

Appendix F

2019 Facility Plan *Recommendation for
Capital Crescent Trail Crossing at Little Falls
Parkway Safety Improvements Project*



MONTGOMERY COUNTY DEPARTMENT OF PARKS
THE MARYLAND-NATIONAL CAPITAL PARK AND PLANNING COMMISSION

MCPB
Item: TBD
Date: 06/13/19

May 23, 2019

MEMORANDUM

TO: Montgomery County Planning Board

VIA: Michael F. Riley, Director of Parks *MR*
Mitra Pedoeem, Deputy Director of Parks
Andy Frank, Acting Chief, Park Development Division *AP*
Patricia McManus, Design Section Supervisor, Park Development Division

FROM: Kim Paniati, P.E., Acting Section Supervisor, Park Development Division *KP*
Andrew Tsai, P.E., Project Manager, Park Development Division *AT*

SUBJECT: Facility Plan Recommendation for Capital Crescent Trail Crossing at Little Falls Parkway Safety Improvements Project

STAFF RECOMMENDATION: APPROVE the Recommended Facility Plan, including the cost estimate and recommendation for future long-term study.

PROJECT DESCRIPTION

Introduction

The purpose of this project is to prepare a facility plan for permanent safety improvements at the Capital Crescent Trail (CCT) crossing of Little Falls Parkway (LFP) in Bethesda. The crossing is located between the intersection of Little Falls Parkway and Arlington Road to the north and Little Falls Parkway and Hillandale Road to the south.

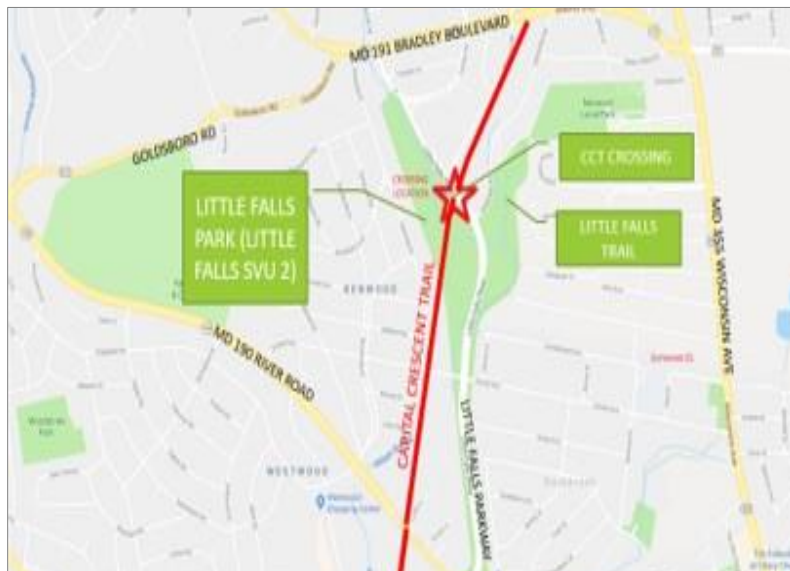


Figure 1: Location Map (Courtesy Google Maps)

The Capital Crescent Trail is an 11-mile hard surface shared use trail extending from Silver Spring to Georgetown in the District of Columbia. Approximately 5.5 miles of the trail are in Montgomery County. The Trail was created in the 1990's from the conversion of the Georgetown Branch of the B&O Railroad, as part of the ongoing nationwide "Rails to Trails" efforts to create off-road trails from abandoned railroad right of ways. The Capital Crescent Trail is the most heavily used trail in Montgomery County (2,000 – 5,000 users/day) and experiences both recreational and commuter traffic.

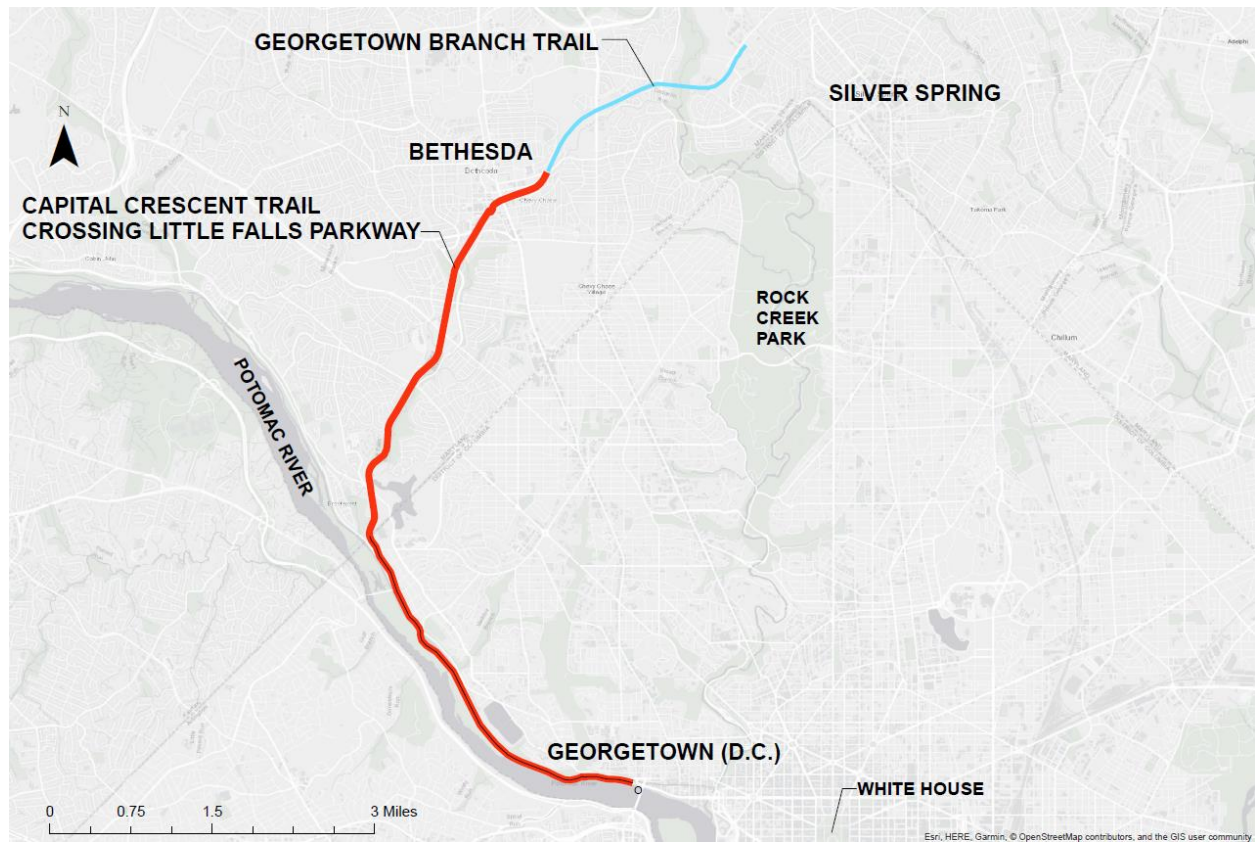


Figure 2: Capital Crescent Trail, Bethesda to Georgetown (D.C.)

The Trail crosses Little Falls Parkway, a four-lane roadway consisting of two lanes in each direction with a grass median, approximately one-third of a mile south of Bradley Boulevard near the Bethesda Pool. Little Falls Parkway was originally constructed in the 1960's to provide access to Little Falls Park and has provided a connection between MD 190 (River Road) and MD 191 (Bradley Boulevard), as well as to the residential communities in the area.

The Capital Crescent Trail crossing at Little Falls Parkway has historically been the site of frequent conflicts between trail users and motorists. The four-lane cross-section on Little Falls Parkway allowed for the multiple-lane threat scenario – a recognized traffic safety problem occurring along road sections that are more than two lanes wide and have an uncontrolled crossing (no traffic signal or stop sign). A multiple-lane threat crash involves a driver stopping in one lane on a multilane road to permit pedestrians/cyclists to cross, and an adjacent oncoming vehicle (in the same direction) striking the pedestrian who is crossing in front of the stopped vehicle. This crash is due to the initial stopped vehicle obscuring the view of the second advancing vehicle who subsequently fails to yield.

Following a bicyclist fatality at the crossing in October 2016, Montgomery Parks in coordination with Park Police and the Montgomery County Department of Transportation implemented a Vision Zero-based interim road diet along Little Falls Parkway between Arlington and Hillandale Roads. Montgomery County is the first suburban county in the United States to commit to [Vision Zero](#): a national initiative to eliminate pedestrian and bicyclist traffic fatalities and conflicts, while increasing safe, healthy, and equitable mobility for all pedestrians.

The interim road diet condition reduced the cross-section of Little Falls Parkway from four lanes to two lanes at the Trail crossing, by closing the outside travel lane in each direction. The travel lanes were closed off with chains and flexible posts, and the speed limit was reduced to 25 mph. The grass median dividing the northbound and southbound travel lanes remained.

In late 2017, Montgomery Parks initiated the Capital Crescent Trail Crossing at Little Falls Parkway Safety Improvements project to develop a facility plan (30% Design) recommendation for permanent safety improvements at the crossing. Sabra & Associates, Inc., a multi-disciplinary engineering firm specializing in transportation engineering was contracted to assist with the project.

Project Funding

Facility planning represents thirty percent complete construction documents, including a proposed design, cost estimate, and determination of regulatory feasibility, including preparation and approval of a stormwater management concept and Natural Resources Inventory/Forest Stand Delineation plan. The facility planning study for the project was funded with \$200,000 from the FY 2018-2019 Capital Improvements Program in the Facility Planning: Non Local Parks project.

Facility Planning Process

The facility planning process included the following sequence of work:

Initiation

1. Collect data and analyze existing site conditions.
2. Meet with the community to gather input.
3. Identify program of requirements and priorities: safety of trail users.

Concept

4. Develop concept alternatives.
5. Present concept alternatives to the community, stakeholders, and neighbors. (June 2018 Community Meeting #1)

Design Development and Preliminary Permits

6. Develop top (3) alternatives based on program priorities and public input.
7. Coordinate alternatives with the community, regulatory agencies, and stakeholders.
8. Present top (3) alternatives to the community, stakeholders, and neighbors. (October 2018 Community Meeting #2)

Facility Plan

9. Develop Recommended Plan based on feedback from the community and regulatory agencies.

10. Prepare stormwater management concept submission and obtain approval from the Department of Permitting Services.
11. Prepare and obtain approval of Natural Resources Inventory/Forest Stand Delineation Summary Map and Forest Conservation Plan Exemption from Montgomery County Planning.
12. Finalize facility plan and prepare 30% construction documents and cost estimate.

Final Documentation and Planning Board Approval

13. Prepare facility plan staff report, cost estimate and operating budget estimates.
14. Present facility plan recommendations and costs to the Montgomery County Planning Board for approval.

MASTER PLAN RECOMMENDATIONS

Montgomery County Vision Zero Two-Year Action Plan, Approved and Adopted November 2017

Montgomery County is one of the first counties in the United States to adopt Vision Zero policy, which originated in Sweden in 1997 and is credited with significant reductions in fatal and severe collisions there. Starting in the 2000's, numerous jurisdictions in the United States adopted Vision Zero policy, including New York City in 2014. As of October 2017, 31 jurisdictions in the United States have adopted the policy. Montgomery County adopted a Two-Year Action Plan with the goal of reducing severe and fatal collisions by 35 percent for vehicle occupants, bicyclists and pedestrians by November 2019. The long-term goal is to completely eliminate fatalities and severe injuries by 2030 by reducing crash frequency as well as severity.

Vision Zero policy consists of several common principles:

1. *Transportation-related deaths and severe injuries are preventable and unacceptable.*
2. *Human life takes priority over mobility and other objectives of the road system. The road system should be safe for all users, for all modes of transportation, in all communities, and for people of all ages and abilities.*
3. *Human error is inevitable; the transportation system should be designed to anticipate error so the consequences are not severe injury or death. Advancements in vehicle design and technology, roadway engineering, personal electronic devices, etc., are necessary components for avoiding the impacts of human errors.*
4. *People are inherently vulnerable, and speed is a fundamental predictor of crash survival. The transportation system should be designed for speeds that protect human life.*
5. *Safe human behaviors, education, and enforcement are essential contributors to a safe system.*
6. *Policies and practices at all levels of government need to align, making safety the highest priority for roadways.*

The Action Plan commits to utilizing non-traditional, evidence based “*safe system approaches*” to improve safety. Examples include narrowing travel lanes, reducing vehicle travel speeds, additional educational outreach, and enforcement. The interim road diet installed at the trail crossing along Little Falls Parkway in January 2017 is included on Page 20 as a highlighted example of corrective action to “*improving dangerous intersections*”.

Westbard Sector Plan, Approved and Adopted July 2016

Westbard is a community in the southwestern part of Montgomery County, approximately two miles from the Bethesda and Friendship Heights commercial areas. The Capital Crescent Trail runs the length of Westbard. The Westbard Shopping Center is planned for mixed use redevelopment in the future, which will include new commercial space, residential homes, and other amenities. The planned Westbard redevelopment spurred approval of an updated Westbard Sector Plan in July 2016.

The Westbard Sector Plan considers the Capital Crescent Trail to be a major amenity as follows:

A tremendous asset in the community is the Capital Crescent Trail (CCT) on the old B&O Railroad right-of-way. This pedestrian and bicycle trail is a major regional connection that also provides limited local service in the Westbard area. Increasing local connectivity to and from the CCT will allow it to be more integrated into the community.

The Sector Plan makes many short and long term recommendations for the Capital Crescent Trail. A sampling of the recommendations include the following:

- *Create a road connection between River Road and Westbard Avenue, adjacent to the Capital Crescent Trail, to provide access to businesses and improve access to the Capital Crescent Trail.*
- *If a future Master Plan recommends additional density on the Whole Foods site, it should also explore options for a park or open space at this site and a trail connecting this site with the Capital Crescent Trail.*
- *If the Washington Episcopal School redevelops, renovate the associated portion of Willett Branch to restore the flood plain and provide a trail connection to the Little Falls Stream Valley and Capital Crescent Trail.*
- *Provide plantings to complete Westbard's Greenway network along the Capital Crescent Trail and Little Falls Parkway.*

Regarding transportation improvements in the area, the Sector Plan states on page 32: *"While the automobile still needs to be accommodated, data trends, at least in this area, indicate a shift in mode choice or commuting patterns away from the automobile. This shift provides the opportunity to use the existing and proposed rights-of-way for needed transportation facilities, other than just road capacity."*

The Sector Plan visualizes the framework for future transportation network development in the area, and includes recommendations for redevelopment of River Road, Westbard Avenue, and other roadways in the area. Consideration for additional bikeways, pedestrian paths, and public transportation networks is included. The Sector Plan includes results from traffic modeling of future traffic growth in the area as follows:

In the 2012 SSP year 2040 TPAR analysis, the Bethesda-Chevy Chase Policy Area is shown to be adequate for the roadway test. Given that the Westbard Sector Plan area is a small subset of a much larger policy area and the planned growth in Westbard is anticipated to be relatively minor compared to what is zoned but not built, the transportation network is considered to be in balance with the land use and densities proposed by the Westbard Sector Plan.

Little Falls Parkway is classified as a *"limited access park road that runs along the eastern boundary of the Sector Plan area with truck restrictions"*. As a park road, Little Falls Parkway is excluded from modeling and planning purposes as a primary means of traffic conveyance (as opposed to state highways and Montgomery County Department of Transportation owned

roads). Page 37 of the Sector Plan endorses Little Falls Parkway with a target 35 mph speed limit and two total lanes of traffic.

Bethesda Downtown Plan, Approved and Adopted May 2017

The Bethesda Downtown Plan provides guidance and recommendations for development in the downtown Bethesda area for approximately twenty years from the date of adoption. The Plan envisions a sustainable downtown area supported by the three major highways (Old Georgetown Road, Wisconsin Avenue, and East-West Highway), a strong public transportation network, and well developed and accessible pedestrian and bicycle infrastructure.

While Little Falls Parkway is outside of the formal downtown Bethesda area, the Plan has numerous recommendations for bikeways that will result in improved access to the Capital Crescent Trail from downtown Bethesda:



Figure 3: The Bethesda Downtown Plan Recommends a Network of Bikeways and Lanes Connecting to Capital Crescent Trail

As bicycle and pedestrian infrastructure in downtown Bethesda is improved in the future, it will result in additional recreational and commuter users on the Capital Crescent Trail.

Montgomery County Bicycle Master Plan, Approved and Adopted November 2018

The Bicycle Master Plan is a comprehensive update and amendment to all existing County bike plans, including the 1978 Master Plan of Bikeways, the 2005 Countywide Bikeways Functional Master Plan, and bikeway recommendations in past functional plans, master plans, and sector plans. The Bicycle Master Plan is a “*key element in Montgomery County’s Vision Zero Two-Year Action Plan to eliminate traffic-related fatalities and serious injuries*”.

The Bicycle Master Plan contains recommendations for a low stress network of bikeways and bicycle infrastructure throughout Montgomery County, including creation of a *Breezeway Network*, a high capacity system of arterial bikeways between major activity centers.

The Capital Crescent Trail, which follows an abandoned rail corridor and connects several major activity centers from Silver Spring through Bethesda into Washington D.C., is considered a major future Breezeway in the Plan:

Breezeways feature intuitive and safe intersection and driveway crossings that minimize delay for pedestrians and bicyclists. The crossings are developed to prioritize non-motorized travel by making it easier and safer to travel through intersections. Breezeway crossings include elements that both separate bicycle movements from motor vehicles and make bicyclists and pedestrians more visible to other road users. Crossings will:

- Slow motor vehicle traffic.*
- Improve bicyclist and pedestrian visibility.*
- Reduce bicyclist and pedestrian exposure.*
- Reduce or eliminate conflicts.*

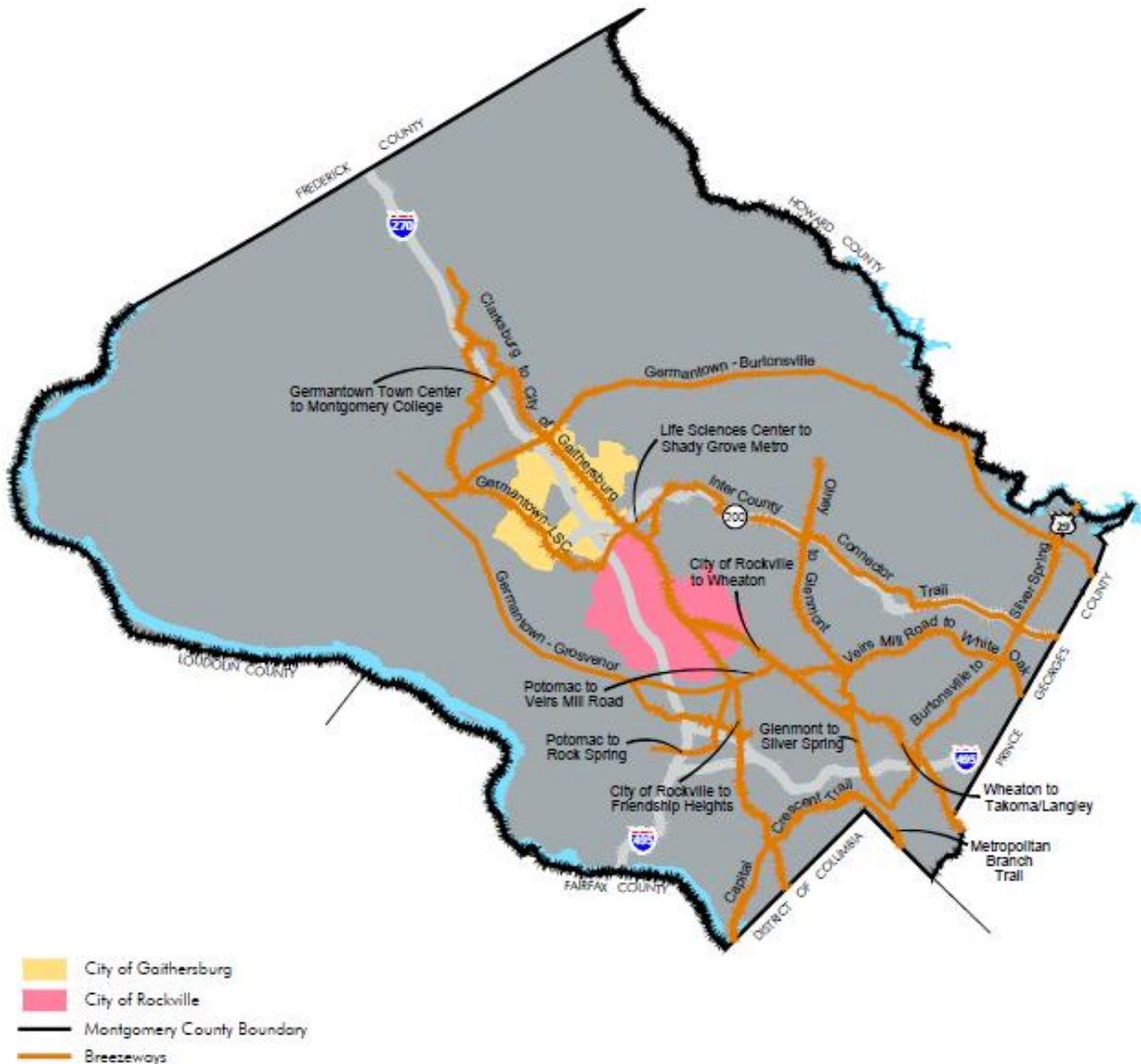


Figure 4: Capital Crescent Trail One of Fourteen Future Major Bicycle Breezeways (Bicycle Master Plan)

The Plan has several other long-term recommendations for the Capital Crescent Trail:

- Widening the trail to 15 feet with 2-foot-wide shoulders between Massachusetts Avenue and Bethesda Avenue, with a 5-7-foot-wide walkway and an 8-10-foot-wide bikeway.
- Added lighting along the trail between Bethesda Avenue and the Silver Spring Transit Center.
- Strongly considering trail lighting between River Road and Bethesda Avenue during the facility planning process.
- Studying an improved connection from the Capital Crescent Trail to MacArthur Boulevard.

Regarding mid-block trail crossings, such as the Capital Crescent Trail crossing at Little Falls Parkway, on Page 83 the Plan recommends “Traffic calming that removes traffic lanes and/or reduces the design speed of the road” as a viable solution to improve safety, in addition to trail re-alignment and grade separation.

EXISTING CONDITIONS

Little Falls Parkway is located within Little Falls Stream Valley Unit 2, consisting of several parcels. The Capital Crescent Trail crossing, and the scope of this project, is limited to Parcel 330. Parcel 330 is a 32.10-acre parcel acquired via a land donation from the United States Government. The Capital Crescent Trail right of way (former railroad right of way) is owned by Montgomery County and maintained by the Maryland-National Capital Park and Planning Commission.

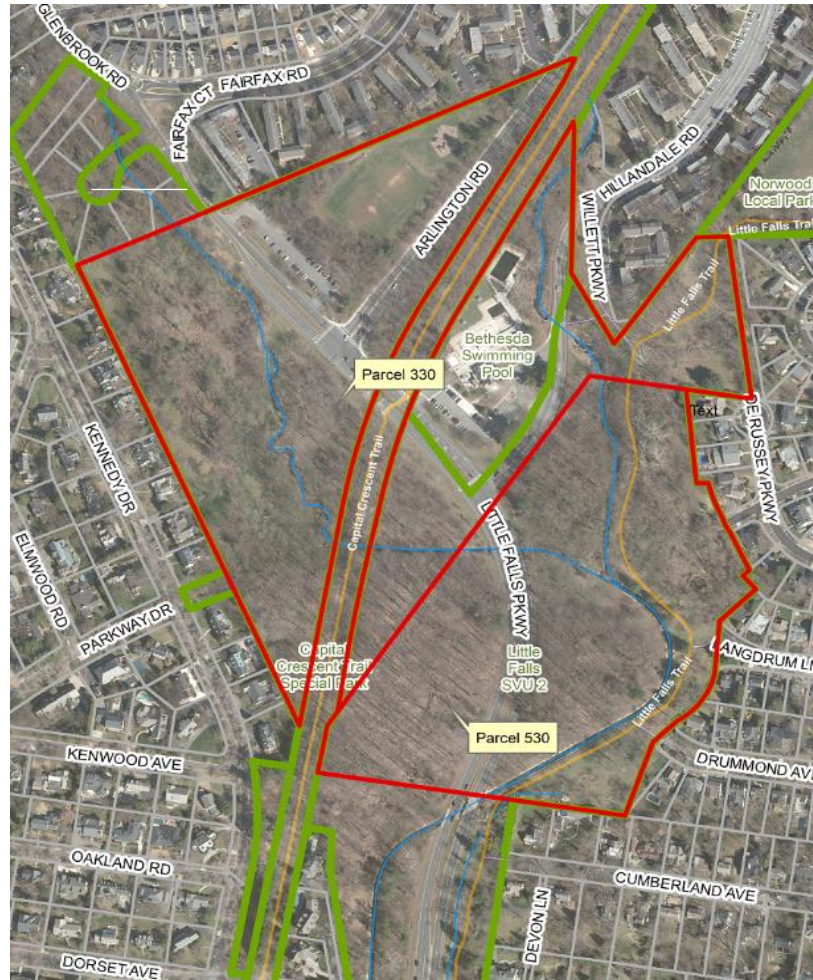


Figure 5: Project Park Boundaries

The project site is located at the Capital Crescent Trail crossing of Little Falls Parkway, between Hillendale Road and Arlington Road. Little Falls Parkway is owned by the Maryland-National Capital Park and Planning Commission (M-NCPPC). Arlington Road is a four-lane arterial roadway owned by Montgomery County Department of Transportation. Hillendale Road is a two-lane residential roadway owned by Montgomery County Department of Transportation. The southernmost portions of Hillendale Road and Arlington Road approaching Little Falls Parkway are M-NCPPC property.

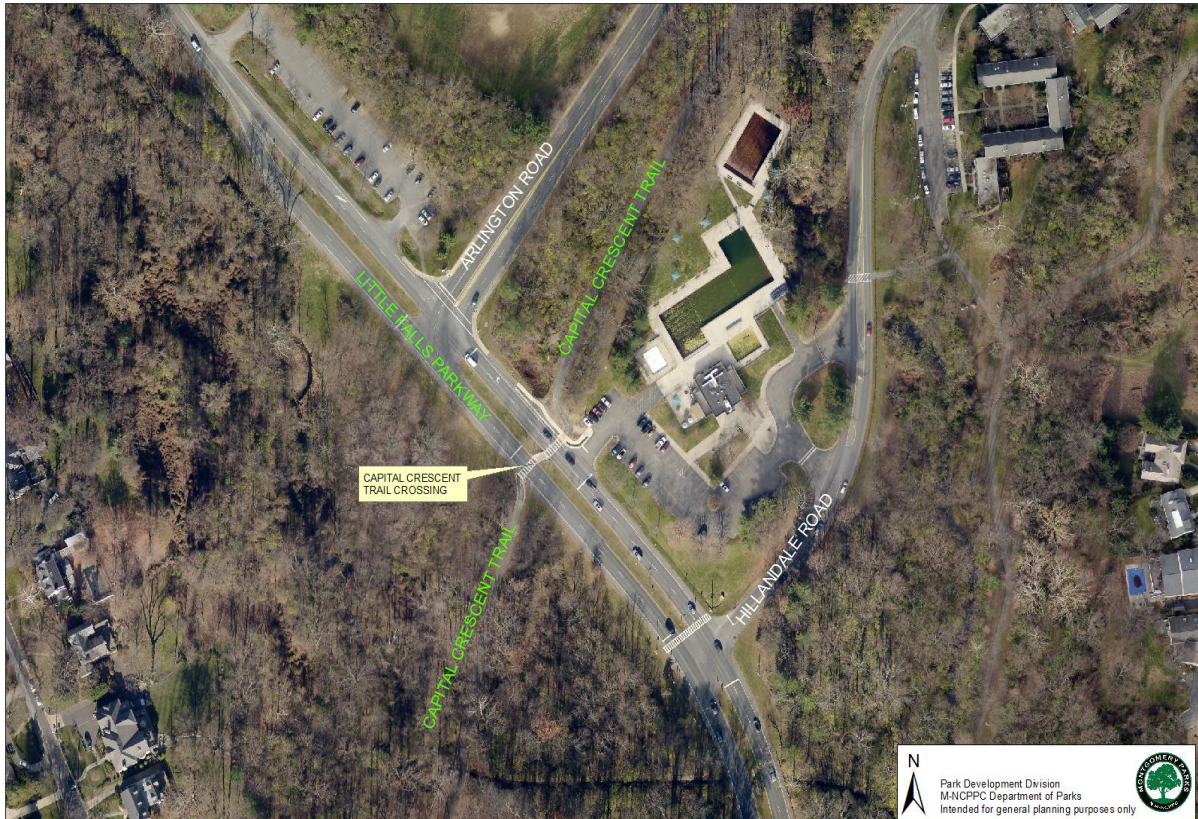


Figure 6: Aerial Image

The area around the project site is predominantly undeveloped, with residential communities interspersed within a half-mile radius. The Bethesda Pool is located adjacent to the Trail along Hillendale Road. The site is within the Willett Branch watershed and stream buffer. Several established diverse forest stands consisting of tulip poplars, white oaks, and other species are located around Little Falls Parkway and the Trail. The Natural Resources Inventory / Forest Stand Delineation (NRI / FSD) is attached to this memorandum.

Little Falls Parkway is four lanes, two in each direction with a grass median at the project site. The Capital Crescent Trail is a ten-foot wide asphalt trail. There are two drive entrances to the Bethesda Pool in the area: one driveway from Little Falls Parkway, and one driveway from Hillendale Road. Vehicular traffic counts conducted in May 2017 along Little Falls Parkway show an average weekday daily traffic (ADT) of 6,030 vehicles/day northbound and 8,030 vehicles/day southbound, with peak traffic typically between 8:00 to 9:30 AM and 4:00 to 7:00 PM. Weekend traffic volumes are 20% to 25% lower than weekday daily traffic. Trail counts for the Capital Crescent Trail range from 2,000 to 5,000 users a day with peaks of over 400 users an hour.

Prior to implementation of the interim road diet and safety improvements in January 2017, the Trail crossing at Little Falls Parkway consisted of a marked crosswalk with associated warning signage. The speed limit was 35 mph. “Stop” signs were present at each end of the crossing for trail users.



Figure 7: Trail Crossing Fall 2016 (Prior to Interim Road Diet), Courtesy Google Maps

The Trail crossing at Little Falls Parkway has historically been the site of frequent conflicts between trail users and motorists. The four-lane cross-section on Little Falls Parkway allowed for the multiple-lane threat scenario – a recognized traffic safety problem occurring along road sections that are more than two lanes wide and have an uncontrolled crossing (no traffic signal or stop sign). A multiple-lane threat crash involves a driver stopping in one lane on a multilane road to permit pedestrians/cyclists to cross, and an adjacent oncoming vehicle (in the same direction) striking the pedestrian who is crossing in front of the stopped vehicle. This crash is due to the initial stopped vehicle occluding the view of the second advancing vehicle who subsequently fails to yield. In addition, the 35 mph speed limit and resulting higher vehicle travel speeds allows less reaction and stopping time for trail users and motorists. In the two-year period from 2014 to 2016, the crossing experienced twelve conflicts, with a conflict defined as an incident that generated a police report. Among these twelve conflicts was a fatality involving a bicyclist in October 2016.

INTERIM ROAD DIET DESCRIPTION

In January of 2017, Montgomery Parks implemented a Vision Zero-based interim road diet along Little Falls Parkway to increase safety for trail users crossing the roadway in response to the fatal crash. The interim road diet reduced the cross-section of Little Falls Parkway from four-lanes to two-lanes between Arlington Road and Hillandale Road, by closing the outside travel lane in each direction approaching the trail crossing. The travel lanes were closed off with chains and flexible bollards. The speed limit was reduced to 25 mph. The grass median dividing the northbound and southbound travel lanes remained.

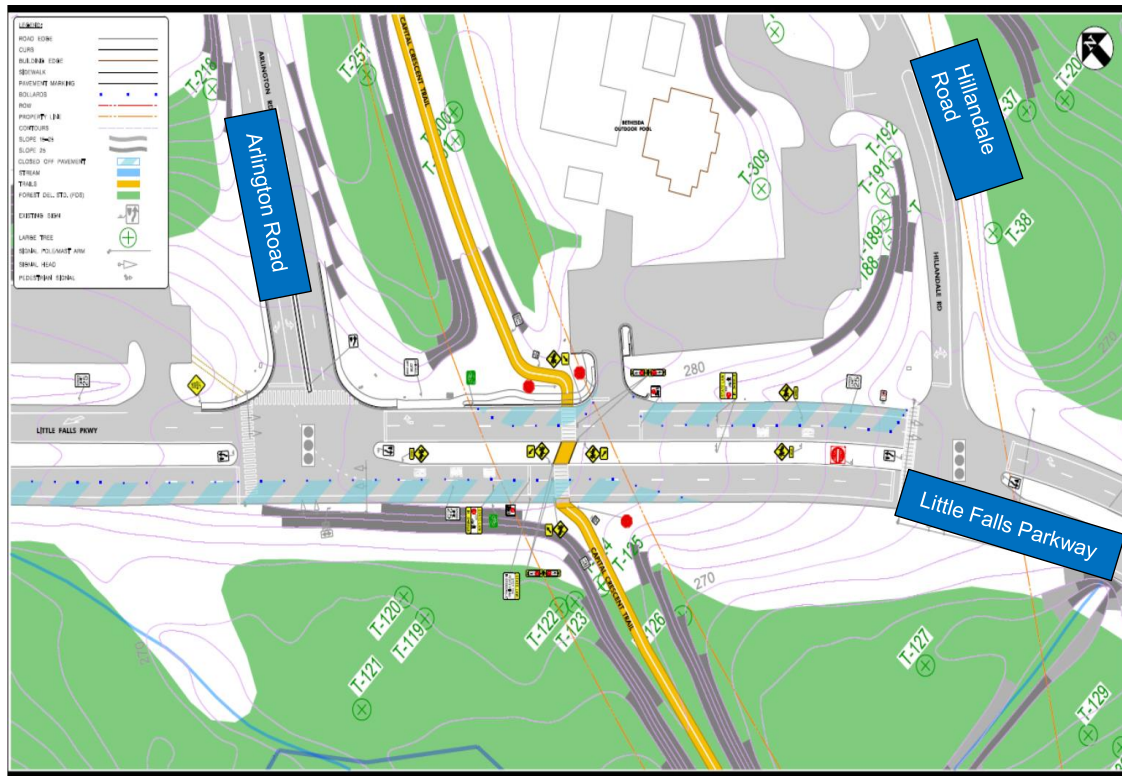


Figure 8: Plan View of Interim Road Diet



Figure 9: Trail Looking North at Little Falls Parkway, With Road Diet

INTERIM ROAD DIET SAFETY OBSERVATIONS

Staff have continually monitored the interim road diet condition since implementation. The interim road diet eliminated the multiple-lane threat condition, while simultaneously slowing vehicle speeds along Little Falls Parkway. The interim road diet has resulted in a significant overall crash reduction, from twelve in the two-year period prior to the road diet (2014-2016) to five from January 2017 to March 2019. The number of pedestrian/bike crashes was reduced 50% from six to three, with no fatalities. “Severity” of conflicts appears reduced due to reduced vehicle speeds.

Multiple observations of interactions between motorists and trail users at the modified trail crossing of Little Falls Parkway showed extremely high levels of drivers yielding to trail users in the crosswalk. Corresponding pedestrian/cyclist delay (or waiting time to cross) was effectively zero seconds.

Additional observations from multiple hours of observation of the interim road diet and crossing by staff and Sabra & Associates during peak rush hours and different times of the year include:

- Northbound traffic on Little Falls Parkway (between Arlington and Hillandale) was observed to have an average speed of 14 mph, while southbound traffic had an average speed of 19 mph. The difference between northbound and southbound speeds is likely due to the uphill grade on Little Falls Parkway for northbound traffic and the downhill grade for southbound traffic.
- Arlington Road had the longest traffic queues of any approach, with drivers waiting to turn left onto southbound Little Falls Parkway varying in number from 15 cars to 0 cars. On several occasions during the peak hour, when Arlington Road received a green light, there were no cars queued up waiting to turn. This is likely due to the fact that the large majority of traffic on Arlington Road originates from Bethesda downtown using southbound Arlington Road, and traversing through Bradley Boulevard. The traffic signal at that location has a cycle length that is twice as long as the one at Arlington Road and Little Falls Parkway, which means that the Bradley Boulevard signal effectively meters traffic arriving at the Arlington Road and Little Falls Parkway intersection.
- Vehicles utilizing the secondary Bethesda Pool driveway along Little Falls Parkway to exit the Pool parking lot occasionally contribute to additional queuing and delay for vehicles on Little Falls Parkway. In addition, the right turn results in the vehicles immediately approaching the trail crossing with less reaction time.
- The short cycle lengths (60 seconds between the beginning of successive green lights) along Little Falls Parkway appear to be the critical factor in allowing traffic to progress through while keeping queues on all approaches to a minimum.

The road diet shifts the queuing space where vehicles are “stored” as they travel through the intersection. The southbound queue is shifted to Arlington Road and the northbound queue is shifted to south of the Hillandale Road intersection. However, northbound travel times are not drastically affected because most of the northbound traffic before and after the road diet is utilizing the right lane only to turn onto Arlington Road. The road diet increased travel time on average by 7 seconds.

INTERIM ROAD DIET TRAFFIC VOLUME OBSERVATIONS

Traffic counts were conducted before and after the interim road diet along the affected segment of Little Falls Parkway. The results are summarized below.

Table 1: Before and After Interim Road Diet Little Falls Parkway Peak Hour Traffic Counts

Direction	Before Road Diet		After Road Diet		Change in Traffic	
	AM	PM	AM	PM	AM	PM
Southbound	835	1045	840	923	5	-122
Northbound	783	863	713	932	-70	69
Total	1618	1908	1553	1855	-65	-53
					-4.0%	-2.8%

The traffic volume counts show that Little Falls Parkway is effectively processing the same amount of traffic before and after the interim road diet. The average 3.4% drop in traffic processed by the collective intersections could entirely be discounted by the fact that the “before” counts were conducted during early December and the “after” counts were taken in mid-May when the trail is more heavily used.

INTERIM ROAD DIET IMPACTS TO ADJACENT ROADWAYS

There are several local roadways that could act as alternate routes for traffic that may wish to divert from Little Falls Parkway: Arlington Road, Dorset Ave, Kennedy Drive, and Hillandale Road.

Arlington Road: Traffic volume counts show that Arlington Road has experienced a decrease in traffic after implementation of the interim road diet. The Montgomery County Department of Transportation is currently conducting a road diet traffic study for Arlington Road independently of this project.

Dorset Avenue: A 48-hour traffic count was conducted on Dorset Avenue in Somerset in the Spring of 2018 when County schools were in session to see if there was noticeable diversion of traffic away from Little Falls Parkway. While no “before” count was conducted on Dorset Ave as part of this project, the Town of Somerset collected morning and afternoon peak period traffic data for vehicles entering the Town on Dorset Avenue in 2015. The traffic count showed that Dorset Avenue has not received any new cut-through traffic, as overall traffic was shown to be currently lower than in 2015.

Kennedy Drive: Kennedy Drive is a residential road connecting Bradley Boulevard and River Road, running through the Kenwood residential community. The Kenwood community is a potential area where cut through drivers could bypass Little Falls Parkway. After community concerns were raised regarding increased cut-through traffic, an origin-destination camera study was performed in Fall 2018 to quantify cut-through traffic through the Kenwood neighborhood.

Using time-stamped video, traffic entering Kenwood at Kennedy Drive from the Bradley Boulevard intersection was counted simultaneously with traffic exiting Kenwood at Dorset Avenue/Little Falls Parkway and at Brookside Drive/River Road. Traffic that entered the Kenwood neighborhood at Kennedy Drive and then exited at one of the other two exit points within 10 minutes was deemed to be a cut-through trip. The results of the study indicated that on average 5 vehicles during the morning peak hour and 3 vehicles during the evening peak hour satisfied the criteria for cut-through traffic. The average speed of cut-through traffic was 20 mph in the morning and 22 mph in the evening, which is below the speed limit of 25 mph.

The observed cut-through volumes were low and well below the minimum County thresholds for further study and implementation of additional traffic access restrictions. Per Montgomery County

Executive Regulation No. 17-94AM *Through Traffic Volume Access Restrictions in Residential Areas*, more than 100 cut-through vehicles per hour are needed to trigger further restrictions for a residential street like Kennedy Drive

The low volume of cut-through traffic through the Kenwood community is likely due to several factors:

- Morning peak hour turn restrictions from Bradley Boulevard onto southbound Kennedy Drive.
- Kennedy Drive is only about 20 feet wide, not including on-street parking.
- The traffic signal at Brookside Drive and River Road has a 2 ½ minutes long cycle length, meaning that any potential time saved by cutting through the Kenwood community to River Road is lost while waiting at the signal.
- Northbound Kennedy Avenue traffic (which is southbound only for most of its length) can only egress to eastbound Bradley Boulevard.

The data indicates that these turn restrictions, along with the partial one-way operation of Kennedy Drive, make it an inconvenient and slower cut-through for drivers that wish to use it to bypass Little Falls Parkway.

