Selective canopy reduction
- Fertilization and irrigation

- Tree 39

Proposed CRZ Impact: Severe at 100%

Condition: Fair

Waiver Request: Approve for removal

Tree 39 is proposed for removal due to its location along the proposed right-of-way. This tree is within the redevelopment area described in the general discussion and removal is necessary to implement the project to current standards. An effort to save this tree would require a significant reduction in usable building space to clear the CRZ and would not be allow reasonable and significant use of the property. Specifically, this tree must be removed to remove existing buildings, grade the site, install utility and stormwater infrastructure, build sidewalks and roads to complete streets standards, and implement a compact, transit-oriented design with new open spaces, a Sector-Planned shared-use path, and forest conservation areas. This tree is also at the location of the new community entrance that is aligned with the confronting street to improve pedestrian and vehicular safety.

- Tree 40

Proposed CRZ Impact: Severe at 98%

Condition: Fair

Waiver Request: Approve for removal with intent to save

Tree 40 is a street tree targeted for retention. Because of required streetscape, including expansion of the sidewalk width, this tree has a significant impact. To retain the tree, however, numerous steps will be implemented in coordination with an arborist to minimize damage to the root structure beyond the tree protection fencing established outside the root flare. While many of the details will be determined during the construction document and design phase in spring when more information can be gathered on tree health, we intend to use several techniques to minimize and mitigate impacts to this tree:

- Tree protection fencing
 - Aeration and root pruning
 - Reduced soil compaction
 - Maintaining post-construction grade elevation
 - Hand demolition of hardscape elements
 - Selective canopy reduction
 - Fertilization and irrigation
-
- Tree 41
Proposed CRZ Impact: Severe at 100%
Condition: Fair/Poor
Waiver Request: Approve for removal
Tree 41 is proposed for removal due to its location along the proposed right-of-way. This tree is within the redevelopment area described in the general discussion and removal is necessary to implement the project to current standards. An effort to save this tree would require a significant reduction in usable building space to clear the CRZ and would not be allow reasonable and significant use of the property. Specifically, this tree must be removed to remove existing buildings, grade the site, install utility and stormwater infrastructure, build sidewalks and roads to complete streets standards, and implement a compact, transit-oriented design with new open spaces, a Sector-Planned shared-use path, and forest conservation areas. This tree is also in failing condition with a poor crown and long-term canopy will be better safeguarded by removing and replanting the street tree now.

 - Tree 42
Proposed CRZ Impact: Severe at 100%
Condition: Fair
Waiver Request: Approve for removal
Tree 42 is proposed for removal due to its location along the proposed right-of-way. This tree is within the redevelopment area described in the general discussion and removal is necessary to implement the project to current standards. An effort to save this tree would require a significant reduction in usable building space to clear the CRZ and would not be allow reasonable and significant use of the property. Specifically, this tree must be removed to remove existing buildings, grade the site, install utility and stormwater infrastructure, build sidewalks and roads to complete streets standards, and implement a compact, transit-oriented design with new open spaces, a Sector-Planned shared-use path, and forest conservation areas. This tree is also in failing condition with a poor crown and long-term canopy will be better safeguarded by removing and replanting the street tree now.

 - Tree 44
Proposed CRZ Impact: Severe at 44%
Condition: Fair/Good
Waiver Request: Approve for removal with intent to save



Tree 44 has a severe impact but is on a slope and may be retained. The impact is due to its location on the proposed Shared-Use Trail recommended by the Sector Plan and the Bicycle Master Plan. While envisioned in MNCPPC planning documents to run through the Coquelin Parkway, the Applicant has proposed a shift onto the subject property to minimize disturbance close to the stream and through an interior forest condition. By placing the trail at this location, near the existing edge of the forest, it will have less impact providing a greater buffer to the stream. It is also more visible and, therefore, safer for users who will now be able to connect from points north to the Capital Crescent Trail and the Purple Line. Impact is necessary to implement County policy under area and functional plan goals.

- Tree 45

Proposed CRZ Impact: Severe at 39%

Condition: Fair

Waiver request: Approve for removal with intent to save

Tree 45 has a severe impact but is on a slope and may be retained. The impact is due to its location on the proposed Shared-Use Trail recommended by the Sector Plan and the Bicycle Master Plan. While envisioned in MNCPPC planning documents to run through the Coquelin Parkway, the Applicant has proposed a shift onto the subject property to minimize disturbance close to the stream and through an interior forest condition. By placing the trail at this location, near the existing edge of the forest, it will have less impact providing a greater buffer to the stream. It is also more visible and, therefore, safer for users who will now be able to connect from points north to the Capital Crescent Trail and the Purple Line. Impact is necessary to implement County policy under area and functional plan goals.

- Tree 46

Proposed CRZ Impact: Severe at 100%

Condition: Fair/Poor

Waiver Request: Approve for removal

Tree 46 is proposed for removal due to its location on the proposed Shared-Use Trail recommended by the Sector Plan and the Bicycle Master Plan. While envisioned in MNCPPC planning documents to run through the Coquelin Parkway, the Applicant has proposed a shift onto the subject property to minimize disturbance close to the stream and through an interior forest condition. By placing the trail at this location, near the existing edge of the forest, it will have less impact providing a greater buffer to the stream. It is also more visible and, therefore, safer for users who will now be able to connect from points north to the Capital Crescent Trail and the Purple Line. Removal is necessary to implement County policy under area and functional plan goals.

- Tree 48

Proposed CRZ Impact: Severe at 100%

Condition: Fair

Waiver Request: Approve for removal



Tree 48 is proposed for removal due to its location on the proposed Shared-Use Trail recommended by the Sector Plan and the Bicycle Master Plan. While envisioned in MNCPPC planning documents to run through the Coquelin Parkway, the Applicant has proposed a shift onto the subject property to minimize disturbance close to the stream and through an interior forest condition. By placing the trail at this location, near the existing edge of the forest, it will have less impact providing a greater buffer to the stream. It is also more visible and, therefore, safer for users who will now be able to connect from points north to the Capital Crescent Trail and the Purple Line. Removal is necessary to implement County policy under area and functional plan goals.

- Tree 50

Proposed CRZ Impact: Severe at 52%

Condition: Fair

Waiver request: Approve for removal

Tree 50 is proposed for removal due to its location directly at the proposed Shared-Use Trail recommended by the Sector Plan and the Bicycle Master Plan and Manor Road, which is required to provide an expanded 6-foot sidewalk. While the trail is envisioned in MNCPPC planning documents to run through the Coquelin Parkway, the Applicant has proposed a shift onto the subject property to minimize disturbance close to the stream and through an interior forest condition. By placing the trail at this location, near the existing edge of the forest, it will have less impact providing a greater buffer to the stream. It is also more visible and, therefore, safer for users who will now be able to connect from points north to the Capital Crescent Trail and the Purple Line. Removal is necessary to implement County policy under area and functional plan goals.

- Tree 53

Proposed CRZ Impact: Moderate at 28%

Condition: Fair

Waiver request: Approve impact

Tree 53 is impacted by the master-planned Shared Use Trail and the Manor Road improvements. While the trail is envisioned in MNCPPC planning documents to run through the Coquelin Parkway, the Applicant has proposed a shift onto the subject property to minimize disturbance close to the stream and through an interior forest condition. By placing the trail at this location, near the existing edge of the forest, it will have less impact providing a greater buffer to the stream. It is also more visible and, therefore, safer for users who will now be able to connect from points north to the Capital Crescent Trail and the Purple Line. Impact is necessary to implement County policy under area and functional plan goals.

- Tree 54

Proposed CRZ Impact: Severe at 51%

Condition: Fair

Waiver Request: Approve for removal with intent to save



Tree 54 is a street tree targeted for retention. Because of required streetscape, including expansion of the sidewalk width, this tree has a significant impact. To retain the tree, however, numerous steps will be implemented in coordination with an arborist to minimize damage to the root structure beyond the tree protection fencing established outside the root flare. While many of the details will be determined during the construction document and design phase in spring when more information can be gathered on tree health, we intend to use several techniques to minimize and mitigate impacts to this tree:

- Tree protection fencing
 - Aeration and root pruning
 - Reduced soil compaction
 - Maintaining post-construction grade elevation
 - Hand demolition of hardscape elements
 - Selective canopy reduction
 - Fertilization and irrigation
-
- Tree 55
Proposed CRZ Impacts: Minor at 2%
Condition: Good
Waiver request: Approve impact
Tree 55 is impacted by Manor Road improvements. Impact is minimal but necessary to implement County policy under area and functional plan goals.

 - Tree 60
Proposed CRZ Impact: Moderate at 28%
Condition: Good
Waiver Request: Approve impact
Tree 60 has a moderate impact and may be retained. The impact is due to its location along the proposed Shared-Use Trail recommended by the Sector Plan and the Bicycle Master Plan and sewer line connection. While the trail is envisioned in MNCPPC planning documents to run through the Coquelin Parkway, the Applicant has proposed a shift onto the subject property to minimize disturbance close to the stream and through an interior forest condition. By placing the trail at this location, near the existing edge of the forest, it will have less impact providing a greater buffer to the stream. It is also more visible and, therefore, safer for users who will now be able to connect from points north to the Capital Crescent Trail and the Purple Line. The sewer only runs along the stream valley and no alternative location is available at this elevation. Impact is necessary to implement County policy under area and functional plan goals.

 - Tree 62
Proposed CRZ Impact: Severe at 40%
Condition: Fair
Waiver request: Approve impact
Tree 62 has a significant impact but may be retained. It is located within a forest stand and the



impacts are isolated to the perimeter of the critical root zone. Impacts are due to a sewer line and the master-planned Shared Use Trail. The existing sewer is in the stream valley, as is typical throughout the County, and the location of the connection was chosen to impact as few trees as possible. The impact from the trail is necessary to implement County Policy under area and functional plan goals.

- Tree 65
Proposed CRZ Impact: Severe at 46%
Condition: Good
Waiver request: Approve impact
Tree 62 has a significant impact but may be retained. It is located within a forest stand and the impacts are isolated to the perimeter of the critical root zone. Impacts are due to a stream restoration project creating step pools from a proposed storm drain outlet, as well as reducing steep stream banks. The impact from the trail is necessary to implement County Policy under area and functional plan goals.
- Tree 66
Proposed CRZ Impact: Minor at 6%
Condition: Good
Waiver request: Approve impact
Tree 66 has a minor impact and may be retained. The impact is due to its location along the proposed Shared-Use Trail recommended by the Sector Plan and the Bicycle Master Plan. While the trail is envisioned in MNCPPC planning documents to run through the Coquelin Parkway, the Applicant has proposed a shift onto the subject property to minimize disturbance close to the stream and through an interior forest condition. By placing the trail at this location, near the existing edge of the forest, it will have less impact providing a greater buffer to the stream. It is also more visible and, therefore, safer for users who will now be able to connect from points north to the Capital Crescent Trail and the Purple Line. Impact is necessary to implement County policy under area and functional plan goals.
- Tree 68
Proposed CRZ Impact: Moderate/Severe at 36%
Condition: Good
Waiver request: Approve for removal with intent to save
Tree 68 has a moderate/severe impact but may be retained. The impact is due to outfall restoration work, primarily fill of an eroded channel and replanting. Impact is necessary to implement County policy under area and functional plan goals.

While many of the details will be determined during the construction document and design phase in spring when more information can be gathered on tree health, we intend to use several techniques to minimize and mitigate impacts to this tree:

- Tree protection fencing

- Aeration and root pruning
 - Reduced soil compaction
 - Maintaining post-construction grade elevation
 - Selective canopy reduction
- Tree 69
Proposed CRZ Impact: Severe at 41%
Condition: Fair
Waiver request: Approve for removal with intent to save
Tree 69 has a severe impact but may be retained. The impact is due to outfall restoration work, primarily fill of an eroded channel and replanting. Impact is necessary to implement County policy under area and functional plan goals.

While many of the details will be determined during the construction document and design phase in spring when more information can be gathered on tree health, we intend to use several techniques to minimize and mitigate impacts to this tree:

- Tree protection fencing
 - Aeration and root pruning
 - Reduced soil compaction
 - Maintaining post-construction grade elevation
 - Selective canopy reduction
- Tree 70
Proposed CRZ Impact: Severe at 59%
Condition: Good
Waiver request: Approve for removal with intent to save
Tree 70 has a severe impact but may be retained. The impact is due to outfall restoration work, primarily fill of an eroded channel and replanting. Impact is necessary to implement County policy under area and functional plan goals.

While many of the details will be determined during the construction document and design phase in spring when more information can be gathered on tree health, we intend to use several techniques to minimize and mitigate impacts to this tree:

- Tree protection fencing
 - Aeration and root pruning
 - Reduced soil compaction
 - Maintaining post-construction grade elevation
 - Selective canopy reduction
- Tree 72
Proposed CRZ Impacts: Minor at 7%



Condition: Fair

Waiver request: Approve impact

Tree 72 is a street tree located on the north side of Manor Road, across from the subject Property. The CRZ is not likely to be actually impacted the proposed development on the south side of Manor Road. Therefore, this tree is proposed to be saved.

- Tree 73

Proposed CRZ Impacts: Minor at 14%

Condition: Fair

Waiver request: Approve impact

Tree 73 is a street tree located on the north side of Manor Road, across from the subject Property. The CRZ is not likely to be actually impacted the proposed development on the south side of Manor Road. Therefore, this tree is proposed to be saved.

- Tree 74

Proposed CRZ Impacts: Minor at 11%

Tree Conditions: Fair to Poor

Waiver request: Approve impact

Tree 74 is a street tree located on the north side of Manor Road, across from the subject Property. The CRZ is not likely to be actually impacted the proposed development on the south side of Manor Road. Therefore, this tree is proposed to be saved.

- Tree 75

Proposed CRZ Impacts: Minor at 16%

Tree Conditions: Poor

Waiver request: Approve impact

Tree 75 is a street tree located on the north side of Manor Road, across from the subject Property. The CRZ is not likely to be actually impacted the proposed development on the south side of Manor Road. Therefore, this tree is proposed to be saved.

- Tree 76

Proposed CRZ Impacts: Minor at 11%

Tree Conditions: Fair

Waiver request: Approve impact

Tree 76 is a street tree located on the north side of Manor Road, across from the subject Property. The CRZ is not likely to be actually impacted the proposed development on the south side of Manor Road. Therefore, this tree is proposed to be saved.

- Tree 77

Proposed CRZ Impact: Severe at 100%

Condition: Good/Fair

Waiver Request: Approve for removal



Tree 77 is proposed for removal due to its location inside the proposed Limits of Disturbance (LOD). This tree is within the redevelopment area described in the general discussion and removal is necessary to implement the project to current standards. An effort to save this tree would require a significant reduction in usable building space to clear the CRZ and would not be allow reasonable and significant use of the property. Specifically, this tree must be removed to remove existing buildings, grade the site, install utility and stormwater infrastructure, build sidewalks and roads to complete streets standards, and implement a compact, transit-oriented design with new open spaces, and an additional recommended pedestrian connection between Manor Road and the Purple Line right-of-way. Further, this tree would need to be removed by pending Purple Line grading within the right-of-way.

- (2) *Describe how enforcement of this Chapter will deprive the landowner of rights commonly enjoyed by others in similar areas;*

Denial of the variance would also deprive Applicant of rights commonly enjoyed by others in similar areas. Despite the constraints the existing environmental features create for redevelopment of the property, this project establishes compatible relationships between new development and existing neighborhoods through limits on density and uses; the Project includes for-sale townhomes including Moderately Priced dwelling Units (MPDUs) at a similar scale to the currently existing rental garden apartments and townhomes. No commercial uses will be developed on the Property, preserving the residential character of the neighborhood. Development was focused on the areas of the property currently improved, which contain many of the trees subject to the variance. Furthermore, the proposed development achieves the *2013 Approved and Adopted Chevy Chase Lake Sector Plan* (the Sector Plan) recommendation of retaining the current zoning and similar development. Avoiding any impacts to the trees would significantly inhibit any compatible redevelopment. Therefore, strict enforcement of the County Code would unfairly prevent the redevelopment of the Property to the same extent as similarly situated properties along Manor Road.

Approval of the variance will allow Applicant to create a community that is consistent with the existing land uses in the area. Approval will also allow for the creation of master-planned pathways connecting the project to the adjacent communities along restored greenways for intangible yet significant community enjoyment of natural areas. All of these goals are consistent with the rights enjoyed by nearby property owners.

- (3) *Verify that State water quality standards will not be violated and that a measurable degradation in water quality will not occur as a result of the granting of the variance;*

The granting of Applicant's variance request will not result in a violation of State water quality standards, nor will a measurable degradation in water quality occur as a result. On the contrary, the Project will implement measures to improve water quality where none currently exist. On-site runoff in the existing condition allows heated and polluted runoff to flow directly through storm drains to nearby streams. This runoff has created areas of significant erosion, which will be remediated by this

project. Further, A significant area within the Stream Valley Buffer (SVB) at the east end of the site will be placed into a Category I Forest Conservation Easement and allow for a perpetual protection of forested area buffering the offsite stream. A hiker/biker path will be installed adjacently to the Category I Forest Conservation easement, allowing low-impact community use and enjoyment of the site's natural areas. The Project also integrates current state-of-the-art stormwater management practices, which will improve upon the Property's previous stormwater management system and enhance water quality. These enhancements include reduced sediment and pollutants, cooler water temperatures, slowed and delayed storm flows, and areas for planting habitat.

(4) *Provide any other information appropriate to support the request.*

In addition to the summary below, please see the revised Statement of Justification and attached narrative on the Sector Plan Environmental Recommendations and the Environmental Guidelines. Approving Applicant's variance request is justified for other reasons as well. The proposed project will advance the County's Sector Plan vision for the Property through the development of an attractive public transit-oriented residential community that contains an open space component. The project also includes attractive landscaped open spaces and amenities while continuing to provide access to the nearby shops, metro, schools, and nature sanctuary. The Project offers recreational and natural habitat preservation near a Metro stop that are consistent with the current character of the surrounding neighborhood.

Additionally, the Project proposes preserving most of the existing forest adjacent to the stream buffer, subject to a Category I Forest Conservation easement. The low-impact path planned for community enjoyment of the natural areas provides access to open spaces and the adjacent communities, which will promote additional pedestrian and cyclist activity in the area. It is envisioned that a significant number of additional plantings will occur throughout the property, including shade trees, evergreen trees, ornamental trees, and planting beds, all of which will serve to improve ecological quality. Focusing redevelopment on existing developed land, increasing Forest Conservation, landscape plantings, and greatly improved stormwater management design all combine to significantly improve water quality protection, wildlife value, carbon sequestration, and reduction in urban heat island effects.

Furthermore, the variance does not arise from a condition related to land or building use, either permitted or nonconforming, on a neighboring property.



Thank you for your consideration of Applicant's tree variance request. The supporting information provided in this letter establishes that denial of the variance would result in unnecessary hardship or practical difficulty, as well as demonstrates Applicant's extensive efforts to minimize impacts. Please contact me with any questions, or if you require additional information.

Sincerely,

VIKA Maryland, LLC

Joshua Sloan, RLA, ASLA, AICP
Director of Planning and Landscape Architecture / Vice President





Department of Permitting Services
Fire Department Access and Water Supply Comments

DATE: 31-Jan-23
TO: Josh Sloan
VIKA, Inc
FROM: Marie LaBaw
RE: Preston Place & Lake Apartments
820220180

PLAN APPROVED

1. Review based only upon information contained on the plan submitted **31-Jan-23** .Review and approval does not cover unsatisfactory installation resulting from errors, omissions, or failure to clearly indicate conditions on this plan.
2. Correction of unsatisfactory installation will be required upon inspection and service of notice of violation to a party responsible for the property.

***** Fire lane orders shall be processed prior to certified site plan and implemented for private roads and alleys as necessary to maintain fire department vehicular access prior to any occupancies *****

***** 2/26/2023 Amendment - Height restriction language added *****



MONTGOMERY COUNTY FIRE AND RESCUE SERVICE

Date: February 17, 2023

Fire Lane Establishment Order

Pursuant to Section 22-33, Montgomery County Code, 1971, as amended, you are hereby notified that a Fire Lane has been established as described in this order. You are hereby ordered to post fire lane signs and paint curbs/pavement as identified below. When signs or paint work has been completed, this order will authorize the enforcement of this Fire Lane by appropriate police or fire officials. Compliance with this order must be achieved within 30 days of receipt when any of the following conditions are met:

- One or more structures addressed from the subject road are occupied;
- The road or accessway is available for use and at least one building permit for an address on the subject road has been issued; or
- The road or accessway is necessary fire department access.

LOCATION: 1. Road B from Manor Road to terminus at southeast of site except where 8' parallel parking spaces are provided.
2. Road C from Manor Road to intersection with Road B.
3. Alley D & Alley F from intersection of Road C to 150 feet from intersection tangent.

Delineate all areas where indicated by signs and/or paint.

☒ **SIGNS** -- (See attached diagram for location of sign placement)



(Red letters on white background)

Signs must be posted so that it is not possible to park a vehicle without being in sight of a sign. Signs may be no further apart than 100 feet.

☐ **PAINT** -- (See attached diagram when painting is required)

Paint must be traffic yellow with lines of Sufficient width to be readily identifiable/readable by motor vehicle operators.