Hillandale Road: Hillandale Road is M-NCPPC-owned from approximately Willett Parkway south to River Road, and Montgomery County Department of Transportation-owned from Willett Parkway north to River Road. The Department of Transportation-owned portion of Hillandale Road includes the Kenwood Forest II condominium community, with on-street parking.

Based on the before and after traffic counts, Hillandale Road has received extra traffic due to the interim road diet. Peak hour morning traffic has increased along Hillandale Road by 24 cars (from 137 to 161 cars per hour); while afternoon peak hour traffic increased by 104 cars (151 to 255 cars per hour).

Montgomery Parks has received citizen complaints of speeding vehicles on Hillandale Road, as well as vehicles not yielding to pedestrians at the Hillandale Road and Chevy Chase Drive crosswalk. Montgomery County Department of Transportation conducted a spot speed study in January 2019 from 1 to 2 PM (as speeds are typically higher during non-rush hour volumes), just north of the Willett Parkway and Hillandale Road intersection, which showed an 85th percentile speed of 30 mph (25 mph speed limit). The observed 85th percentile speed of 30 mph is below the 34 mph requirement for speed humps, per Montgomery County *Executive Regulation 1-18 AM – Speed Humps for Residential Streets*. The Montgomery County Department of Transportation installed a radar speed sign along Hillandale Road near the Kenwood Forest condominiums in March 2019 and has been coordinating with the community on additional traffic calming measures, including curb bump outs and additional signage.

FACILITY PLANNING PROCESS

Program of Requirements

The scope of the Capital Crescent Trail Crossing at Little Falls Parkway Safety Improvements project is to develop the safest permanent crossing solution possible for trail users, consistent with Vision Zero design principles, while balancing construction costs, environmental impacts, maintenance costs, and traffic flow. Trail user safety is the priority for this project.

Staff met with the community on several occasions to discuss the project and obtain input for permanent safety improvements. The first community meeting was held in June 2018 to present

preliminary alternatives and gather community input. The second community meeting was held in October 2018, where the “top” three concept alternatives were presented. Staff has also met with various community groups and regulatory agencies throughout the facility planning process.

Overview of Facility Planning Process

Community Meeting #1

The first community meeting was held on June 13, 2018 at Somerset Elementary School in Bethesda. At the meeting, twelve concept alternatives were presented, which were grouped into three different safety approaches:

- Controlling the trail crossing: Concepts included diverting the Capital Crescent Trail to Arlington Road or Hillandale Road (Alternates #1 and #2), a new traffic signal or HAWK signal at the existing crossing location (Alternates #3 and #4), or stop signs at the crossing (Alternate #5).
- Removing trail user and motorist conflicts: Complete separation of the trail and road with a tunnel or bridge (Alternates #6 and #7), or complete closure of that segment of Little Falls Parkway to vehicle traffic (Alternate #8).
- Reducing trail user and motorist conflict potential: Concepts included a permanent road diet with speed table crossing (Alternate #9), dynamic lane usage along Little Falls Parkway (Alternate #10), a roundabout intersection at Arlington (Alternate #11), and a permanent road diet from Arlington Road south to Dorset Avenue (Alternate #12).

Parks utilized an informal sticker voting system at the Community Meeting to allow attendees to vote for their most popular and least popular alternates.

Based on the sticker votes, the most popular alternates were:

1. Diversion of the Trail to the Arlington Road Intersection
2. Diversion of the Trail to the Hillandale Road Intersection
3. Pedestrian Bridge

The three least popular alternates were:

1. HAWK pedestrian signal crossing
2. Dynamic lane use along Little Falls Parkway
3. Complete closure of Little Falls Parkway to vehicle traffic

The following observations, comments, and concerns were expressed by the attending community:

- Many attendees expressed concern about some of the bicyclists who use the trail. Some travel at a high rate of speed, do not yield to pedestrians, and do not slow down at the trail crossings.
- Concerns were raised about the interim road diet backing up and slowing down traffic in the area or contributing to cut through traffic on adjacent roadways.
- The interim road diet closure of two lanes has improved the safety for users, as they now only must cross two lanes instead of four, and vehicles are moving slower.
- There were split opinions on whether Little Falls Parkway should remain two lanes or be re-opened back to four lanes.
- There was a desire for additional lighting at the trail crossing.

- Requests were made for additional measures and enforcement to slow bicyclists on the trail.
- Concerns were expressed about the safety of the Capital Crescent Trail crossing of Dorset Avenue, further south.
- Requests were made to consider trail connectors from surrounding roads and communities in any permanent design.

During the Summer of 2018, the twelve concepts were narrowed down and revised to three concept alternatives based on the following factors:

- Trail user safety
- Consistency with Vision Zero policy
- Construction and operational / maintenance costs
- Environmental impacts
- Vehicle delay and traffic impacts
- Coordination and input from Montgomery County Department of Transportation
- Consistency with applicable master plans
- Public input obtained from Community Meeting #1 and additional project correspondence from the community received by Parks

Community Meeting #2

The second community meeting was held on October 9, 2018 at Bethesda Chevy Chase High School. This was a joint meeting with the Montgomery County Department of Transportation to discuss downtown Bethesda bicycle and pedestrian facility improvements. Park staff presented the top three concept alternatives and maintained an informational booth to answer questions and gather feedback. All three concepts proposed reconfiguring Little Falls Parkway to a two lane road with one travel lane in each direction.

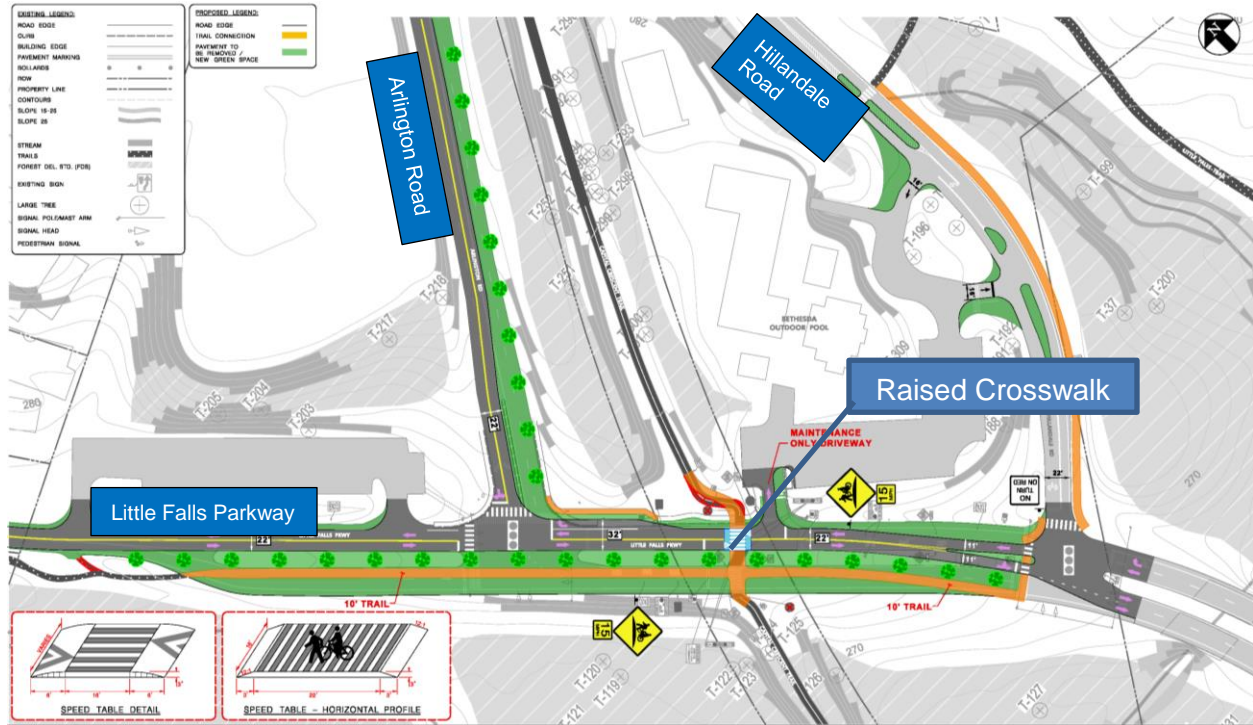


Figure 10: Alternate A: Permanent Road Diet (As Presented At Community Meeting #2)

Alternate A builds on the successful interim road diet. The existing southbound travel way is removed completely, and the northbound travel way is converted to bi-directional traffic flow resulting in one travel lane in each direction with no median. This permanently eliminates the multiple-lane threat potential, while the removal of the wide median provides a further safety benefit by simplifying the driver's decision-making for when to yield the right-of-way to trail users in the crossing or approaching the crossing. The raised crosswalk (speed table) will enhance safety by helping to ensure vehicles slow down as they approach the crossing.

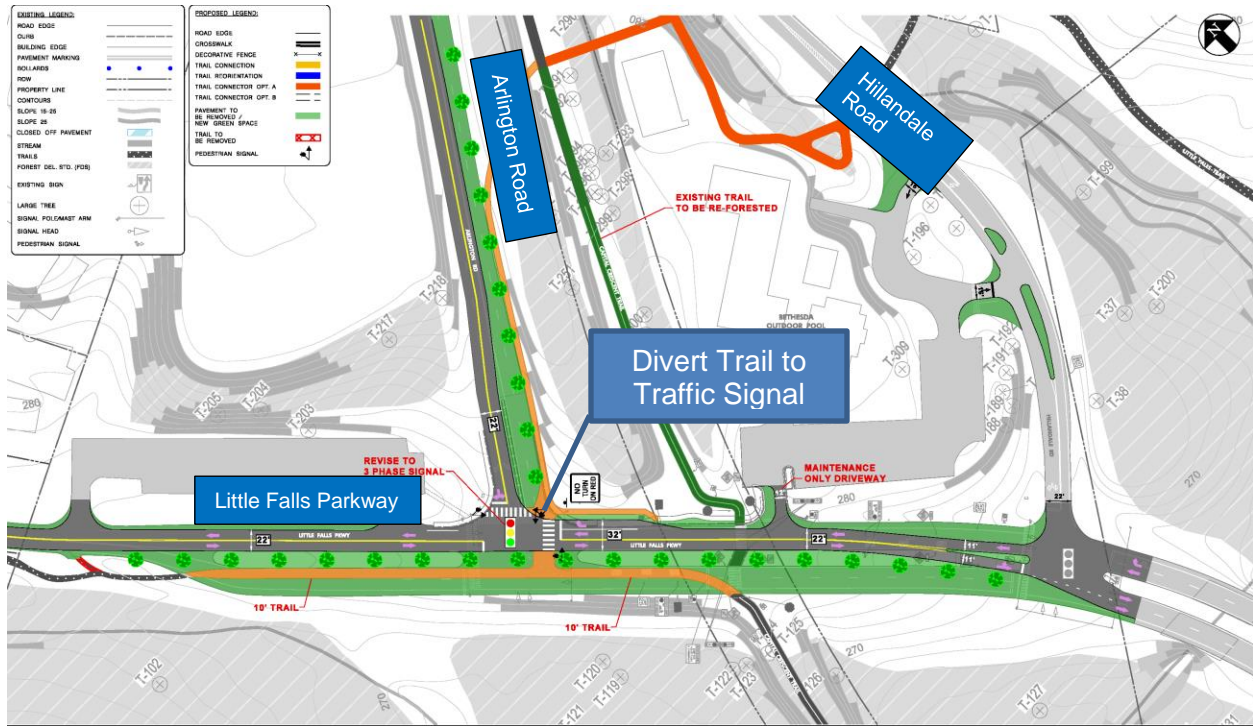


Figure 11: Alternate B, Trail Reorientation to Arlington Road Signal

Alternate B shifts the Capital Crescent Trail to the existing signalized intersection at Arlington Road and Little Falls Parkway. The existing traffic signal would be re-configured to control conflicts between trail users and motorists with the addition of a pedestrian-only signal phase. The pedestrian only signal phase would provide red indications for all vehicular movements while trail users are given the walk indication. The northbound right-turn pocket from Little Falls Parkway to Arlington Road would remain, but a “no turn on red” restriction would be required for safety. As with Alternate A, the existing southbound travel way is removed completely, and the northbound travel way is converted to bi-directional traffic flow resulting in one travel lane in each direction with no median. The reduction in travel lanes and removal of the wide median reduces the crossing distance for trail users, further limiting their exposure to conflicts with vehicular traffic. The reduction in travel lanes also reduces the time required for the pedestrian only signal phase, which limits delays for vehicles. In order to prevent people from jaywalking across Little Falls Parkway instead of utilizing the new signalized crossing, this alternative would also require physical barriers along the edge of northbound and southbound Little Falls Parkway.

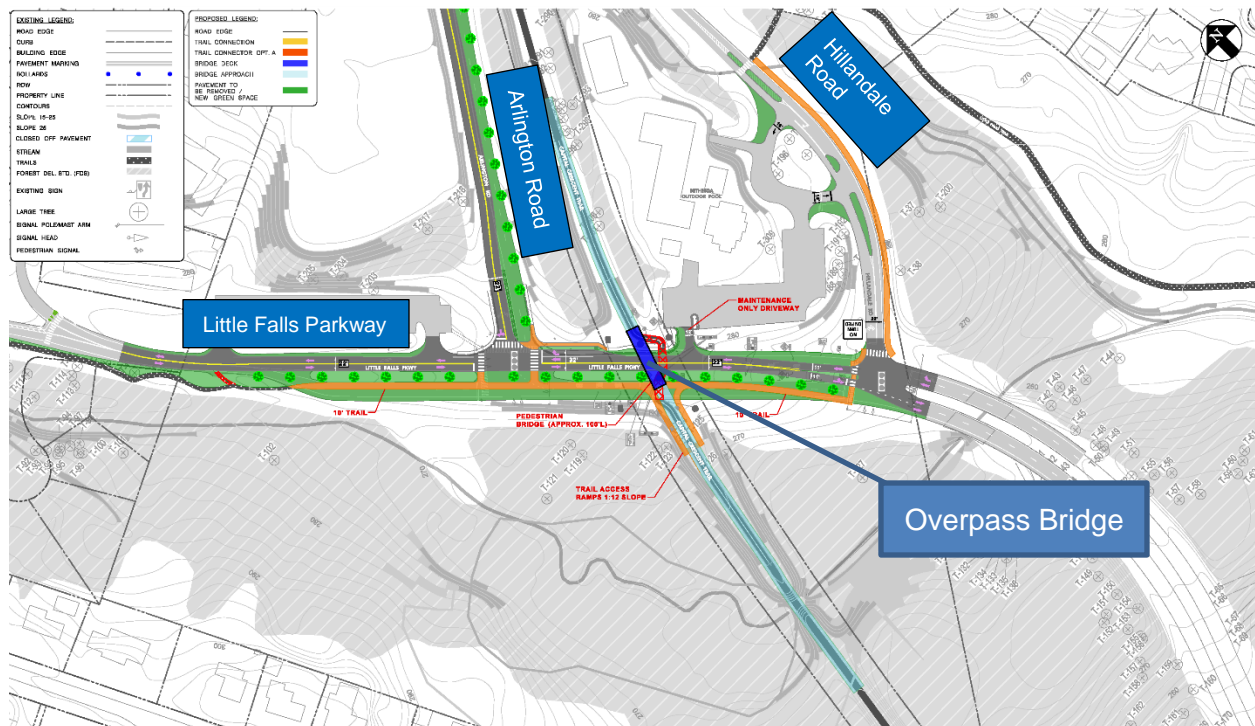


Figure 12: Alternate C, Pedestrian Bridge

Alternate C is a grade-separated trail crossing via a pedestrian bridge over Little Falls Parkway. The grade separation would provide superior safety benefits over all other alternates under consideration by eliminating conflict points between trail users and vehicular traffic on Little Falls Parkway. Alternate C includes the re-configuration of Little Falls Parkway to a two-lane road (as with Alternates A and B) in order to minimize the length of the bridge span (i.e. the distance between bridge abutments) and impacts to adjacent environmental features, while maintaining vehicle capacity. This alternative requires the construction of significant retaining walls to provide trail connections from the Little Falls Trail and the neighborhood to the west.

Staff distributed surveys at the community at the meeting, asking interested attendees to vote on their favorite alternative and to provide any other input. The results of the survey:

- Alternate A Permanent Road Diet: 45% chose this as most preferred.
- Alternate B Trail Reorientation to Arlington Road Signal: 6% chose this as most preferred.
- Alternate C Pedestrian Bridge: 49% chose this as most preferred.

Many attendees expressed concern about future traffic in the downtown Bethesda area, such as from the Westbard re-development. Some expressed a desire that Little Falls Parkway be configured to function more like Rock Creek Parkway in Washington D.C., which is 6 lanes total, 3 in each direction. There was concern that the interim road diet was negatively affecting adjacent neighborhood roadways. However, some attendees supported the interim road diet and its success in increasing safety for trail users. Most attendees agreed that the interim flexible posts and signage along Little Falls Parkway could be confusing for drivers. Many attendees requested a solution that minimized travel delays for motorists and trail users.

The following observations, comments, and concerns were also expressed by the attending community:

- The Little Falls trail connector behind the Bethesda Pool shown on Alternate B was not popular.
- There was significant support for a permanent pedestrian bridge.
- A request was made for lighting at the crossing.
- There was concern that trail users and cyclists would not obey a “Don’t Walk” signal in Alternate B, some bicyclists would just cross at the former crossing unless a barrier was installed, and that vehicles would ignore “No Right on Red” restrictions.
- There was a desire to minimize environmental impacts.
- Many attendees expressed concern about bicyclists who use the trail. Some travel at a high rate of speed, do not yield to pedestrians, and do not slow down at the trail crossings.
- Many attendees expressed concern that the interim road diet was causing additional cut-through traffic in adjacent neighborhood roadways, decreasing safety in the neighborhood.

Open Town Hall

An online Open Town Hall forum was opened by Montgomery Parks in November 2018 and closed February 2019. The format allows for on-line comments and was identical to the survey distributed at the October 2018 community meeting. Montgomery Parks received the following 318 individual positions on the alternatives:

- Alternate A: 51 votes (16%)
- Alternate B: 94 votes (30%)
- Alternate C: 156 votes (49%)
- No position: 17 votes (5%)

Many Town Hall users supported a pedestrian overpass bridge, and many commented that the existing interim road diet could be confusing. Some users also requested a return to four lanes of travel on Little Falls Parkway. Some users indicated that the interim road diet had resulted in increased traffic on adjacent neighborhood roadways.

Analysis of Top Three Alternates

After Community Meeting #2, and based on community feedback and coordination with other community groups and agencies, Alternates A, B, and C were refined, modeled and assessed in detail. Table 2 below is a summary of several metrics. The “No Build” in Table 2 is a baseline scenario representing Little Falls Parkway pre-January 2017 with two lanes of travel in each direction.

Prior to the construction of the interim condition, traffic counts were taken in late Fall of 2016 along Hillandale Road, Arlington Road, and Little Falls Parkway. Subsequently in May of 2017, traffic counts were conducted along the same roads to determine how the interim road diet restricted vehicle travel or diverted traffic to alternate streets. Along with multiple field visits, additional data collected included:

- Trail user delays experienced at the crossing of Little Falls Parkway under the existing interim road diet;
- Observations of interactions between trail users and motorists at the trail crossing;

- Existing interim road diet queuing observations;
- Before and after crash data for Little Falls Parkway between Arlington Road and Hillandale Road;
- Dorset Road peak hour traffic counts in Somerset;
- Average peak hour speed for vehicles along Little Falls Parkway between Hillandale and Arlington Roads;
- Cut-through data collected along Kennedy Avenue in Kenwood; and
- Current traffic signal timing for Hillandale/Little Falls Parkway and Arlington/Little Falls Parkway.

This data was used to determine and estimate impacts to:

- Trail user safety;
- Trail user delay;
- Vehicle delay (modeled in VISSIM multimodal traffic modeling software);
- Diversion of traffic onto other local roads (modeled in VISSIM software); and
- Adjacent environmental features including forests, streams, water channels, and wetlands.

Table 2: Assessment of Alternates

Metric	No Build (pre-January 2017 condition)	Interim Road Diet	Alt A – Perm Road Diet	Alt B – Arlington Road Signal	Alt C - Bridge
Trail User Safety	Unsafe – 12 conflicts in 2 years including 1 fatality	Safe – 5 conflicts in 2+ years, no fatalities	Safe – includes raised crosswalk	Safe – controlled crossing	Safe – complete separation
Trail User Delay	Minimal	Minimal	Minimal	~30+ seconds	None
Vehicle Delay	0 (baseline)	+7 seconds	+7 seconds	+13 seconds	-3 seconds
Diverted Traffic	None (baseline)	Additional diversion to Hillandale Road	Consistent with Interim Road Diet	Slightly higher diversion to Hillandale Road than Interim Road Diet	None
Design and Construction Cost	\$0 (baseline)	N/A	\$1.4 M	\$2.0 M	\$5.8 M
Operational Budget Impact	\$0 (baseline)	N/A	\$8,000 / year	\$17,500 / year	\$50,000 / year, eventual complete bridge replacement / reconstruction
Environmental Impacts	None	Negligible	Negligible	Moderate	Major
Public Input	N/A	Public support	Public support	Public support	Significant public support

All three alternatives maintain a two-lane configuration for Little Falls Parkway, consisting of one travel lane in each direction. This is a similar section as other Park roads, including Sligo Creek Parkway and Beach Drive. Crossing two lanes is always safer for trail users than crossing four lanes.



Figure 13: Alternate A Permanent Road Diet (Looking North Towards Crossing)

Alternate A: Permanent Road Diet

Alternate A has many advantages: it is an extension of the interim road diet that has been in place for over two years which has demonstratively increased safety at the crossing, it is the least expensive to maintain and construct, and has negligible environmental impacts.

The drawbacks for Alternate A are slightly higher travel times for motorists, and without additional improvements may not decrease the traffic that has been diverted to Hillandale Road during the interim road diet.



Figure 14: Alternate B Trail Reorientation to Arlington Road Signal Rendering (Little Falls Parkway and Arlington Road Intersection Looking South)

Alternate B: Trail Reorientation to Arlington Road Signal

Alternate B controls the crossing by shifting the Trail to the Arlington Road intersection traffic signal. There would be a full pedestrian phase for the signal where all traffic is stopped, and Trail users have the right of way to cross. The pedestrian phase results in increased delay for both motorists and trail users compared to other alternatives. This may result in increased frustration among trail users and vehicles.

There is a likelihood that some trail users may not comply with a “Don’t Walk” pedestrian signal and attempt to cross between gaps in traffic, decreasing safety and increasing the potential for a conflict. Additionally, staff has concerns regarding motorist compliance with “no turn on red” for the right turn from Little Falls Parkway onto Arlington Road, which will decrease safety. While these actions are against the law, Vision Zero policy acknowledges that safe solutions must account for real world behavior of motorists and trail users.

Expansion of Little Falls Parkway back to four lanes of traffic, as requested by some motorists, is not recommended because it increases the travel distance and time of exposure for trail users, decreasing safety. Four lanes would also not decrease motorist travel times through Little Falls Parkway because the pedestrian phase of the traffic signal would be increased to account for the additional crossing distance, negating any increased roadway capacity and resulting in additional motorist delay and additional traffic diverted to adjacent neighborhood roads. Traffic models indicate that under this Alternate, motorist and trail user delays will always be higher than any other alternative whether with two or four lanes open on Little Falls Parkway, due to the requirement of a full pedestrian crossing phase at the Arlington Road signal.



Figure 15: Alternate C Pedestrian Bridge (Little Falls Parkway Looking North at Trail Crossing)

Alternate C: Bridge

An overpass trail bridge (Alternate C) is a very popular option amongst the community because it completely separates vehicle traffic and trail users. It is the only alternative which would decrease motorist travel times while still maintaining safety for trail users.

However, Alternate C has significant disadvantages, including substantial construction (\$6M) and maintenance costs.

Alternate C has significant impacts to adjacent environmental features. The pedestrian bridge would be 15 feet above Little Falls Parkway, requiring placement of substantial amounts of fill in sensitive natural areas, and retaining walls for the ramps to the bridge. The ramps and retaining walls would extend to the bridge elevation and block viewsheds of the adjacent stream valley. The ramps would introduce unavoidable steeper segments than the current trail, which may deter some less able-bodied users from using the bridge. The bridge abutments, ramps, and associated sidewalk connections would impact an existing mixed wetland forest stand as well as the Willett Branch stream valley buffer, thus requiring additional environmental mitigation.

Restoring Little Falls Parkway to four lanes would require a substantially longer bridge span, mitigation of significant environmental impacts to Willett Branch and adjacent forest stands, and much higher costs.

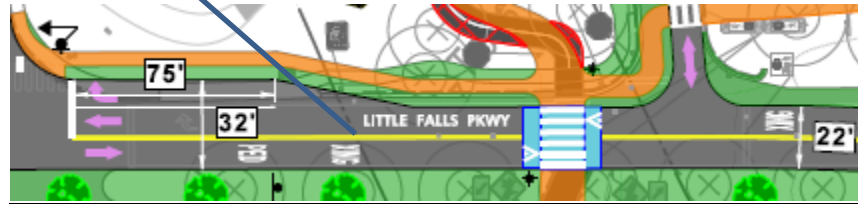
Alternate C also has the longest design and construction timeframe and impacts to Capital Crescent Trail users during construction, including temporary closures and detours, would be required to construct the bridge.

Installation of the interim road diet along Little Falls Parkway approaching the Capital Crescent Trail was quantitatively shown to significantly improve safety at the trail crossing while also minimizing adverse impacts to vehicular operations along the corridor. The interim road diet has eliminated the multi-lane threat, slowed vehicle speeds through this segment of Little Falls Parkway, and increased visibility between trail users and drivers. Before and after traffic count data reveals minimal reductions in vehicular throughput along Little Falls Parkway due to the road diet, with some traffic diverting from Arlington Road to Hillandale Road. However, Little Falls Parkway is capably processing traffic with minimal delays.

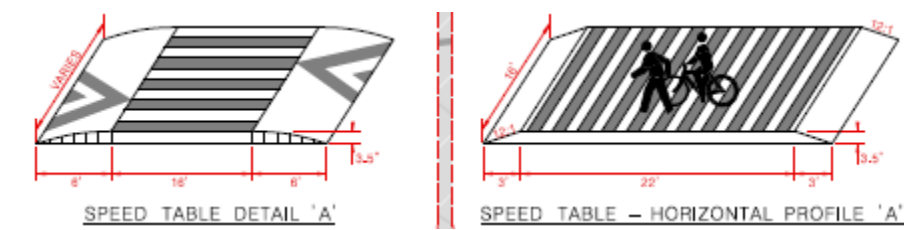
26

Key elements include:

Little Falls Parkway
two total lanes at
Crossing



Permanent Road Diet – Conversion of Little Falls Parkway from four lanes to two lanes reduces vehicle speeds, eliminates the “multi-threat” lane condition, and is consistent with Vision Zero policy.

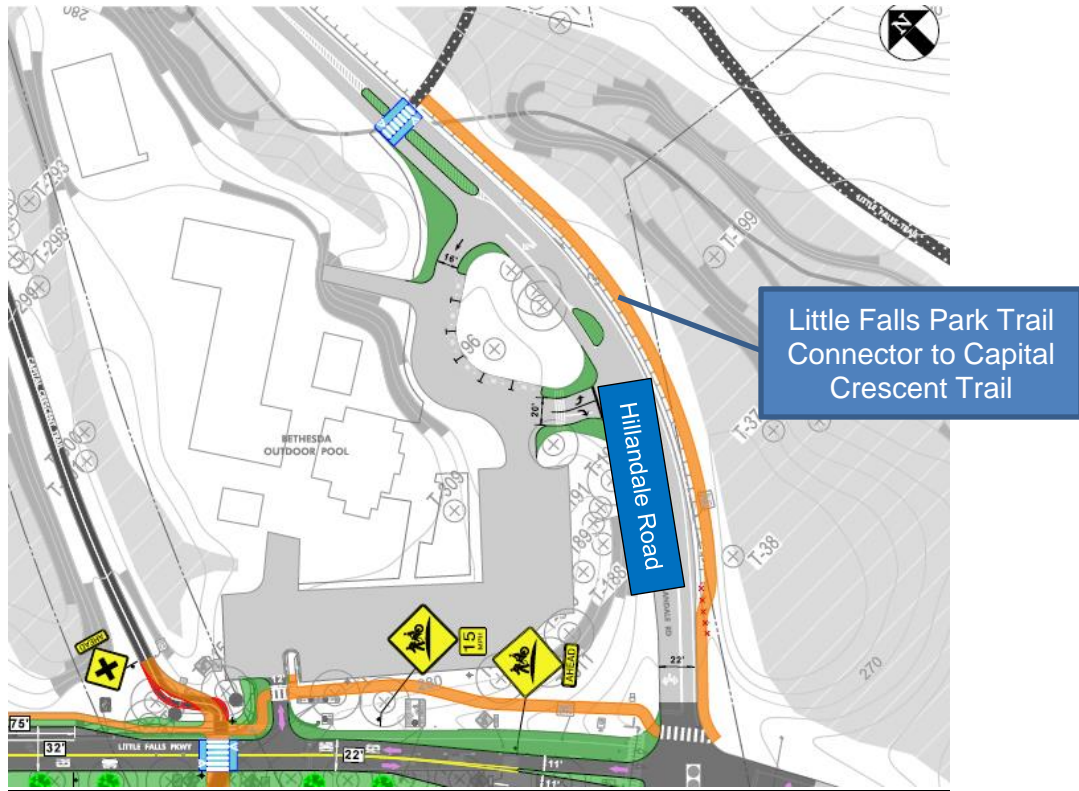


Speed Table Crossing – The Capital Crescent Trail crossing will be converted to a speed table, which is an elevated hump with a flat top. Speed tables effectively reduce vehicle speeds and increase driver awareness. Park staff have found similar installations along Sligo Creek Parkway to be effective at reducing speeds and improving crossing safety.

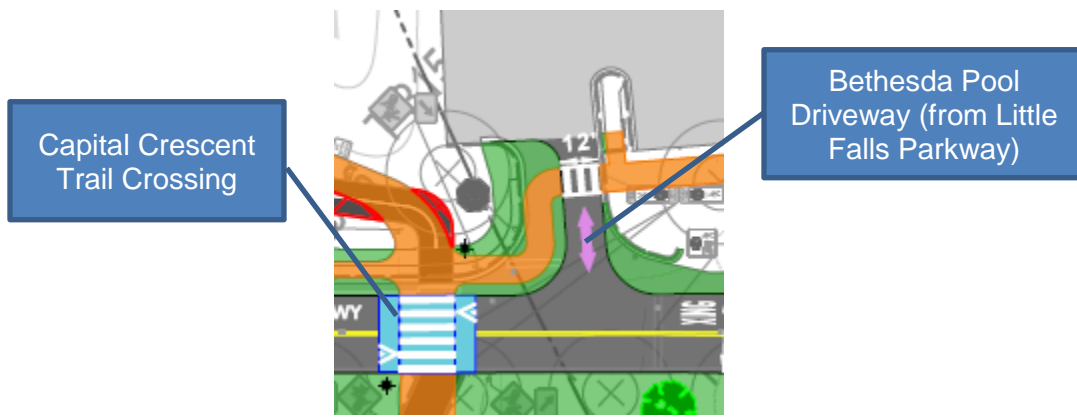
Removal of Median – Reconfiguration of Little Falls Parkway to two total lanes also removes the existing median. Median removal is recommended after coordination with Park Police, as it increases confusion and miscommunication between drivers and trail users at the crossing and has been a factor in several crashes. One common scenario is that approaching drivers in the far lane believe oncoming trail users will stop in the median and proceed through the crossing, while trail users are in the crosswalk, have the right-of-way and believe drivers will stop.

Improved Lighting – New street lighting will improve visibility at the crossing for all users.

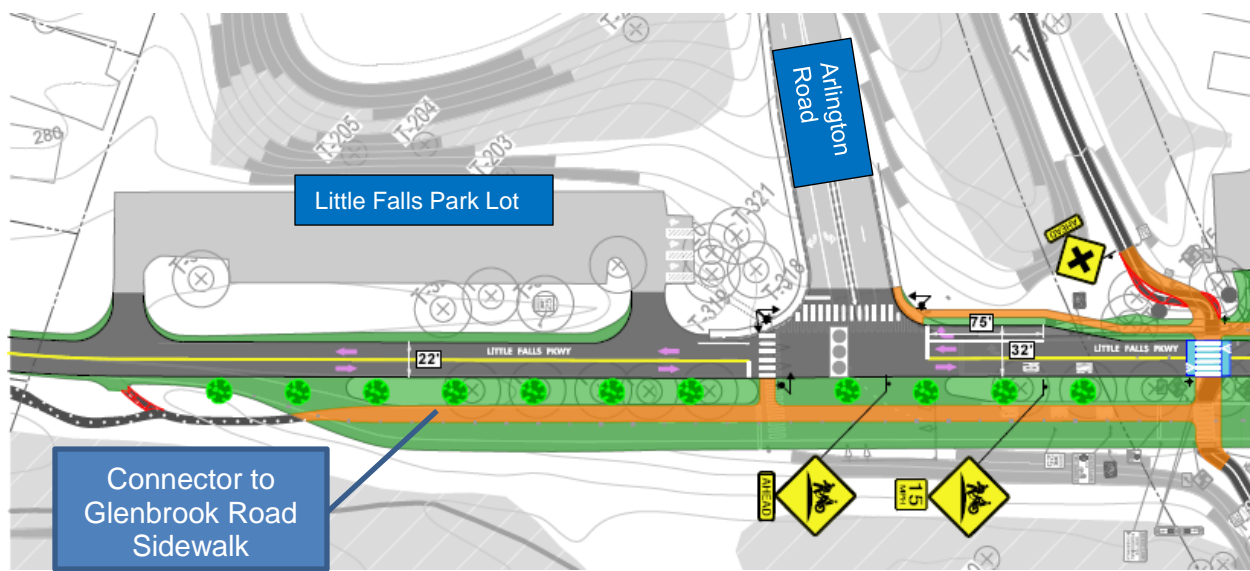
“Stop” Signs for Trail Users – The “Stop” signs for trail users on both ends of the crossing will remain. The “Stop” signs make it clear to users that it is unsafe to cross without stopping and observing traffic.



Connection to Little Falls Park Trail – The Little Falls Trail parallels a portion of the Capital Crescent Trail east of Hillandale Road, allowing users to access the Trail and other areas of Bethesda from Norwood Local Park and points east. A new 8' asphalt trail connection along Hillandale Road will connect Little Falls Park Trail directly with the Capital Crescent Trail. The trail connection will be designed in conjunction with the Hillandale Road traffic calming to minimize impacts to the stream valley buffer.



Conversion of Bethesda Pool Driveway – The existing driveway to the Bethesda Pool from Little Falls Parkway would be narrowed and converted to a maintenance and event egress only driveway. This modification will increase safety of the crossing by reducing the number of vehicles utilizing Little Falls Parkway to enter and exit the pool. It will also reduce the chances of conflicts at the crossing by minimizing cars exiting the pool onto Little Falls Parkway, a turning movement that takes them directly into the crossing and proposed Little Falls Trail connector. The modifications will not affect parking at the pool.



Connectivity with Little Falls Park Lot and Glenbrook Road – an existing parking lot at the intersection of Little Falls Parkway and Arlington Road provides overflow parking for Bethesda Pool, Little Falls Park, the Capital Crescent Trail, and other park amenities in the area. There is an existing sidewalk along Glenbrook Road that ends at Little Falls Parkway and the Little Falls parking lot. The sidewalk along Glenbrook Road will be extended and connected to the Capital Crescent Trail, and the existing trail connection from the Capital Crescent Trail to the Little Falls parking lot will be upgraded. Users on the Capital Crescent Trail will have the option of traveling to the intersection of Little Falls Parkway and Arlington Road to cross Little Falls Parkway at the existing traffic signal if desired.

Stormwater Management – the Recommended Facility Plan removes a substantial amount of impervious asphalt surfaces, reducing the amount of stormwater runoff and pollutants discharging to Willett Branch. A bio-swale is proposed alongside Little Falls Parkway to provide additional water quality treatment and address project stormwater management requirements. Storm drainage improvements will include replacement of a trench drain at the Bethesda Pool entrance and replacement of a culvert at Little Falls Parkway and Hillandale Road.

Open Space – With the reduction in Little Falls Parkway from four lanes to two lanes, portions of the previous asphalt roadway will be converted to green open space.

Landscape Improvements – There are negligible impacts to existing forest stands. Several individual trees may need to be removed to construct the new Little Falls trail connection. Additional shade trees will be planted throughout the project. Low maintenance groundcovers will be used on the project periphery to enhance the space and provide environmental benefits.

Hillandale Road Traffic Calming – Hillandale Road is a residential road that has experienced an increase in traffic volumes since implementation of the interim road diet. The Recommended Facility Plan includes installation of traffic calming measures on the Park owned portion of Hillandale Road, including a speed table crossing for the Little Falls Trail at the Bethesda Pool entrance, an additional speed hump closer to the Kenwood Forest condominiums, curbed grass medians, and roadway narrowing.

Community Group, Agency Coordination, and Regulatory Approvals

The following is a summary of coordination performed with agency and community groups for this project:

M-NCPPC Department of Planning

Planners from Area 1 have been part of the Planning, Design, Construction, and Operations (PDCO) project team process since project inception. A Natural Resources Inventory/Forest Stand Delineation (NRI/FSD) and Forest Conservation Plan Exemption for the Recommended Facility Plan was approved in March 2019 (Project #42019124E). The Forest Conservation Plan exemption was approved because the Recommended Facility Plan is classified as a County highway project with minimal forest clearing and no impacts to champion, specimen, or significant trees. To comply with the requirements of the Forest Conservation Plan exemption, a Tree Save Plan will be submitted for approval during the detailed design phase.

M-NCPPC Park Police

Park Police staff have been part of the PDCO project team process since project inception. Park Police continue to monitor the crossing and Capital Crescent Trail.

Montgomery County Department of Permitting Services (DPS)

The stormwater management concept for the Recommended Facility Plan was submitted in February 2019 (#284594). Staff met with Montgomery County Department of Permitting Services staff to present the project in March 2019, and a revised concept was re-submitted in March 2019. The concept proposes the installation of a bio-swale alongside Little Falls Parkway to address stormwater management requirements. If future geotechnical testing determines high groundwater levels, the concept proposes replacement of the bio-swale with approved bio-filtration structures.

Montgomery County Department of Transportation (DOT)

Staff has coordinated with the Montgomery County Department of Transportation throughout the project process. Montgomery County Department of Transportation staff provided review and support for implementation of the interim road diet in January 2017. Park staff met with Department of Transportation personnel from the Division of Traffic Engineering in August 2018 and November 2018 to discuss the alternatives being evaluated. The Department of Transportation's Division of Traffic Engineering personnel have also reviewed the Recommended Facility Plan for general compliance with County traffic standards.

Montgomery County Fire and Rescue Service

Little Falls Parkway is utilized by emergency vehicles to respond to nearby service calls. Staff met with Montgomery County Fire and Rescue Service in August 2018 to present the concept alternates. Fire and Rescue Service reviewed the alternatives, including the Recommended Facility Plan, and confirmed that they would have minimal effect on emergency response times.

Bethesda Area Homeowner Associations

Representatives from several homeowner associations and communities around the project area requested a meeting with Staff in February 2019. Communities represented included the Town of Somerset, the Citizens Coordinating Committee on Friendship Heights, Kenwood Citizens Association, Kenwood House, Hillandale Town Houses, and the Kenwood Community. The communities re-iterated their desire that any permanent improvements include opening Little Falls Parkway back to four lanes of traffic, and they expressed their concerns with future traffic impacts from the Westbard re-development and planned residential communities. Staff shared the results of ongoing traffic studies on adjacent roadways and observations of the interim road diet.

Coalition for the Capital Crescent Trail (CCCT)

The Coalition for the Capital Crescent Trail is a non-profit volunteer group that was instrumental in development of the Trail and provides continued advocacy for improvements and trail upgrades. Staff met with the Coalition in March 2019 to give an overview of the project and present the Recommended Facility Plan. The Coalition presented their preferred solution of shifting the Capital Crescent Trail to the intersection of Little Falls Parkway and Arlington Road, having trail users cross with the traffic signal, and re-opening all four lanes of traffic. Park staff and the Coalition discussed a potential future study for re-alignment of the Little Falls Parkway to curve into Arlington Road to improve traffic flow. The Coalition also stated their concerns with future traffic from planned development in the Bethesda area.

Pedestrian, Bicycle, and Traffic Safety Advisory Committee (PBTSAC)

The Pedestrian, Bicycle, and Traffic Safety Advisory Committee is a group of citizens, elected officials, and government representatives focused on pedestrian and bicycle safety issues in Montgomery County. During the November 2018 PBTSAC meeting, a vote was held that favored the Recommended Facility Plan, except for removing the median refuge. However, staff believe removal of the median will reduce confusion between drivers and trail users and increase overall safety, particularly in combination with the proposed speed table crossing.

COST ESTIMATE

Design and Construction Costs – Recommended Facility Plan

A summary of design and construction costs for the Recommended Facility Plan (two lane roadway with speed table crossing) is outlined in the table below. A detailed cost estimate is included in the attachments.

Item	Total Cost
Site Preparation and Maintenance of Traffic	\$61,000
Erosion and Sediment Controls	\$36,500
Demolition	\$99,125
Earthwork and Grading	\$14,000
Storm Drainage	\$39,030
Stormwater Management	\$47,520
Asphalt Pavement and Trail	\$67,200
Traffic Signage	\$7,375
Lighting and Electrical	\$48,000
Pavement Markings	\$19,100
Curb and Gutter	\$32,500
Speed Tables and ADA Pads	\$20,000
Miscellaneous Traffic / Shoulder Improvements	\$11,400
Relocate Signal Poles and Mast Arms (Arlington Road)	\$150,000
Landscape Improvements	\$52,250
Hillandale Road Traffic Calming and Narrowing	\$200,000
CONSTRUCTION SUBTOTAL	\$905,000
General Contractor Overhead and Profit @ 8%	\$72,400
Construction Contingency @ 15% of Construction Subtotal	\$135,750
CONSTRUCTION TOTAL	\$1,113,150
Design, Permitting and Long-Term Study	\$181,000
Staff Chargebacks for Detailed Design	\$39,000
Construction Management and Inspections	\$44,500
TOTAL PROJECT COST	\$1,378,000

Operating Budget Impact

The total operating budget impact is estimated at \$8,000 per year. Operating budget costs include landscaping, spot trail repairs at the crossing, maintenance of stormwater management structures and storm drains, and general maintenance and upkeep.

LONG-TERM SITE PLAN STUDY RECOMMENDATION

The community has voiced continual concerns with anticipated traffic increases in the Bethesda area under proposed redevelopment plans. Therefore, staff recommends the traffic counts and analysis be repeated five years after construction of the Recommended Facility Plan, with the assistance of a traffic engineering consultant and coordination with the Montgomery County Department of Transportation. The analysis will assess conditions at Little Falls Parkway, the trail crossing, and adjacent roadways post-improvements. If this analysis shows results that are significantly different than current predictions, the Department of Parks will assess the viability of an “ultimate” site plan solution that prioritizes minimal delays for motorists and trail users. A concept level “ultimate” site plan building on the Recommended Facility Plan has been developed:

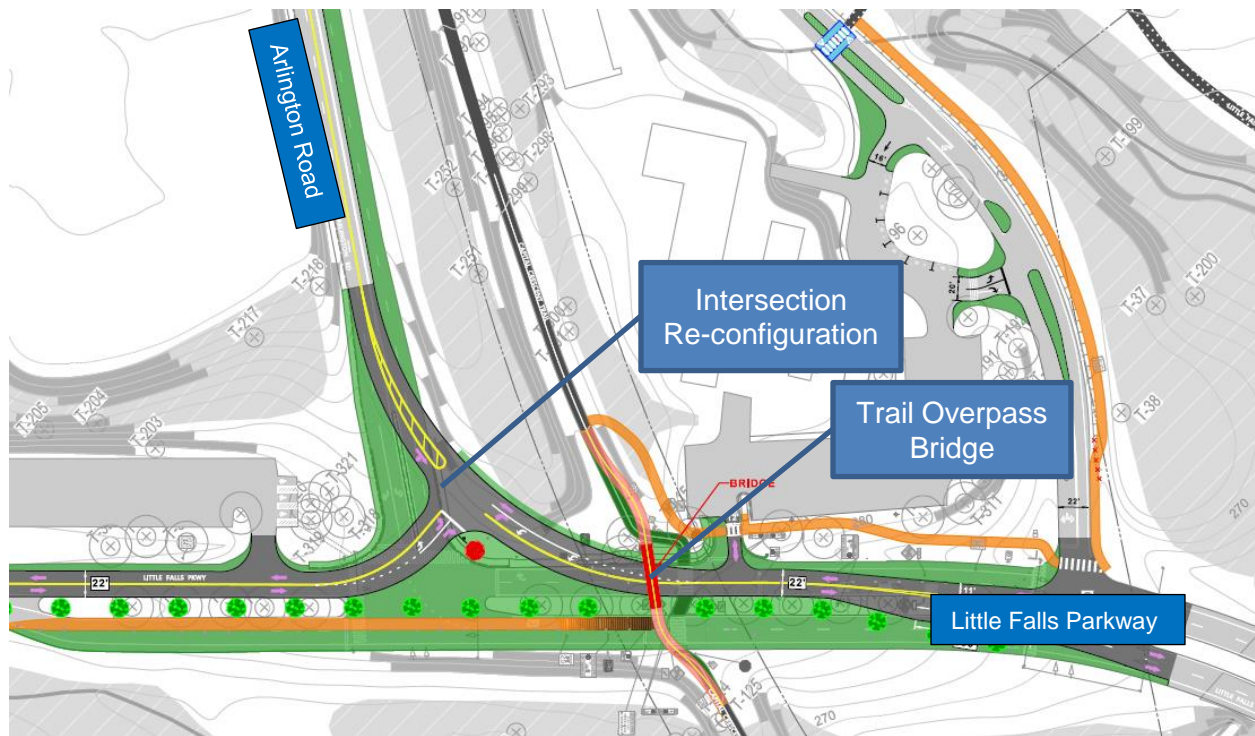


Figure 17: Concept “Ultimate” Site Plan

Key elements of the “Ultimate” Site Plan include:

Capital Crescent Trail Overpass Bridge – A completely separated crossing for trail users minimizes vehicle and trail user delays and maintains safety.

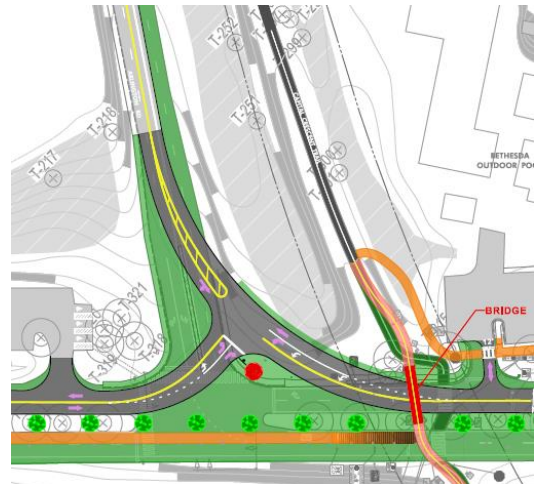


Figure 18: Existing Intersection (Left) and Potential "Ultimate" Re-configuration (Right)

Re-configuration of Intersection of Little Falls Parkway and Arlington Road – The predominant traffic movement for northbound Little Falls Parkway traffic is to turn right onto Arlington Road, while the predominant movement for southbound Arlington Road traffic is to turn left onto Little Falls Parkway. Most vehicles from Glenbrook Road approaching Little Falls Parkway turn right onto the Parkway. Re-configuration of the intersection to merge Little Falls Parkway with Arlington Road, with removal of the traffic signals, will consolidate the dominant turning movements and reduce vehicle travel delays. Glenbrook Road will intersect with the new consolidated roadway with "Stop" sign control. Coordination and approval from Montgomery County Department of Transportation is required to re-configure the intersection as the portion of Arlington Road is managed by the Department of Transportation.

The Recommended Facility Plan cost estimate includes funding to perform the future traffic analysis and study.

CONCLUSION

Staff recommends approval of the Recommended Facility Plan, consisting of the conversion of Little Falls Parkway at the Capital Crescent Trail crossing to a two-lane road with a speed table crossing, and associated improvements. Five years after construction of the Recommended Facility Plan, staff recommends a traffic study and counts be re-done to assess post-improvement conditions along Little Falls Parkway and adjacent neighborhood roadways.

Vision Zero represents a paradigm shift in traffic policy, stating that pedestrian and bicyclist severe injuries or fatalities are unacceptable, and putting the onus on system designers rather than road and trail users to ensure a safe transportation system. By removing the multi-lane threat, slowing vehicles, and reducing confusion between trail users and motorists, the Recommended Facility Plan will significantly increase safety and meet Montgomery County's Vision Zero policy.

Attachments:

- Appendix A: 30% Construction Documents
- Appendix B: Natural Resource Inventory / Forest Stand Delineation Drawings and Approval Letter
- Appendix C: Stormwater Management Concept
- Appendix D: Cost Estimates
- Appendix E: Traffic Engineering Study Data
- Appendix F: Concept Site Plan Sketches
- Appendix G: Correspondence

Appendix A

30% Construction Documents

The Maryland-National Capital Park and Planning Commission

CAPITAL CRESCENT TRAIL

PEDESTRIAN CROSSING IMPROVEMENT PLANS

LITTLE FALLS PARKWAY: FROM ARLINGTON ROAD TO HILLANDALE ROAD

FINAL SCANNED:


























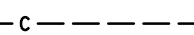
PLAN SCANNED:

PARK CODE: NA

Wednesday, April 17, 2019 AT 12:38 PM

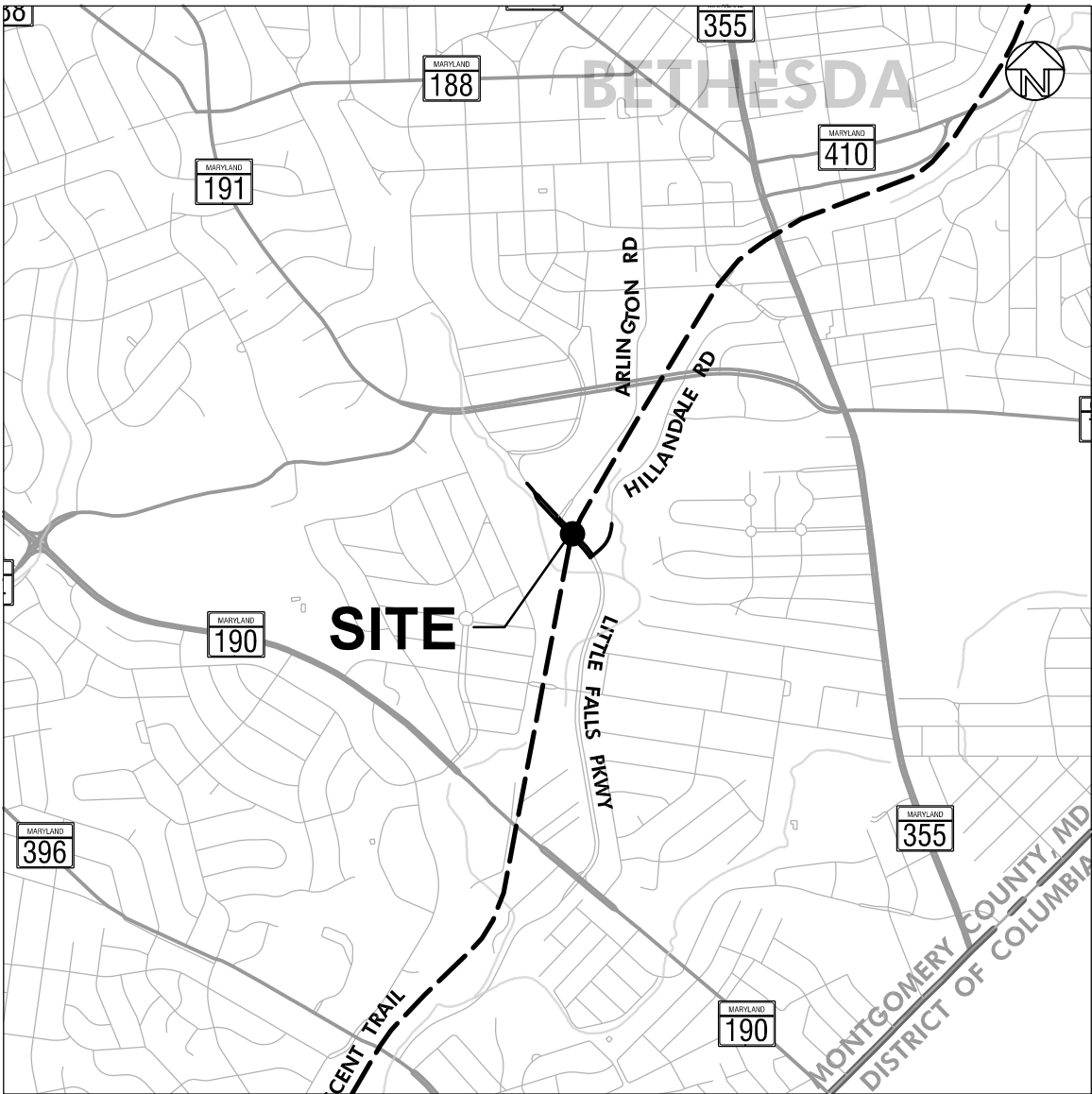
R:\2017\41 CapitalCrescent Trail_MNCPPC_\$148(DWG)30% Plans\p1-000%_CCT.dgn PLOTTED BY: deller -

LEGEND:

	EXISTING CONTOUR
	EXISTING TREE LINE
	EXISTING TREES
	PROPERTY LINE
	EXISTING EDGE OF PAVEMENT
	EXISTING STORM DRAINS
	EXISTING FLEX POSTS
	EXISTING GROUND MOUNTED SIGN
	EXISTING TRAFFIC SIG. MOUNTED SIGNS
	EXISTING SIGNAL POLE AND MAST ARM
	EXISTING SIGNAL HEAD
	EXISTING PEDESTRIAN SIGNAL
	EXISTING GROUND MOUNTED CONTROL CABINET
	EXISTING HAND BOX
	EXISTING METER SERVICE PEDESTAL
	EXISTING SIGNAL POLE MOUNTED LUMINAIRE
	EXISTING STORM DRAIN
	EXISTING ELECTRICAL (UNDERGROUND)
	EXISTING ELECTRICAL (OVERHEAD)
	EXISTING GUARDRAIL
	EXISTING CURB AND GUTTER
	EXISTING DETECTABLE WARNING SURFACE
	LIMITS OF DISTURBANCE
	PROPOSED TRAFFIC FLOW ARROW
	PROPOSED CUT LINE
	PROPOSED FILL LINE

GENERAL NOTES

- Detail drawings and schedules describe construction at given areas. The General Contractor and all subcontractors shall utilize equivalent construction methods in all areas.
- All notes shown on the drawings shall be assumed as typical unless otherwise shown or noted.
- It shall be the duty of the contractor to verify all dimensions and conditions given on the drawings and to report to the M-NCPPC's Construction Manager any error or inconsistency with the actual circumstances in the field before commencing work.
- The General Contractor shall be responsible for any and all items required to provide a site clear of obstructions (above and below grade) and rough grade to specified elevations.
- To All Bidders: The General Contractor and all major subcontractors shall visit the site to familiarize themselves with existing conditions prior to submitting Bid.
- Any work within the public right-of-way, shall be performed in accordance with the latest MCDPWT standards and specifications. This work includes, but not limited to: Repairing; Restoring; and obtaining final inspection approvals.
- Prior to vegetative stabilization, all disturbed areas must be topsoiled per the Montgomery County "Standards and Specifications for Topsoil" (see DPS sheet 4 of 8) and M-NCPPC Technical Specifications. If on-site materials do not meet requirements of topsoil, coordinate with M-NCPPC regarding tilling-in of certified compost to on-site soils in order to meet specifications.
- Any discrepancies, omissions, ambiguities, or conflicts in or among the construction documents or doubt about their meaning, shall be brought to the attention of the M-NCPPC Construction Manager for direction before proceeding with work. If ambiguities exist, the better quality and greater quantity of work shall be bid upon and installed by the Contractor unless otherwise indicated by the M-NCPPC's Construction Manager in writing.



VICINITY MAP
SCALE : 1"= 2000'

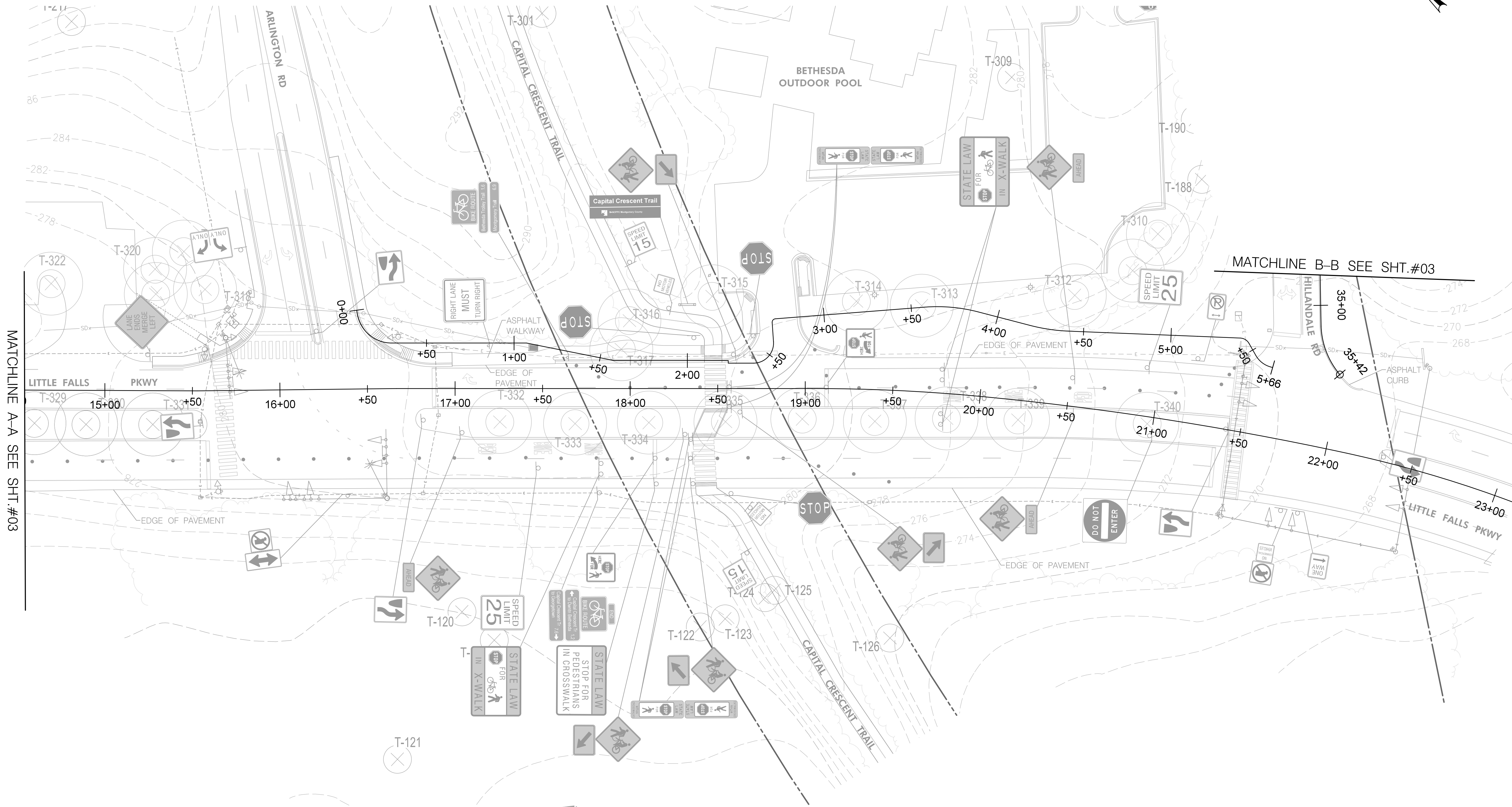
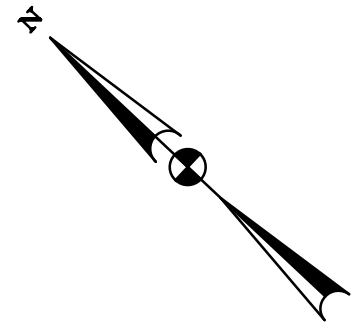
INDEX OF DRAWINGS

DPS SHEET#	SHEET#	DESCRIPTION
	01	TITLE SHEET
	02-03	EXISTING CONDITIONS PLAN
	04-07	TYPICAL SECTIONS
	08-09	ROADWAY PLAN
	10	CONSTRUCTION DETAILS
	11-12	GRADING PLAN
	13	CROSS SECTIONS
	14-15	STORMWATER MANAGEMENT PLAN
	16	SWM BIO-RETENTION DETAILS
	17-18	TRAFFIC SIGNAL PLAN
	19-20	SIGNING AND MARKING PLAN
	21	SIGNING AND MARKING DETAILS
	22-23	LANDSCAPE PLAN
	24	LANDSCAPE DETAILS

30% SUBMITTAL
APRIL 19, 2019

NOT FOR CONSTRUCTION

Sabra & Associates, Inc. Designer's Name 7055 Samuel Morse Drive, #100 Address Columbia, MD 21046 City/State/Zip (443) 741-3500 Telephone Number	DESIGN		Professional Certification. I hereby certify that these documents were prepared or approved by me, and that I am a duly licensed professional engineer under the laws of the State of Maryland. License No. _____ Expiration Date _____ 	 The Maryland-National Capital Park and Planning Commission Montgomery County Department of Parks 9500 Brunett Avenue Silver Spring, Maryland 20901 (301) 495-2535	REVIEW AND APPROVAL		ISSUED FOR PROCUREMENT ON _____		Cover Sheet Capital Crescent Trail SCALE: N/A	DWG. # <u> 1 </u> of <u> 1 </u> SHT. # <u> 01 </u> of <u> 24 </u>
	Landscape Architect	Date			Checked By:	Project Manager	Date	REVISIONS		
	Architect	Date	Checked By:	Construction Manager	Date	Rev. No.	Date	Description		
	Engineer	Date	Checked By:	Park Manager	Date					
	CK	04/2019								
	Drawn By	Date	Checked By:							



FINAL SCANNED:

PLAN SCANNED:

PARK CODE: NA

Wednesday, April 17, 2019 AT 12:43 PM

R:\2017\41 CapitalCrescent Trail_MNCP\PCP_30% Plans\p10-Ed01_DCC.dgn PLOTTED BY: seller

<div>Sabra & Associates, Inc. Designer's Name 7055 Samuel Morse Drive, #100 Address Columbia, MD 21046 City/State/Zip (443) 741-3500 Telephone Number</div>		<table><tr><th colspan="3">DESIGN</th></tr><tr><td>Landscape Architect</td><td>Date</td><td>Checked By:</td></tr><tr><td>Architect</td><td>Date</td><td>Checked By:</td></tr><tr><td>BW</td><td>04/2019</td><td></td></tr><tr><td>Engineer</td><td>Date</td><td>Checked By:</td></tr><tr><td>CK</td><td>04/2019</td><td></td></tr><tr><td>Drawn By</td><td>Date</td><td>Checked By:</td></tr></table>		DESIGN			Landscape Architect	Date	Checked By:	Architect	Date	Checked By:	BW	04/2019		Engineer	Date	Checked By:	CK	04/2019		Drawn By	Date	Checked By:	<div>Professional Certification. I hereby certify that these documents were prepared or approved by me, and that I am a duly licensed professional engineer under the laws of the State of Maryland. License No. _____ Expiration Date _____</div> <div>PROFESSIONAL STAMP</div>		<div>The Maryland-National Capital Park and Planning Commission Montgomery County Department of Parks 9500 Brunett Avenue Silver Spring, Maryland 20901 (301) 495-2535</div>		<table><tr><th colspan="2">REVIEW AND APPROVAL</th></tr><tr><td>Project Manager</td><td>Date</td></tr><tr><td>Construction Manager</td><td>Date</td></tr><tr><td>Park Manager</td><td>Date</td></tr></table> <table><tr><th colspan="3">ISSUED FOR PROCUREMENT ON _____</th></tr><tr><th colspan="3">REVISIONS</th></tr><tr><th>Rev. No.</th><th>Date</th><th>Description</th></tr><tr><td> </td><td> </td><td> </td></tr><tr><td> </td><td> </td><td> </td></tr><tr><td> </td><td> </td><td> </td></tr><tr><td> </td><td> </td><td> </td></tr></table>		REVIEW AND APPROVAL		Project Manager	Date	Construction Manager	Date	Park Manager	Date	ISSUED FOR PROCUREMENT ON _____			REVISIONS			Rev. No.	Date	Description													<div><h2>Existing Conditions Plan</h2><h3>Capital Crescent Trail</h3><p>SCALE: 1"=30'</p></div>		<div>DWG. # <u>1</u> of <u>2</u></div> <div>SHT. # <u>02</u> of <u>24</u></div>	
DESIGN																																																															
Landscape Architect	Date	Checked By:																																																													
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BW	04/2019																																																														
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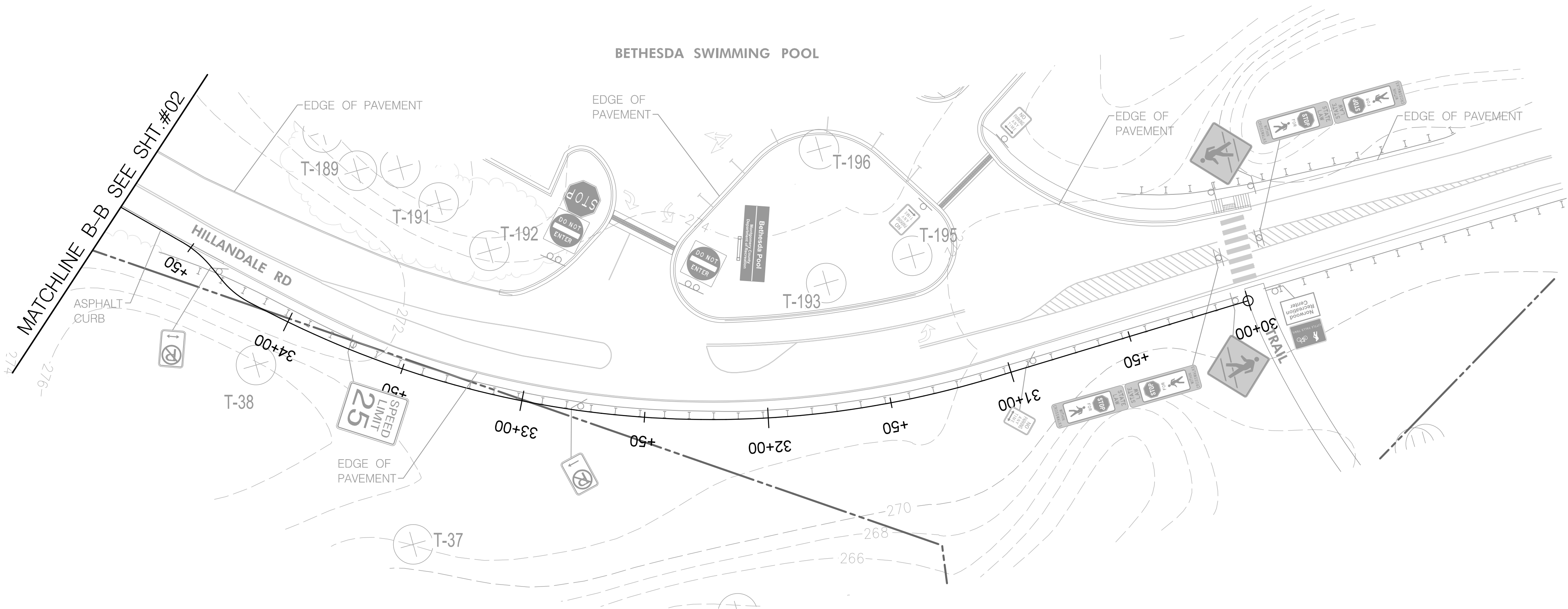
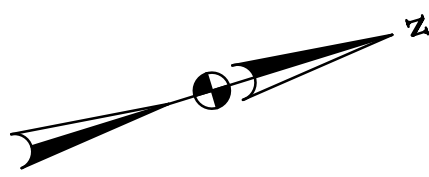
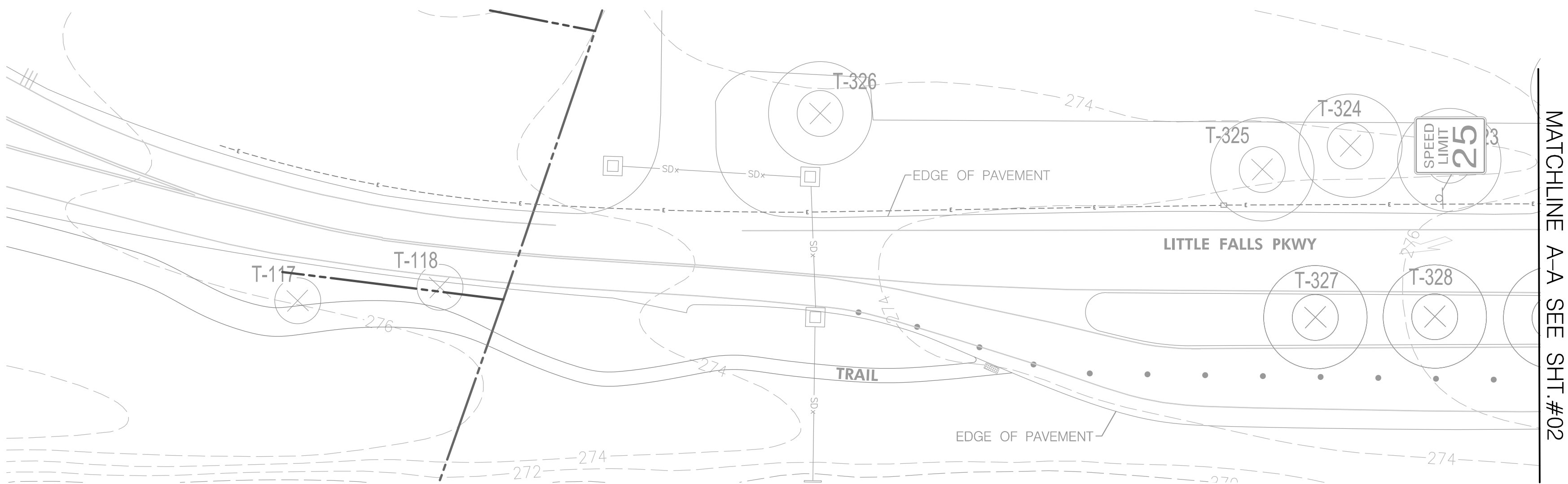
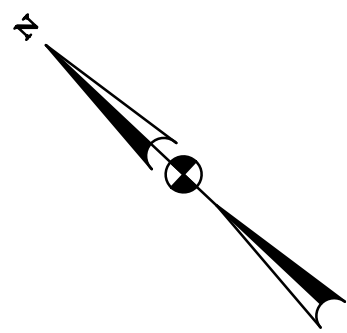
FINAL SCANNED:

PLAN SCANNED:

PARK CODE: NA

Wednesday, April 17, 2019 AT 01:29 PM

R:\2017\41 CapitalCrescent Trail_MNCPPC_\$148(DWG\$30% Plans)\p10-Ex02_CCT.dgnPLOTTED BY: seller -



Sabra & Associates, Inc. Designer's Name 7055 Samuel Morse Drive, #100 Address Columbia, MD 21046 City/State/Zip (443) 741-3500 Telephone Number	DESIGN		
	Landscape Architect	Date	Checked By:
	Architect	Date	Checked By:
	BW	04/2019	
	Engineer	Date	Checked By:
	CK	04/2019	
	Drawn By	Date	Checked By:

Professional Certification. I hereby certify that these documents were prepared or approved by me, and that I am a duly licensed professional engineer under the laws of the State of Maryland.

License No. _____

Expiration Date _____

PROFESSIONAL
STAMP

**The Maryland-National Capital
Park and Planning Commission**

Montgomery County Department of Parks
9500 Brunett Avenue
Silver Spring, Maryland 20901
(301) 495-2535

REVIEW AND APPROVAL	
Project Manager	Date
Construction Manager	Date
Park Manager	Date

ISSUED FOR PROCUREMENT ON _____		
REVISIONS		
Rev. No.	Date	Description

Existing Conditions Plan Capital Crescent Trail

SCALE: 1"=30'

DWG. # 2 of 2

SHT. # 03 of 24

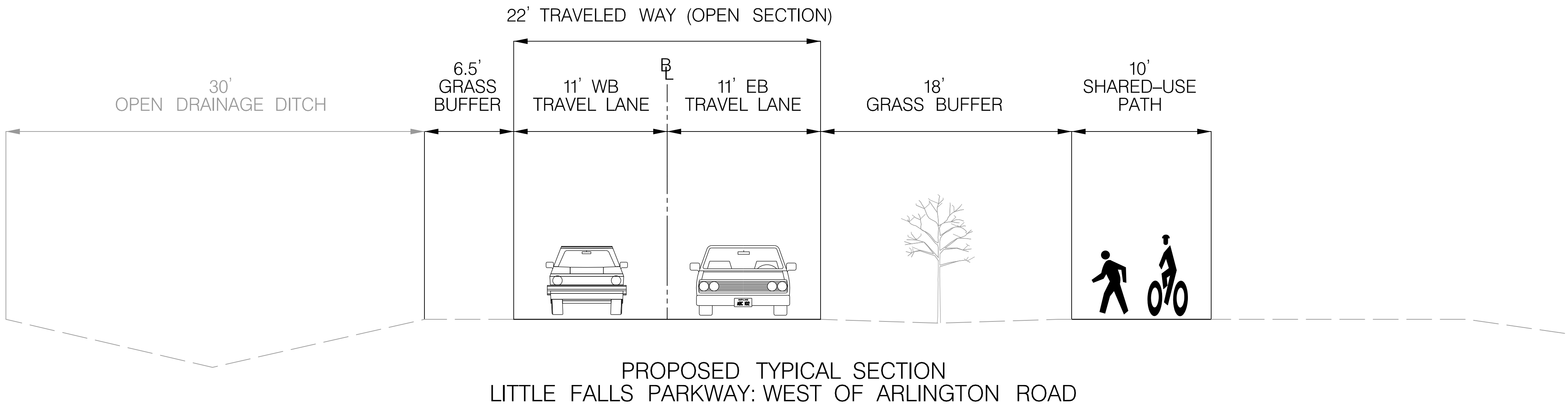
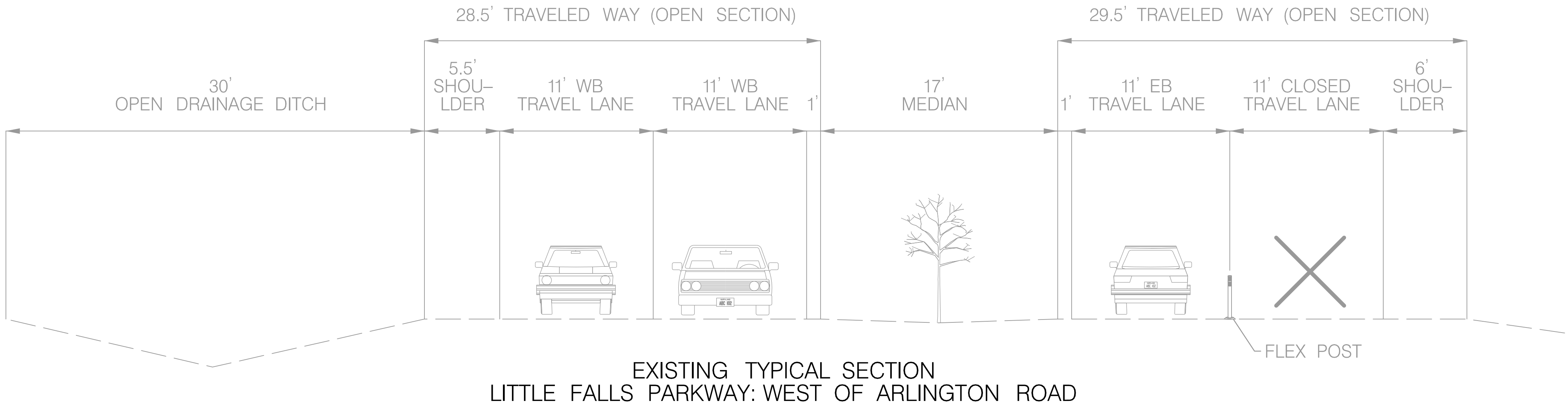
FINAL SCANNED:

PLAN SCANNED:

PARK CODE: NA

Thursday, April 04, 2019 AT 10:46 AM

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Sabra & Associates, Inc.	DESIGN		
Designer's Name			
7055 Samuel Morse Drive, #100	Landscape Architect	Date	Checked By:
Address	Architect	Date	Checked By:
Columbia, MD 21046	BW	04/2019	
City/State/Zip	Engineer	Date	Checked By:
(443) 741-3500	CK	04/2019	
Telephone Number	Drawn By	Date	Checked By:

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Expiration Date _____		



The Maryland-National Capital Park and Planning Commission
Montgomery County Department of Parks
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Silver Spring, Maryland 20901
(301) 495-2535

REVIEW AND APPROVAL		ISSUED FOR PROCUREMENT ON _____	
		REVISIONS	
Project Manager	Date	Rev. No.	Date
Construction Manager	Date		
Park Manager	Date		

Typical Sections
Capital Crescent Trail

SCALE: 1"=5'

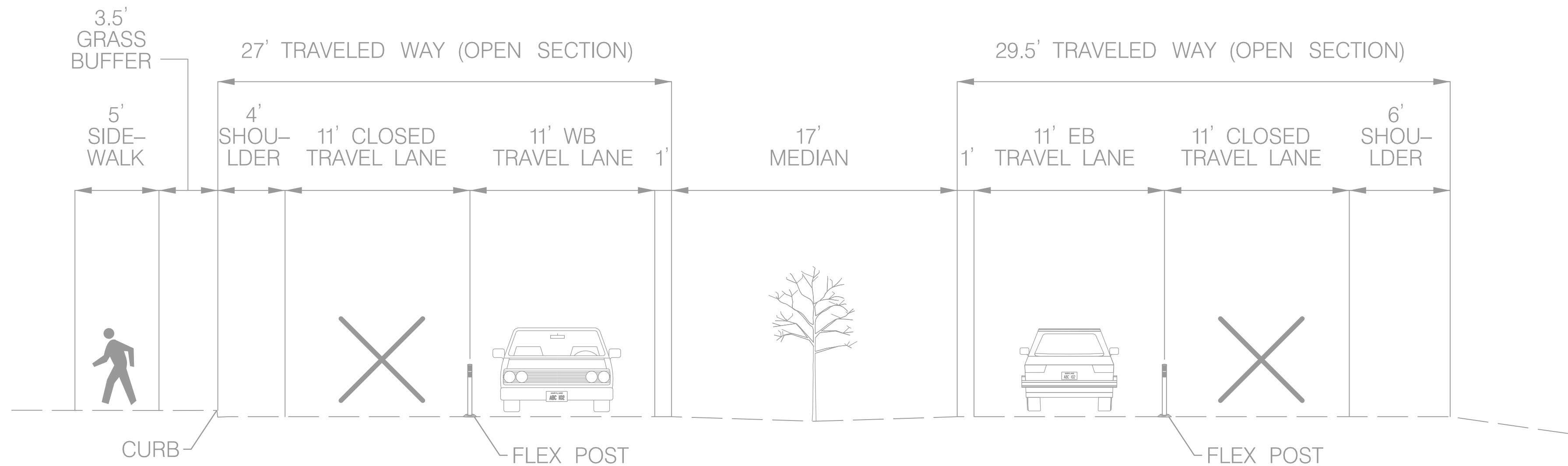
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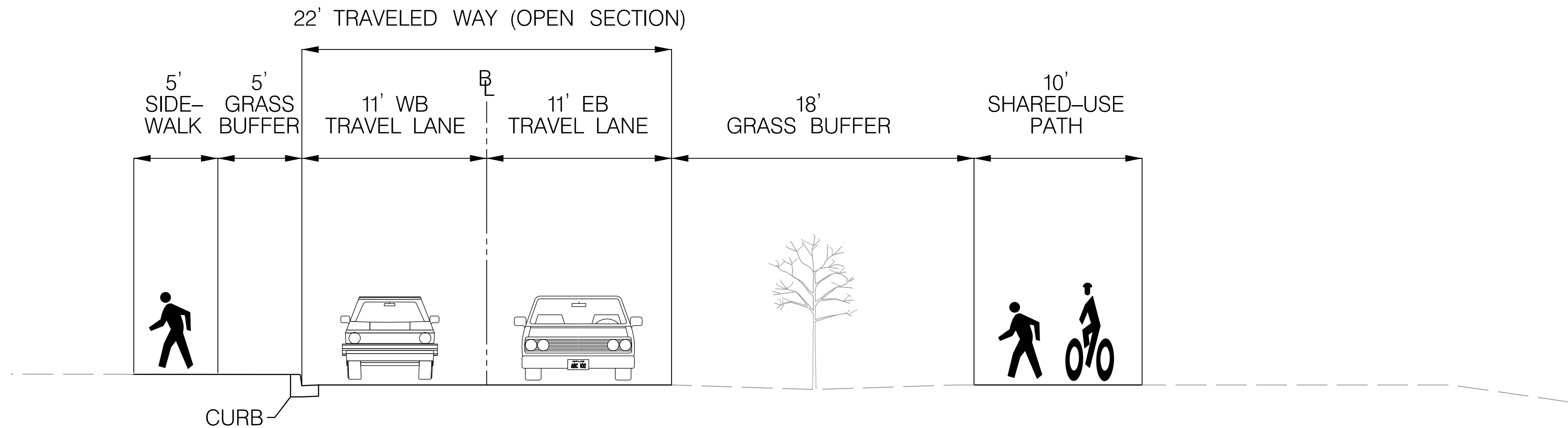
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Wednesday, April 17, 2019 AT 01:50 PM

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EXISTING TYPICAL SECTION
LITTLE FALLS PARKWAY: BETWEEN ARLINGTON ROAD AND BETHESDA SWIMMING POOL ENTRANCE