Signature of Order Writer/I.D. #

Cc: Fire Code Enforcement Section
Attachment: Fire Lane Diagram

FIRE LANE LOCATION/ADDRESS: South side of Manor Road at intersection with Jones Bridge Road.

See attached drawing for designated fire lanes:

I have received the drawing and instructions for installing the designated fire lanes.

NAME AND TITLE OF PROPERTY REPRESENTATIVE

NAME: Jeffery S. Driscoll TITLE: VP, Land Development, DC Metro

SIGNATURE: 

PHONE: 410.381.3263 DATE: February 17, 2023

EMAIL ADDRESS (where processed order will be sent):

sloan@vika.com

The designated fire lanes are the minimum necessary for fire/rescue access and are in accordance with Section 22-33 of the Fire Safety Code.

NAME: _____ SIGNATURE: _____

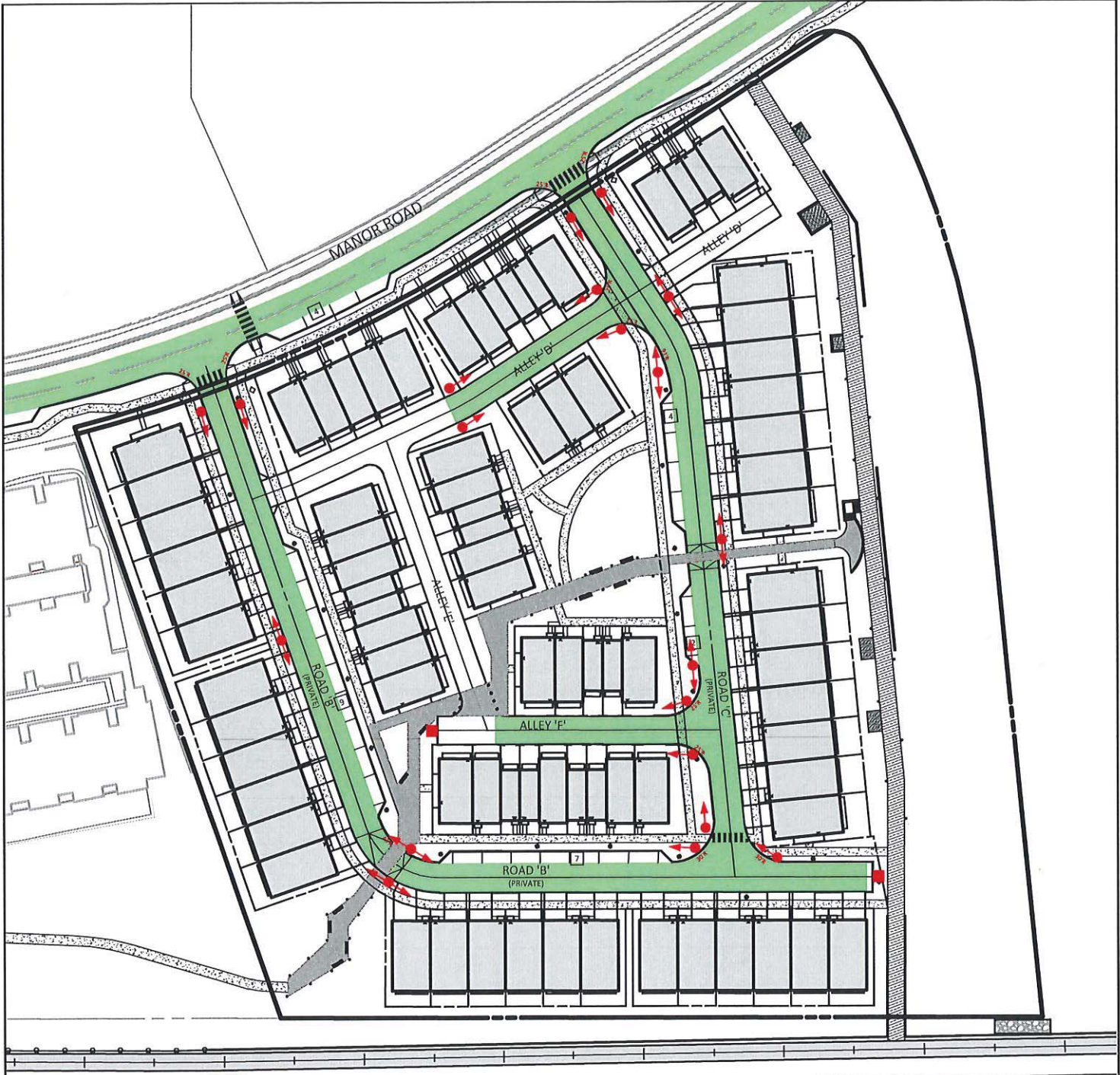
STA.#: _____ I.D.#: _____ DATE: _____

Comments: _____

Fire Lane Installed Per Order

NAME: _____ DATE: _____




0092N/23



ORDER FOR TRAFFIC CONTROL SIGNS

Pursuant to Section 31-2 and 31-10, Montgomery County Code, 2004 as amended, you are hereby directed to install the following traffic and or parking control devices at the designated location(s).

LEGEND

-  Fire Lane Sign/Direction
-  Fire Lane Sign (end of alley)
-  Fire Access Path



VIKA MARYLAND, LLC ■
ENGINEERS ■ PLANNERS ■ LANDSCAPE ARCHITECTS ■
SURVEYORS ■ 3D LASER SCANNING ■
20251 CENTURY BOULEVARD SUITE 400 ■
GERMANTOWN, MD 20874 ■
(301)918-4100 ■ FAX (301)918-2282 ■
WWW.VIKA.COM ■

PRESTON PLACE AND LAKE APARTMENTS

7TH ELECTION DISTRICT MONTGOMERY COUNTY, MARYLAND
WSSC GRID: 210NW04
TAX MAP: HN43

FIRE LANE ORDER

DATE: JANUARY 26, 2023	
DRAWN: NT	CHECKED: JS
2 OF 2	



0 50' 100' 200'
SCALE: 1" = 100'

PRIVATE ROAD A &
PRIVATE ALLEYS B, C & D- PRESTON PLACE APARTMENTS
FIRE LANE ORDER - NO PARKING SIGN LOCATIONS



MONTGOMERY COUNTY FIRE AND RESCUE SERVICE

Date: February 17, 2023

Fire Lane Establishment Order

Pursuant to Section 22-33, Montgomery County Code, 1971, as amended, you are hereby notified that a Fire Lane has been established as described in this order. You are hereby ordered to post fire lane signs and paint curbs/pavement as identified below. When signs or paint work has been completed, this order will authorize the enforcement of this Fire Lane by appropriate police or fire officials. Compliance with this order must be achieved within 30 days of receipt when any of the following conditions are met:

- One or more structures addressed from the subject road are occupied;
- The road or accessway is available for use and at least one building permit for an address on the subject road has been issued; or
- The road or accessway is necessary fire department access.

LOCATION:

1. Road A from Manor Road to terminus at southeast of site except where 8' parallel parking spaces are provided.
2. Alley B & Alley C from intersection of Road A to 150 feet from intersection tangent.

Delineate all areas where indicated by signs and/or paint.

☒ **SIGNS** -- (See attached diagram for location of sign placement)



(Red letters on white background)

Signs must be posted so that it is not possible to park a vehicle without being in sight of a sign. Signs may be no further apart than 100 feet.

☐ **PAINT** -- (See attached diagram when painting is required)

Paint must be traffic yellow with lines of Sufficient width to be readily identifiable/readable by motor vehicle operators.

Signature of Order Writer/I.D. #

Cc: Fire Code Enforcement Section
Attachment: Fire Lane Diagram

FIRE LANE LOCATION/ADDRESS: South side of Manor Road across from intersection
with Village Park Place.

See attached drawing for designated fire lanes:

I have received the drawing and instructions for installing the designated fire lanes.

NAME AND TITLE OF PROPERTY REPRESENTATIVE

NAME: Jeffery S. Driscoll TITLE: VP, Land Development, DC Metro

SIGNATURE: 

PHONE: 410.381.3263 DATE: February 17, 2023

EMAIL ADDRESS (where processed order will be sent):

sloan@vika.com

The designated fire lanes are the minimum necessary for fire/rescue access and are in accordance with
Section 22-33 of the Fire Safety Code.

NAME: _____ SIGNATURE: _____

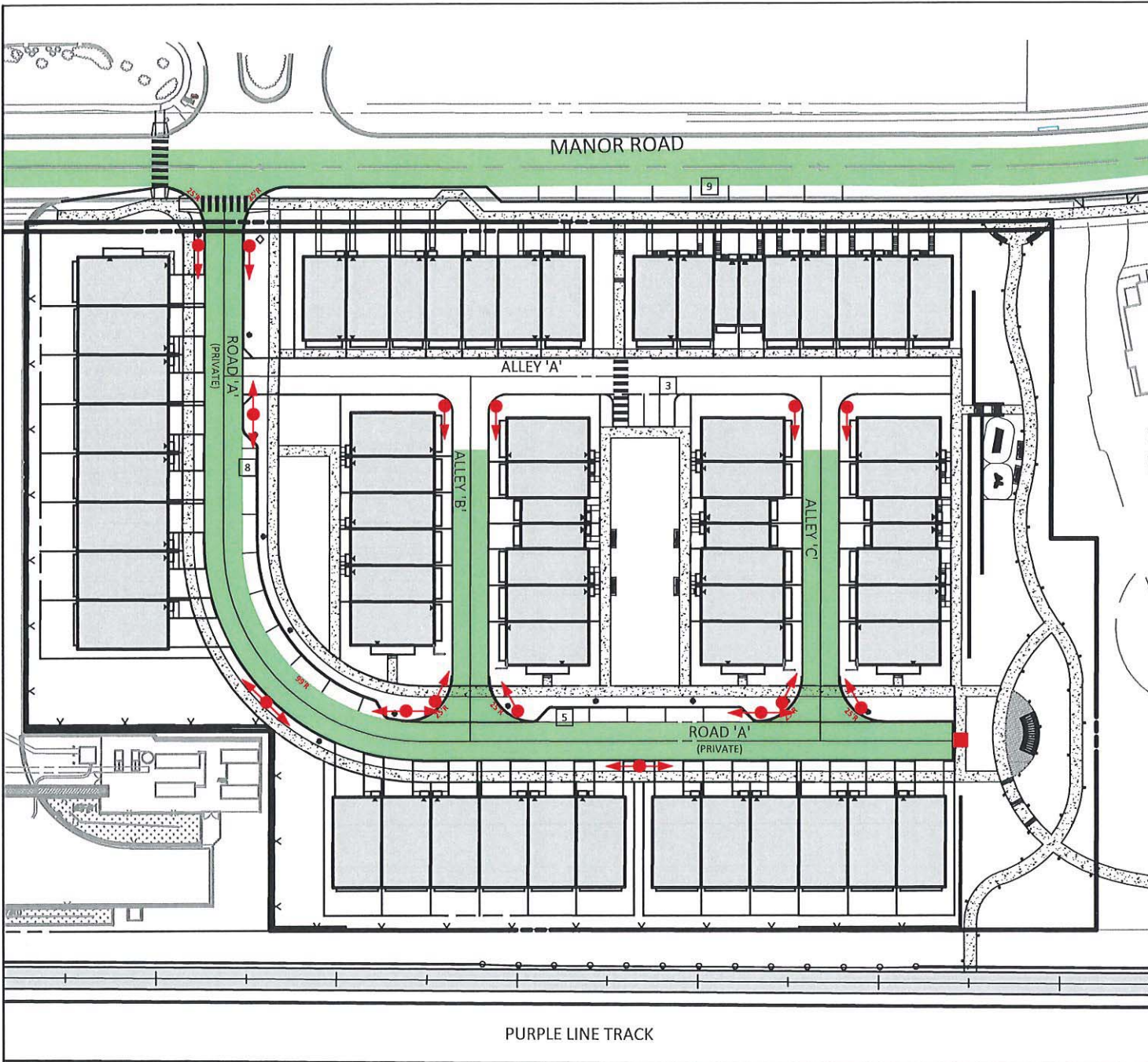
STA.#: _____ I.D.#: _____ DATE: _____

Comments: _____

Fire Lane Installed Per Order

NAME: _____ DATE: _____

0092N/23



ORDER FOR TRAFFIC CONTROL SIGNS

Pursuant to Section 31-2 and 31-10, Montgomery County Code, 2004 as amended, you are hereby directed to install the following traffic and or parking control devices at the designated location(s).

LEGEND

- Fire Lane Sign/Direction
- Fire Lane Sign (end of alley)
- Fire Access Path



VKA MARYLAND, LLC ■
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SURVEYORS ■ 3D LASER SCANNING ■
20251 CENTURY BOULEVARD SUITE 400 ■
GERMANTOWN, MD 20874 ■
(301)916-4100 ■ FAX (301)916-2282 ■
WWW.VKA.COM ■

PRESTON PLACE AND LAKE APARTMENTS

7TH ELECTION DISTRICT MONTGOMERY COUNTY, MARYLAND
WSSC GRID: 210NW04
TAX MAP: HN43

FIRE LANE ORDER

DATE: JANUARY 26, 2023

DRAWN: NT

CHECKED: JS

1 OF 2



0 40' 80' 1160'

SCALE: 1" = 80'

PRIVATE ROAD A &
PRIVATE ALLEYS B & C- LAKE APARTMENTS
FIRE LANE ORDER - NO PARKING SIGN LOCATIONS

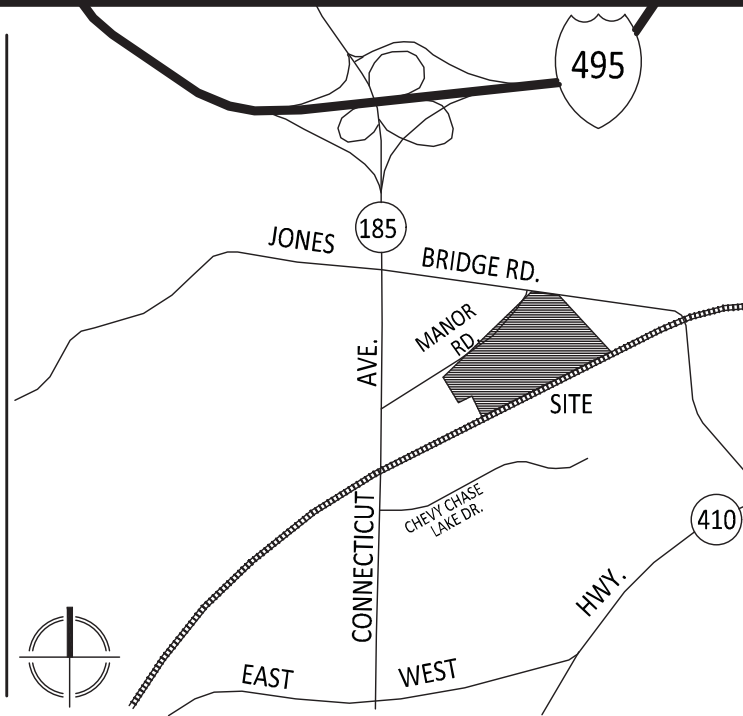


NOTE: SIDE ENTRY ONLY ALLOWED WHERE PORCH IS VISIBLE FROM SAME FIRE DEPARTMENT VEHICULAR ACCESS AS OTHER UNITS IN THE STRING



NOTE: NO DECKS ABOVE 11' HEIGHT ALLOWED ON ALLEY-FACING SIDE.

REAR LOAD
SIDE ELEVATION



VICINITY MAP
SCALE: 1" = 2000'



VIKA MARYLAND, LLC
20251 Century Blvd., Suite 400
Germantown, MD 20874
301.916.4100 | vika.com
Our Site Set on the Future.

PREPARED FOR:
TOLL MID-ATLANTIC LP
COMPANY, INC
6731 COLUMBIA GATEWAY DRIVE
SUITE 120
COLUMBIA, MD 21046
CONTACT: JEFFREY S. DRISCOLL
JDRISCOLL@TOLLBROTHERS.COM

DESIGN CONSULTANTS
PLANNER, CIVIL ENGINEER
VIKA MARYLAND, LLC
20251 CENTURY BOULEVARD
SUITE 400
GERMANTOWN MD, 20874
301.916.4100
CONTACT: JOSHUA SLOAN, RLA

ATTORNEY
LERCH, EARLY & BREWER,
CHTD
7600 WISCONSIN AVENUE
SUITE 700
BETHESDA MD, 20814
301.841.3837
CONTACT: PATRICIA A. HARRIS

HEIGHT RESTRICTION DEFINITION:
ONE & TWO-FAMILY DWELLINGS OF 3 STORIES (27'
TO HIGHEST SILL, INCLUDING FALSE DORMERS) OR
LESS MUST PROVIDE ACCESS TO THE OCCUPIED
INTERIOR THROUGH A MAIN, SIDE-HINGE DOOR
WITHIN 150' OF A FIRE DEPARTMENT ACCESS
ROUTE.

NOTES:
UNITS MAY HAVE 4TH FLOOR ROOFTOP TERRACE;
DEPENDENT ON ACCESS CONDITION, UNITS MAY BE
HEIGHT RESTRICTED AND, IF HEIGHT RESTRICTED,
MUST NOT HAVE A 4TH FLOOR TERRACE.

- IF FRONT DOOR IS WITHIN 50 FEET OF
COMPLIANT FIRE DEPARTMENT VEHICULAR
ACCESS, NO RESTRICTION.
- IF FRONT DOOR IS WITHIN 150 FEET OF
COMPLIANT FIRE DEPARTMENT VEHICULAR
ACCESS AND REAR WALL OF UNIT IS WITHIN
50 FEET OF ALLEY ACCESS, NO RESTRICTION.
- NO STACKED, SUPERIMPOSED, OR
PIGGYBACKED DWELLING UNITS PERMITTED.
- NO UNITS REQUIRE A FIRE DEPARTMENT
CONNECTION.

LEGEND

- 10"W
- FIRE HYDRANT
- PRIMARY BUILDING ENTRY
- FIRE ACCESS PATH
- FIRE ACCESS FOOT PATH
- 100' RADIUS

0 15' 30' 60'
SCALE: 1" = 30'

FIRE CODE ENFORCEMENT

Fire Department Access Review

Review based only upon information contained on
this plan. Does not cover unsatisfactory layout
resulting from omissions, errors or failure to
clearly indicate conditions on this plan. Correction
of such unsatisfactory layout to afford required
access will be required if found upon inspection
after installation.

BY: S.M.C. PM: 43 DATE: 2/26/2023
original 1/31/2023

*** Height restriction language added ***

REVISIONS
REVISED PER DRC
HEIGHT RESTRICTION

DATE
09/02/22
02/02/23

PRESTON
PLACE AND LAKE
APARTMENTS
7TH ELECTION DISTRICT
MONTGOMERY COUNTY,
MARYLAND
WSSC GRID: 210NW04
TAX MAP: HN43

FIRE ACCESS
PLAN

SITE PLAN
#820220180

PROFESSIONAL SEAL



I HEREBY CERTIFY THAT THESE DOCUMENTS WERE
PREPARED OR APPROVED BY ME, AND THAT I AM
A QUALIFIED PROFESSIONAL ENGINEER
UNDER THE LAWS OF THE STATE OF MARYLAND.
ENGINEER'S NAME: DONALD NELSON, P.E.
LICENSE NO.: 5259
EXPIRATION DATE: JULY 31, 2024

THE INFORMATION, DESIGN AND CONTENT OF THESE
DRAWINGS OR DOCUMENTS ARE PROPRIETARY TO VIKA
MARYLAND, LLC AND CONSTITUTE ITS PROPRIETARY
INTELLECTUAL PROPERTY. THESE DRAWINGS AND/OR
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COPIED, DIGITALLY CONVERTED, MODIFIED OR USED
FOR ANY PURPOSE, IN ANY FORMAT, WITHOUT PRIOR
WRITTEN AUTHORIZATION FROM VIKA MARYLAND, LLC.
VIOLATIONS MAY RESULT IN PROSECUTION. ONLY
APPROVED, SIGNED AND SEALED PLANS OR DRAWINGS
MAY BE UTILIZED FOR CONSTRUCTION PURPOSES.

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DRAWN BY:
DESIGNED BY:
DATE ISSUED: 03/23/2022

VIKA
PROJECT
DRAWING
NO. FDA-002

SHEET NO.

"FOR LOCATION OF UTILITIES CALL
8-1-1 or 1-800-257-7777 OR LOG ON TO
www.call811.com or http://www.missutility.net
48 HOURS IN ADVANCE OF ANY
WORK IN THIS VICINITY"
The excavator must notify all public utility companies with underground
facilities in the area of proposed excavation and have those facilities
located by the utility companies prior to commencing excavation. The
excavator is responsible for compliance with requirements of Chapter
36A of the Montgomery County Code.