PROPOSED TYPICAL SECTION
LITTLE FALLS PARKWAY: BETWEEN ARLINGTON ROAD AND BETHESDA SWIMMING POOL ENTRANCE

<div>Sabra & Associates, Inc. Designer's Name 7055 Samuel Morse Drive, #100 Address Columbia, MD 21046 City/State/Zip (443) 741-3500 Telephone Number</div>			<div>DESIGN Landscape Architect Date Checked By: Architect Date Checked By: Engineer Date Checked By: CK 04/2019 Drawn By Date Checked By:</div>			<div>Professional Certification. I hereby certify that these documents were prepared or approved by me, and that I am a duly licensed professional engineer under the laws of the State of Maryland. License No. _____ Expiration Date _____</div> <div>PROFESSIONAL STAMP</div>			<div>The Maryland-National Capital Park and Planning Commission Montgomery County Department of Parks 9500 Brunett Avenue Silver Spring, Maryland 20901 (301) 495-2535</div>			<div>REVIEW AND APPROVAL Project Manager Construction Manager Park Manager</div>			<div>ISSUED FOR PROCUREMENT ON _____ REVISIONS Rev. No. Date Description</div>			<div>Typical Sections Capital Crescent Trail SCALE: 1"=5'</div>			<div>DWG. # 2 of 4 SHT. # 05 of 24</div>		
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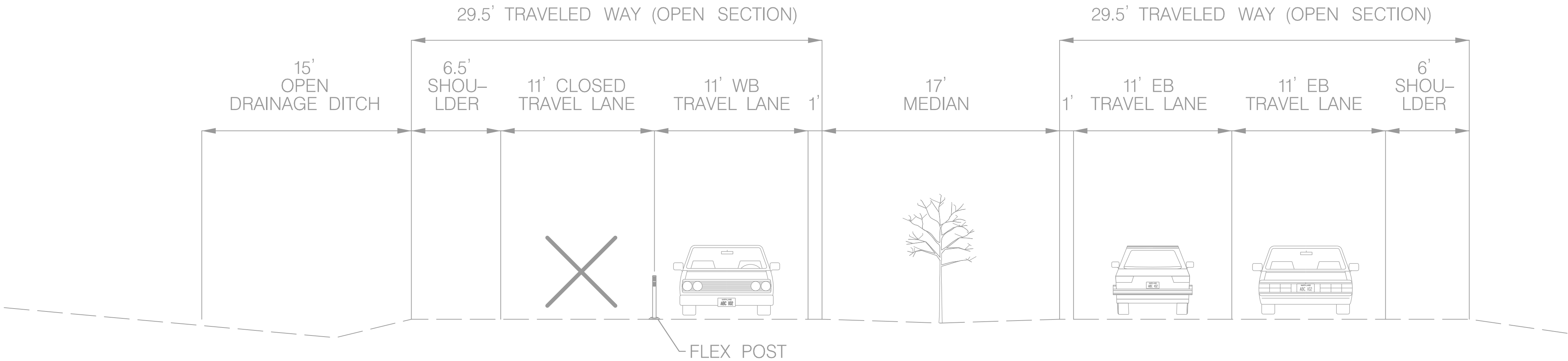
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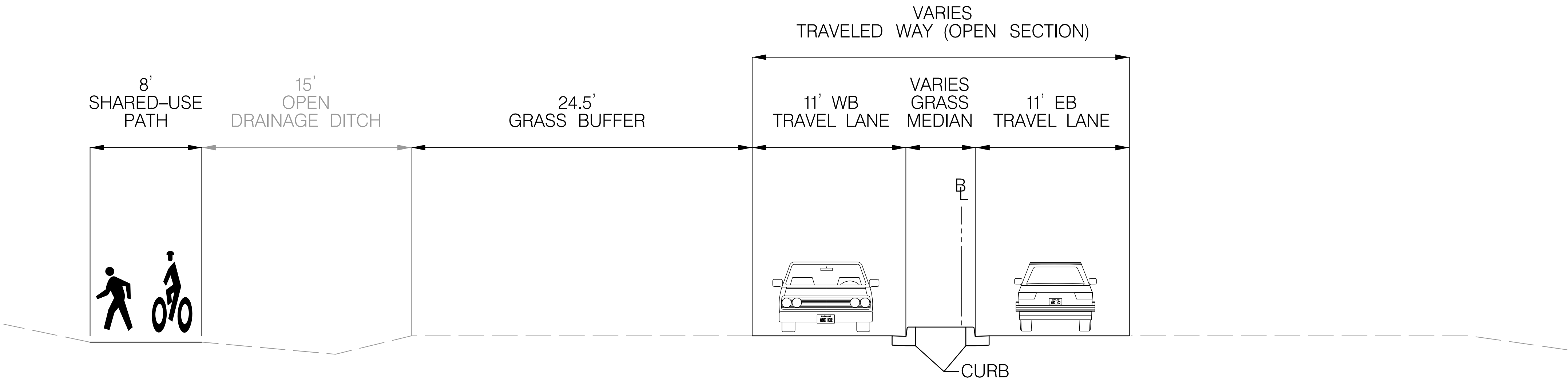
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Wednesday, April 17, 2019 AT 01:32 PM


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EXISTING TYPICAL SECTION
LITTLE FALLS PARKWAY: BETWEEN BETHESDA SWIMMING POOL ENTRANCE AND HILLANDALE ROAD



PROPOSED TYPICAL SECTION
LITTLE FALLS PARKWAY: BETWEEN BETHESDA SWIMMING POOL ENTRANCE AND HILLANDALE ROAD

<div>Sabra & Associates, Inc. Designer's Name 7055 Samuel Morse Drive, #100 Address Columbia, MD 21046 City/State/Zip (443) 741-3500 Telephone Number</div>			<table><tr><th colspan="3">DESIGN</th></tr><tr><td>Landscape Architect</td><td>Date</td><td>Checked By:</td></tr><tr><td>Architect</td><td>Date</td><td>Checked By:</td></tr><tr><td>BW</td><td>04/2019</td><td></td></tr><tr><td>Engineer</td><td>Date</td><td>Checked By:</td></tr><tr><td>CK</td><td>04/2019</td><td></td></tr><tr><td>Drawn By</td><td>Date</td><td>Checked By:</td></tr></table>			DESIGN			Landscape Architect	Date	Checked By:	Architect	Date	Checked By:	BW	04/2019		Engineer	Date	Checked By:	CK	04/2019		Drawn By	Date	Checked By:	<div>Professional Certification. I hereby certify that these documents were prepared or approved by me, and that I am a duly licensed professional engineer under the laws of the State of Maryland. License No. _____ Expiration Date _____</div> <div>PROFESSIONAL STAMP</div>			<div>The Maryland-National Capital Park and Planning Commission Montgomery County Department of Parks 9500 Brunett Avenue Silver Spring, Maryland 20901 (301) 495-2535</div>			<table><tr><th colspan="2">REVIEW AND APPROVAL</th></tr><tr><td>Project Manager</td><td>Date</td></tr><tr><td>Construction Manager</td><td>Date</td></tr><tr><td>Park Manager</td><td>Date</td></tr></table>			REVIEW AND APPROVAL		Project Manager	Date	Construction Manager	Date	Park Manager	Date	<table><tr><th colspan="3">ISSUED FOR PROCUREMENT ON _____</th></tr><tr><th colspan="3">REVISIONS</th></tr><tr><th>Rev. No.</th><th>Date</th><th>Description</th></tr><tr><td> </td><td> </td><td> </td></tr><tr><td> </td><td> </td><td> </td></tr><tr><td> </td><td> </td><td> </td></tr><tr><td> </td><td> </td><td> </td></tr></table>			ISSUED FOR PROCUREMENT ON _____			REVISIONS			Rev. No.	Date	Description													<div><h2>Typical Sections</h2><h3>Capital Crescent Trail</h3><p>SCALE: 1"=5'</p></div>			<div>DWG. # <u>3</u> of <u>4</u> SHT. # <u>06</u> of <u>24</u></div>		
DESIGN																																																																									
Landscape Architect	Date	Checked By:																																																																							
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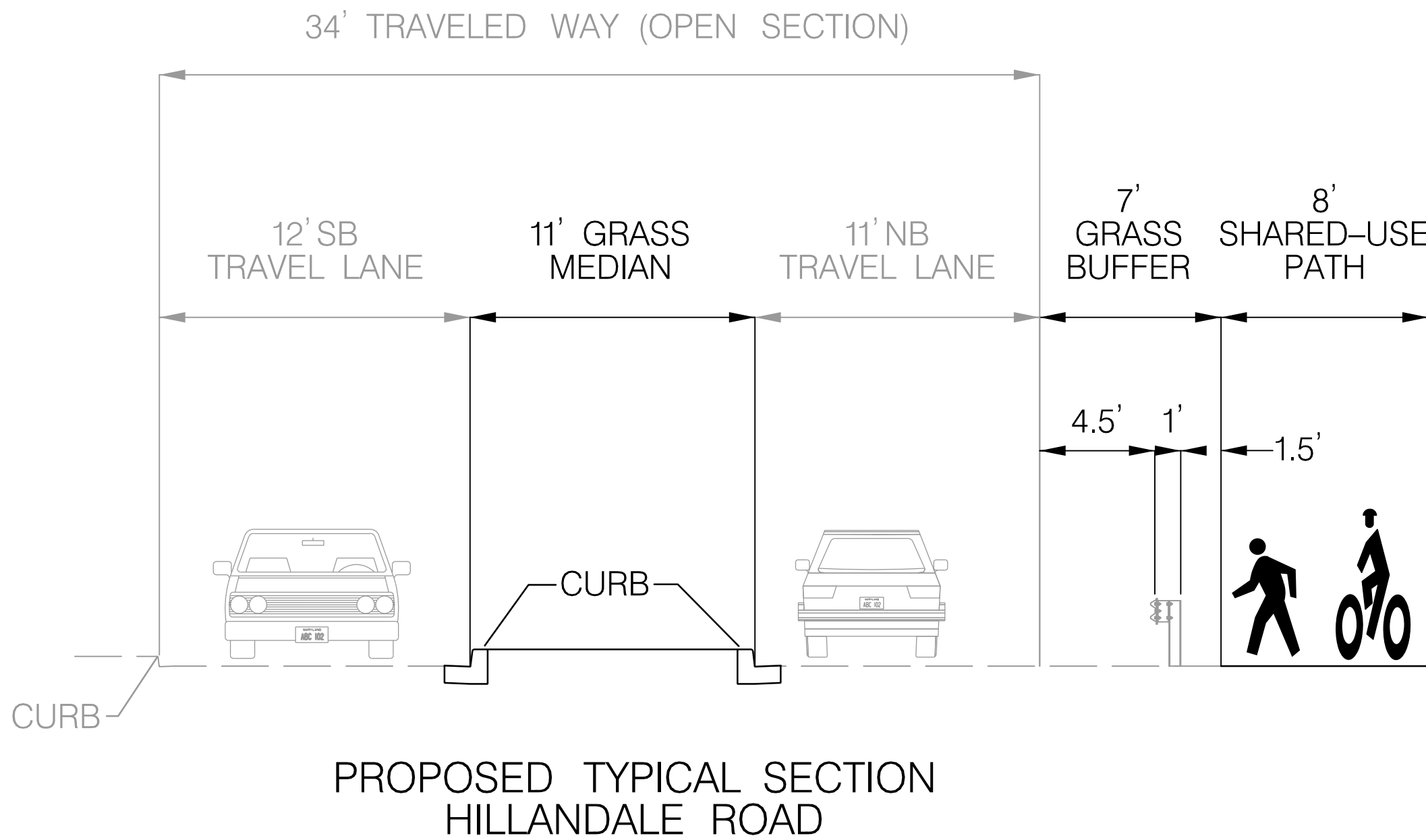
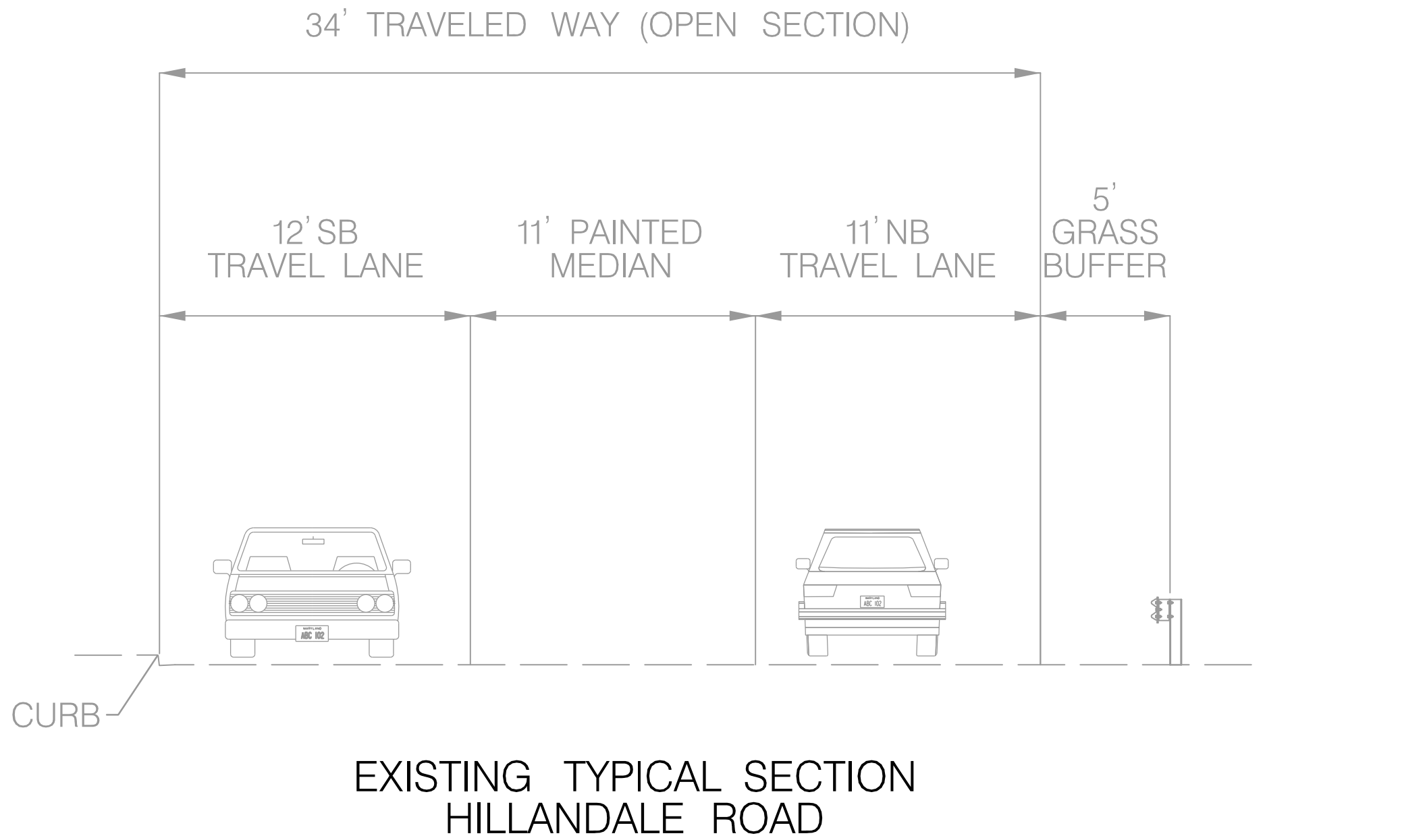
FINAL SCANNED:

PLAN SCANNED:

PARK CODE: NA

Wednesday, April 17, 2019 AT 01:53 PM

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Sabra & Associates, Inc.	DESIGN		
Designer's Name			
7055 Samuel Morse Drive, #100	Landscape Architect	Date	Checked By:
Address			
Columbia, MD 21046	Architect	Date	Checked By:
City/State/Zip	BW	04/2019	
(443) 741-3500	Engineer	Date	Checked By:
Telephone Number	CK	04/2019	
	Drawn By	Date	Checked By:

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License No. _____		
Expiration Date _____		



The Maryland-National Capital
Park and Planning Commission
Montgomery County Department of Parks
9500 Brunett Avenue
Silver Spring, Maryland 20901
(301) 495-2535

REVIEW AND APPROVAL		ISSUED FOR PROCUREMENT ON _____	
		REVISIONS	
Project Manager	Date	Rev. No.	Date
Construction Manager	Date		
Park Manager	Date		

Typical Sections
Capital Crescent Trail

SCALE: 1"=5'

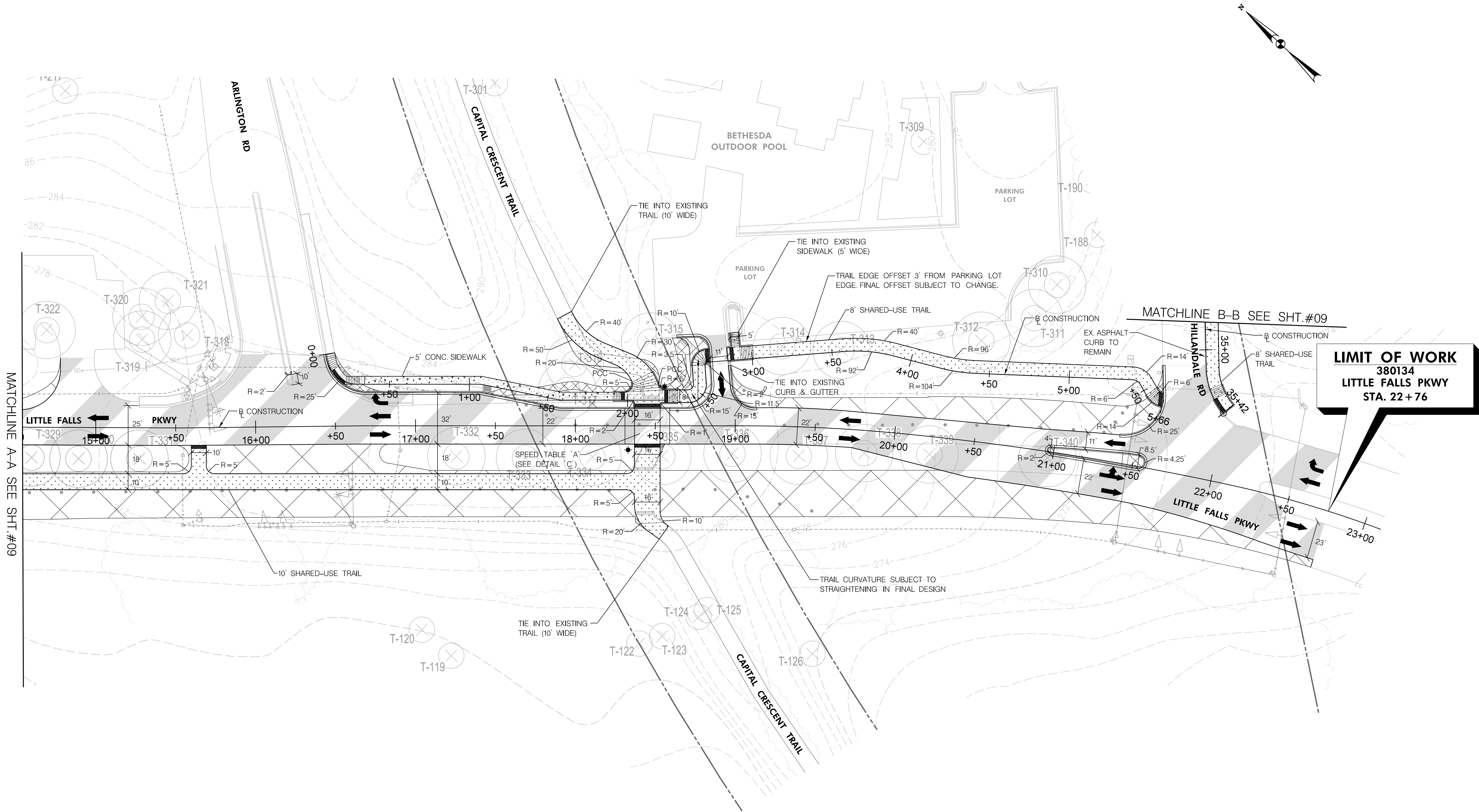
FINAL SCANNED:

PLAN SCANNED:

PARK CODE: NA

Wednesday, April 17, 2019 AT 01:52 PM

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LIMIT OF WORK
380134
LITTLE FALLS PKWY
STA. 22 + 76

PROPOSED LEGEND	
	SHARED USE PATH
	5 INCH CONCRETE SIDEWALK
	GRIND AND RESURFACE EXISTING PAVEMENT
	EXISTING SIDEWALK TO BE REMOVED
	EXISTING PAVEMENT TO BE REMOVED
	PROPOSED CURB AND GUTTER

Sabra & Associates, Inc. Designer's Name 7055 Samuel Morse Drive, #100 Address Columbia, MD 21046 City/State/Zip (443) 741-3500 Telephone Number	DESIGN			Professional Certification. I hereby certify that these documents were prepared or approved by me, and that I am a duly licensed professional engineer under the laws of the State of Maryland. License No. _____ Expiration Date _____ <div>PROFESSIONAL STAMP</div>
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	Drawn By	Date	Checked By:	



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(301) 495-2535

REVIEW AND APPROVAL		ISSUED FOR PROCUREMENT ON _____	
		REVISIONS	
Rev. No.	Date	Description	
Project Manager	Date		
Construction Manager	Date		
Park Manager	Date		

Roadway Plan Capital Crescent Trail

SCALE: 1"=30'

DWG. # 1 of 3

SHT. # 08 of 24

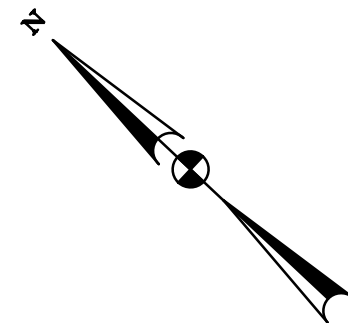
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PLAN SCANNED:

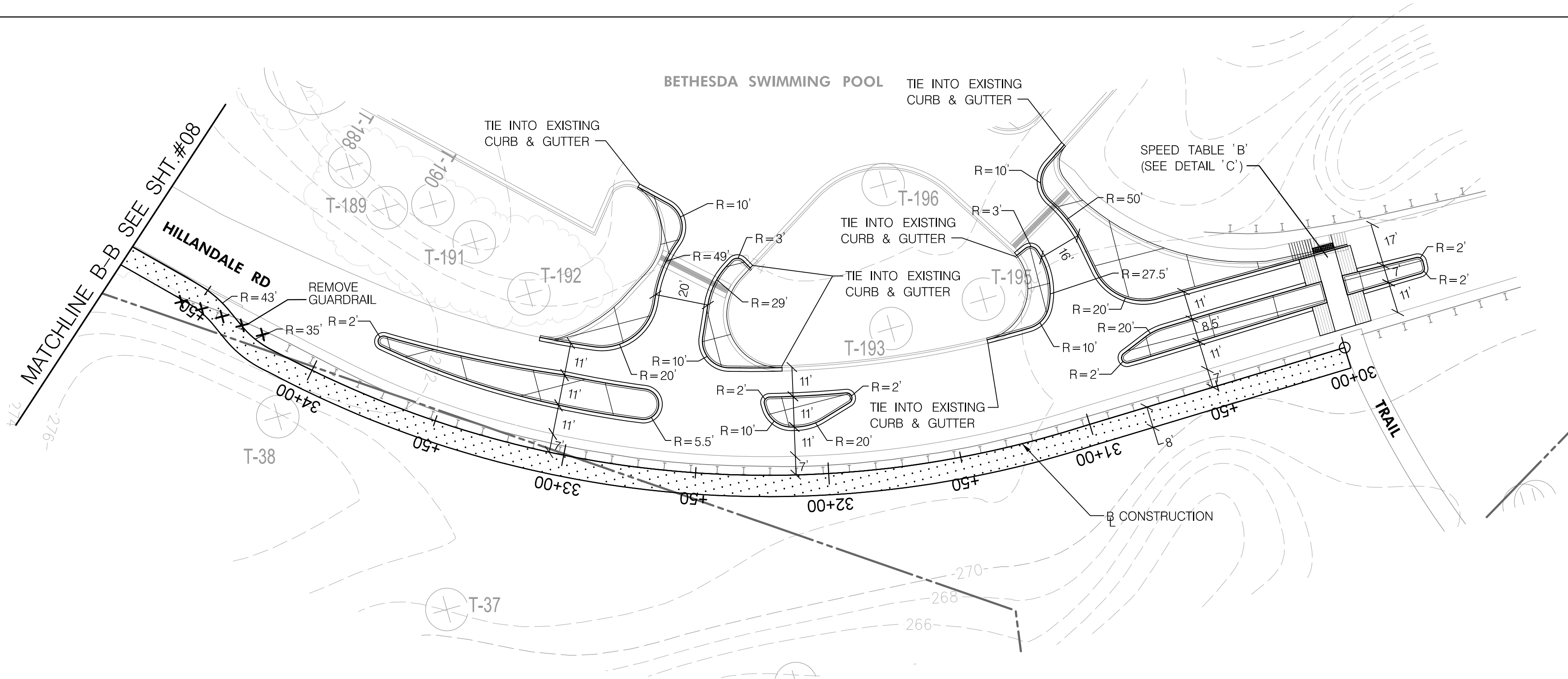
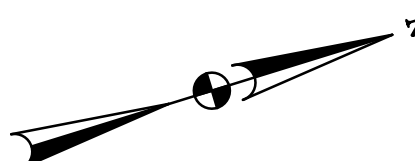
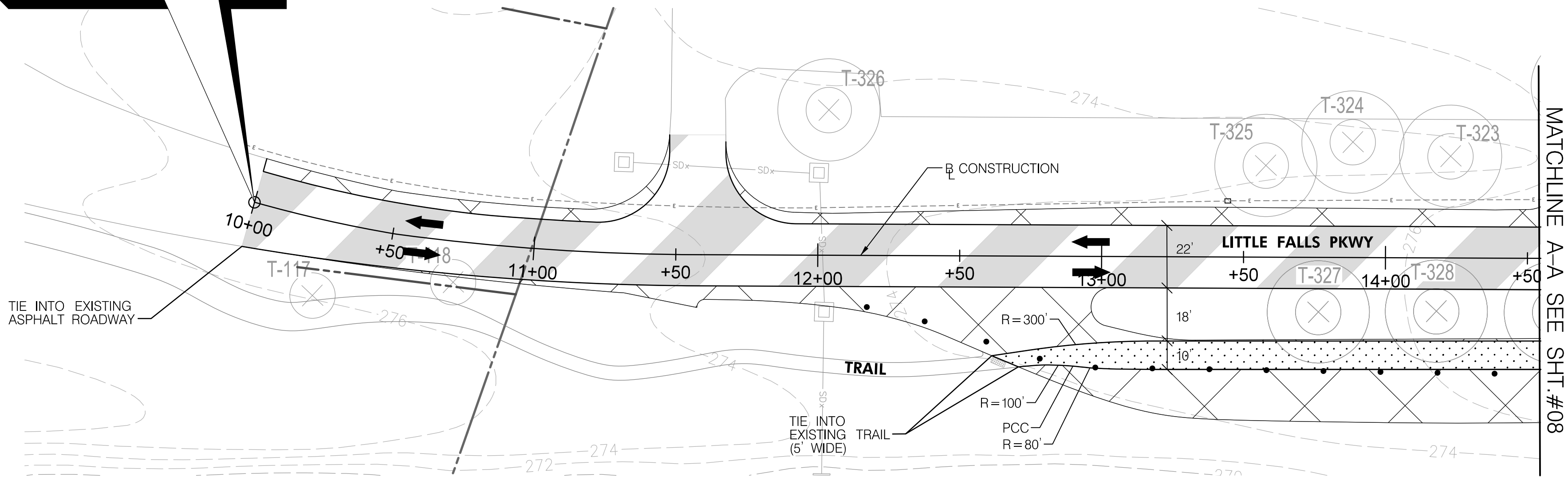
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Wednesday, April 17, 2019 AT 01:54 PM

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LIMIT OF WORK
380134
LITTLE FALLS PKWY
STA. 10 + 00



PROPOSED LEGEND

- SHARED USE PATH
- 5 INCH CONCRETE SIDEWALK
- GRIND AND RESURFACE EXISTING PAVEMENT
- EXISTING SIDEWALK TO BE REMOVED
- EXISTING PAVEMENT TO ME REMOVED
- PROPOSED CURB AND GUTTER

Sabra & Associates, Inc.	DESIGN			Professional Certification. I hereby certify that these documents were prepared or approved by me, and that I am a duly licensed professional engineer under the laws of the State of Maryland. License No. _____ Expiration Date _____
Designer's Name	Landscape Architect	Date	Checked By:	
7055 Samuel Morse Drive, #100				
Address	Architect	Date	Checked By:	
Columbia, MD 21046	BW	04/2019		
City/State/Zip	Engineer	Date	Checked By:	
(443) 741-3500	CK	04/2019		
Telephone Number	Drawn By	Date	Checked By:	



The Maryland-National Capital Park and Planning Commission
Montgomery County Department of Parks
9500 Brunett Avenue
Silver Spring, Maryland 20901
(301) 495-2535

REVIEW AND APPROVAL	
Project Manager	Date
Construction Manager	Date
Park Manager	Date

ISSUED FOR PROCUREMENT ON _____		
REVISIONS		
Rev. No.	Date	Description

Roadway Plan
Capital Crescent Trail

SCALE: 1"=30'