LAYOUT: SP-FDA-2, Plotted By: nperaz



DEPARTMENT OF PERMITTING SERVICES

Marc Elrich
County Executive

Mitra Pedoeem
Director

December 7, 2022

Mr. Don Nelson
Vika, Inc
20251 Century Boulevard, Suite 400,
Germantown, MD 20874

Re: **COMBINED STORMWATER MANAGEMENT
CONCEPT/SITE DEVELOPMENT
STORMWATER MANAGEMENT PLAN** for
Preston Place
Preliminary Plan #: 120220130
SM File #: 288047
Tract Size/Zone: 12.61 ac
Total Concept Area: 12.11 ac
Lots/Block: L 1-4, B 2
Parcel(s): B
Watershed: Lower Rock Creek (I)

Dear Mr. Nelson:

Based on a review by the Department of Permitting Services Review Staff, the stormwater management concept for the above-mentioned site is **acceptable**. The plan proposes to meet required stormwater management goals via the use of 6 Microbioretention, 7 Storm Filters with Underground Pipe Storage.

The following items will need to be addressed during the detailed sediment control/stormwater management plan stage:

1. A detailed review of the stormwater management computations will occur at the time of detailed plan review.
2. An engineered sediment control plan must be submitted for this development.
3. All filtration media for manufactured best management practices, whether for new development or redevelopment, must consist of MDE approved material.
4. Landscaping in areas located within the stormwater management easement which are shown on the approved Landscape Plan as part of the approved Site Plan are illustrative purpose only and may be changed at the time of detailed plan review of the Sediment Control/Storm Water Management plans by the Montgomery County Department of Permitting Services, Water Resources Section.

This list may not be all-inclusive and may change based on available information at the time.

Payment of a stormwater management contribution in accordance with Section 2 of the Stormwater Management Regulation 4-90 **is not required**.



2425 Reedy Drive, 7th Floor, Wheaton, Maryland 20902 | 240-777-0311
www.montgomerycountymd.gov/permittingservices

Mr. Don Nelson
December 7, 2022
Page 2 of 2

This letter must appear on the sediment control/stormwater management plan at its initial submittal. The concept approval is based on all stormwater management structures being located outside of the Public Utility Easement, the Public Improvement Easement, and the Public Right of Way unless specifically approved on the concept plan. Any divergence from the information provided to this office; or additional information received during the development process; or a change in an applicable Executive Regulation may constitute grounds to rescind or amend any approval actions taken, and to reevaluate the site for additional or amended stormwater management requirements. If there are subsequent additions or modifications to the development, a separate concept request shall be required.

If you have any questions regarding these actions, please feel free to contact Bill Musico PE at 240-777-6340.

Sincerely,



Mark Etheridge, Manager
Water Resources Section
Division of Land Development Services

cc: Neil Braunstein
SM File # 288047

ESD: Required/Provided 55,331 cu.ft. / 13,228 cu.ft.
PE: Target/Achieved: 2.0 inches / 0.5 inches
STRUCTURAL: 42,103 cu.ft.
WAIVED: 0.00 cf.



DEPARTMENT OF TRANSPORTATION

Marc Elrich
County Executive

Christopher Conklin
Director

January 20, 2023

Mr. Adam Bossi, Planner III
Downcounty Planning Division
The Maryland-National Capital
Park & Planning Commission
2425 Reedie Drive
Wheaton, MD 20902

RE: Preliminary Plan No. 120220130
Preston Place & Lake Apartments

Dear Mr. Bossi:

We have completed our review of the revised preliminary plan uploaded to eplans on December 27, 2022. A previous version of this plan was reviewed by the Development Review Committee (DRC) at its meeting on June 7, 2022. We recommend approval of the plans subject to the following comments:

Significant Plan Review Comments

1. The proposal for private streets, as shown on the preliminary plan uploaded to eplans on December 27, 2022 (and documented in the December 22, 2022, Statement of Justification), is acceptable to this Department. The proposed private streets are not in the master plan and are not needed for general circulation. We support Planning Board approval of the private streets subject to execution and recordation of a Declaration of Restrictive Covenants (for private roads). The deed reference for this document is to be identified on the record plat.
2. The applicant will be required to replace the existing sidewalk on the south side of Manor Road, from the northwest corner of the property to Jones Bridge Road, and construct a six-foot (6') wide, concrete sidewalk in its place with a five-and-one-half-foot (5.5') buffer from traffic.
3. Provide dedication thirty-five (35') feet from centerline along the property's Manor Road frontage per the 2013 Chevy Chase Lake Sector Plan.
4. MCDOT does not approve of the proposed Manor Road midblock crossing, shown on Sheet 07-PREL-120220130-04, connecting the sidewalk on the east side of the Road B entrance to the sidewalk on the north side of Manor Road. On the Certified Preliminary Plan, remove the ADA ramps and pavement crossing striping. In addition, on the Certified Preliminary Plan, remove all pavement crossing striping shown in the Manor Road right-of-way on Sheet 07-PREL-120220130-03 and Sheet

Office of the Director

101 Monroe Street 10th Floor · Rockville Maryland 20850 · 240-777-7170 · 240-777-7178 FAX
www.montgomerycountymd.gov

Located one block west of the Rockville Metro Station

07-PREL-120220130-04.

Standard Plan Review Comments

5. All Planning Board Opinions relating to this plan or any subsequent revision, project plans or site plans should be submitted to the Montgomery County Department of Permitting Services in the package for record plats, storm drain, grading or paving plans, or application for access permit. This letter and all other correspondence from this department should be included in the package.
6. The sight distance study has been accepted. A copy of the Sight Distance Evaluation certifications form is included with this letter.
7. The storm drain analysis was reviewed and is acceptable to MCDOT. No improvements are needed to the downstream County storm drain system for this plan.
8. Design all vehicular access points to be at-grade with sidewalk, dropping down to street level between the sidewalk and roadway.
9. Provide on-site handicap access facilities, parking spaces, ramps, etc. in accordance with the Americans with Disabilities Act.
10. Relocation of utilities along existing roads to accommodate the required roadway improvements shall be the responsibility of the applicant.
11. In all underground utility installations, install identification tape or other "toning" device approximately two feet above the utility.
12. If the proposed development will alter any existing streetlights, replacement of signing, and/or pavement markings, please contact Mr. Dan Sanayi of our Traffic Engineering Design and Operations Section at (240) 777-2190 for proper executing procedures. All costs associated with such relocations shall be the responsibility of the applicant.
13. Trees in the County rights of way – spacing and species to be in accordance with the applicable MCDOT standards. Tree planning within the public right of way must be coordinated with DPS Right-of-Way Plan Review Section.
14. Posting of a ROW permit bond is a prerequisite to MCDPS approval of the record plat. The permit will include, but not necessarily be limited to, the following improvements:
 - a. Paving, curb, gutter, sidewalk, handicap ramps and street trees along Manor Road.
 - b. Permanent monuments and property line markers, as required by Section 50-4.3(G) of the Subdivision Regulations.

- c. Erosion and sediment control measures as required by Montgomery County Code 19-10(02) and on-site stormwater management where applicable shall be provided by the Developer (at no cost to the County) at such locations deemed necessary by the Department of Permitting Services (DPS) and will comply with their specifications. Erosion and sediment control measures are to be built prior to construction of streets, houses and/or site grading and are to remain in operation (including maintenance) as long as deemed necessary by MCDPS.

Thank you for the opportunity to review this preliminary plan. If you have any questions or comments regarding this letter, please contact me at william.whelan@montgomerycountymd.gov or (240) 777-2173.

Sincerely,

William Whelan

William Whelan
Development Review Team
Office of Transportation Policy

[SharePoint/transportation/directors office/development review/WhelanW/120220130 Preston Place & Lake Apartments-MCDOT letter 012023.docx](#)

Enclosures (1)

Sight Distance Certifications

cc: Correspondence folder FY 2023

cc-e:	Joshua Sloan	VIKA
	Katie Mencarini	MNCP&PC
	Stephanie Dickel	MNCP&PC
	Grace Bogdan	MNCP&PC
	Mark Terry	MCDOT DTEO
	Oscar Yen	MCDOT DTEO
	Sam Farhadi	MCDPS RWPR

820220180 Preston Place & Lake Apts

Contact: Sam Farhadi at 240 777-6333

We have reviewed site plan files:

“07-SITE-820220180-003.pdf V4” uploaded on/ dated **“12/23/2022”** and
“07-SITE-820220180-004.pdf V4” uploaded on/ dated **“12/23/2022”** and

The followings need to be addressed prior to the certification of site plan:

1. Access points on public roads:
 - a. Provide truck turning movement for all (especially right turn) movements and ensure of minimum curb radii that will accommodate the site traffic.
 - b. Provide easement for the Chevy Chase Lake Driveway apron that crosses the frontage.
2. Provide public sidewalk:
 - a. to ADA standards and label it accordingly.
 - b. ensure of/ show ADA compliant connection of proposed sidewalks/ ramps at intersections and at the site peripheries to the existing ones.
3. On landscaping plan, provide street trees per approved tree species list at the required spacing and clearances.

And the following needs to be a condition of the certified site plan:

1. Private streets to be built to the corresponding public road classification standards per County Code 50.4.3.E.4.c.



DEPARTMENT OF HOUSING AND COMMUNITY AFFAIRS

Marc Elrich
County Executive

Aseem K. Nigam
Director

January 9, 2023

Mr. Adam Bossi
Montgomery County Planning Department
2425 Reedie Drive
Wheaton, Maryland 20902

Re: Preston and Lake Apartments
Site Plan No. 820220180

Dear Mr. Bossi:

The Montgomery County Department of Housing and Community Affairs (DHCA) has reviewed the above referenced plans and recommends Approval.

Sincerely,

Julia Chen

Julia Chen, Planning Specialist
Affordable Housing Programs Section

cc:

Division of Housing

Affordable Housing

Common Ownership Communities

Landlord-Tenant Affairs

Multifamily Housing

1401 Rockville Pike, 4th Floor • Rockville, Maryland 20852 • 240-777-0311 • 240-777-3691 FAX • www.montgomerycountymd.gov/dhca

July 26, 2022

Re: Preston Place and Lake Apartments on Manor Road, Chevy Chase MD
Preliminary Plan Number 120220130
Site Plan Number 820220180

Dear Mr. Bossi
Emailed to : Adam.Bossi@montgomeryplanning.org

We are writing to register our concerns with the Toll Brothers proposed development of Preston Place and Lake Apartment on Manor Road in Chevy Chase Maryland (the "Project"). The following are our concerns:

1. Traffic and entrance and exits onto Manor Road. We live in the development, Chevy Chase Park ("CCP"), that is directly across from the Project and very close to the Chevy Chase Lake development at the corner of Manor Road and Connecticut Ave that Montgomery County recently approved ("CC Lake").

CC Lake is a large development and dense with both residential and commercial uses. A large majority of its traffic will enter and exit on Manor Road, very close to the corner of Connecticut Ave. CC Lake has both an entrance and exit on Manor Road as well as a parking garage entrance and exit.

CCP's entrance and exit is in the first $\frac{1}{4}$ of Manor Road and it is very close to CC Lake's entrance/exit onto Manor.