DWG. # 2 of 3

SHT. # 09 of 24

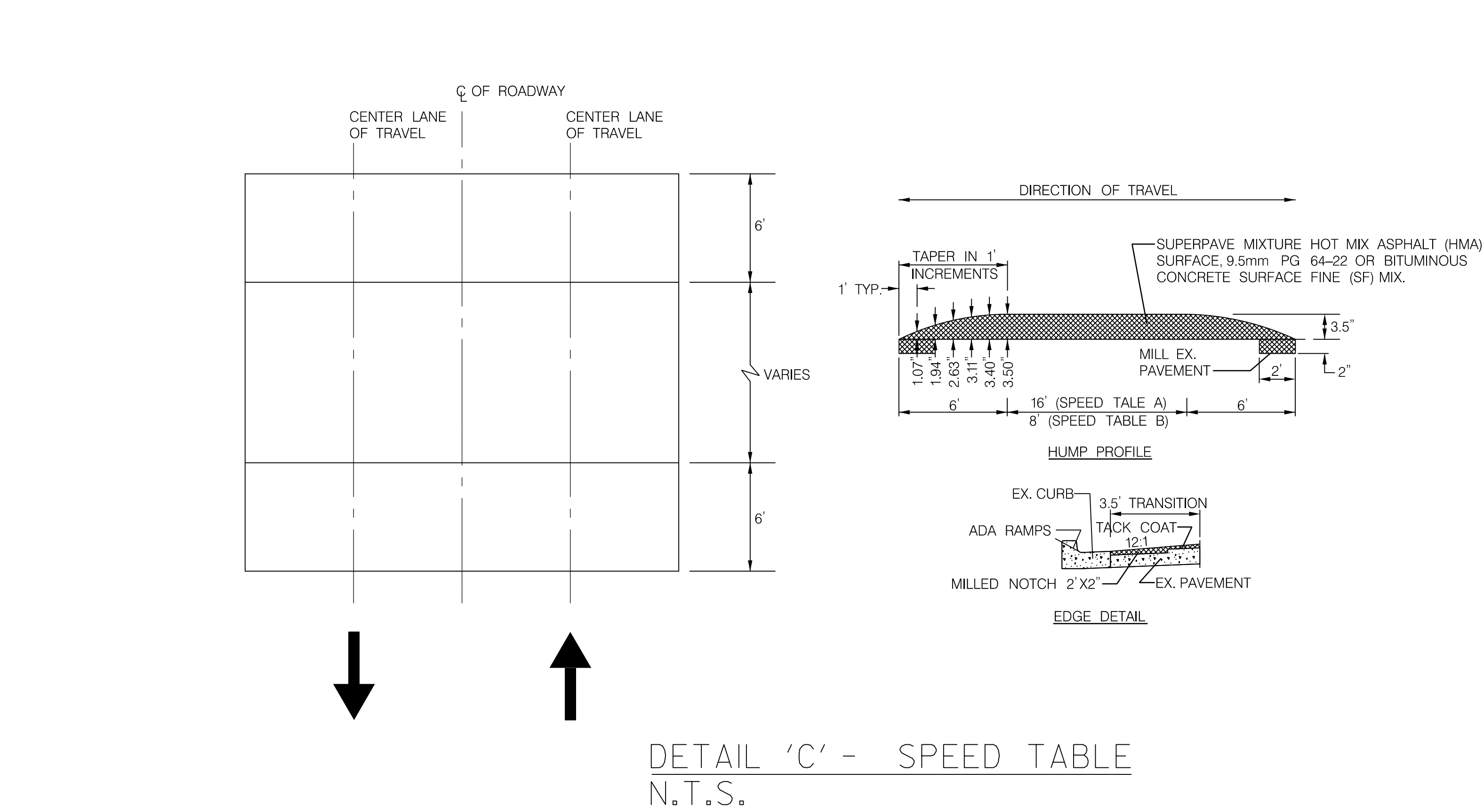
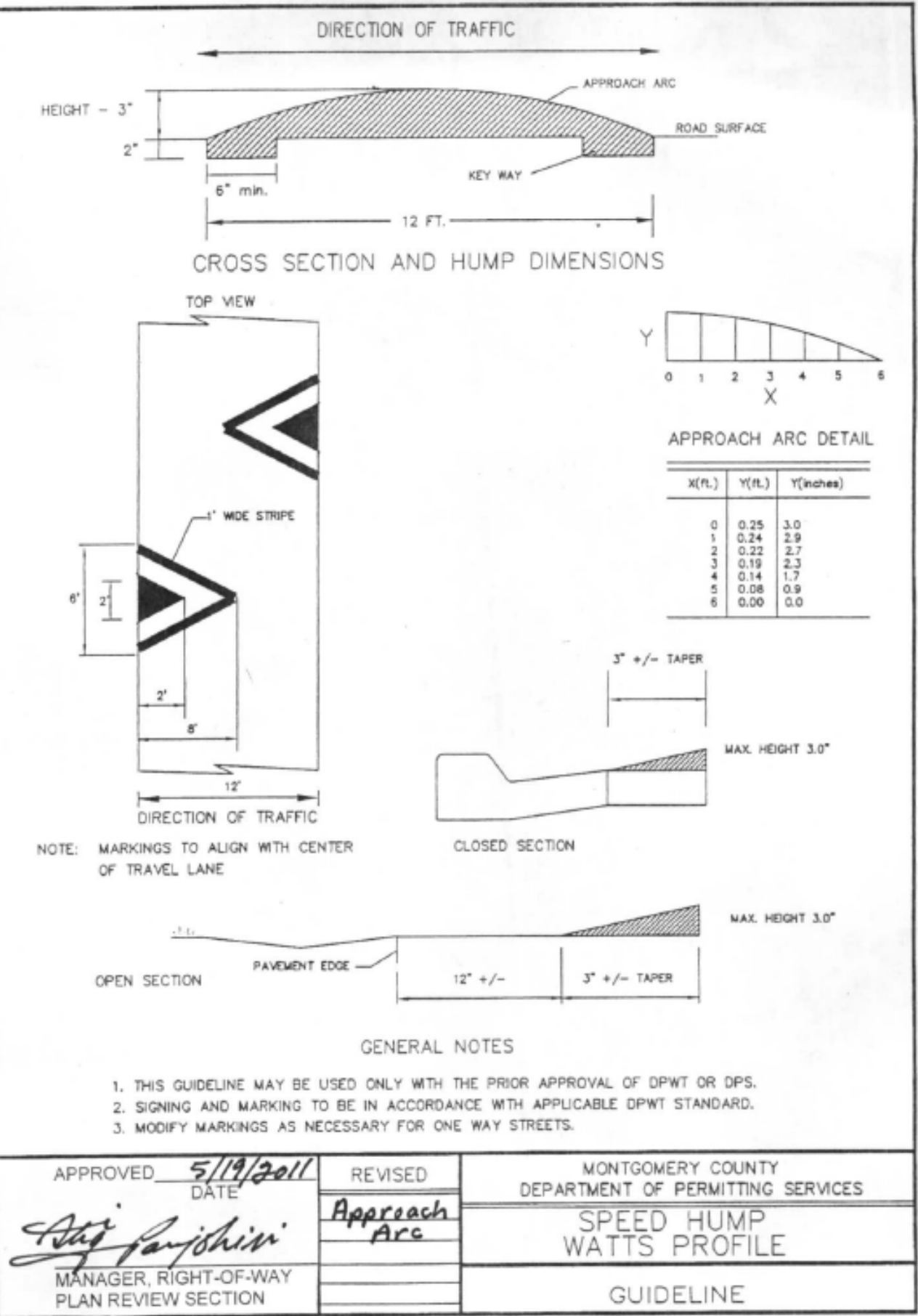
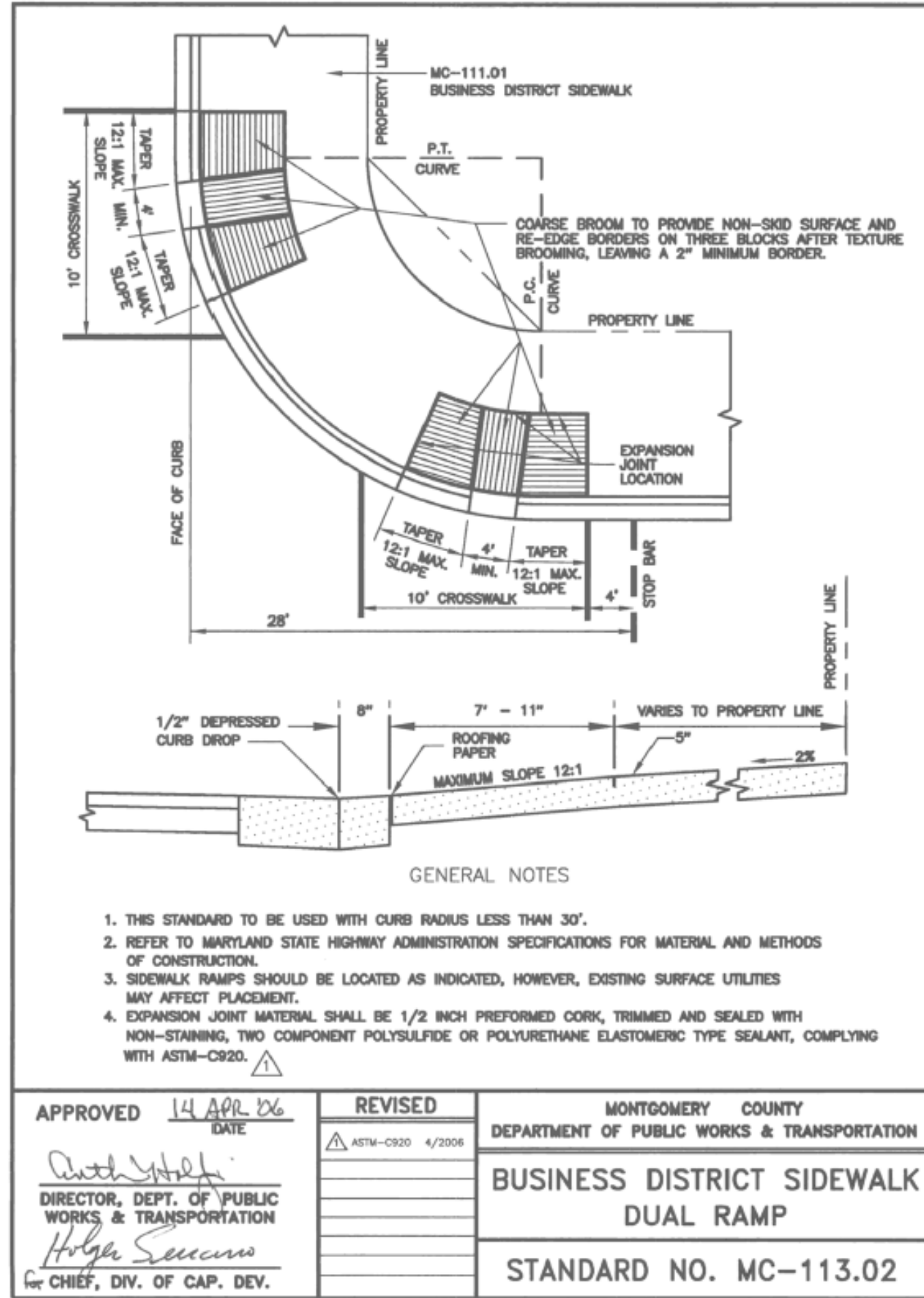
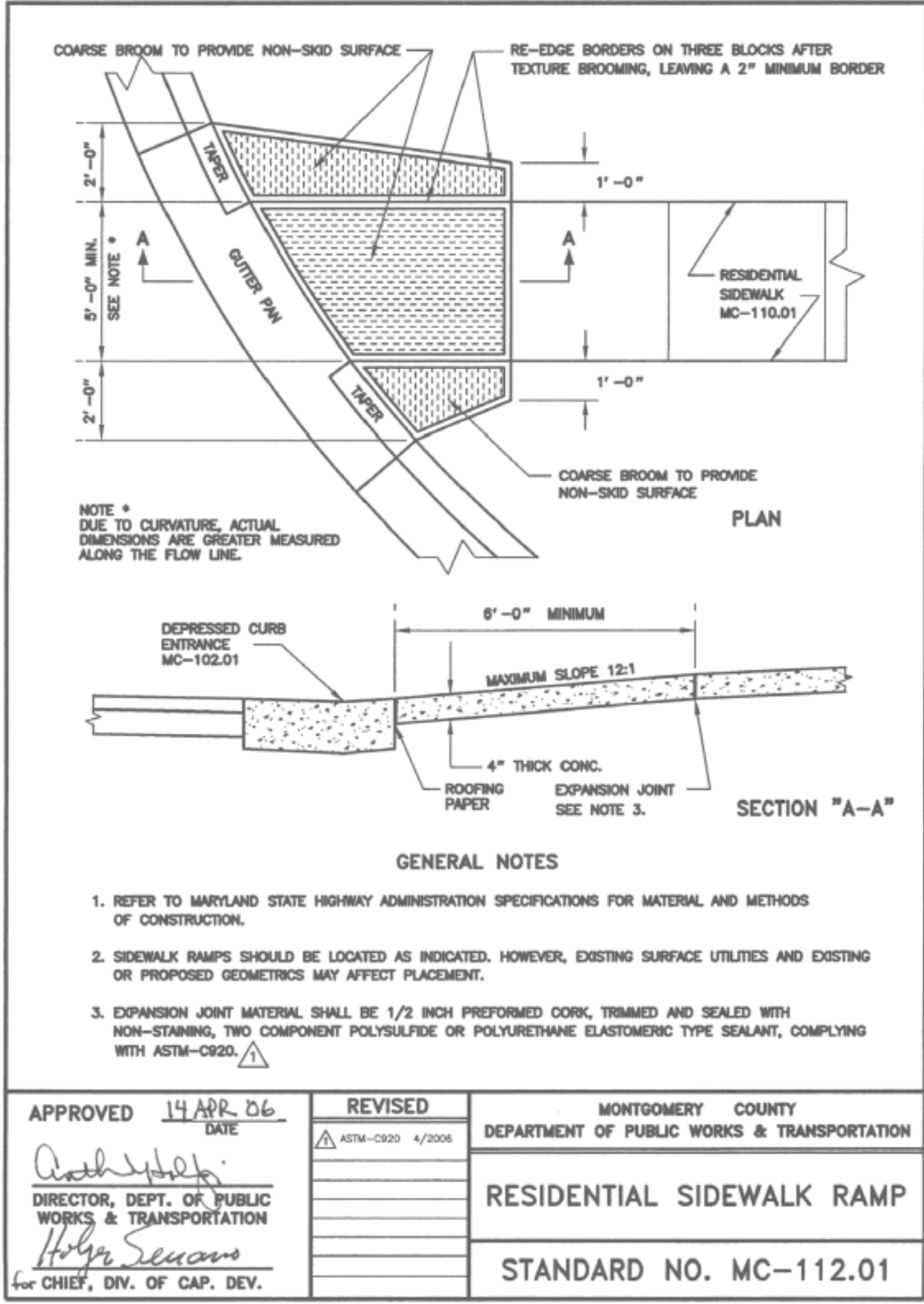
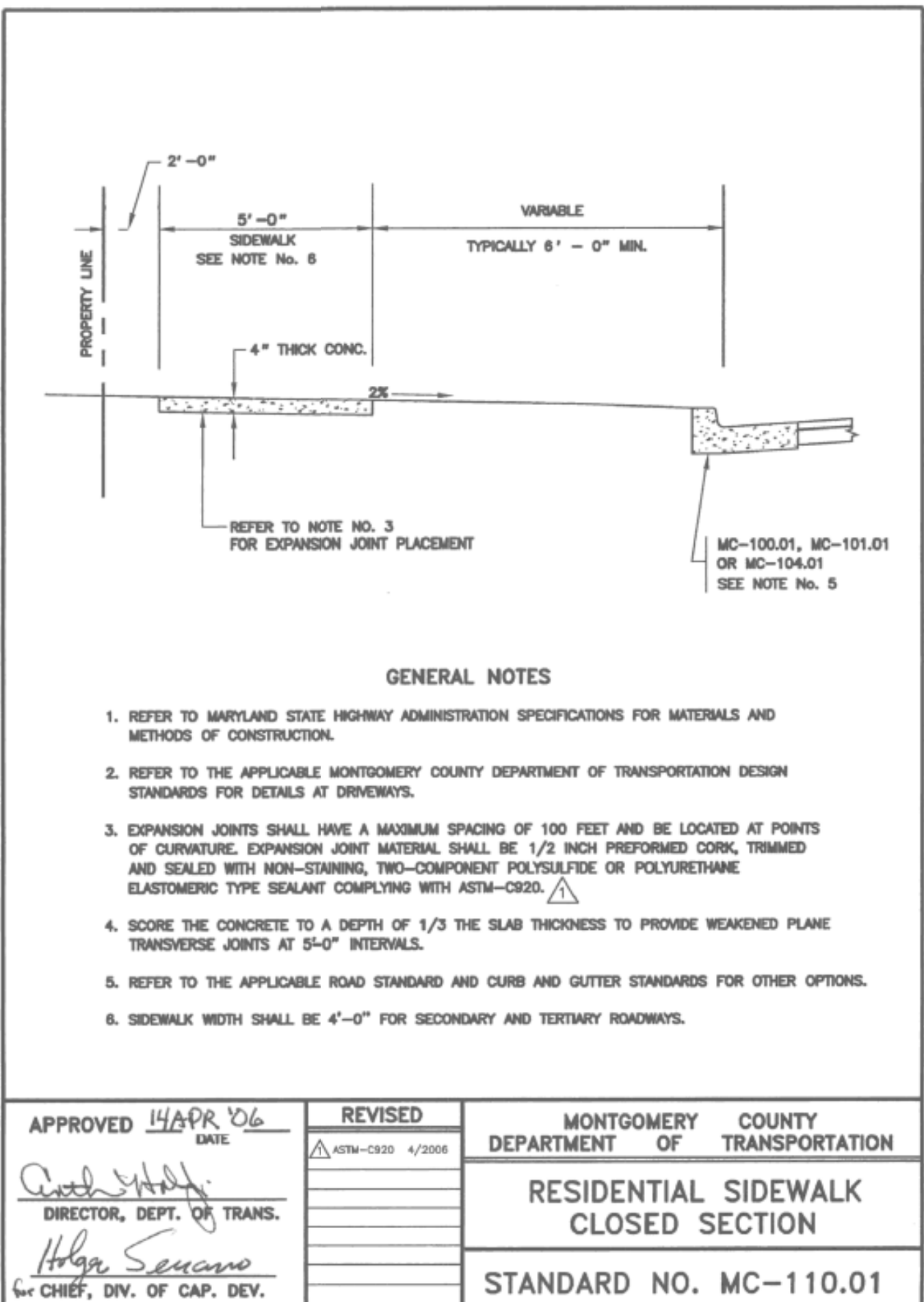
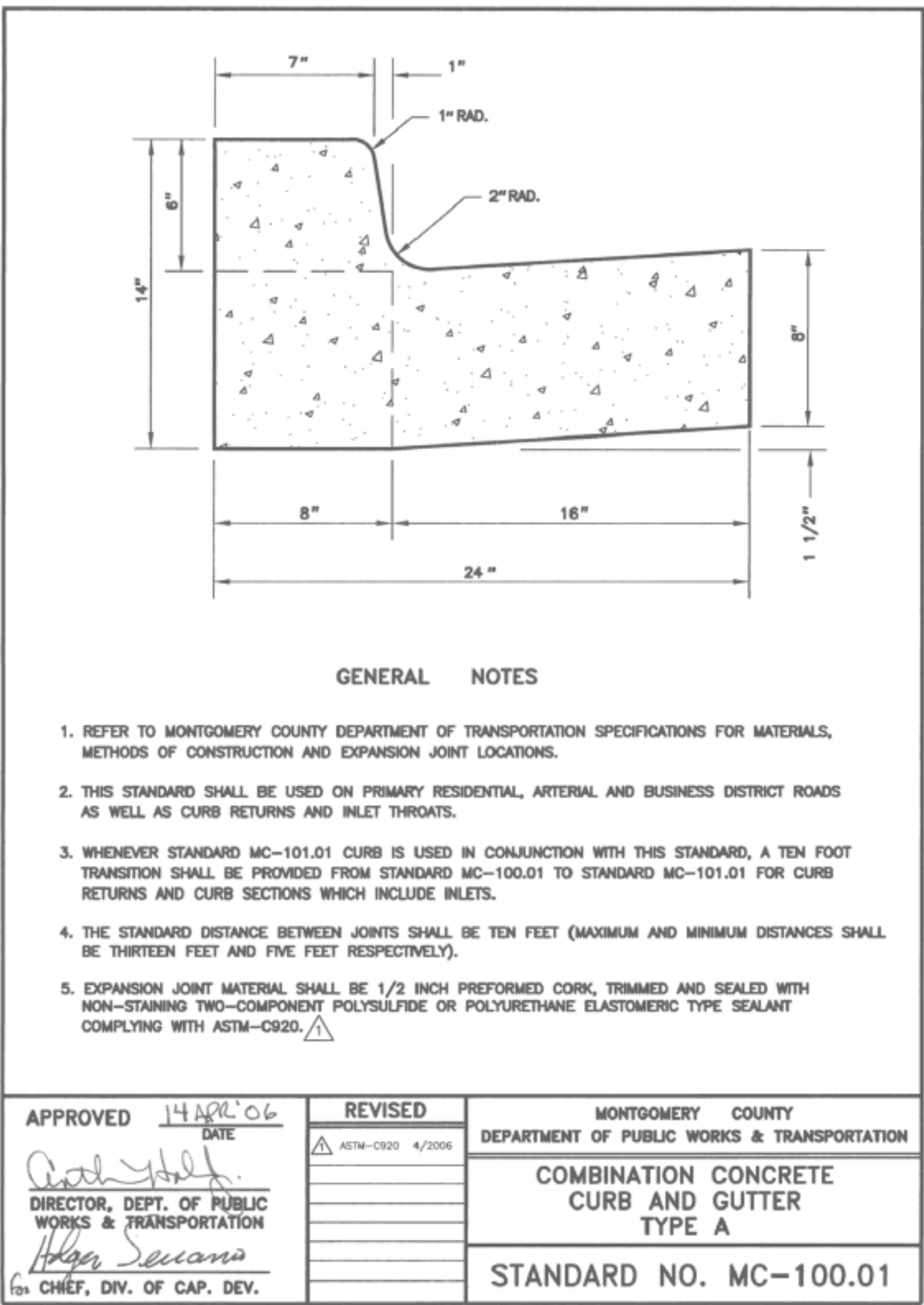
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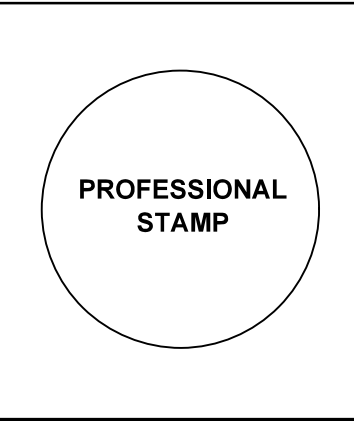
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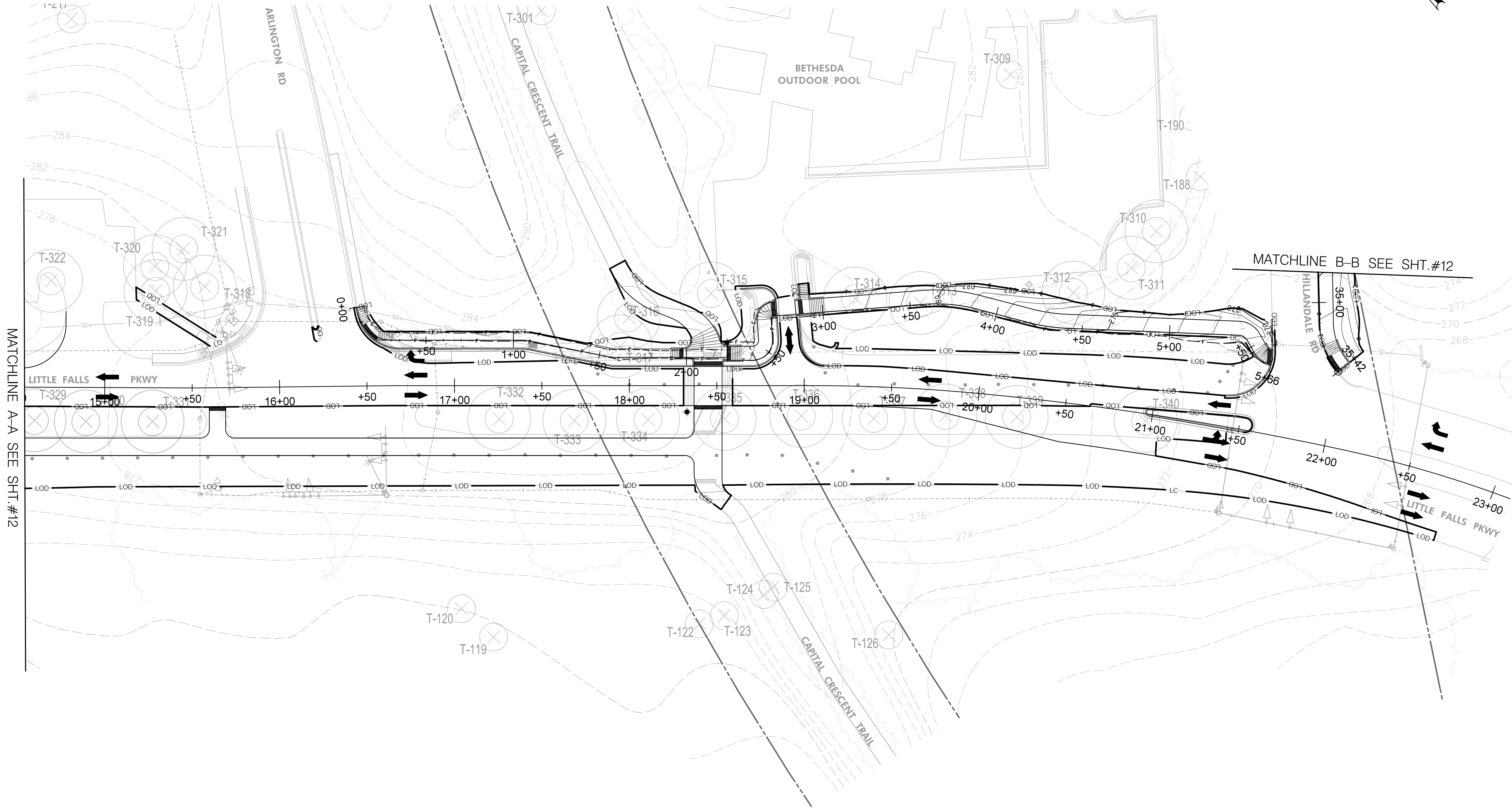
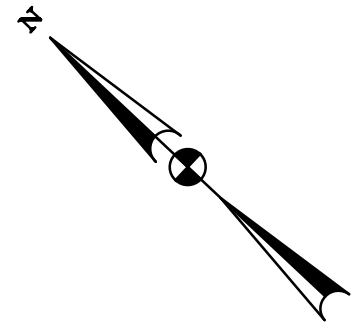
DESIGN		
Sabra & Associates, Inc.		
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7055 Samuel Morse Drive, #100		
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(443) 741-3500		
Telephone Number		

Landscape Architect	Date	Checked By:
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BW	04/2019	
Engineer	Date	Checked By:
CK	04/2019	
Drawn By	Date	Checked By:



REVIEW AND APPROVAL		ISSUED FOR PROCUREMENT ON	
		REVISIONS	
Project Manager	Date	Rev. No.	Date
Construction Manager	Date		
Park Manager	Date		

Construction Details		DWG. # 3 of 3
Capital Crescent Trail		SHT. # 10 of 24
SCALE: N/A		



FINAL SCANNED:

PLAN SCANNED:

PARK CODE: NA

Wednesday, April 17, 2019 AT 09:04 AM

R:\2017\41 CapitalCrescent Trail\MNCP\PC_\$148\DWG\30% Plans\035-FRM01_CCT.dgn PLOTTED BY: seller -

<div>Sabra & Associates, Inc. Designer's Name 7055 Samuel Morse Drive, #100 Address Columbia, MD 21046 City/State/Zip (443) 741-3500 Telephone Number</div>		<table><tr><th colspan="3">DESIGN</th></tr><tr><td>Landscape Architect</td><td>Date</td><td>Checked By:</td></tr><tr><td>Architect</td><td>Date</td><td>Checked By:</td></tr><tr><td>BW</td><td>04/2019</td><td></td></tr><tr><td>Engineer</td><td>Date</td><td>Checked By:</td></tr><tr><td>CK</td><td>04/2019</td><td></td></tr><tr><td>Drawn By</td><td>Date</td><td>Checked By:</td></tr></table>		DESIGN			Landscape Architect	Date	Checked By:	Architect	Date	Checked By:	BW	04/2019		Engineer	Date	Checked By:	CK	04/2019		Drawn By	Date	Checked By:	<div>Professional Certification. I hereby certify that these documents were prepared or approved by me, and that I am a duly licensed professional engineer under the laws of the State of Maryland. <div>PROFESSIONAL STAMP</div><div><div>The Maryland-National Capital Park and Planning Commission Montgomery County Department of Parks 9500 Brunett Avenue Silver Spring, Maryland 20901 (301) 495-2535</div></div></div>		<table><tr><th colspan="2">REVIEW AND APPROVAL</th></tr><tr><td>Project Manager</td><td>Date</td></tr><tr><td>Construction Manager</td><td>Date</td></tr><tr><td>Park Manager</td><td>Date</td></tr></table> <table><tr><th colspan="3">ISSUED FOR PROCUREMENT ON _____</th></tr><tr><th colspan="3">REVISIONS</th></tr><tr><th>Rev. No.</th><th>Date</th><th>Description</th></tr><tr><td> </td><td> </td><td> </td></tr><tr><td> </td><td> </td><td> </td></tr><tr><td> </td><td> </td><td> </td></tr><tr><td> </td><td> </td><td> </td></tr></table>		REVIEW AND APPROVAL		Project Manager	Date	Construction Manager	Date	Park Manager	Date	ISSUED FOR PROCUREMENT ON _____			REVISIONS			Rev. No.	Date	Description													<div><h1>Grading Plan</h1><h2>Capital Crescent Trail</h2><p>SCALE: 1"=30'</p></div>		<div>DWG. # <u>1</u> of <u>2</u> SHT. # <u>11</u> of <u>24</u></div>	
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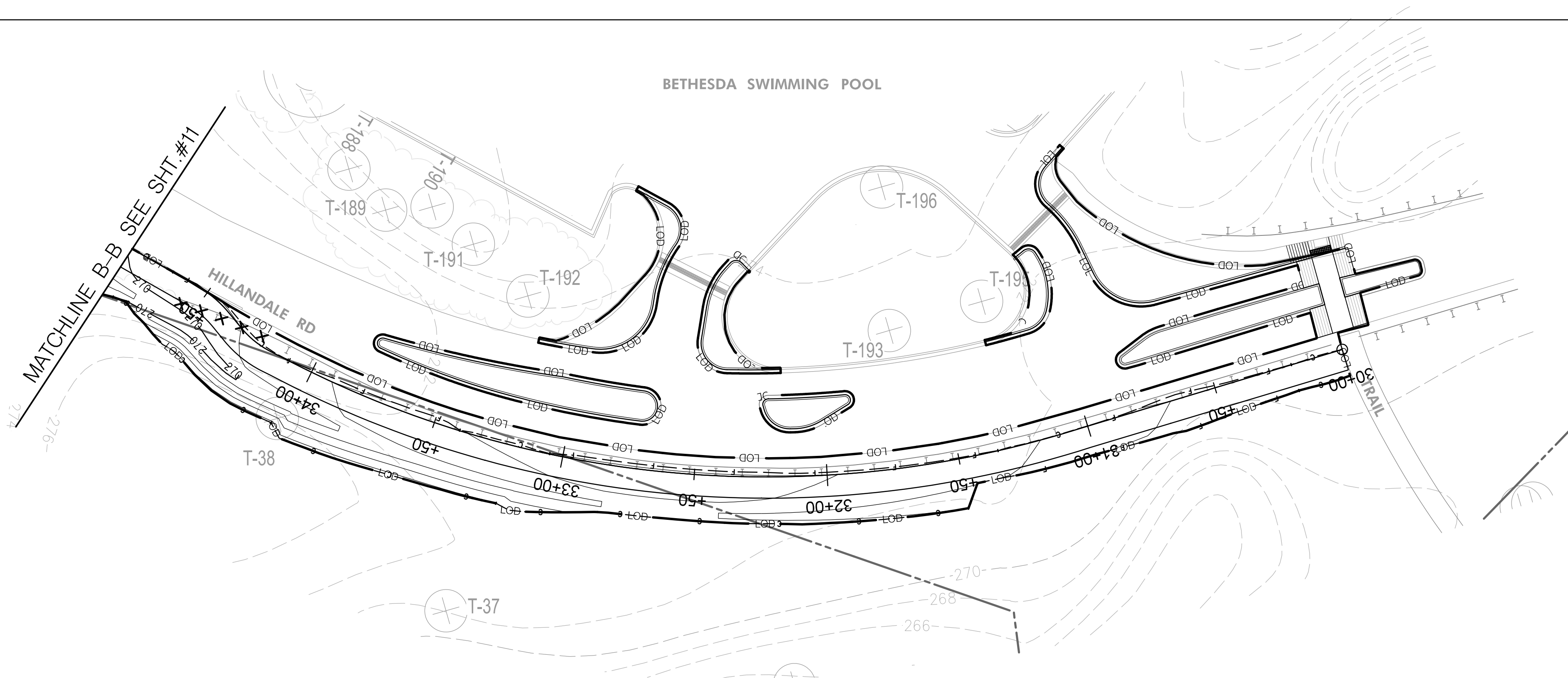
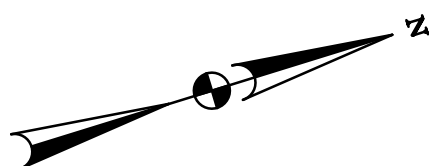
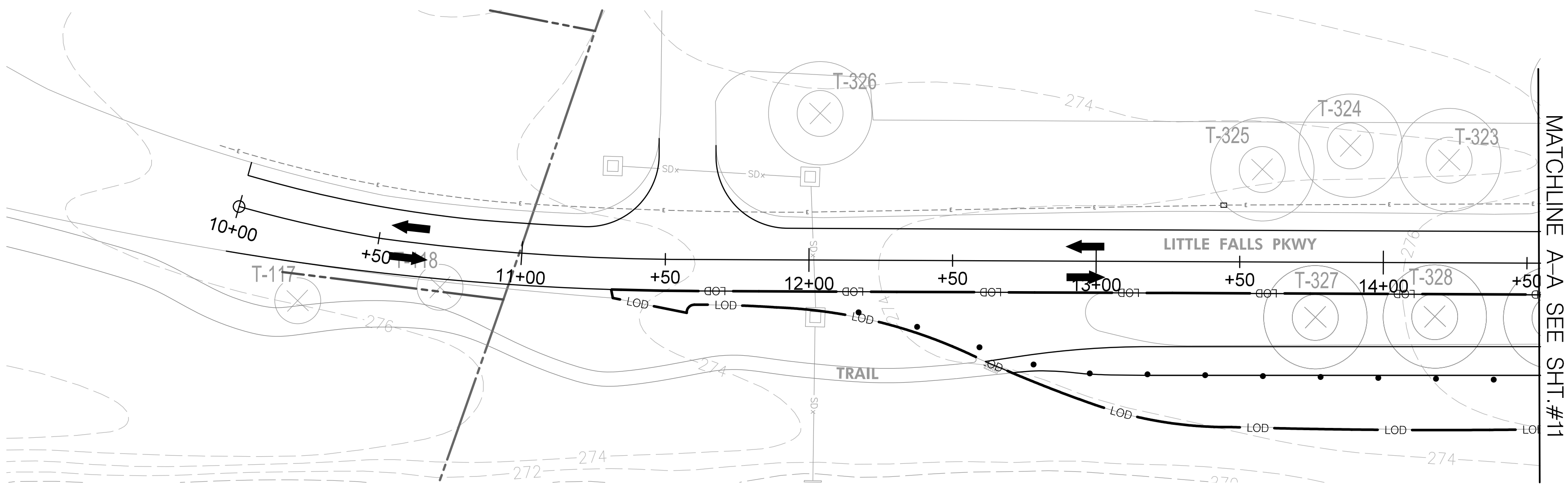
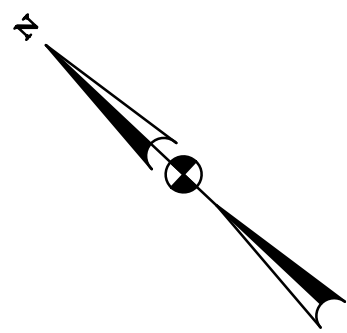
FINAL SCANNED:

PLAN SCANNED:

PARK CODE: NA

Wednesday, April 17, 2019 AT 01:58 PM

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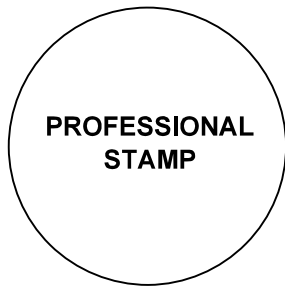


Sabra & Associates, Inc. Designer's Name 7055 Samuel Morse Drive, #100 Address Columbia, MD 21046 City/State/Zip (443) 741-3500 Telephone Number	DESIGN		
	Landscape Architect	Date	Checked By:
	Architect	Date	Checked By:
	BW	04/2019	
	Engineer	Date	Checked By:
	CK	04/2019	
	Drawn By	Date	Checked By:

Professional Certification. I hereby certify that these documents were prepared or approved by me, and that I am a duly licensed professional engineer under the laws of the State of Maryland.

License No. _____

Expiration Date _____



The Maryland-National Capital Park and Planning Commission
Montgomery County Department of Parks
9500 Brunett Avenue
Silver Spring, Maryland 20901
(301) 495-2535

REVIEW AND APPROVAL	
Project Manager	Date
Construction Manager	Date
Park Manager	Date

ISSUED FOR PROCUREMENT ON _____		
REVISIONS		
Rev. No.	Date	Description

Grading Plan Capital Crescent Trail

SCALE: 1"=30'

DWG. # 2 of 2

SHT. # 12 of 24

FINAL SCANNED:

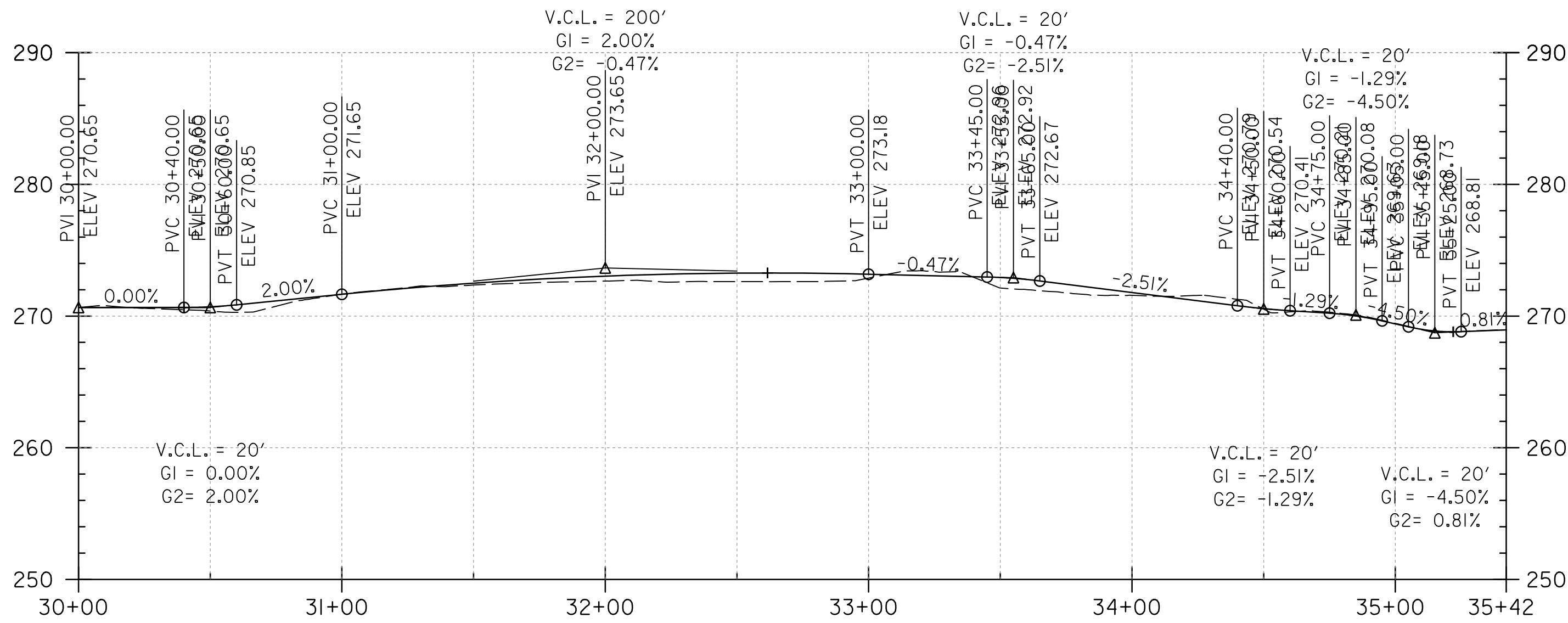
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PARK CODE: NA

Wednesday, April 17, 2019 AT 01:55 PM

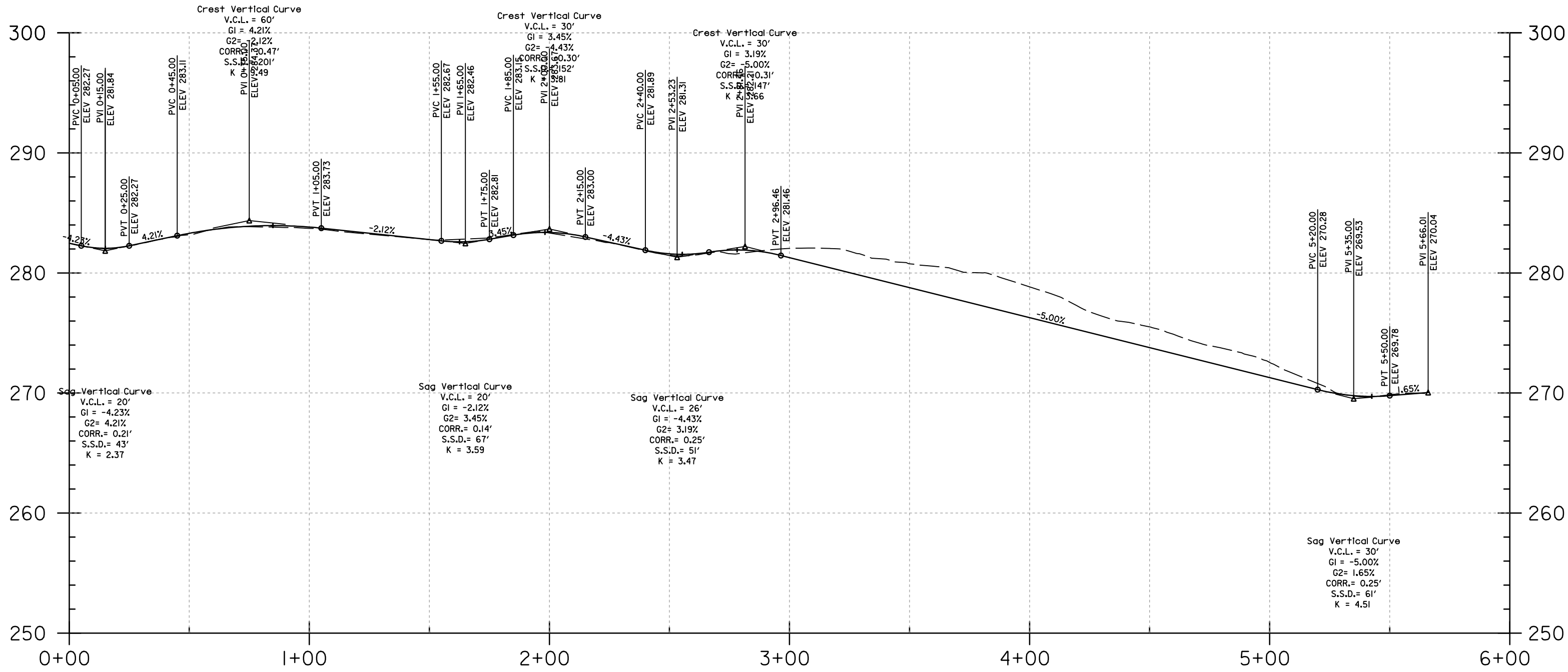
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Hillandale Rd Trail



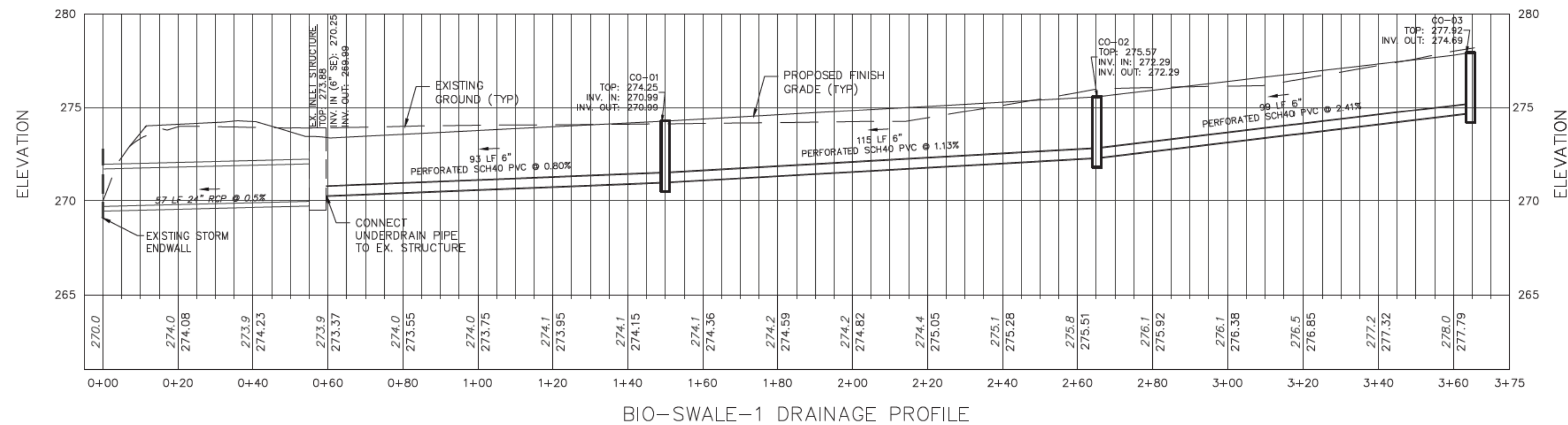
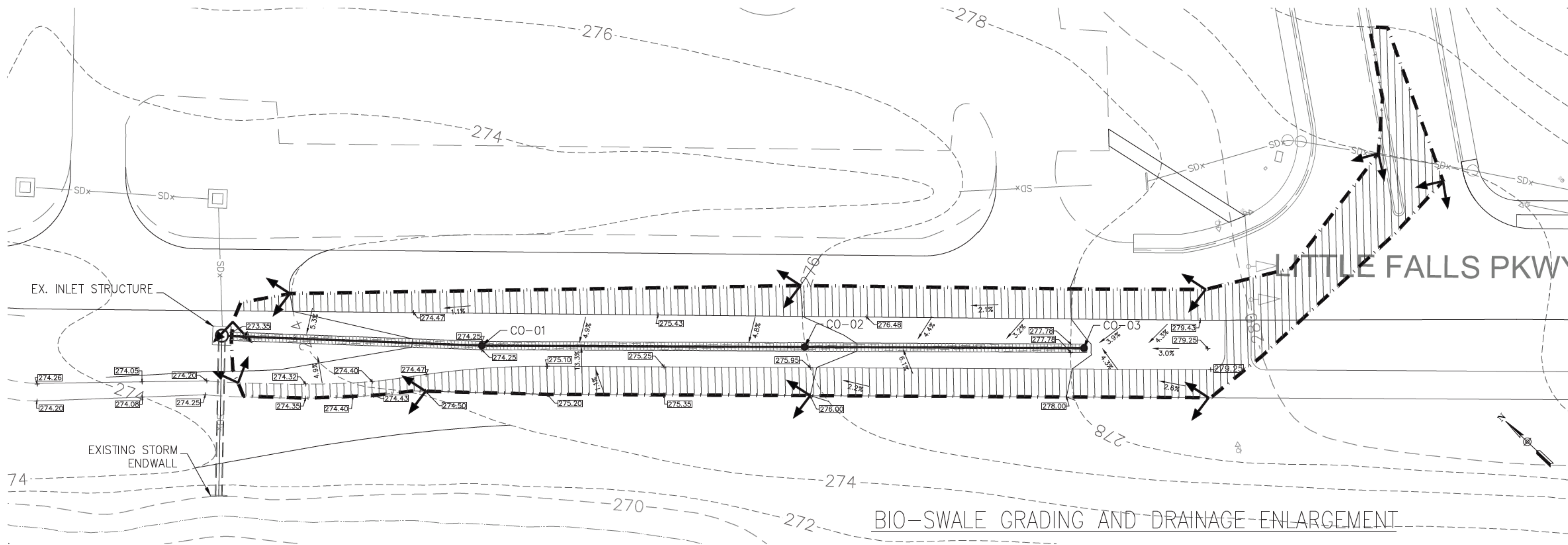
LINE	SURFACE	OFFSET
--	GIS Contours	0.0000
Scaled 5.0000 Times Ver. Scaled 1.0000 Times Hor.		

Little Falls Rd Trail



LINE	SURFACE	OFFSET
--	GIS Contours	0.0000
Scaled 5.0000 Times Ver. Scaled 1.0000 Times Hor.		