To compound this problem, we understand that Toll Brothers plans to put the Project's proposed entrance and exit where the current Lake Apartments entrance/exit is. This happens to be directly across from CCP and ALSO WITHIN THE FIRST $\frac{1}{4}$ of MANOR ROAD.

Given the density of the CC Lake development with both commercial and residential use and the very close locations of CC Lake's and CCP's entrances and exits, traffic and congestion will reign supreme. All left turns out of any of the 3 developments will be very difficult to make given the high volume of traffic. Right turns may also be difficult given that cars will likely be backed fairly far on Manor waiting for the light at Connecticut Ave. Also note that Manor Road serves as the main exit for much of the larger north chevy chase neighborhood that wants to access Connecticut Ave going south, so the impact is not contained to the developments on Manor Road.

We respectfully ask that the Project's main entrance/exit be placed further down on Manor Road (away from Connecticut Ave) in the remaining $\frac{3}{4}$ of Manor Road. This will alleviate traffic from 3 developments entering and exiting at almost the same spot that is so close to Connecticut Ave (let alone the traffic from the rest of the north chevy chase

area that uses Manor Rd as the main way to access Connecticut Ave going south). Please also consider putting 4 way stops or better yet, a roundabout, to address all of the traffic that will soon be flowing onto Manor Road.

2. Park/Green space. We understand from a Toll Brothers presentation that they will be putting a green space/park/playground in the back of their development. While we are pleased that they are incorporating park/ green space into their design, ***we strongly urge you to require the green space to be moved to the front of the development, fronting Manor Road.*** This is why:

1. The plans for CC Lake that were approved by Montgomery County have only large buildings (not green space) facing Manor Road.
2. As 20 + year residents of the area along Manor Road, we have enjoyed the park-like setting of the current apartments and townhomes that will be demolished in order to build the Project. While we are not contesting the type or size of the townhomes being planned for the Project, they will now take up most of the land.
3. ThriveMontgomery puts an emphasis on green space and green space that the neighborhood can enjoy. If the green space is at the back, then only the residents of the Project will be able to enjoy its use. More importantly, only the residents of the Project will enjoy the visual calm and beauty that green space provides if it remains in the back. Why wouldn't this be shared with the entire neighborhood ? Why wouldn't you stay true to the principles of ThriveMontgomery? The existing north chevy chase neighborhood has had to put up with a lot of from all the development associated with the purple line.
Please let the greenspace shine front and center on Manor Road for all to enjoy.

3. Construction Parking. Construction parking related to the building of CC Lake has been a nightmare and the neighborhood has borne the brunt of it. Despite repeated calls, police rarely ticket on Manor Road even when workers are parked in no parking areas, visibly obstructing the street and the ability to pull out onto Manor in safety. In the Toll Brothers presentation, they promised that their workers would be able to park on the property being constructed, so this would not be a problem in the future. ***Please make as a condition of approval that the construction workers all park on the property being developed.***

Sincerely,

Vanessa Chernick

Olafur Gudmundsson

3821 Village Park Dr./ Chevy Chase MD

Cc: Partap Verma Partap.Verma@mncppc-mc.org

From: [Bogdan, Grace](#)
To: [Al Carr](#)
Cc: [Dickel, Stephanie](#); [Hisel-McCoy, Elza](#); [Kronenberg, Robert](#); [Bossi, Adam](#)
Subject: RE: Question about Preston Place & Lake Apartments
Date: Wednesday, February 8, 2023 3:36:31 PM
Attachments: [image001.png](#)
[image002.png](#)
[image003.png](#)
[image004.png](#)
[image005.png](#)

Good Afternoon Mr. Carr-

Thank you for your inquiry. The Preliminary and Site Plan applications are currently scheduled for February 23rd Planning Board hearing. During the review of the applications we determined that undergrounding the utilities along the Manor Road frontage would necessitate removal of all of the mature street trees in that area. To minimize the loss of those trees that contribute so much to the community character, staff is not recommending undergrounding.

If you have any further questions please do not hesitate to reach out to me or Adam Bossi (cc'd), who is co-leading the Project review.

Thanks,

Grace



Grace Bogdan, AICP (she/her/hers)
Planner III, DownCounty Planning Division

Montgomery County Planning Department
2425 Reedie Drive, 13th Floor, Wheaton, MD 20902
grace.bogdan@montgomeryplanning.org
o: 301.495.4533



From: Al Carr <alfred.carr@gmail.com>
Sent: Wednesday, February 8, 2023 9:04 AM
To: Bogdan, Grace <grace.bogdan@montgomeryplanning.org>
Cc: Dickel, Stephanie <Stephanie.Dickel@montgomeryplanning.org>; Hisel-McCoy, Elza <elza.hisel-mccoy@montgomeryplanning.org>; Kronenberg, Robert <robert.kronenberg@montgomeryplanning.org>; Leftwich, Troy <Troy.Leftwich@montgomeryplanning.org>
Subject: Re: Question about Preston Place & Lake Apartments

[EXTERNAL EMAIL] Exercise caution when opening attachments, clicking links, or responding.

Hi Grace,

Can you provide any updates on this application?

Will the applicant be going before the planning board before their extension expires?

Did the applicant agree to bury the wires?

Thanks,

Al

On Fri, May 27, 2022, 11:33 AM Bogdan, Grace <grace.bogdan@montgomeryplanning.org> wrote:

Good Morning Delegate Carr-

I apologize for my delayed response, as I was out of the office. This project has just been assigned to me and is at the very beginning of the review process and we will be reviewing it closely in the next couple of weeks for conformance with the Zoning Ordinance and Sector Plan. We will be asking the Applicant to explore undergrounding utilities and expand the tree canopy as noted in the Sector Plan.

The application materials are available for viewing online and linked below

Preliminary Plan 120220130: <https://eplans.montgomeryplanning.org/daiclinks/pdoxlinks.aspx?apno=120220130&projname=Preston%20Place%20&%20Lake%20Apts>

Site Plan 820220180: <https://eplans.montgomeryplanning.org/daiclinks/pdoxlinks.aspx?apno=820220180&projname=Preston%20Place%20&%20Lake%20Apts>

If you have additional questions please feel free to reach out.

Grace



Grace Bogdan, AICP

Planner III, DownCounty Planning Division

Montgomery County Planning Department
2425 Reedy Drive, 13th Floor, Wheaton, MD 20902
grace.bogdan@montgomeryplanning.org
o: 301.495.4533



2D690ACD



From: Al Carr <alfred.carr@gmail.com>
Sent: Wednesday, May 25, 2022 9:06 AM
To: Bogdan, Grace <grace.bogdan@montgomeryplanning.org>
Cc: Delegate Al Carr <alfred.carr@house.state.md.us>
Subject: Question about Preston Place & Lake Apartments

[EXTERNAL EMAIL] Exercise caution when opening attachments, clicking links, or responding.

Good morning Grace,

I see that Preston Place and Lake Apartments is on the DRC agenda for June 7, 2022.
(Site Plan Number: 820220180 & Preliminary Plan Number: 120220130)

The Chevy Chase Lake Sector Plan has language about protecting/expanding the tree canopy and about placing utilities underground.

Have there been conversations between staff and the developer about placing the utilities on Manor Road underground in order to maximize the potential tree canopy there?

Thanks,

Al

Del. Al Carr
Maryland's 18th Legislative District
301 641-8460

2/20/2023

Dear Chairman Zyontz,

When you consider the Preston Place Preliminary Plan 120220130 and Site Plan 820220180 on February 23rd, 2023, please add conditions requiring that environmental and bike/ped provisions of the Chevy Chase Lake Sector Plan be followed.

The applicant must:

- *Bury the overhead wires along Manor Rd
- *Plant tall overstory street trees on Manor Rd

The tree canopy near Chevy Chase Lake has been devastated in recent years by road widening, development, Pepco and clear cutting for the purple line. It is vitally important that the Sector Plan's environmental and bicycle and pedestrian recommendations be followed so that the tree canopy can be restored and to protect our health, safety and water quality.
Thank you for considering my views.

Sincerely,

Carlos Torres
3569 Hamlet Pl
Chevy Chase, MD 20815

2/20/2023

Dear Chairman Zyontz -

As a neighbor who lives across the Purple Line from Preston Place, I am shocked at the amount of variances being proposed concerning the mature trees on the site. How can the Dept of Environmental Protection call themselves environmental protectors when they agree to the felling of 34 large-caliber mature trees? I am not sure if that number includes the areas to the north of Preston Place where they intend to put in stones instead of trees to fortify the Coquelin Run tributary, another travesty.

The tree canopy near Chevy Chase Lake has been devastated in recent years by road widening, development, Pepco and primarily by clear cutting for the Purple Line. We need to keep all mature trees. Replacing them with saplings and fees does nothing for our tree canopy.

If Preston Place is required to pay MoCo, the money would be put to much better use in modifying the site plan to accommodate our trees. No amount of money can bring a mature tree canopy back.

When you consider the Preston Place Preliminary Plan 120220130 and Site Plan 820220180 on February 23rd, 2023, please add conditions requiring that mature trees not be felled.

- *Modify the plans around the mature trees

- *Plant tall overstory street trees on Manor Rd

Thank you for considering my views.

Sincerely,

Aletta Schaap

3515 Hamlet Place

Chevy Chase MD 20815

2/22/2023 7:30 PM

Received Email

Email from: Allie Arias

Re: [Chevy Chase Hills] Re: Note from Pat Leibowitz [EXTERNAL EMAIL] Exercise caution when opening attachments, clicking links, or responding.

What a shame to hear more trees are going down. Since there are some new families in the neighborhood I wanted to resend this link for Montgomery County's Free Shade Tree program! You can literally type in your address and get on their list to get a free shade tree.

I went through the process and they came and planted it today. Got to pick the type of tree I wanted out of about 20 or so that they offered. Super great program! At least we can try and make a difference on our own private properties.

<https://www.montgomerycountymd.gov/green/trees/tree-montgomery.html>

Allie

2/21/2023 5:51 PM

Received Email

Email from: Lewis Leibowitz

Trees are important. Keep them.

Lewis Leibowitz

lewis.leibowitz@gmail.com

From: Pat Leibowitz <pleibowitz@yahoo.com>

Sent: Tuesday, February 21, 2023 12:38:05 PM

To: mcp-chair@mncppc-mc.org <mcp-chair@mncppc-mc.org>

Cc: cchills@googlegroups.com <cchills@googlegroups.com>

Subject: Note from Pat Leibowitz

I really think that the planning board should come and look at the development near Conn. Ave and Manor Road. Our open spaces have been decimated and now the project on Manor Road will be taking away affordable housing as well as cutting mature trees. I really feel that our local government has turned a blind eye to what is going on here in our neighborhood. Are there no environmental rules in place for Maryland and our Chevy Chase Lake area?! I would like to discuss this with someone on the council. Thank you, Pat Leibowitz

Created on: 2/21/2023 6:24 PM

Received Email

Email from: Suzie Moss

Dear Chairman Zyontz,

The environmental and bike/pedestrian provisions must be followed when planning Preston Place 120220130 and Site Plan 820220180.

*Bury the overhead wires along Manor Rd

*Plant tall overstory street trees on Manor Rd

The tree canopy near Chevy Chase Lake has been devastated in recent years by road widening, development, Pepco and clear cutting for the purple line. It is vitally important that the Sector Plan's environmental and bicycle and pedestrian recommendations be followed so that the tree canopy can be restored and to protect our health, safety and water quality.

Thank you for considering my views.

Sincerely,

Susan Moss

3517 Hamlet Pl

Chevy Chase

Created on: 2/21/2023 8:49 PM
Received Email
Email from: Alexandra Kennedy

Dear Chairman Zyontz,

I'm a Chevy Chase resident in the Manor Road neighborhood writing to you with concerns about the future of Preston Place. While I'm very worried about the Toll Brothers and Chevy Chase Land Company's redevelopment plans in terms of affordability (planning to knock down 133 dwellings for luxury housing with only 15% affordable units), I am writing to you today chiefly about sustainability. The developers' plans to cut down more of our local tree canopy is alarming.

At tomorrow's hearing, when you consider the Preston Place Preliminary Plan 120220130 and Site, I urge you to add conditions requiring that environmental and cyclist and pedestrian provisions of the Chevy Chase Lake Sector Plan be followed:

The applicant must:

*Bury the overhead wires along Manor Rd

*Plant tall overstory street trees on Manor Rd

The tree canopy near Chevy Chase Lake has been devastated in recent years by road widening, development, Pepco, and clear cutting for the Purple Line. After losing our beloved portion of the Capital Crescent Trail, it's hard to countenance more tree canopy loss. Signs of the climate crisis are hard to ignore even here in our backyard. It's only late February, but already daffodils and cherry trees are in bloom, crocuses are shooting up, and we have a forecasted temperature pushing 80°F this coming Thursday. Trees remain an excellent way to naturally capture carbon, preserve animal habitats, purify air, prevent soil erosion, and provide temperature control (cooling and wind blocking). I am begging you to consider these environmental imperatives. I understand that the area of our neighborhood is changing and growing, but there surely must be ways that we can achieve growth while protecting crucial elements of our natural environment.

In closing, it is vitally important that the Sector Plan's environmental and bicycle and pedestrian recommendations be followed so that the tree canopy can be restored and to protect our health, safety, and water quality.

Thank you for your time and consideration of my views.

Yours sincerely,

Alexandra Kennedy
4002 Laird Place, Chevy Chase, MD 20815

Created on: 2/21/2023 8:10 PM
Received Email
Email from: Tom Kennedy

Dear Chairman Zyontz:

When you consider the Preston Place Preliminary Plan 120220130 and Site Plan 820220180 at your afternoon hearing on Feb. 23, (item 7), I would respectfully ask that you add conditions requiring that the environmental and bike/pedestrian provisions of the Chevy Chase Lake Sector be followed completely,

In particular the applicant must: 1) Bury the overhead wires along Manor Road, and 2) Plant tall overstory street trees along Manor Road.

The tree canopy near Chevy Chase Lake has been decimated by road construction (widening) in the area, as well as by development (the Bozzuto project), Pepco incursions, and serious clear-cutting to make room for the Purple Line. It has been very frustrating to watch as protections disappear thanks to developments that seem intent on systematically overturning the Sector Plan's recommendations for environmental, bicycle and pedestrian accommodations that were so much a valued part of our previous pre-development existence.

My wife and I have lived in our Chevy Chase Hills neighborhood since 1987 and raised our daughter here at this locathetion. As a family, we depended on the Capital Crescent Trail for easy access to Bethesda as well as all manner of recreational activity. While promises have been made regarding the Purple Line and restoration of pedestrian/bicycle access that would mimic what previously existed, I have little confidence that will occur.

I think it is imperative that you ensure that the Sector Plan be followed with respect to environmental, pedestrian, and bicycle access recommendations and that our tree canopy be fully restored in the neighborhood, particularly along Manor Road. I see these steps as vital to preserving quality of life in our neighborhood as well as supporting health, public safety, and water quality in the watershed.

Thank you in advance for your consideration and attention.

Sincerely,
Thomas R. Kennedy

Thomas R. Kennedy
4002 Laird Place
Chevy Chase, MD 20815
240.601.8041 Cell

Created on: 2/21/2023 8:52 PM

Received Email

Email from: Tom Mitchell

Dear Chairman Zyontz:

I have resided at 8507 Longfellow Place, Chevy Chase, Maryland for about 15 years.

You are scheduled to consider the Preston Place Preliminary Plan No. 120220130, and Site Plan No. 820220180 on February 23, 2023.

I have learned from numerous reports, including Maryland State Delegates, that the developer wants permission to further degrade aspects of my neighborhood by requesting an exception to the Plans that would allow them clear cut trees on Manor Road in order to avoid the necessity of burying power lines, as required in the original site plan for further development. I urge you in the strongest terms to add conditions requiring that the environmental and bike/pedestrian provisions of the Chevy Chase Lake Sector Plan be followed.

More specifically, you must ensure that the developer/builder (applicant): (1) bury all overhead wires along Manor Rd., and (2) plant tall overstory street trees on Manor Rd. You should allow any deviation from these requirements.

Over the past several years, the old trees in the Chevy Chase Lake area have been cut down by road widening, development, Pepco, and clear cutting for the Purple Line. It is vitally important that the Sector Plan's environmental and bicycle and pedestrian recommendations be followed so that the tree canopy can be restored and to protect our health, safety and water quality.

There is no good reason to allow the wealthy developer and builder to cutdown any more established trees and to avoid having to bury power and utility lines. you should require them to adhere to the plans that they do so.

Thank you for considering my views. I look forward to seeing you at the public hearing.

Sincerely,

Thomas W. Mitchell

8507 Longfellow Place

Chevy Chase, MD 20815

THOMAS W. MITCHELL

MITCHELL FIRM, PLLC

7625 Wisconsin Avenue

Third Floor

Bethesda, Maryland 20814

240-482-3597 (direct)

240-482-3599 (fax)

tmitchell@themitchellfirmplc.com

Created on: 2/21/2023 9:11 PM

Received Email

Email from: Darko Filipi

Dear Chairman Zyontz,

I am writing to request your attention to the upcoming consideration of the Preston Place Preliminary Plan 120220130 and Site Plan 820220180 on February 23rd, 2023. I would like to request that you add conditions to the approval of these plans, which require that the environmental and bike/ped provisions of the Chevy Chase Lake Sector Plan be followed.

Specifically, I ask that the applicant is required to bury the overhead wires along Manor Rd and plant tall overstory street trees on Manor Rd. The tree canopy in the Chevy Chase Lake area has suffered greatly in recent years due to road widening, development, Pepco, and clear cutting for the Purple Line. It is crucial that the Sector Plan's environmental and bicycle/pedestrian recommendations be followed to restore the tree canopy and protect the health, safety, and water quality of the community.

Thank you for considering my views on this matter.

Sincerely,

Darko Filipi
3905 Larid Place
Chevy Chase, MD 20815



February 23, 2023

The Montgomery County Group of the Maryland Sierra Club Chapter asks that the Planning Board reject Preliminary Plan no. 120220130 and Site Plan no. 820220180 (Preston Place) because it fails to conform to the Chevy Chase Lake Sector Plan.

The Sierra Club strongly supports smart growth, transit oriented, pedestrian-friendly development and environmental protection. The Chevy Chase Lake Sector Plan approved by the County Council in 2013 has clear language that requires that the tree canopy be restored and protected and that overhead utilities be relocated underground when redevelopment occurs¹. The staff report is misinformed in stating that burying the utilities along Manor Road would harm existing mature trees. The single-phase Pepco feeder line along this stretch could be buried underneath the roadway pavement, thus avoiding harm to the few mature street trees that will remain. This is the best plan for the long-term health of the tree canopy and will allow for the planting of large overstory trees along Manor Rd. Large trees will provide the most environmental benefits to shade and cool the area and to soak up stormwater.

The project includes a modest increase in density (net increase of 14 dwelling units). The Sector Plan did not contemplate a change in use, zoning or density for these parcels. The project involves the gentrification of 1, 2 and 3 bedroom naturally occurring affordable rental apartments into 3 bedroom luxury townhomes with 15% smaller units as MPDU. As such there is no overriding smart growth value that would justify waiving important environmental provisions of the Sector Plan.

As evidence that electric lines can be buried under roadway pavement without harming trees, look no further than the nearby Pepco's Sligo to Linden undergrounding project which buried much higher capacity lines under narrower roads. This project (DPS permits no. 365021 and 365022) began in 2019 and included the burial of three regional 69,000 volt subtransmission lines between Pepco substations. The lines were buried under County roads such as Seminary Rd, Linden Lane, Fraser Ave, Hale Pl, Birch Dr, and Montgomery St.

Waiving the requirement to protect the tree canopy by burying Pepco's wires along Manor Rd would create an unfortunate precedent that future applicants are certain to cite when properties west of Connecticut redevelop.

We also wish to express disappointment that the applicant proposes to serve this development with natural gas infrastructure, presumably for gas hot water, gas cooking and gas space heating. This represents a missed opportunity to electrify and use efficient heat pumps for heating and hot water as a more environmentally friendly option to minimize climate impact. The current Preston Place rental townhomes use electric stoves, so gas cooking would be a step backward from the status quo.

Please reject this proposal and instruct the applicant and staff to come back with amended plans that include burial of the Pepco feeder line that conflicts with the longterm health of tree canopy.

Sincerely,

Darian Unger, Chair
Sierra Club Montgomery County Group
dwunger@howard.edu

Al Carr, Executive Committee Member
Sierra Club Montgomery County Group
alfred.carr@gmail.com

cc: Montgomery County Council

¹[Approved and adopted \(2013\) Chevy Chase Lake Sector Plan](#)

Summary of Recommendations

Environment (p22)

Preserve and restore environmental features while minimizing the impact of future development.

- Restore the Coquelin Run stream valley by promoting stream and forest restoration and creating a conservation easement.
- Expand the existing tree canopy.
- Promote sustainable site and building design to mitigate negative environmental impacts.
- Accommodate future transit options, including the Purple Line.

Access

Pedestrian and Bicycle (p41)

Today, Connecticut Avenue divides the Center as two separate places. The supermarket and shops of the Chevy Chase Lake shopping center on the east side of the avenue are isolated from the coffee shop, eateries, and other businesses on the west side. While it is possible to cross Connecticut Avenue on foot, few people do.