<div>Sabra & Associates, Inc.</div> <div>Designer's Name</div> <div>7055 Samuel Morse Drive, #100</div> <div>Address</div> <div>Columbia, MD 21046</div> <div>City/State/Zip</div> <div>(443) 741-3500</div> <div>Telephone Number</div>	<table><tr><th colspan="3">DESIGN</th></tr><tr><td>Landscape Architect</td><td>Date</td><td>Checked By:</td></tr><tr><td>Architect</td><td>Date</td><td>Checked By:</td></tr><tr><td>BW</td><td>04/2019</td><td></td></tr><tr><td>Engineer</td><td>Date</td><td>Checked By:</td></tr><tr><td>CK</td><td>04/2019</td><td></td></tr><tr><td>Drawn By</td><td>Date</td><td>Checked By:</td></tr></table>	DESIGN			Landscape Architect	Date	Checked By:	Architect	Date	Checked By:	BW	04/2019		Engineer	Date	Checked By:	CK	04/2019		Drawn By	Date	Checked By:	<div>Professional Certification. I hereby certify that these documents were prepared or approved by me, and that I am a duly licensed professional engineer under the laws of the State of Maryland.</div> <div>License No. _____</div> <div>Expiration Date _____</div> <div>PROFESSIONAL STAMP</div> <div><div>The Maryland-National Capital Park and Planning Commission Montgomery County Department of Parks 9500 Brunett Avenue Silver Spring, Maryland 20901 (301) 495-2535</div></div>	<table><tr><th colspan="2">REVIEW AND APPROVAL</th><th colspan="2">ISSUED FOR PROCUREMENT ON _____</th></tr><tr><td colspan="2"></td><td colspan="2">REVISIONS</td></tr><tr><td>Rev. No.</td><td>Date</td><td colspan="2">Description</td></tr><tr><td>Project Manager</td><td>Date</td><td colspan="2"></td></tr><tr><td>Construction Manager</td><td>Date</td><td colspan="2"></td></tr><tr><td>Park Manager</td><td>Date</td><td colspan="2"></td></tr></table>	REVIEW AND APPROVAL		ISSUED FOR PROCUREMENT ON _____				REVISIONS		Rev. No.	Date	Description		Project Manager	Date			Construction Manager	Date			Park Manager	Date			<div><h2>Cross Sections</h2><h2>Capital Crescent Trail</h2><div>SCALE: AS SHOWN</div></div>	<div>DWG. # <u>1</u> of <u>1</u></div> <div>SHT. # <u>13</u> of <u>24</u></div>
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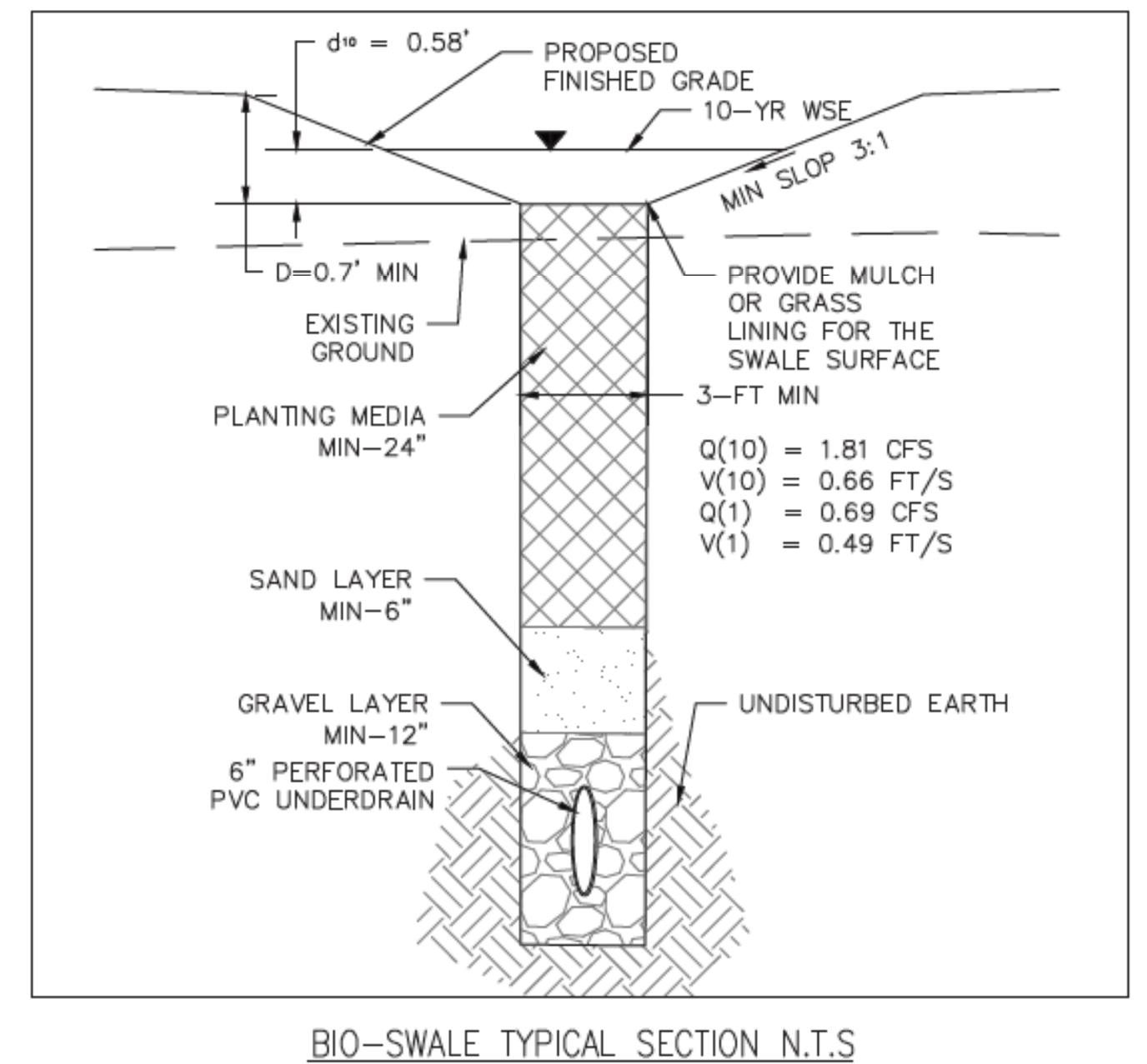


HORIZONTAL SCALE: 1"=20'

20' 0 20' 40'

VERTICAL SCALE: 1"=4'

4' 0 4' 8'



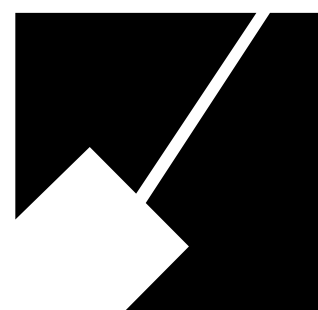
DRAINAGE DIVIDE

TREATED IMPERVIOUS COVER

PROPOSED SPOT ELEVATION

Bio Swale-1 (Proposed Parraller to SB-Little Falls Parkway Before Arlington Rd.)				
Impervious Area	8980			
Total DA to Bio-Swale	15960			
	I	56%	Proposed Area (SF)	9159
	Rv	0.56	Proposee Planting Medium Depth (Ft.)	2
Pe (MAX)	2.6		Gravel Layer Depth (Ft.)	1
Pe (MIN)	1.0		Sand Layer Depth (Ft.)	0.5
			Void Ratio for Sand, Gravel, and Planting Medium	0.4
ESDv (MAX)	1924		Required ESDv (CF)	1278
ESDv (MIN)	740		ESDv (provided in the filter Media) (CF)	1281
			ESDv Met	Yes

**PROFESSIONAL
STAMP**



REVIEW AND APPROVAL

ISSUED FOR PROCUREMENT ON

REVISIONS		
Rev. No.	Date	Description

Stormwater Management Plan

Capital Crescent Trail

SCALE: AS SHOWN

DWG. # 1 of 3

SHT. # 14 of 24

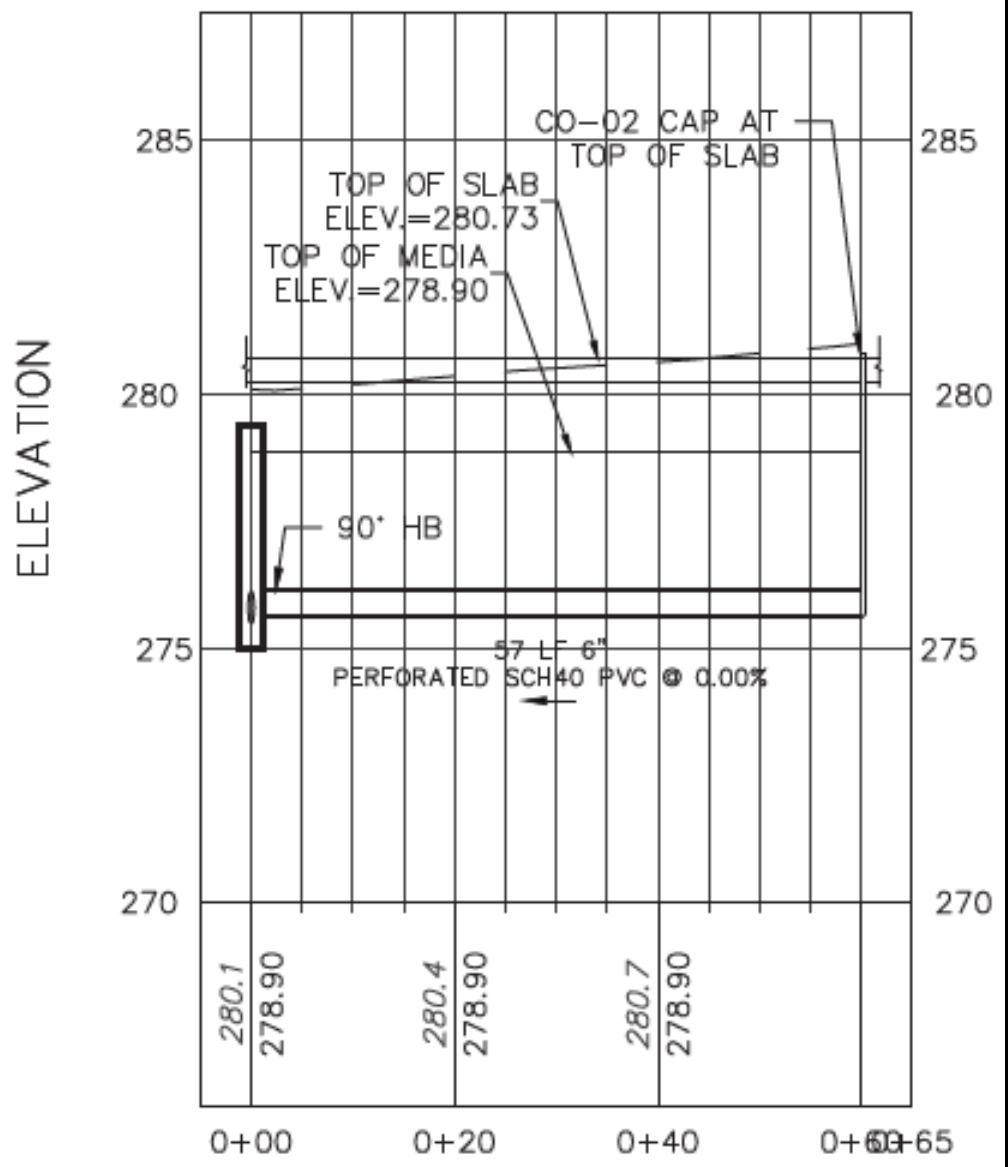
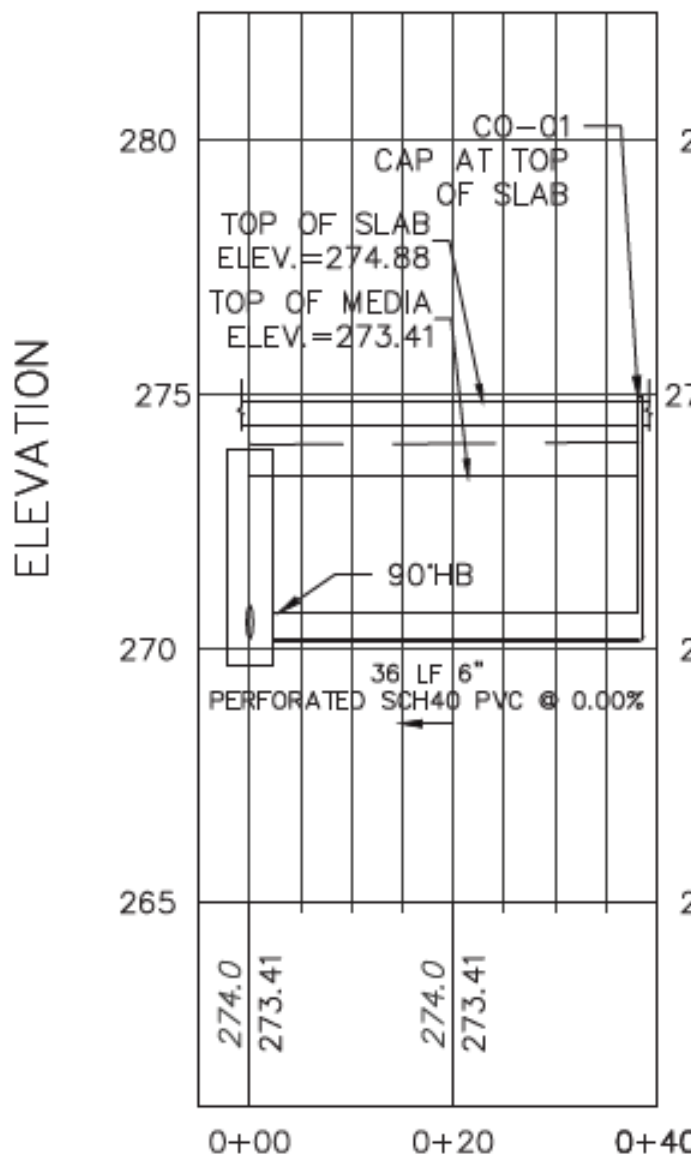
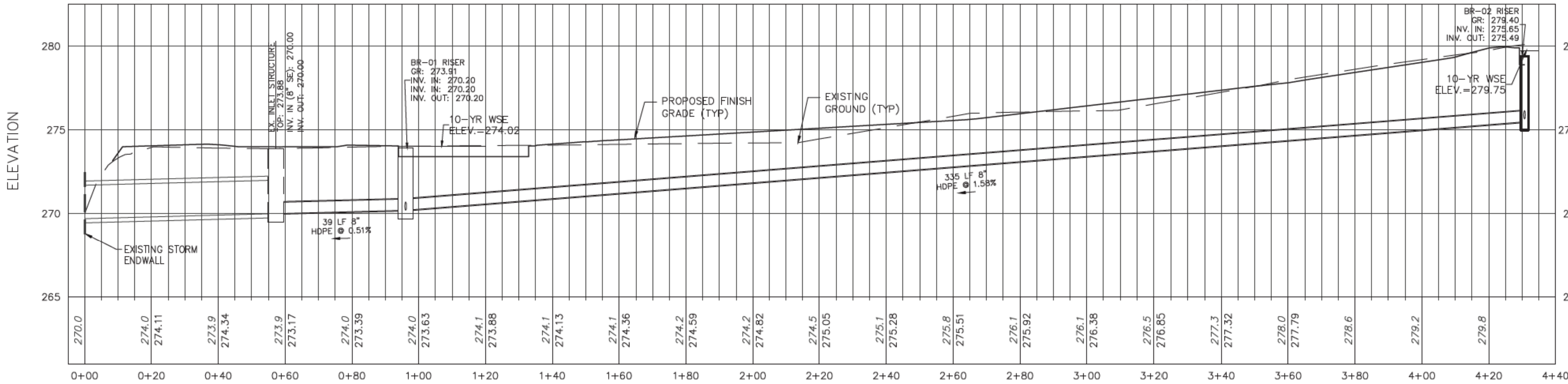
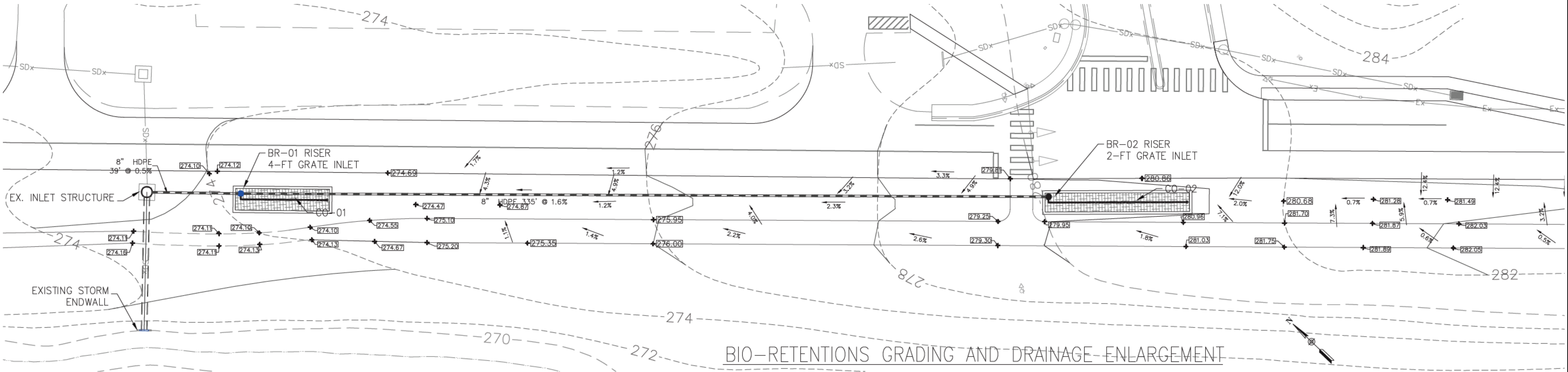
FINAL SCANNED:

PLAN SCANNED:

PARK CODE: NA

Wednesday, April 10, 2019 AT 03:31 PM

R:\2017\41 CapitalCrescent Trail_MNCP\PC \$148 (DWG) 30% Plans\USW-P002_CCT_Tier1PLOTTED BY: Keller -



BIO-RETENTION BR-01			
Impervious Area	6890		
Total DA-1	13450	Proposed Area (SF)	312
I	51%	Ponding depth	0.5
Rv	0.51	Proposee Planting Medium Depth (Ft.)	2
Pe (MAX)	2.6	Gravel Layer Depth (Ft.)	1
Pe (MIN)	1.0	Sand Layer Depth (Ft.)	0.5
		Void Ratio for Sand, Gravel, and Planting Medium	0.4
ESDv (MAX)	1489		
ESDv (MIN)	573	ESDv (provided in the filter Media) (CF)	593
BIO-RETENTION BR-02			
Impervious Area	10848		
Total DA-2	16125	Proposed Area (SF)	468
I	67%	Ponding depth	0.5
Rv	0.66	Proposee Planting Medium Depth (Ft.)	2
Pe (MAX)	2.6	Gravel Layer Depth (Ft.)	1
Pe (MIN)	1.0	Sand Layer Depth (Ft.)	0.5
		Void Ratio for Sand, Gravel, and Planting Medium	0.4
ESDv (MAX)	2290		
ESDv (MIN)	881	ESDv (provided in the filter Media) (CF)	889
		Required ESDv (CF)	1276
		Total ESDv Provided	1482
		ESDv Met	Yes

LEGEND

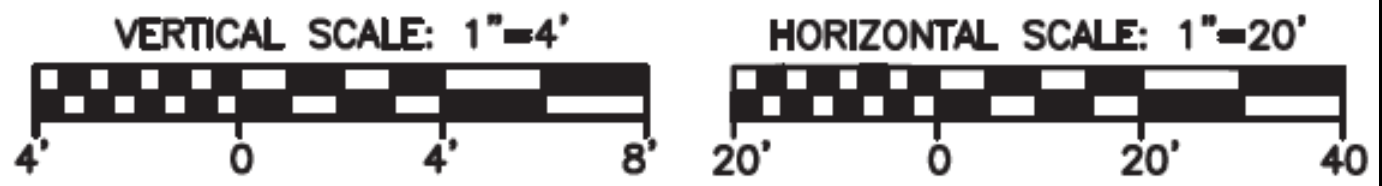
PROPOSED SPOT ELEVATION

DRAINAGE PROFILE

NOTE: OWNER IS RESPONSIBLE FOR ALL MAINTENANCE AND REGULAR INSPECTIONS.

BR-01 DRAINAGE PROFILE

BR-02 DRAINAGE PROFILE



ALPHA Corporation Designer's Name 21351 Ridgetop Circle, Ste. 200 Address Dulles, VA 20166 City/State/Zip (703) 450-0800 Telephone Number	DESIGN <table><tr><td>Landscape Architect</td><td>Date</td><td>Checked By:</td></tr><tr><td>Architect</td><td>Date 04/2019</td><td>Checked By:</td></tr><tr><td>Engineer</td><td>Date 04/2019</td><td>Checked By:</td></tr><tr><td>Drawn By</td><td>Date</td><td>Checked By:</td></tr></table>	Landscape Architect	Date	Checked By:	Architect	Date 04/2019	Checked By:	Engineer	Date 04/2019	Checked By:	Drawn By	Date	Checked By:	Professional Certification. I hereby certify that these documents were prepared or approved by me, and that I am a duly licensed professional engineer under the laws of the State of Maryland. License No. _____ Expiration Date _____ PROFESSIONAL STAMP	The Maryland-National Capital Park and Planning Commission Montgomery County Department of Parks 9500 Brunett Avenue Silver Spring, Maryland 20901 (301) 495-2535	REVIEW AND APPROVAL <table><tr><td>Project Manager</td><td>Date</td></tr><tr><td>Construction Manager</td><td>Date</td></tr><tr><td>Park Manager</td><td>Date</td></tr></table>	Project Manager	Date	Construction Manager	Date	Park Manager	Date	ISSUED FOR PROCUREMENT ON _____ REVISIONS <table><tr><th>Rev. No.</th><th>Date</th><th>Description</th></tr><tr><td> </td><td> </td><td> </td></tr><tr><td> </td><td> </td><td> </td></tr><tr><td> </td><td> </td><td> </td></tr><tr><td> </td><td> </td><td> </td></tr></table>	Rev. No.	Date	Description													SWM Plan - Alternate Capital Crescent Trail SCALE: AS SHOWN	DWG. # 2 of 3 SHT. # 15 of 24
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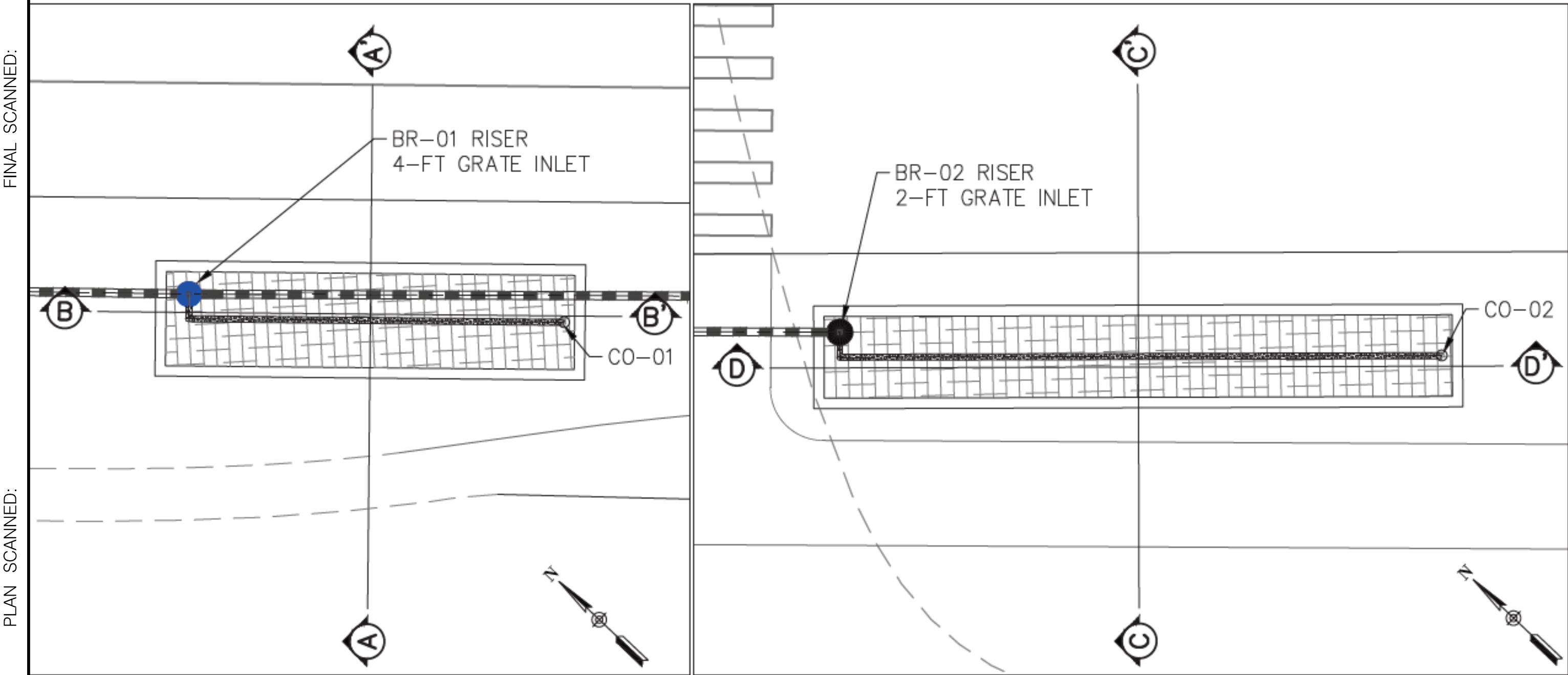
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PLAN SCANNED:

PARK CODE: NA

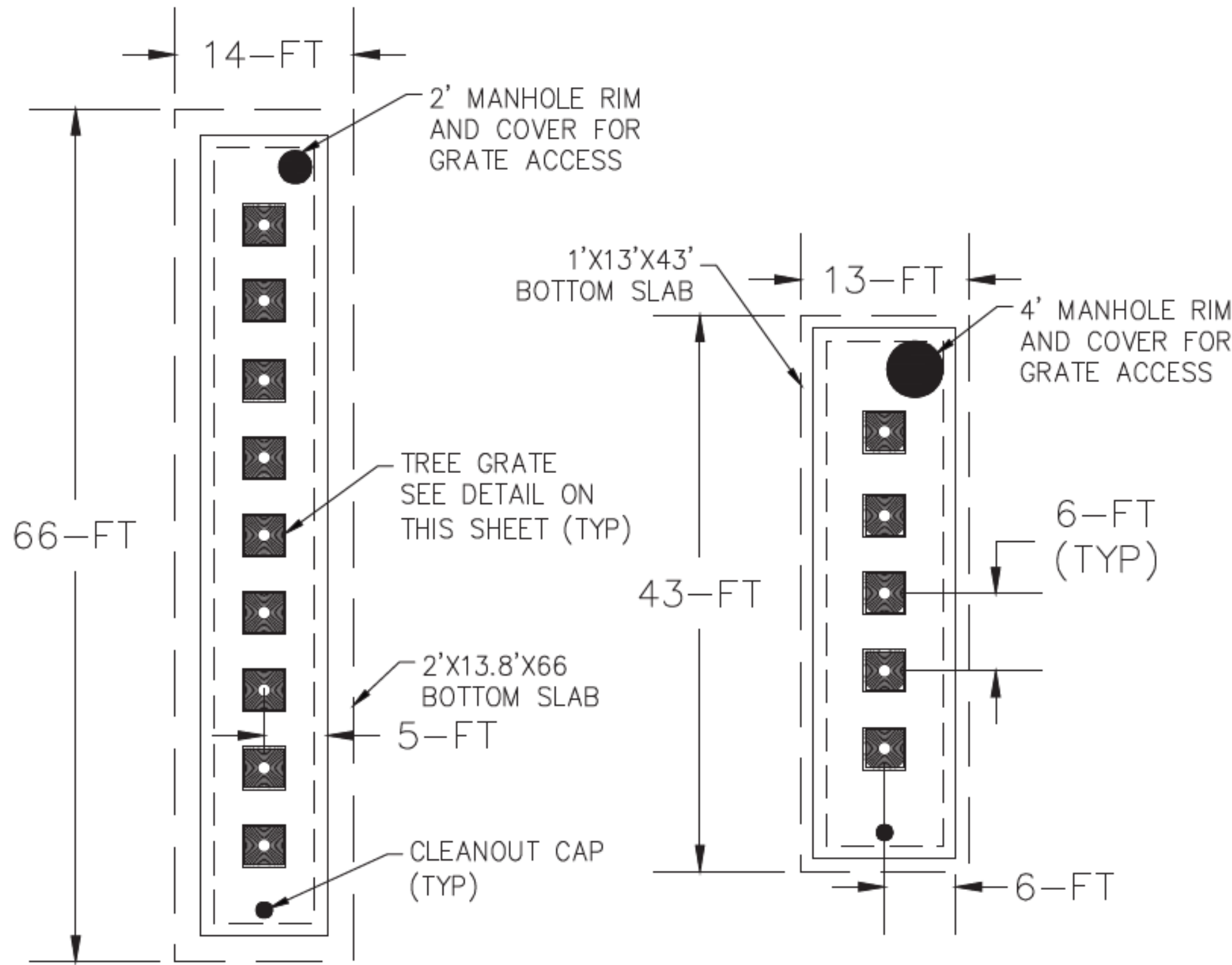
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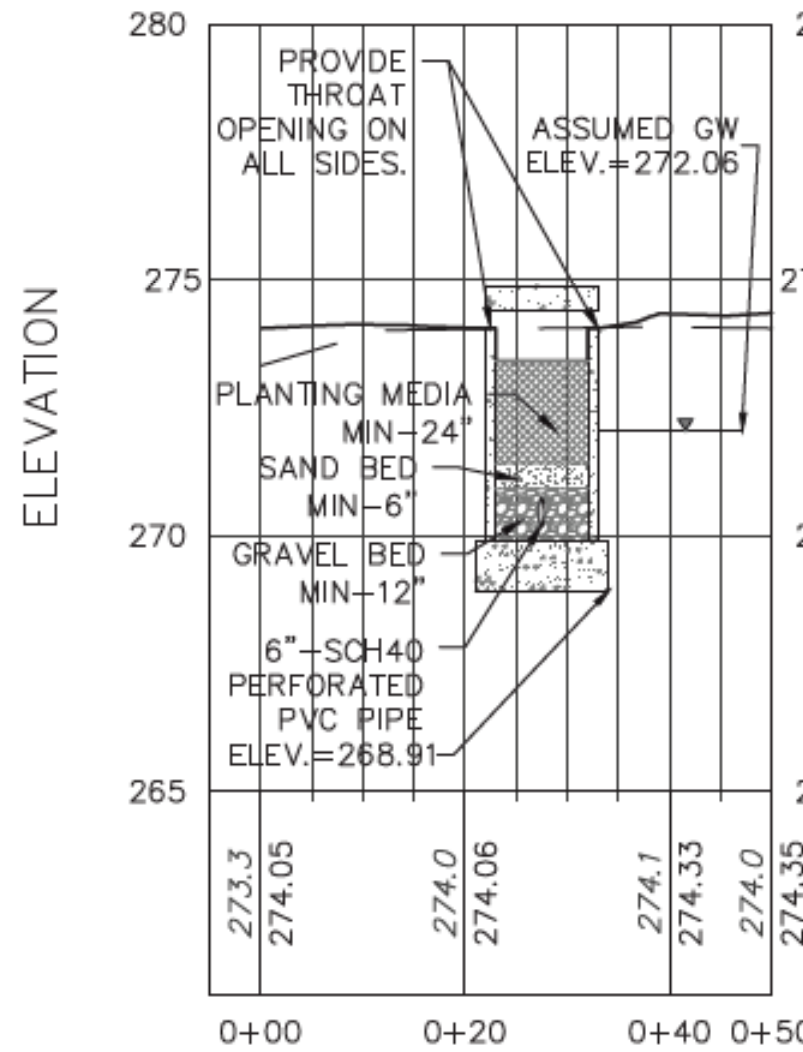


BIO-RETENTION ENLARGEMENT 1"=10'

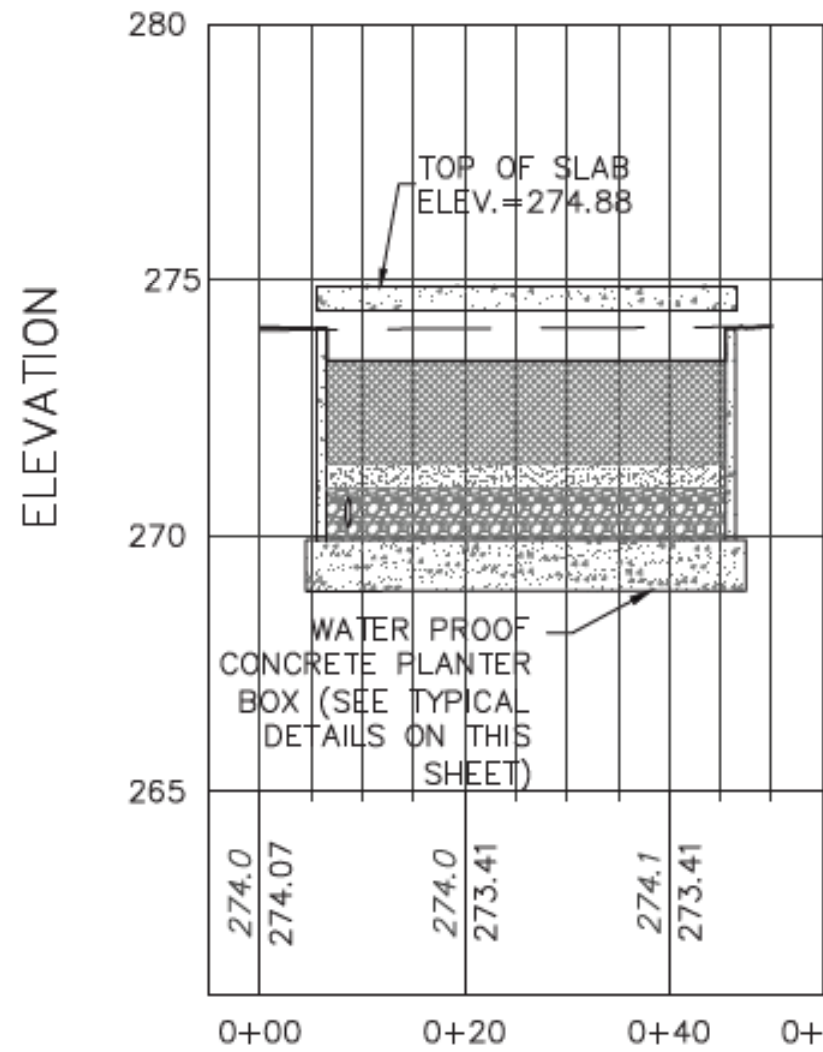
STRUCTURE INFORMATION		
DIMENSIONS	BIO-RETENTION BR-01	BIO-RETENTION BR-02
PLANTER BOX SIZES (FT)		
WALL THICKNESS	1	1
WIDTH	9	7.8
LENGTH	39	60
TOP SLAB SIZES (FT)		
THICKNESS	1	1
WIDTH	11	9.8
LENGTH	41	62
BASE SLAB SIZES (FT)		
THICKNESS	1	2
WIDTH	13	13.8
LENGTH	43	66
CONCRETE VOLUME (CF)	1361	2897
WEIGHT GRATES AND COVER (LBS.)	800	900
WEIGHT OF STRUCTURE (LBS.)	204950	435480
BACKFILL AND SLIDING RESISTANCE FORCES		
ASSUMED BACKFILL γ_{sub}	70	70
BURIED STRUCTURAL PERIMETER (FT)	104	144
P (LBS./FT)	306	306
SLIDING RESISTANCE (LBS.)	9558	13197
WEIGHT SOIL (LBS.)	31374	66856
BOX BOTTOM ELEV.	268.9	273.4
ASSUMED GW ELEV.	272.1	278.6
SUBMERGED DEPTH	3.1	5.2
BUOYANCY ANALYSIS RESULTS		
TOTAL RESISTING FORCES (LB.)	245882	515533
BUOYANT FORCE (LB.)	88649	197154
F.S. MIN 2.0	2.8	2.6



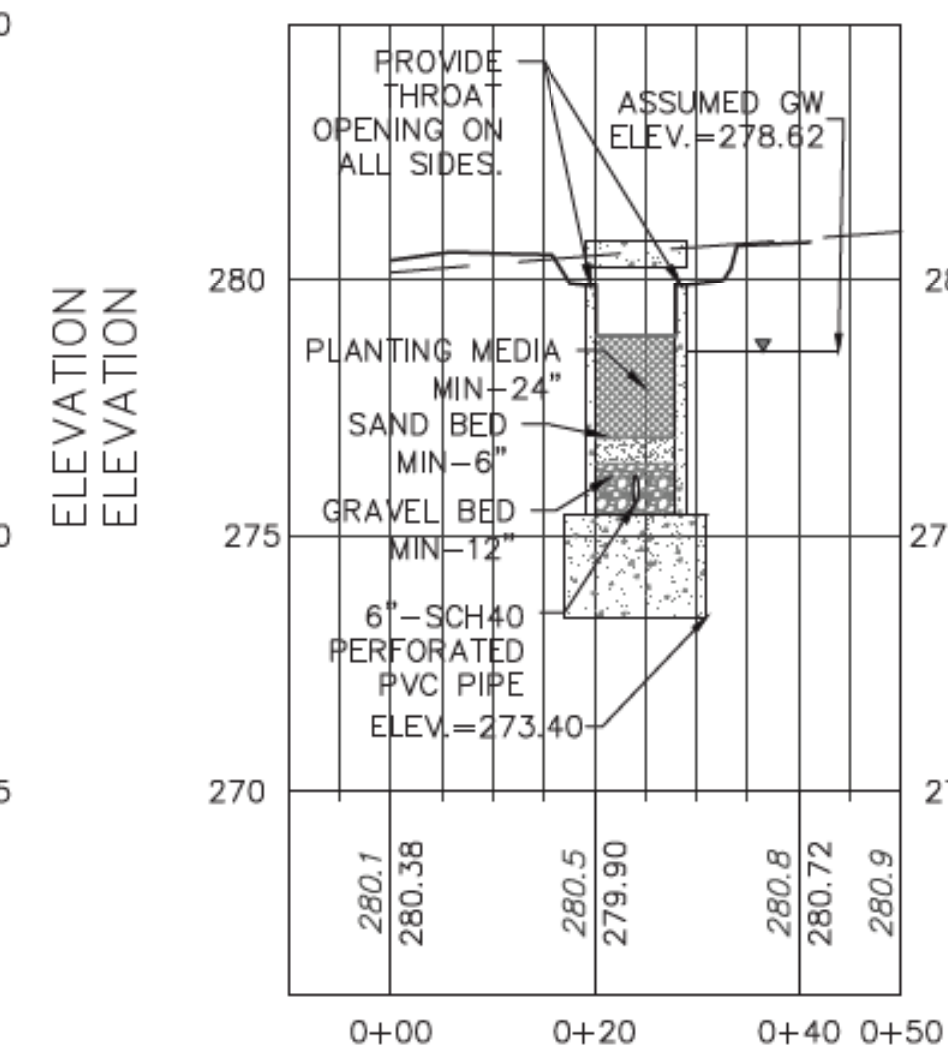
BIO-RETENTION PLAN DETAIL 1"=10'



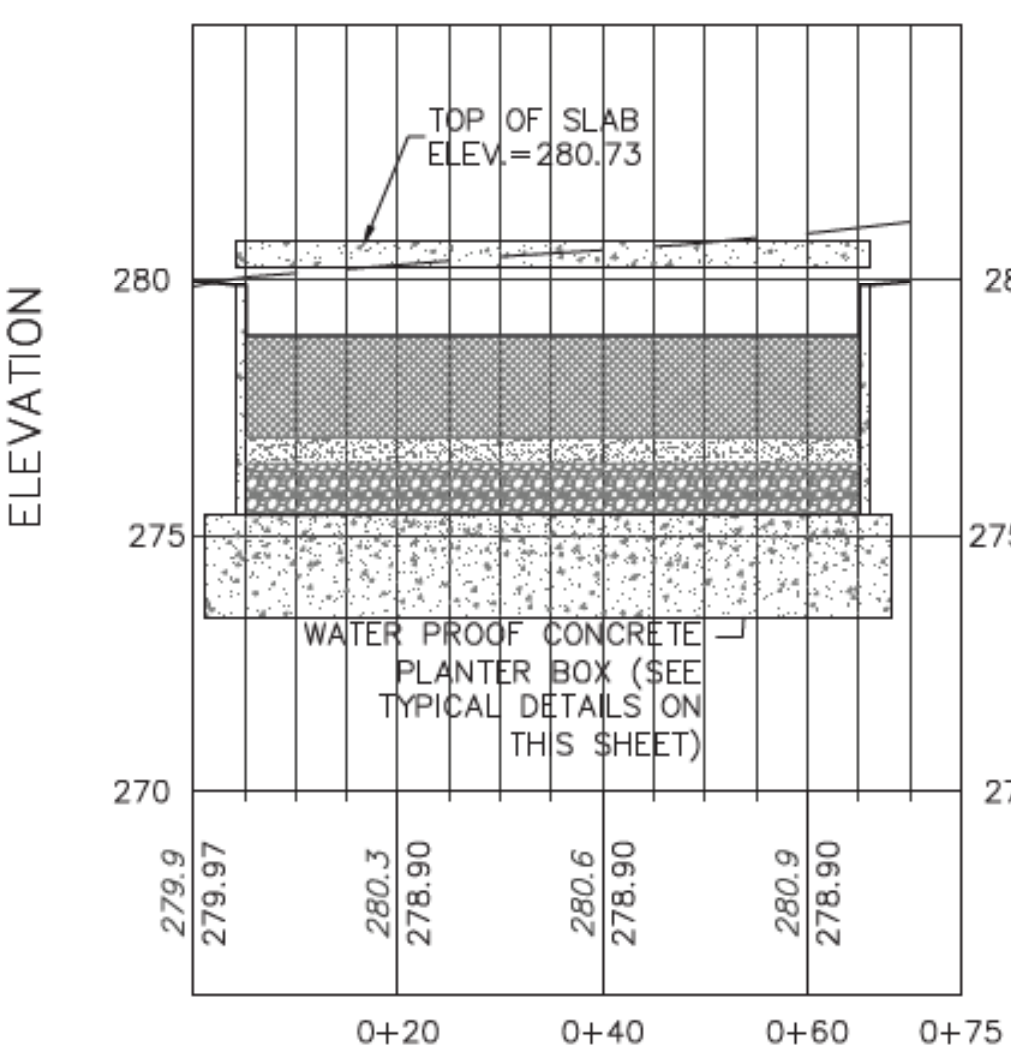
BR-01 SECTION A-A'



BR-01 SECTION B-B'



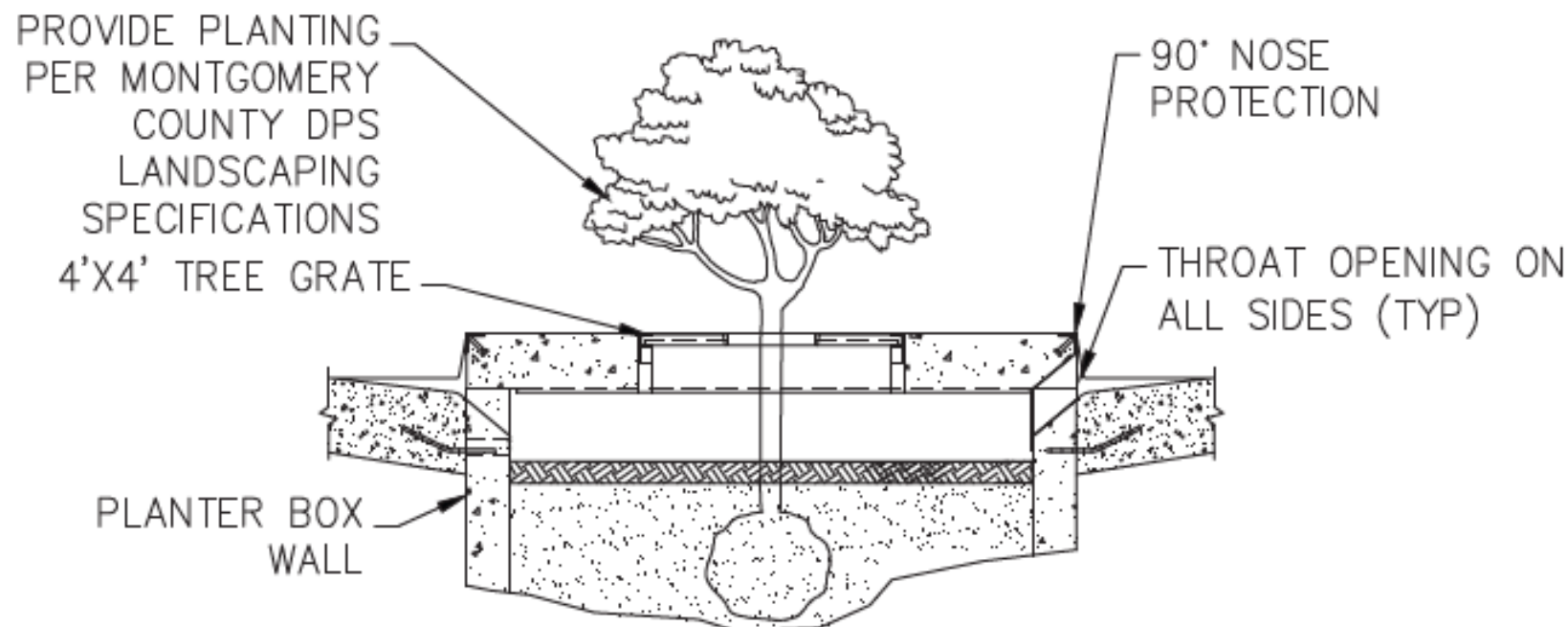
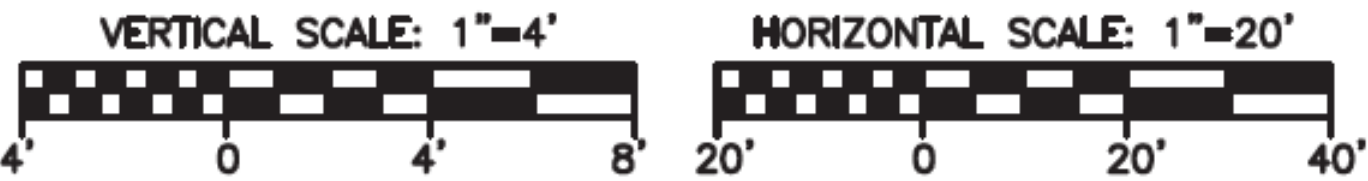
BR-02 SECTION C-C'



BR-02 SECTION D-D'

NOTE: OWNER IS RESPONSIBLE FOR ALL MAINTENANCE AND REGULAR INSPECTIONS.

BIO-RETENTION SECTION PROFILES

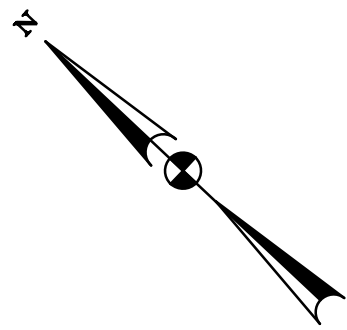


BIO-RETENTION TREE GRATE SECTION N.T.S

ALPHA Corporation	DESIGN		Professional Certification. I hereby certify that these documents were prepared or approved by me, and that I am a duly licensed professional engineer under the laws of the State of Maryland.			The Maryland-National Capital Park and Planning Commission Montgomery County Department of Parks 9500 Brunett Avenue Silver Spring, Maryland 20901 (301) 495-2535	REVIEW AND APPROVAL	ISSUED FOR PROCUREMENT ON _____	SWM Bio-Retention Details-Alternate Capital Crescent Trail	DWG. # <u>3</u> of <u>3</u>				
Designer's Name	Landscape Architect	Date									Checked By:	Rev. No.	Date	Description
21351 Ridgeway Circle, Ste. 200														
Address	Architect	Date									Checked By:			
Dulles, VA 20166														
City/State/Zip	Engineer	Date									Checked By:			
(703) 450-0800														
Telephone Number	Drawn By	Date	Checked By:											

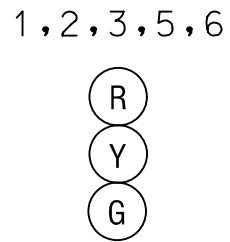
SCALE: AS SHOWN

SHT. # 16 **of** 24

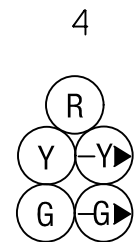


LITTLE FALLS PARKWAY IS ASSUMED TO RUN
IN AN EAST-WEST DIRECTION

PROPOSED SIGNALS



12" BLACK-
FACED LED
SIGNAL



12" BLACK-
FACED LED
SIGNAL



12"
2-SECTION LED
COUNTDOWN
PEDESTRIAN
SIGNAL

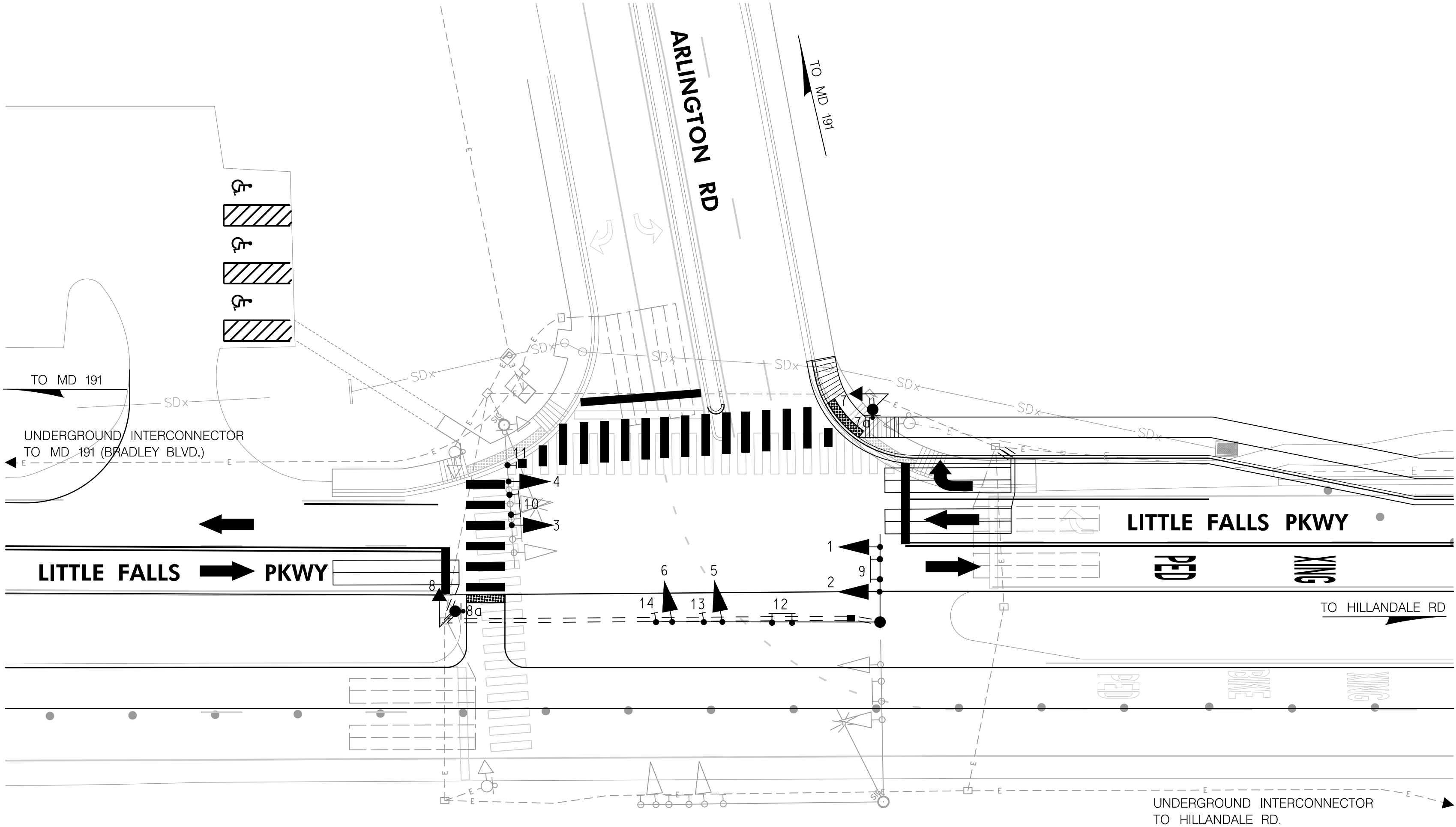
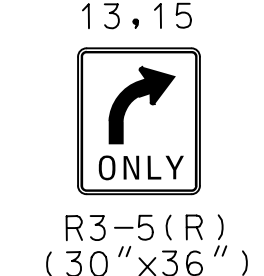
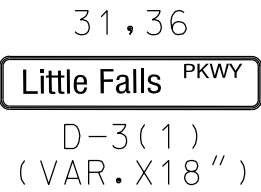
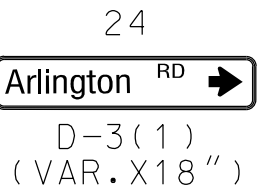
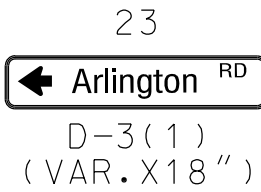
PROPOSED SIGNS



R10-3(1)
9" x 15"



R10-3(1)
9" x 15"

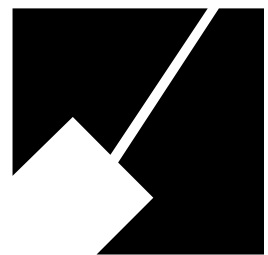
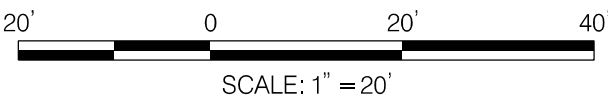


GEOMETRIC LEGEND

EXISTING
PROPOSED

UTILITY LEGEND

SDx STORM DRAIN
E ELECTRICAL



The Maryland-National Capital
Park and Planning Commission
Montgomery County Department of Parks
9500 Brunett Avenue
Silver Spring, Maryland 20901
(301) 495-2535



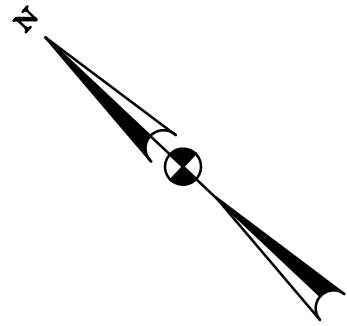
NO.	REVISION	BY	APP'D	DATE	DESIGNED BY: B. WHITE	DATE 04/2019
					DRAWN BY: C. KELLER	DATE 04/2019
					CHECKED BY: S. DU	DATE 04/2019
					DRAWING NO.:	
					Approved: _____ Chief, Traffic Engineering and Operations	
					Reviewed: _____ Manager, Transportation Systems Engineering	
					Reviewed: _____ Manager, Traffic Control and Lighting Engineering	
					Recommended: _____ Engineer, Transportation Systems Engineering	

DEPARTMENT OF TRANSPORTATION
DIVISION OF TRAFFIC ENGINEERING AND OPERATIONS
MONTGOMERY COUNTY, MARYLAND

CAPITAL CRESCENT TRAIL
PED. CROSSING IMPROVEMENT PLAN
LITTLE FALLS PKWY. AT ARLINGTON RD.
BETHESDA, MARYLAND
TRAFFIC SIGNAL PLAN

SCALE: 1"=20'

DATE: APRIL 2019



LITTLE FALLS PARKWAY IS ASSUMED TO RUN
IN AN EAST-WEST DIRECTION

RELOCATED SIGNALS

PROPOSED SIGNALS

1-2
R
Y
G
12" BLACK-
FACED LED
SIGNAL

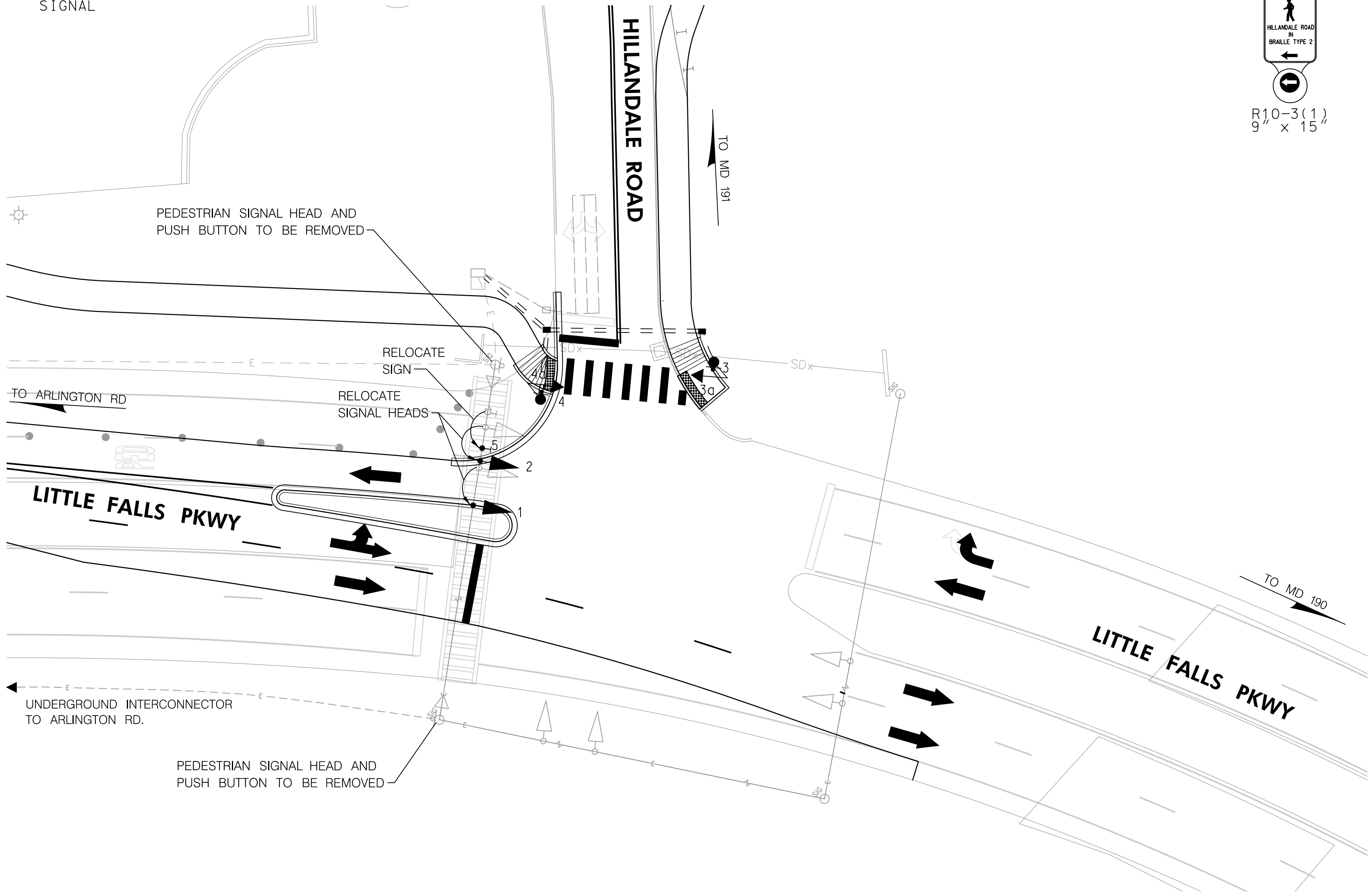
3-4
12"
2-SECTION LED
COUNTDOWN
PEDESTRIAN
SIGNAL

PROPOSED SIGNS

RELOCATED SIGN

3a-4a
PUSH BUTTON
TO CROSS
HILLDALE ROAD
START CROSSING
WATCH FOR
TRAVELING VEHICLE
COUNT START
IF STOPPED
TIME REMAINING
TO FINISH CROSSING
PEDESTRIANS
SHOULD NOT BE
IN CROSSING
HILLDALE ROAD
IN
BRAILLE TYPE 2
R10-3(1)
9' x 15'

5
ONLY
R3-5(R)
(30"x36")



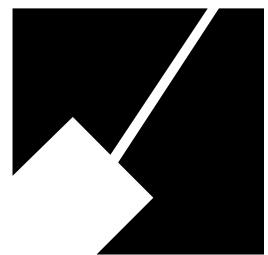
GEOMETRIC LEGEND

EXISTING
PROPOSED

UTILITY LEGEND

SDx STORM DRAIN
E ELECTRICAL

20' 0 20' 40'
SCALE: 1" = 20'



The Maryland-National Capital
Park and Planning Commission
Montgomery County Department of Parks
9500 Brunett Avenue
Silver Spring, Maryland 20901
(301) 495-2535



NO.	REVISION	BY	APP'D	DATE	DESIGNED BY: B. WHITE	DATE 04/2019
					DRAWN BY: C. KELLER	DATE 04/2019
					CHECKED BY: S. DU	DATE 04/2019
					DRAWING NO.:	
					Approved: _____ Chief, Traffic Engineering and Operations	
					Reviewed: _____ Manager, Transportation Systems Engineering	
					Reviewed: _____ Manager, Traffic Control and Lighting Engineering	
					Recommended: _____ Engineer, Transportation Systems Engineering	

DEPARTMENT OF TRANSPORTATION
DIVISION OF TRAFFIC ENGINEERING AND OPERATIONS
MONTGOMERY COUNTY, MARYLAND

CAPITAL CRESCENT TRAIL
PED. CROSSING IMPROVEMENT PLAN
LITTLE FALLS PKWY.AT HILLDALE RD.
BETHESDA, MARYLAND
TRAFFIC SIGNAL PLAN

SCALE: 1"=20'

DATE: APRIL 2019

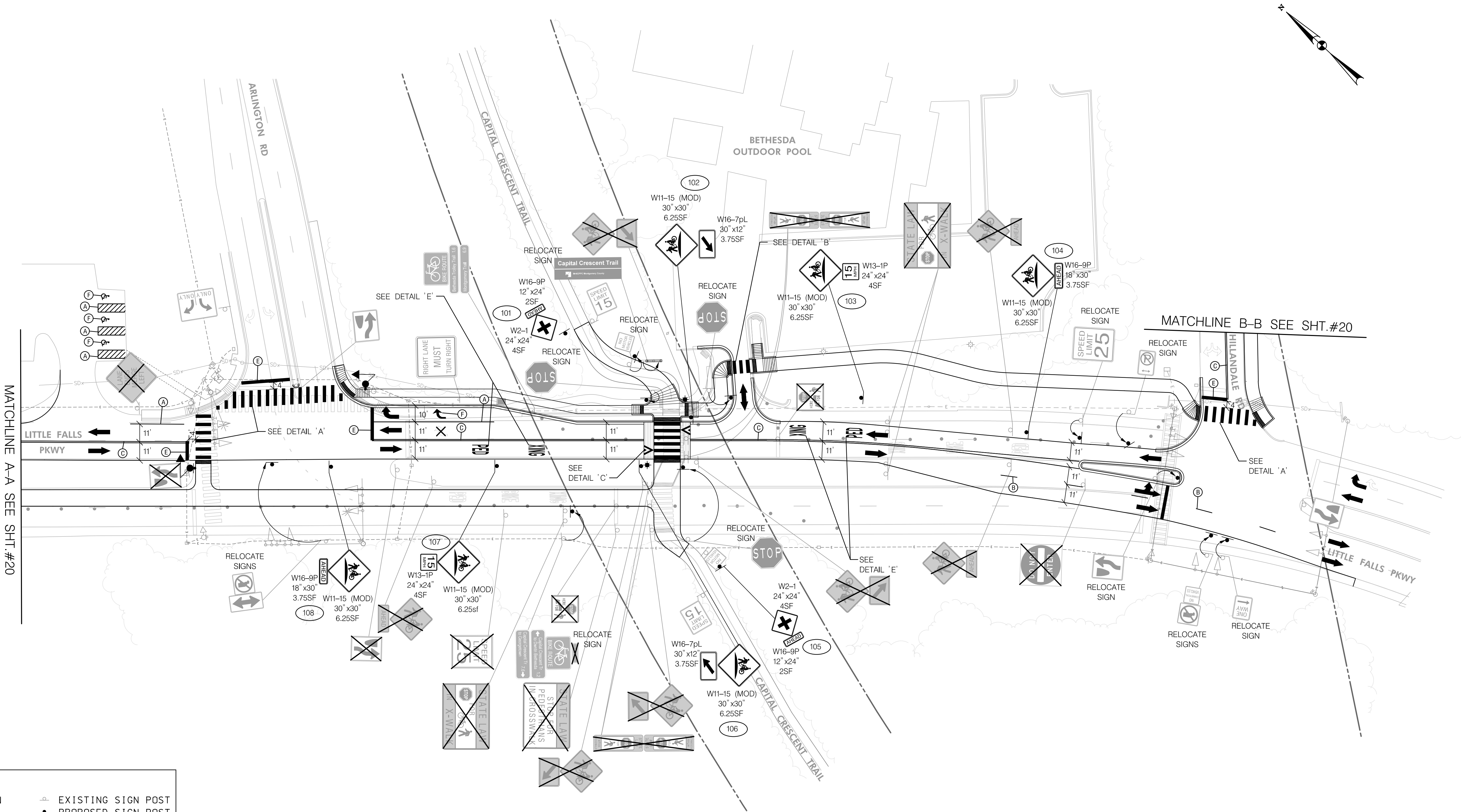
FINAL SCANNED:

PLAN SCANNED:

PARK CODE: NA

Wednesday, April 17, 2019 AT 02:48 PM

R:\2017\41 CapitalCrescent Trail_MNCP\PCP \$148 (DWG)30% Plans\30%LAYOUT.DCT dgn PLOTTED BY: seller -



SIGNING LEGEND:

EXISTING SIGN TO REMAIN

EXISTING SIGN TO BE REMOVED

PROPOSED SIGN

EXISTING SIGN POST

PROPOSED SIGN POST

PAVEMENT MARKING LEGEND:

5 INCH WHITE THERMOPLASTIC PAVEMENT MARKING - SOLID

5 INCH WHITE THERMOPLASTIC PAVEMENT MARKING - 10' STRIPE, 30' SKIP

5 INCH DOUBLE YELLOW THERMOPLASTIC PAVEMENT MARKING

12 INCH WHITE THERMOPLASTIC PAVEMENT MARKING

24 INCH WHITE THERMOPLASTIC PAVEMENT MARKING

WHITE PREFORMED THERMOPLASTIC PAVEMENT MARKING LEGEND AND SYMBOLS

SIGNAGE SCHEDULE			
SIGN ID	SIGN TYPE	QTY. (SHEET ALUMINUM)	SIGN SUPPORT
101	W2-1, W16-9P	6 SF	ONE 14' SQUARE PERFORATED STEEL TUBULAR SIGN POST
102	W11-15 (MOD), W16-7PL	10 SF	ONE 14' SQUARE PERFORATED STEEL TUBULAR SIGN POST
103	W11-15 (MOD), W13-1P	10.25 SF	ONE 14' SQUARE PERFORATED STEEL TUBULAR SIGN POST
104	W11-15 (MOD), W16-9P	10 SF	ONE 14' SQUARE PERFORATED STEEL TUBULAR SIGN POST
105	W2-1, W16-9P	6 SF	ONE 14' SQUARE PERFORATED STEEL TUBULAR SIGN POST
106	W11-15 (MOD), W16-7PL	10 SF	ONE 14' SQUARE PERFORATED STEEL TUBULAR SIGN POST
107	W11-15 (MOD), W13-1P	10.25 SF	ONE 14' SQUARE PERFORATED STEEL TUBULAR SIGN POST
108	W11-15 (MOD), W16-9P	10 SF	ONE 14' SQUARE PERFORATED STEEL TUBULAR SIGN POST

Sabra & Associates, Inc.

Designer's Name

7055 Samuel Morse Drive, #100

Address

Columbia, MD 21046

City/State/Zip

(443) 741-3500

Telephone Number

DESIGN

Landscape Architect

Date

Checked By:

Architect

Date

Checked By:

Engineer

Date

Checked By:

Drawn By

Date

Checked By:

Professional Certification. I hereby certify that these documents were prepared or approved by me, and that I am a duly licensed professional engineer under the laws of the State of Maryland.

License No. _____

Expiration Date _____

PROFESSIONAL STAMP

The Maryland-National Capital Park and Planning Commission

Montgomery County Department of Parks

9500 Brunnet Avenue

Silver Spring, Maryland 20901

(301) 495-2535

REVIEW AND APPROVAL

Project Manager

Date

Construction Manager

Date

Park Manager

Date

ISSUED FOR PROCUREMENT ON _____

REVISIONS

Rev. No.

Date

Description

Signing and Marking Plan

Capital Crescent Trail

SCALE: 1"=30'

DWG. # 1 of 3

SHT. # 19 of 24

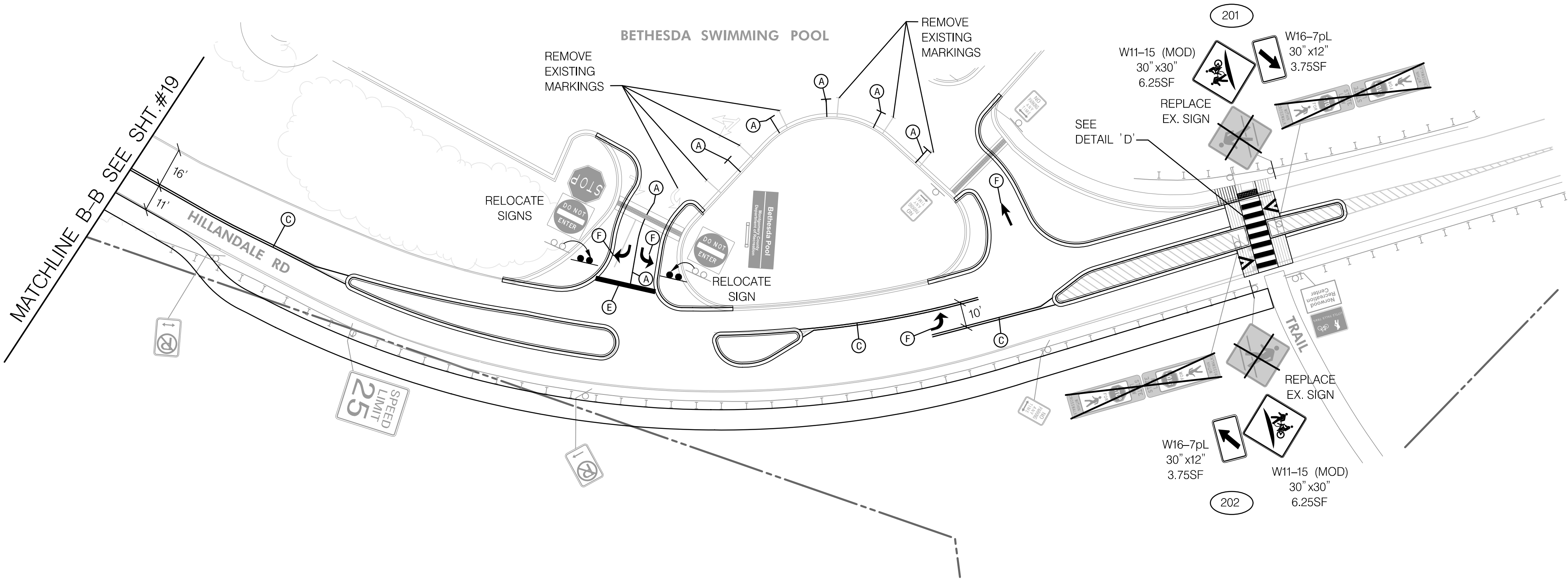
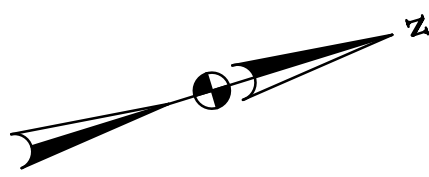
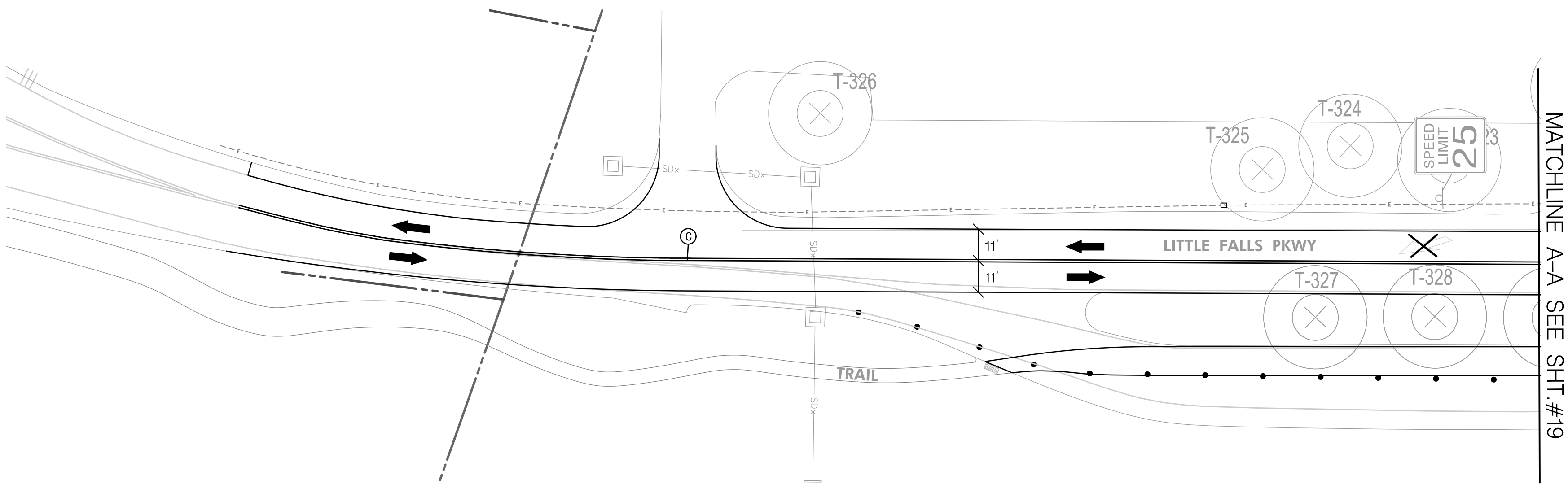
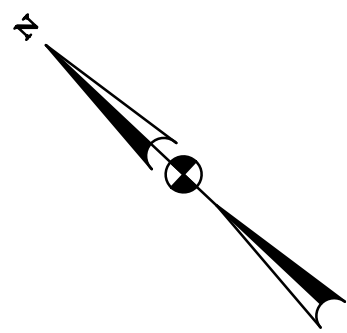
FINAL SCANNED:

PLAN SCANNED:


PARK CODE: NA


Wednesday, April 17, 2019 AT 02:00 PM


R:\2017\41 Capital Crescent Trail_MNCP\PC \$148 (DWG)30% Plans\30%L-Plan2 CCT.dgnPLOTTED BY: seller -





SIGNING LEGEND:

 EXISTING SIGN TO REMAIN


 EXISTING SIGN TO BE REMOVED


 PROPOSED SIGN


 EXISTING SIGN POST


 PROPOSED SIGN POST


PAVEMENT MARKING LEGEND:


 5 INCH WHITE THERMOPLASTIC PAVEMENT MARKING - SOLID

 5 INCH WHITE THERMOPLASTIC PAVEMENT MARKING - 10' STRIPE, 30' SKIP

 5 INCH DOUBLE YELLOW THERMOPLASTIC PAVEMENT MARKING

 12 INCH WHITE THERMOPLASTIC PAVEMENT MARKING

 24 INCH WHITE THERMOPLASTIC PAVEMENT MARKING

 WHITE PREFORMED THERMOPLASTIC PAVEMENT MARKING LEGEND AND SYMBOLS

SIGNAGE SCHEDULE			
SIGN ID	SIGN TYPE	QTY. (SHEET ALUMINUM)	SIGN SUPPORT
201	W11-15 (MOD), W16-7PL	10 SF	ONE 14' SQUARE PERFORATED STEEL TUBULAR SIGN POST
202	W11-15 (MOD), W16-7PL	10 SF	ONE 14' SQUARE PERFORATED STEEL TUBULAR SIGN POST

Sabra & Associates, Inc.

Designer's Name

7055 Samuel Morse Drive, #100

Address

Columbia, MD 21046

City/State/Zip

(443) 741-3500

Telephone Number

DESIGN

Landscape Architect

Date

Checked By:

Architect

Date

Checked By:

BW

04/2019

Engineer

Date

Checked By:

CK

04/2019

Drawn By

Date


Checked By:

Professional Certification. I hereby certify that these documents were prepared or approved by me, and that I am a duly licensed professional engineer under the laws of the State of Maryland.

LICENSE NO. _____

EXPIRATION DATE _____

PROFESSIONAL STAMP



The Maryland-National Capital Park and Planning Commission

Montgomery County Department of Parks

9500 Brunett Avenue

Silver Spring, Maryland 20901

(301) 495-2535

REVIEW AND APPROVAL

Project Manager

Construction Manager

Park Manager

Date

Date

Date

ISSUED FOR PROCUREMENT ON _____

REVISIONS

Rev. No.

Date

Description

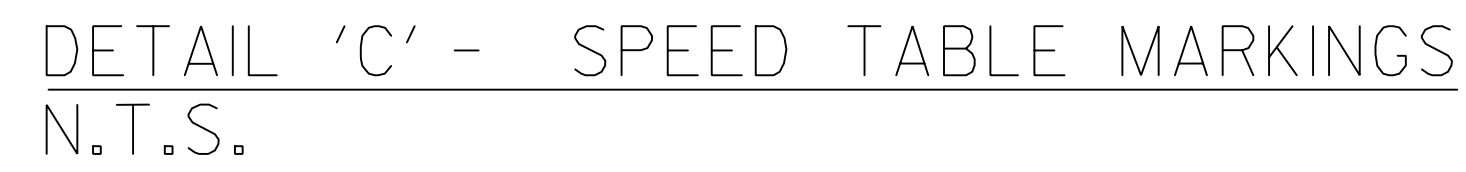
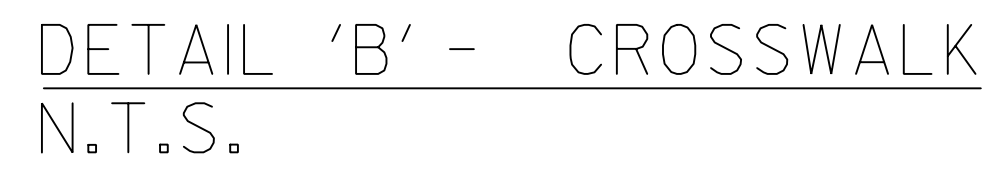
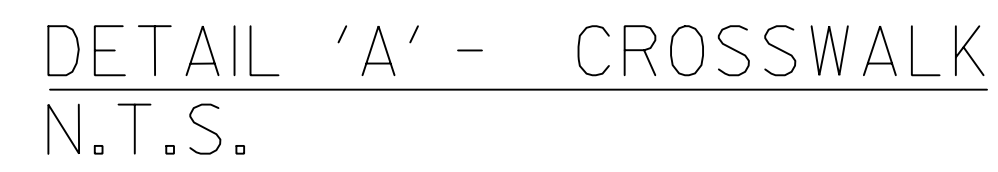
DWG. # 2 of 3


SHT. # 20 of 24

Signing and Marking Plan

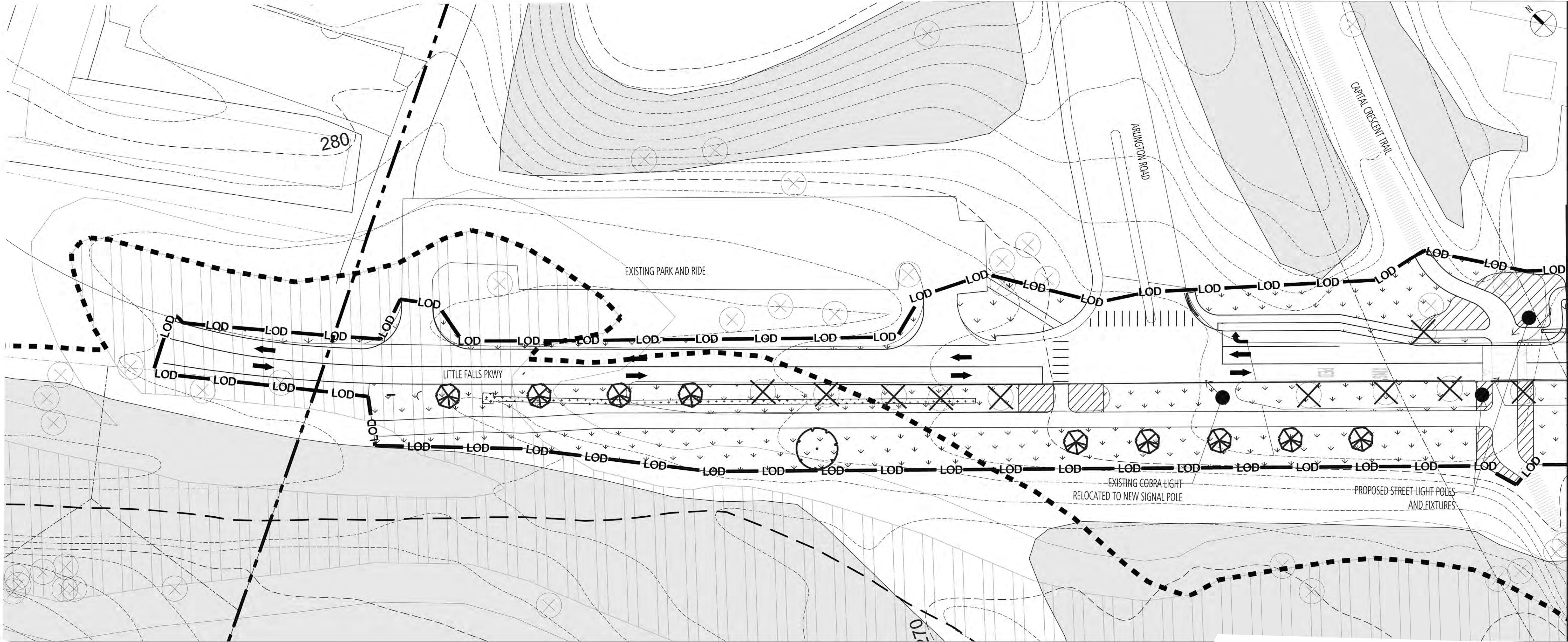
Capital Crescent Trail

SCALE: 1"=30'



<div>Sabra & Associates, Inc.</div> <div>Designer's Name</div> <div>7055 Samuel Morse Drive, #100</div> <div>Address</div> <div>Columbia, MD 21046</div> <div>City/State/Zip</div> <div>(443) 741-3500</div> <div>Telephone Number</div>	DESIGN		<div>Professional Certification. I hereby certify that these documents were prepared or approved by me, and that I am a duly licensed professional engineer under the laws of the State of Maryland.</div> <div>License No. _____</div> <div>Expiration Date _____</div> <div>PROFESSIONAL STAMP</div>	<div></div> <div>The Maryland-National Capital Park and Planning Commission</div> <div>Montgomery County Department of Parks</div> <div>9500 Brunett Avenue</div> <div>Silver Spring, Maryland 20901</div> <div>(301) 495-2535</div>	REVIEW AND APPROVAL		ISSUED FOR PROCUREMENT ON _____		<div>Signing and Marking Details</div> <div>Capital Crescent Trail</div> <div>SCALE: N/A</div>	<div>DWG. # <u>3</u> of <u>3</u></div> <div>SHT. # <u>21</u> of <u>24</u></div>		
							REVISIONS					
	Landscape Architect	Date			Checked By:	Project Manager	Date	Rev. No.			Date	Description
	Architect	Date			Checked By:	Construction Manager	Date					
	Engineer	04/2019			Checked By:	Park Manager	Date					
	CK	04/2019										
	Drawn By	Date	Checked By:									

NOTE:
1. TREES LOCATED IN THE EXISTING MEDIAN ARE ALL RELATIVELY IMMATURE AND ARE RECOMMENDED FOR RELOCATION ON SITE WITH LITTLE TO NO HEALTH IMPACT ANTICIPATED.



LEGEND

- PROPOSED STREET LIGHT
- PROPERTY LINE
- EXISTING TREE
- EXISTING FOREST COVER
- STREAM
- STREAM BUFFER
- 100 YEAR FLOODPLAIN & BLR
- INDEX CONTOUR
- INTERMEDIATE CONTOUR
- SOIL LINE
- WETLAND
- EXISTING TRAIL
- LOD LIMIT OF DISTURBANCE
- PROPOSED ROADWAY
- EXISTING TREE TO BE REMOVED
- PROPOSED PERENNIAL PLANTINGS
- PROPOSED TURFGRASS SOD
- PROPOSED SWM FACILITY (ORNAMENTAL GRASS PLANTINGS)
- PROPOSED LARGE CANOPY TREE
- PROPOSED SMALL/MEDIUM CANOPY TREE



PLANT SCHEDULE (This Sheet Only)							
Key	Quantity	Botanical Name	Common Name	Size	Root	Spacing/	Comments
Shade Tree Species (Overstory)							
ST	1			2" Cal.	B&B / CG		
Ornamental Tree Species (Understory)							
OT	10			5' Ht.	B&B / CG	Multi-stem (3 or 5)	
Perennial Beds							
PB	134	Perennial / Planting Beds (SY)					
Stormwater Management							
SWM	36	SWM Planting Zone (SY)					
Seed Mixes and Treatment							
TURF	1,391	Turfgrass Sod Establishment (SY)					
Misc.							
MULCH	178	Mulch - 3" Depth (SY)					

Floura Teeter

Designer's Name

800 North Charles Street, Ste. 300

Address

Baltimore, MD 21201

City/State/Zip

(410) 528-8395

Telephone Number

DESIGN

Landscape Architect	Date	Checked By:
Architect	Date	Checked By:
MM	04/2019	
Engineer	Date	Checked By:
AL	04/2019	MM
Drawn By	Date	Checked By:

Professional Certification. I hereby certify that these documents were prepared or approved by me, and that I am a duly licensed professional engineer under the laws of the State of Maryland.