For the Center to be successful, pedestrians must have a distinct and welcoming experience. Especially for those blocks between Chevy Chase Lake Drive and Manor Road, Connecticut Avenue must be transformed from what is primarily a through-way, dedicated to moving vehicles through Chevy Chase Lake, to a pedestrian-oriented avenue that invites people to stay, making the Center a whole place and more inviting to pedestrians and cyclists. The Design Guidelines provide further details on this topic.

The Plan recommends knitting the two sides of the Center together, across Connecticut Avenue, by:

- Enhancing existing street crossings with sidewalk, crosswalk, and median improvements.
- Providing a new signalized crosswalk at the Connecticut Avenue/Laird Place intersection.
- Allowing on-street parallel parking during off-peak hours along Connecticut Avenue, between Manor Road and Chevy Chase Lake Drive.
- Installing a distinctive and human-scaled streetscape, including sidewalk paving, streetlights, street trees, integrated stormwater management, and placing all utilities underground.

Environment (p49)

This section includes recommendations to maintain and improve the quality of the natural environment in Chevy Chase Lake.

The Sector Plan recommends that any further development achieve the following objectives:

- Protect the Coquelin Run stream valley network and its associated forested slopes (not including the area of the Coquelin Parkway) through a Category I Conservation Easement.
- Reduce direct stormwater discharge into Coquelin Run.
- Support stream and forest restoration projects to stabilize banks, remove invasive species, restore native forest structure, and prevent further stream erosion.

- Enhance Coquelin Parkway as a public right-of-way that accommodates only bicycle and pedestrian use.

Additionally, the expansive tree cover in Chevy Chase Lake—50 percent of the Plan Area—is an important part of the community's character, as well as its ecological health. One of the goals of the Plan is to maximize tree cover for new development with overall goals of 25 to 30 percent tree canopy cover in the Chevy Chase Lake Center area. New development should also retain or improve canopy cover throughout the Plan Area, within street medians, along new and existing streets, and in surface parking areas.

The likely removal of many existing, mature trees along the Capital Crescent Trail during the construction of the Purple Line reinforces the need to plant new ones. To expand the tree canopy in Chevy Chase Lake, they must be planted in association with new development or redevelopment projects.

Email message from MCDOT Director regarding burial of electric lines under roadways

From: Conklin, Christopher <Christopher.Conklin@montgomerycountymd.gov>
Date: Wed, Feb 15, 2023 at 11:27 AM
Subject: RE: Question about Pepco's sligo to linden undergrounding project
To: Al Carr <alfred.carr@gmail.com>
Cc: Sabbakhan, Rabbiah <Rabbiah.Sabbakhan@montgomerycountymd.gov>

The preference is for the utilities to be located in a utility easement adjacent to the County ROW. The main reason for this is to avoid the disruption and expense associated with utility repairs under the road itself. In many locations, it is not feasible to locate the utility infrastructure outside the ROW. When this happens, sidewalk is preferred so that they can be accessed more easily. If sidewalk isn't feasible, then in the road is possible.

Christopher Conklin, Director
Montgomery County Department of Transportation

From: Al Carr <alfred.carr@gmail.com>
Sent: Wednesday, February 15, 2023 11:24 AM
To: Conklin, Christopher <Christopher.Conklin@montgomerycountymd.gov>
Cc: Sabbakhan, Rabbiah <Rabbiah.Sabbakhan@montgomerycountymd.gov>
Subject: Re: Question about Pepco's sligo to linden undergrounding project

[EXTERNAL EMAIL]

Thanks.

Does MCDOT have any issue with electric lines being buried under roadways?

The Complete Streets Design Guide references a 2016 document which states that MCDOT prefers that "dry utilities" be buried under the sidewalk.

But Pepco buried the sligo to linden lines under the roadway pavement, so did they have to get a waiver?

One of the benefits of burying electric lines under the roadway pavement seems to be that it reduces conflicts with tree roots. I believe this is also the practice in DC for Pepco's large scale project to bury lines there.

Al

On Wed, Feb 15, 2023 at 11:09 AM Conklin, Christopher
<Christopher.Conklin@montgomerycountymd.gov> wrote:

Al,

There would not have been Mandatory Referral for utility reconstruction. The permits would have been managed by DPS for County right of way and MDOT/SHA for work on State right of way. I've copied the new DPS Director Rabbiah Sabbakhan in case he can provide more information about the permitting of this type of utility reconstruction in County right of way.

Christopher Conklin, Director
Montgomery County Department of Transportation
240-777-7198

From: Al Carr <alfred.carr@gmail.com>
Sent: Wednesday, February 15, 2023 11:02 AM
To: Conklin, Christopher <Christopher.Conklin@montgomerycountymd.gov>
Subject: Question about Pepco's sligo to linden undergrounding project

[EXTERNAL EMAIL]

Good day Chris,

Between 2019 and 2022 ish, Pepco completed a project to bury three 69kv subtransmission lines between substations in downtown Silver Spring and Montgomery Hills. They buried the wires under Georgia Ave, Seminary Rd, Hale, Birch, Linden, Fraser, Montgomery, Brookville Rd, etc.

The wires were generally buried under the roadway pavement.

Can you help me find out what type of permits or approvals were needed for Pepco to bury their wires under roads owned by MCDOT?

Did DPS have to approve or just MCDOT?

I don't see any mention of this project on MNCPPC's website so I assume there was no mandatory referral.

Thanks,

Al

2/15/2023

Dear Planning Board Chair,

I would like to provide my view on this item in writing because I am unable to be present at the hearing. I have lived on Village Park Drive for 22 years. This neighborhood was the MI Homes development of 49 houses as well as a group of townhouses on Manor Rd. These developments are adjacent to the proposed development at Preston Place. One of the most appealing aspects of the MI Development was that they preserved large number of trees and buried the utility wires underground. I argue that these same conditions be placed on the Toll Brothers development at Preston Place. With the density of development at the corner of Connecticut Ave and Manor Rd the overhead wires give a closed in feeling of crisscrossed wires everywhere overhead. The same feeling will be created at Preston Place if the wires are not buried. Given the great benefit given to Toll Brothers of building at this site it is only reasonable to demand they bury the wires. If they do not the improvements to the community will be measurably compromised for decades to come. I am happy to answer any questions you may have.

Henry Levin

Henry_levin@nih.gov

3808 Village Park Dr
Chevy Chase MD 20815

2/22/23

Dear Chairman Zyontz,

When you consider the Preston Place Preliminary Plan 120220130 and Site Plan 820220180 on February 23rd, 2023, please add conditions requiring that environmental and bike/ped provisions of the Chevy Chase Lake Sector Plan be followed.

The applicant must:

- *Bury the overhead wires along Manor Rd

- *Plant tall overstory street trees on Manor Rd

The tree canopy near Chevy Chase Lake has been devastated in recent years by road widening, development, Pepco and clear cutting for the purple line. It is vitally important that the Sector Plan's environmental and bicycle and pedestrian recommendations be followed so that the tree canopy can be restored and to protect our health, safety and water quality.

Sincerely,

Lynda Maudlin

8510 Lynwood Pl

Chevy Chase 20815

Dear Chairman Zyontz,

I am a 3rd generation Chevy Chase resident, a former resident of Preston Place and current resident as well as Past President of Board at Hamlet Place. The construction at Chevy Chase Lake and the Purple Line have put an extreme strain on our neighborhood. Regarding trees, we are already experiencing erosion issues that are causing trees to fall and flooding into our community. Preston Place backs up to Hamlet Place on the other side of the Purple Line track.

While most of the residents in the area are opposed to this development, when you consider the Preston Place Preliminary Plan 120220130 and Site Plan 820220180 on February 23rd, 2023, please add conditions requiring that environmental and bike/ped provisions of the Chevy Chase Lake Sector Plan be followed.

The applicant must:

- *Bury the overhead wires along Manor Rd
- *Plant tall overstory street trees on Manor Rd

The tree canopy near Chevy Chase Lake has been devastated in recent years by road widening, development, Pepco and clear cutting for the purple line. It is vitally important that the Sector Plan's environmental and bicycle and pedestrian recommendations be followed so that the tree canopy can be restored and to protect our health, safety and water quality.

Thank you for considering my views.

Sincerely,
Wendy Soroka

February 15, 2023

Written testimony for Item 7: Preston Place: Preliminary Plan No. 120220130 and Site Plan No. 820220180 (2/23 Public Hearing)¹



natureforward.org

Dear Montgomery County Planning Board,

Nature Forward (formerly Audubon Naturalist Society) is the oldest independent environmental organization in the DC metro region. Our mission is to inspire residents of the greater Washington, DC, region to appreciate, understand, and protect their natural environment through outdoor experiences, education, and advocacy. We thank the Planning Board for the opportunity to provide testimony on Item 7: Preston Place: Preliminary Plan No. 120220130 and Site Plan No. 820220180 head of the 2/23 Public Hearing.

It has come to our attention that the Toll Brothers/Chevy Chase Land Co are asking for a waiver to place wires above ground. The approved and adopted 2013 Chevy Chase Lake Sector Plan states that all utilities must be placed underground.² The exact same "high-voltage transmission line" was recently buried immediately to the south where the purple line bridge will cross Connecticut Ave. Identical lines are routinely being buried in the county including in Silver Spring (Linden to Sligo project) and in White Flint (new substation). The fact that these lines are not routinely buried does not negate the Sector Plan's requirement to do so. Nature Forward is requesting that you revisit your decision to grant this waiver and require the developer to comply to the letter of the law.

The recently released Hughes Center Report on Maryland's Forests found that "Montgomery and Prince George's counties accounted for more than 44% of the state's total tree canopy loss."³ Large, mature shade trees will contribute positively to the County's ability to meet its own climate change greenhouse reduction goals of 80% by 2027 and 100% by 2035. A healthy tree canopy is vital to protecting water quality in Rock Creek and its tributaries, and to providing a walkable, livable streetscape to this transit-oriented area as it grows and attracts more pedestrians.

On behalf of Nature Forward and our 28,000 members and supporters, we recommend that the Planning board does not approve waiving the Sector Plan requirement to bury the wires along Manor Rd.

Sincerely,

Denisse Guitarra, MD Conservation Advocate at Nature Forward

¹ Item 7. Available at: <https://montgomeryplanningboard.org/agenda-item/february-23-2023/>

² Chevy Chase Lake Sector Plan (2013) Page 41.

<https://www.montgomeryplanning.org/community/chevychaselake/documents/chevychaseApprovedandAdopted52015.pdf>

³ Maryland Forest Technical Study. Nov 2022. <https://www.chesapeakeconservancy.org/mdforeststudy2022>