License No. _____

Expiration Date _____

PROFESSIONAL STAMP

The Maryland-National Capital Park and Planning Commission

Montgomery County Department of Parks

9500 Brunett Avenue

Silver Spring, Maryland 20901

(301) 495-2535

REVIEW AND APPROVAL

Project Manager	Date
Construction Manager	Date
Park Manager	Date

ISSUED FOR PROCUREMENT ON _____

REVISIONS

Rev. No.	Date	Description

Landscaping Plan

Capital Crescent Trail

SCALE: 1"=20'

DWG. # 1 of 3



SHT. # 22 of 24

1. TREES LOCATED IN THE EXISTING MEDIAN ARE ALL RELATIVELY IMMATURE AND ARE RECOMMENDED FOR RELOCATION ON SITE WITH LITTLE TO NO HEALTH IMPACT ANTICIPATED.



Age Group	Condition A (%)	Condition B (%)	Condition C (%)	Condition D (%)
7	0	0	0	0
10	15	15	15	15
13	30	15	30	30
16	60	15	30	30

PLANT SCHEDULE (This Sheet Only)						
Key	Quantity	Botanical Name	Common Name	Size	Root	Spacing/ Comments
Shade Tree Species (Overstory)						
ST	1			2" Cal.	B&B / CG	
Ornamental Tree Species (Understory)						
OT	5			5' Ht.	B&B / CG	Multi-stem (3 or 5)
Perennial Beds						
PB	168	Perennial / Planting Beds (SY)				
Stormwater Management						
SWM	0	SWM Planting Zone (SY)				
Seed Mixes and Treatment						
TURF	1,041	Turfgrass Sod Establishment (SY)				
Misc.						
MULCH	172	Mulch - 3" Depth (SY)				

		DESIGN		Professional Certification. I hereby certify that these documents were prepared or approved by me, and that I am a duly licensed professional engineer under the laws of the State of Maryland.						The Maryland-National Capital Park and Planning Commission Montgomery County Department of Parks 9500 Brunett Avenue Silver Spring, Maryland 20901 (301) 495-2535		REVIEW AND APPROVAL		ISSUED FOR PROCUREMENT ON _____		<div> <div>DWG. # <u> 2 </u> of <u> 3 </u></div> <div>SHT. # <u> 23 </u> of <u> 24 </u></div> </div>		<div> <div>Landscaping Plan</div> <div>Capital Crescent Trail</div> </div>		SCALE: 1"=20'	
Floura Teeter Designer's Name 800 North Charles Street, Ste. 300 Address Baltimore, MD 21201 City/State/Zip (410) 528-8395 Telephone Number		Landscape Architect Date Checked By: _____ Architect Date Checked By: _____ MM 04/2019 Engineer Date Checked By: _____ AL 04/2019 MM Drawn By Date Checked By: _____										Project Manager Date Construction Manager Date Park Manager Date		REVISIONS Rev. No. Date Description 							

FINAL SCANNED:

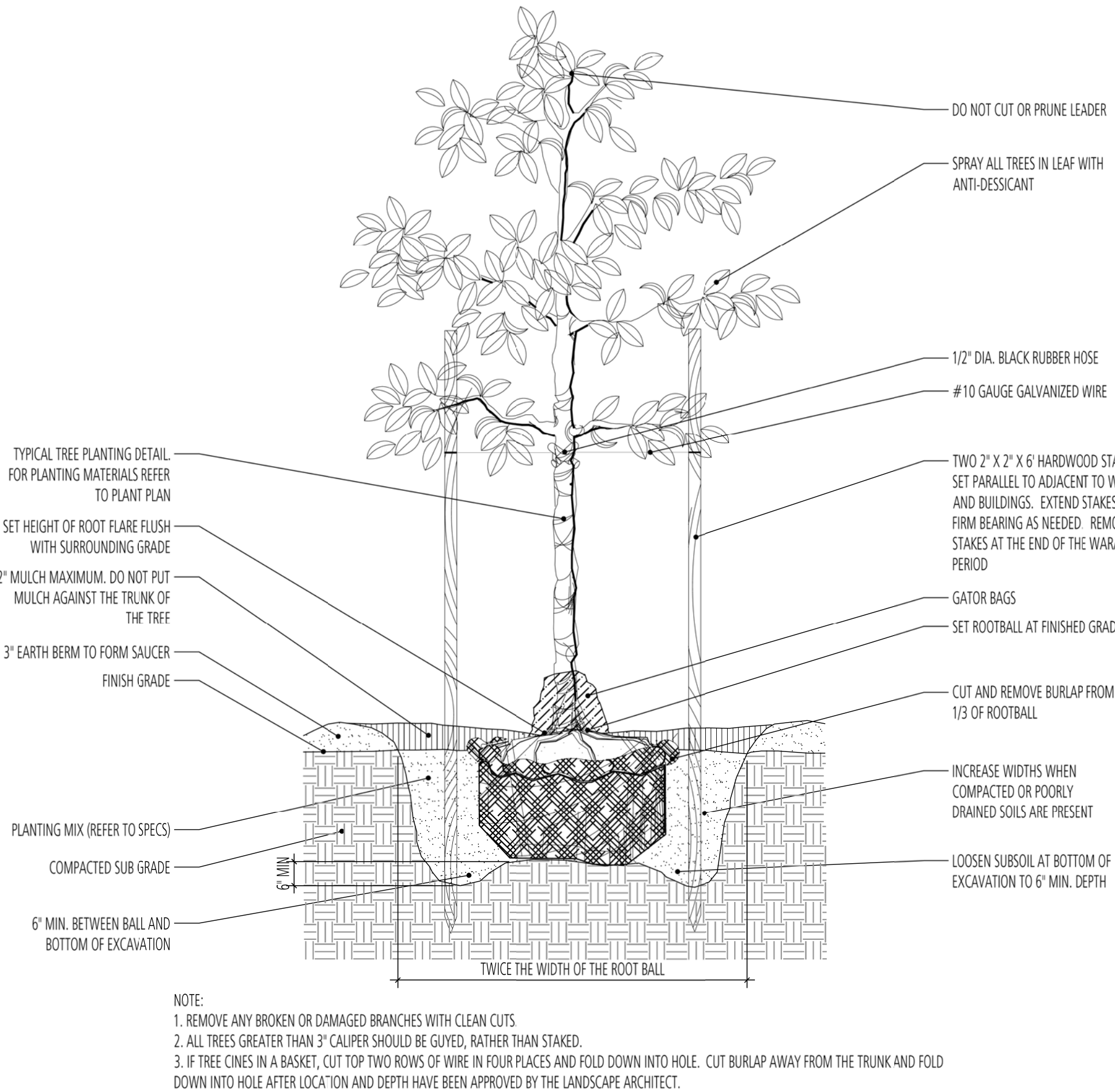
PLAN SCANNED:

PARK CODE: NA

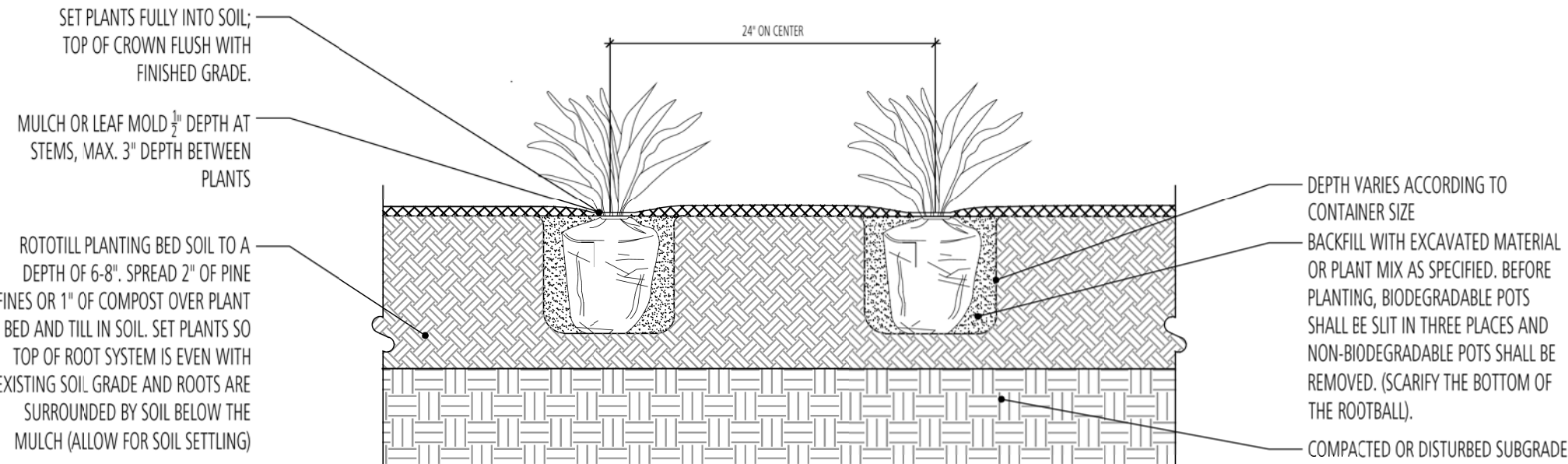
SCATTERING

PLOTTED BY: SUBSERVANTS

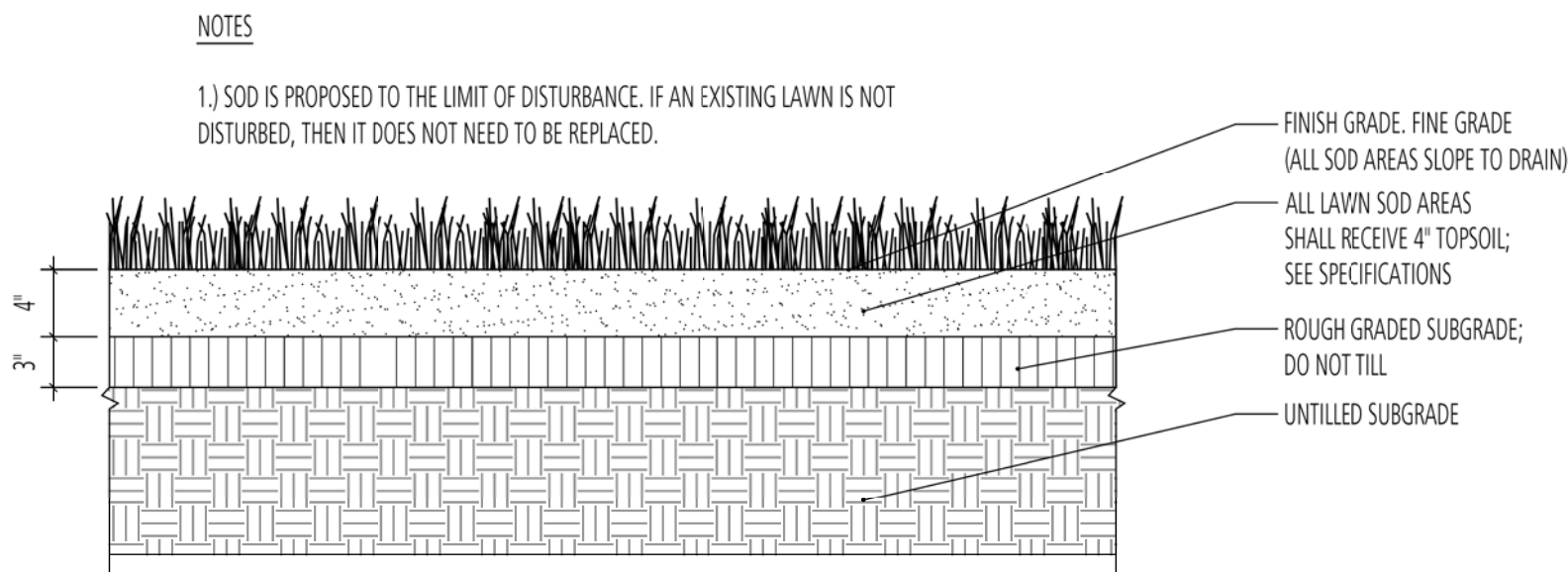
SPILLS



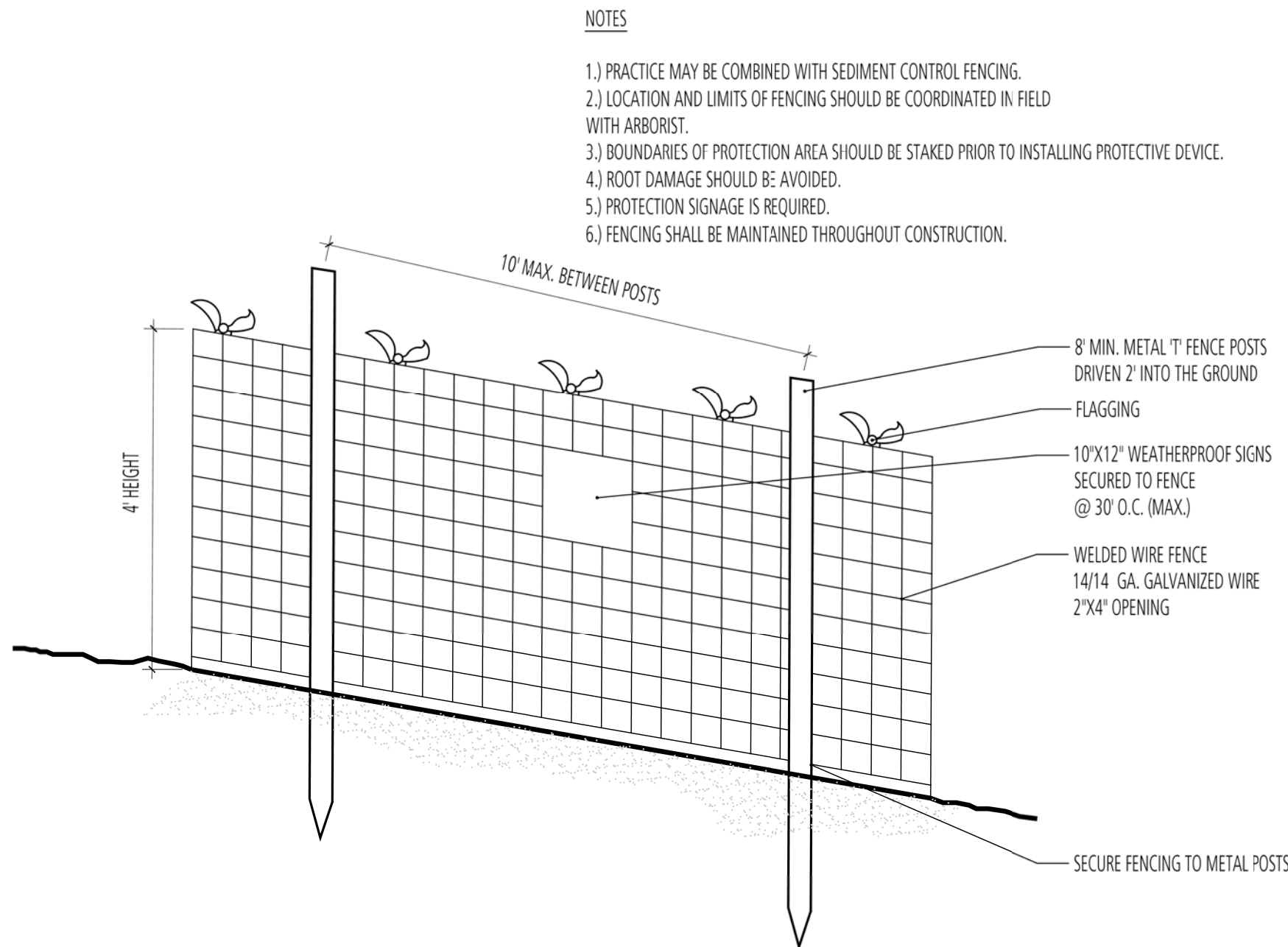
1 TREE PLANTING DETAIL SECTION
d-tree_planting.dwg
SCALE: 1/2" = 1'-0"



2 PERENNIAL PLANTING DETAIL SECTION
d-grndcover_planting.dwg
SCALE: 1/2" = 1'-0"



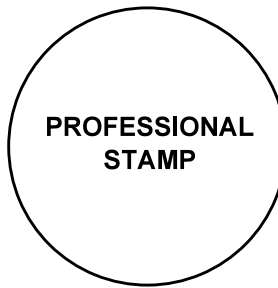
3 TURFGRASS SOD DETAIL SECTION
d-lawn detail sod.dwg
SCALE: 1" = 1'-0"



4 TREE PROTECTION FENCE - MONTGOMERY COUNTY DETAIL
d-TPF.dwg
SCALE: 1" = 1'-0"

MASTER PLANT SCHEDULE						
Key	Quantity	Botanical Name	Common Name	Size	Root	Spacing/ Comments
Shade Tree Species (Overstory)						
ST	2			2" Cal.	B&B / CG	
Ornamental Tree Species (Understory)						
OT	15			5' Ht.	B&B / CG	Multi-stem (3 or 5)
Perennial Beds						
PB	302	Perennial / Planting Beds (SY)				
Stormwater Management						
SWM	36	SWM Planting Zone (SY)				
Seed Mixes and Treatment						
TURF	2,432	Turfgrass Sod Establishment (SY)				
Misc.						
MULCH	350	Mulch - 3" Depth (SY)				

DESIGN			Professional Certification. I hereby certify that these documents were prepared or approved by me, and that I am a duly licensed professional engineer under the laws of the State of Maryland. License No. _____ Expiration Date _____
Floura Teeter			
Designer's Name			
800 North Charles St., Ste. 300			
Address	Landscape Architect	Date	
Baltimore, MD 21201	Architect	Date	
City/State/Zip	MM	04/2019	
(410) 528-8395	Engineer	Date	
Telephone Number	AL	04/2019	Checked By: MM
	Drawn By	Date	Checked By:



The Maryland-National Capital Park and Planning Commission
Montgomery County Department of Parks
9500 Brunett Avenue
Silver Spring, Maryland 20901
(301) 495-2535

REVIEW AND APPROVAL		ISSUED FOR PROCUREMENT ON _____	
		REVISIONS	
Rev. No.	Date	Description	
Project Manager	Date		
Construction Manager	Date		
Park Manager	Date		

Landscape Details Capital Crescent Trail

SCALE: AS SHOWN

DWG. # 3 of 3

SHT. # 24 of 24

Appendix B

Natural Resource Inventory / Forest Stand
Delineation Drawings and Approval Letter



APPENDIX B: NATURAL RESOURCE INVENTORY / FOREST STAND
DELINEATION

MONTGOMERY COUNTY PLANNING DEPARTMENT
THE MARYLAND NATIONAL CAPITAL PARK AND PLANNING COMMISSION

March 25, 2019

Mr. Andrew Tsai
Montgomery County Parks
9500 Brunett Avenue
Silver Spring, MD 20901

Re: Capital Crescent Trail Crossing at Little Falls Parkway
Forest Conservation Exemption Request and Simplified NRI/FSD Plan No. 42019124E
Action Taken: Confirmed and Approved on 3/25/2019

Dear Mr. Andrew Tsai:

On March 21, 2019, the Development Applications and Regulatory Coordination staff of the Montgomery County Planning Department, received a revised Simplified Natural Resource Inventory / Forest Stand Delineation "Simplified NRI/FSD" for the Capital Crescent Trail Crossing at Little Falls Parkway. This Simplified NRI/FSD is part of a Chapter 22A-5(e) forest conservation exemption request for safety improvements within the right-of-way of Little Falls Parkway. This Chapter 22A-5(e) exemption request is subject to Chapter 22A-9 for County Highway Projects.

The review of the exemption request is complete. The Simplified NRI/FSD includes a limit of disturbance. No forest will be impacted by the project. The required forest conservation law inspections to be requested by the Applicant are listed on the attached sequence of events. Notes stating that a Tree Save Plan will be submitted for review and approval at the time of sediment control permit application are included on the Simplified NRI/FSD.

Forest Conservation Exemption Request No. 42019124E for Capital Crescent Trail Crossing at Little Falls Parkway is confirmed. The Simplified NRI/FSD submitted on March 21, 2019 for the project is approved with the condition that a Tree Save Plan will be submitted for review and approval at the time of the sediment control permit application.

If, in the future, changes are planned to the confirmed Exemption Request, another forest conservation review may be required.

Sincerely,

A handwritten signature in black ink that reads "Stephen Peck".

Stephen Peck
Senior Planner and Inspector
Development Applications and Regulatory Coordination
M-NCPPC - Montgomery County Planning Department

CC: Megan Maffeo, Floura Teeter

FOREST STAND DELINEATION: NARRATIVE

STAND A (12.10 AC) - PRIORITY 1

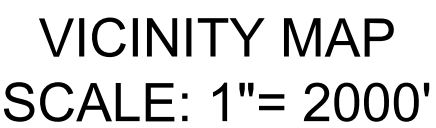
STAND B (3.27 AC) - PRIORITY 1

STAND C (10.17 AC) - PRIORITY 2

[illegible]

NOTE: AREA CALCULATION EXCLUDE RIGHT-OF-WAY FOR FOR TRAIL

1 KEY MAP
PLAN
SCALE: 1"=200'-0"



1. THE SUBJECT PROPERTY IS LOCATED IN MONTGOMERY COUNTY AT THE CROSSROADS OF LITTLE FALLS PARKWAY AND HILLELAND ROAD, LATITUDE 38°58'21.8"N, LONGITUDE 77°06'01.7"W.
2. THE SUBJECT PROPERTY IS LOCATED WITHIN THE WILLET BRANCH WATERSHED, 12-DIGIT # #: 021402020844 (USE CLASS I-P). DATA RETRIEVED FROM MONTGOMERY COUNTY GIS DATABASE, CREDIT: MDE, MD IMAP.
3. ALL PARCELS LOCATED BY MARYLAND NATIONAL CAPITOL PARK & PLANNING COMMISSION. THE STUDY AREA FOR SITE IMPROVEMENTS INCLUDES ONLY PORTIONS OF THE SUBJECT PROPERTY.
4. TWO WATER BODIES FLOW THROUGH THE SUBJECT PROPERTY: THE WILLET BRANCH AND AN UNNAMED TRIBUTARY TO THE WILLET BRANCH (USE CLASS I-P). DATA RETRIEVED FROM MONTGOMERY COUNTY GIS DATABASE, CREDIT: MDE, MD IMAP.
5. THE SUBJECT PROPERTY IS NOT LOCATED WITHIN A SPA OR PMA.
6. THERE ARE NO KNOWN EPHEMERAL STREAMS WITHIN 50 FEET OF THE SUBJECT PROPERTY. THE WILLET BRANCH AND ITS UNNAMED TRIBUTARY ARE CULVERTED IN FIVE LOCATIONS AS THEY RUN THROUGH THE SUBJECT PROPERTY.
7. A PRELIMINARY 100 YEAR FLOODPLAIN, INCLUDING NON-TIDAL WETLAND AREA, IS SITED WITHIN THE SUBJECT PROPERTY, OR WITHIN 100 FEET THEREOF. THE SUBJECT PROPERTY IS LOCATED ON FEMA FIRM MAP 24031C0455D. DATA RETRIEVED FROM MONTGOMERY COUNTY GIS DATABASE, CREDIT: MDE, MD IMAP.
8. NO RARE, THREATENED, OR ENDANGERED SPECIES WERE OBSERVED WITHIN THE STUDY AREA AS PER FLOURA TEETER STAFF SITE VISIT AND USFWS IPaC REVIEW SYSTEM.
9. SLOPES OF 25% OR STEEPER ARE PRESENT THROUGHOUT THE STUDY AREA. THERE ARE SLOPES BETWEEN 15-25% ON ERODIBLE SOILS PRESENT IN THE STUDY AREA.
10. THERE ARE NO HISTORICAL OR CULTURAL FEATURES WITHIN THE STUDY AREA AS PER FLOURA TEETER STAFF SITE VISIT OR PER DATA FROM THE MONTGOMERY COUNTY DIGITAL ATLAS OF HISTORIC RESOURCES.
11. INVASIVE PLANTS LOCATED IN THE STUDY AREA INCLUDE: BUSH HONEYSUCKLE, JAPANESE HONEYSUCKLE, ORIENTAL BITTERSWEET, ENGLISH IVY, AND COMMON BARBERY.
12. FIELDWORK WAS CONDUCTED BY FLOURA TEETER STAFF BETWEEN DECEMBER 2017 AND JANUARY 2018 WITH OVERSIGHT FROM MEGAN MAFFEO, PLA (LICENSE #: 3385). TREE D.B.H. WAS MEASURED USING FORESTRY DIAMETER TAPE.
13. FLOURA TEETER STAFF RECORDED A *CARPINUS CAROLINIANA* DURING FIELDWORK THAT WILL BE SUBMITTED TO THE MARYLAND BIG TREE PROGRAM STAFF FOR THEIR CONSIDERATION AS A STATE CHAMPION SPECIMEN. NO OTHER TREES WITHIN 75% SIZE OF ANY CURRENT STATE OR MONTGOMERY COUNTY CHAMPIONS WERE OBSERVED.


THE FOLLOWING EXEMPTION IS BEING REQUESTED:

CHAPTER 22A OF THE MONTGOMERY COUNTY FOREST CONSERVATION LAW 2014, SEC. 22A-5 EXEMPTIONS, PARAGRAPH E STATES, "A STATE OR COUNTY HIGHWAY CONSTRUCTION ACTIVITY THAT IS SUBJECT TO SECTION 5-103 OF THE NATURAL RESOURCES ARTICLE OF THE MARYLAND CODE, OR SECTION 22A-9."

THIS PROJECT IS A COUNTY HIGHWAY PROJECT WITH CAPITAL IMPROVEMENT PLAN FUNDING AND MINIMAL FOREST CLEARING. NO CHAMPION, SPECIMEN, OR SIGNIFICANT TREES WILL BE IMPACTED BY IMPLEMENTATION OF THIS PLAN. THE IMPACTS ARE DUE TO CHANGES TO ROADWAY/INTERSECTIONS AND TRAIL CROSSING IMPROVEMENTS TO MAXIMIZE SAFETY AND ENSURE ADEQUATE SIGHT DISTANCE. A TOTAL OF NINE INDIVIDUAL TREES LOCATED OUTSIDE OF FORESTED AREAS WILL NEED TO BE REMOVED AND/OR RELOCATED WITHIN THE PROJECT LOD. THE CALCULATED CANOPY AREA OF IMPACTED TREES TOTALS 3,620 SF AS SHOWN IN THE CHART BELOW.

OF THE NINE TREES, NONE ARE LARGER THAN 16" DBH. ONE TREE (T-338) SHOWS SEVERE HEALTH ISSUES AND IS RECOMMENDED FOR REMOVAL REGARDLESS OF PROJECT IMPACT. EIGHT TREES (T-314, T-315, T-317, T-334, T-336, T-338, T-339, T-340) ALL RELATIVELY IMMATURE, ARE RECOMMENDED FOR RELOCATION ON SITE WITH LITTLE TO NO HEALTH IMPACT ANTICIPATED.

*Canopy Area calculated at a radius of 1.5' x DBH.



FLOURA TEETER
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Baltimore, Maryland 21201
Phone: 410.528.8395
Fax: 410.528.8425

REVISION: DATE:
2019-03-11

DRAWN: JB, AL

CHECKED: MM, JF

NATURAL RESOURCE
INVENTORY / FOREST STAND
DELINEATION PLAN
SNRI-42019124E

DATE: 2019-03-06

SCALE: 1" = 200'

FSD 1.0

SIGNIFICANT AND SPECIMEN TREES (STUDY AREA ONLY)

TAG NO.	COMMON NAME	BOTANICAL NAME	DBH	CONDITION	COMMENTS
T-1	Silver Maple	<i>Acer saccharinum</i>	25	Good	Located in maintained lawn area
T-12	Black Walnut	<i>Juglans nigra</i>	25	Good	Located in maintained lawn area
T-13	Slippery Elm	<i>Ulmus rubra</i>	30.5	Good	Located in maintained lawn area
T-21	American Sycamore	<i>Platanus occidentalis</i>	39	Excellent	Located in maintained lawn area
T-22	American Sycamore	<i>Platanus occidentalis</i>	28.5	Excellent	Located in maintained lawn area
T-23	Dawn Redwood	<i>Metasequoia glyptostroboides</i>	25.5	Excellent	Located in maintained lawn area
T-32	Slippery Elm	<i>Ulmus rubra</i>	33	Excellent	Located in maintained lawn area
T-33	White Pine	<i>Pinus strobus</i>	35	Excellent	Located in maintained lawn area
T-37	Tulip Poplar	<i>Liriodendron tulipifera</i>	27	Good	Located within forested area
T-38	Virginia Pine	<i>Pinus virginiana</i>	26	Good	Located within forested area
T-39	Tulip Poplar	<i>Liriodendron tulipifera</i>	28	Good	Located within forested area
T-40	Tulip Poplar	<i>Liriodendron tulipifera</i>	24	Good	Located within forested area
T-41	Red Oak	<i>Quercus rubra</i>	34	Good	Located within forested area
T-42	Eastern Cottonwood	<i>Populus deltoides</i>	35	Good	Located within forested area
T-43	Red Oak	<i>Quercus rubra</i>	24	Good	Located within forested area
T-44	Red Oak	<i>Quercus rubra</i>	72	Good	Located within forested area
T-45	Red Oak	<i>Quercus rubra</i>	24	Good	Located within forested area
T-46	Tulip Poplar	<i>Liriodendron tulipifera</i>	27	Good	Located within forested area
T-47	Red Oak	<i>Quercus rubra</i>	26	Good	Located within forested area
T-48	Tulip Poplar	<i>Liriodendron tulipifera</i>	40	Good	Located within forested area
T-49	Tulip Poplar	<i>Liriodendron tulipifera</i>	51	Good	Located within forested area
T-50	Red Oak	<i>Quercus rubra</i>	24	Good	Located within forested area
T-51	Tulip Poplar	<i>Liriodendron tulipifera</i>	25	Good	Located within forested area
T-52	Tulip Poplar	<i>Liriodendron tulipifera</i>	33	Good	Located within forested area
T-53	Tulip Poplar	<i>Liriodendron tulipifera</i>	26	Good	Located within forested area
T-54	Tulip Poplar	<i>Liriodendron tulipifera</i>	29	Good	Located within forested area
T-55	Tulip Poplar	<i>Liriodendron tulipifera</i>	25	Good	Located within forested area
T-56	Tulip Poplar	<i>Liriodendron tulipifera</i>	29	Good	Located within forested area
T-57	Tulip Poplar	<i>Liriodendron tulipifera</i>	27	Good	Located within forested area
T-58	Tulip Poplar	<i>Liriodendron tulipifera</i>	27	Good	Located within forested area
T-59	Tulip Poplar	<i>Liriodendron tulipifera</i>	33	Good	Located within forested area
T-60	Tulip Poplar	<i>Liriodendron tulipifera</i>	25	Good	Located within forested area
T-61	Tulip Poplar	<i>Liriodendron tulipifera</i>	29	Good	Located within forested area
T-62	Tulip Poplar	<i>Liriodendron tulipifera</i>	33	Good	Located within forested area
T-63	Tulip Poplar	<i>Liriodendron tulipifera</i>	33	Good	Located within forested area
T-64	Tulip Poplar	<i>Liriodendron tulipifera</i>	45	Good	Located within forested area
T-65	Tulip Poplar	<i>Liriodendron tulipifera</i>	46	Good	Located within forested area
T-66	Tulip Poplar	<i>Liriodendron tulipifera</i>	43	Good	Located within forested area
T-67	Tulip Poplar	<i>Liriodendron tulipifera</i>	26	Good	Located within forested area
T-68	Red Oak	<i>Quercus rubra</i>	26	Good	Located within forested area
T-69	Tulip Poplar	<i>Liriodendron tulipifera</i>	38	Good	Located within forested area
T-70	Pin Oak	<i>Quercus palustris</i>	45	Good	Located within forested area
T-71	Tulip Poplar	<i>Liriodendron tulipifera</i>	31	Good	Located within forested area
T-72	Tulip Poplar	<i>Liriodendron tulipifera</i>	30	Good	Located within forested area
T-73	Tulip Poplar	<i>Liriodendron tulipifera</i>	28.5	Good	Located within forested area
T-74	Tulip Poplar	<i>Liriodendron tulipifera</i>	32	Good	Located within forested area
T-75	Tulip Poplar	<i>Liriodendron tulipifera</i>	54	Good	Located within forested area
T-76	White Oak	<i>Quercus alba</i>	26	Good	Located within forested area
T-77	Tulip Poplar	<i>Liriodendron tulipifera</i>	40	Good	Located within forested area
T-78	Tulip Poplar	<i>Liriodendron tulipifera</i>	39	Good	Located within forested area
T-79	Tulip Poplar	<i>Liriodendron tulipifera</i>	35	Good	Located within forested area
T-80	Tulip Poplar	<i>Liriodendron tulipifera</i>	38	Good	Located within forested area
T-81	Tulip Poplar	<i>Liriodendron tulipifera</i>	43	Good	Located within forested area
T-82	Tulip Poplar	<i>Liriodendron tulipifera</i>	38	Good	Located within forested area
T-83	Tulip Poplar	<i>Liriodendron tulipifera</i>	31	Good	Located within forested area
T-84	Tulip Poplar	<i>Liriodendron tulipifera</i>	39	Good	Located within forested area
T-85	Tulip Poplar	<i>Liriodendron tulipifera</i>	37	Good	Located within forested area
T-86	Tulip Poplar	<i>Liriodendron tulipifera</i>	32	Good	Located within forested area
T-87	Tulip Poplar	<i>Liriodendron tulipifera</i>	34	Good	Located within forested area
T-88	Tulip Poplar	<i>Liriodendron tulipifera</i>	35	Good	Located within forested area
T-89	Tulip Poplar	<i>Liriodendron tulipifera</i>	29	Good	Located within forested area
T-90	Tulip Poplar	<i>Liriodendron tulipifera</i>	29	Good	Located within forested area
T-91	Tulip Poplar	<i>Liriodendron tulipifera</i>	42	Good	Located within forested area
T-92	Tulip Poplar	<i>Liriodendron tulipifera</i>	29	Good	Located within forested area
T-93	Tulip Poplar	<i>Liriodendron tulipifera</i>	31	Good	Located within forested area
T-94	Tulip Poplar	<i>Liriodendron tulipifera</i>	26	Good	Located within forested area
T-95	Tulip Poplar	<i>Liriodendron tulipifera</i>	29	Good	Located within forested area
T-96	Tulip Poplar	<i>Liriodendron tulipifera</i>	27	Good	Located within forested area
T-97	Tulip Poplar	<i>Liriodendron tulipifera</i>	27	Good	Located within forested area
T-98	Tulip Poplar	<i>Liriodendron tulipifera</i>	29	Good	Located within forested area
T-99	Tulip Poplar	<i>Liriodendron tulipifera</i>	24	Good	Located within forested area
T-100	Tulip Poplar	<i>Liriodendron tulipifera</i>	31	Good	Located within forested area
T-101	Red Oak	<i>Quercus rubra</i>	24	Good	Located within forested area
T-102	Tulip Poplar	<i>Liriodendron tulipifera</i>	34	Good	Located within forested area
T-103	Tulip Poplar	<i>Liriodendron tulipifera</i>	27	Good	Located within forested area
T-104	Tulip Poplar	<i>Liriodendron tulipifera</i>	26	Good	Located within forested area
T-105	Tulip Poplar	<i>Liriodendron tulipifera</i>	28	Good	Located within forested area
T-106	Tulip Poplar	<i>Liriodendron tulipifera</i>	31	Good	Located within forested area
T-107	Green Ash	<i>Fraxinus pennsylvanica</i>	26	Fair	Located adjacent to trail; edge of forest
T-108	Green Ash	<i>Fraxinus pennsylvanica</i>	33	Fair	Located adjacent to trail; edge of forest
T-109	Tulip Poplar	<i>Liriodendron tulipifera</i>	24	Good	Located within forested area

NATURAL RESOURCE INVENTORY / FOREST STAND
DELINEATION

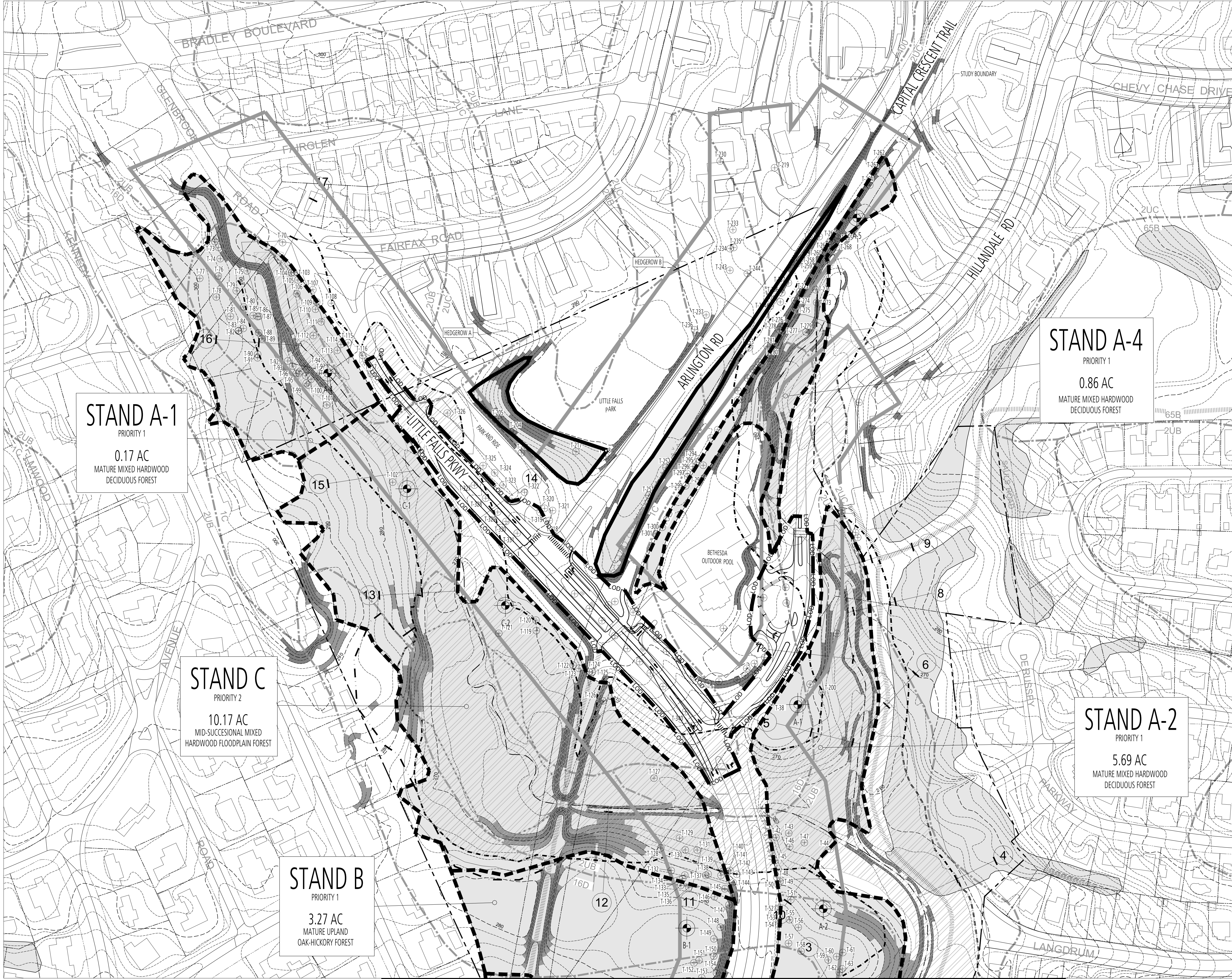
TAG NO.	COMMON NAME	BOTANICAL NAME	DBH	CONDITION	COMMENTS
T-110	Tulip Poplar	<i>Liriodendron tulipifera</i>	28	Good	Located within forested area
T-111	Tulip Poplar	<i>Liriodendron tulipifera</i>	27	Good	Located within forested area
T-112	Tulip Poplar	<i>Liriodendron tulipifera</i>	26	Good	Located within forested area
T-113	Tulip Poplar	<i>Liriodendron tulipifera</i>	26	Good	Located within forested area
T-114	Tulip Poplar	<i>Liriodendron tulipifera</i>	25	Good	Located within forested area
T-115	Red Maple	<i>Acer rubrum</i>	25	Good	Located within forested area
T-116	Red Maple	<i>Acer rubrum</i>	27	Good	Located within forested area
T-117	Red Maple	<i>Acer rubrum</i>	29	Good	Located within forested area
T-118	American Sycamore	<i>Platanus occidentalis</i>	45	Good	Located within forested area
T-119	Red Maple	<i>Acer rubrum</i>	25	Good	Located within forested area
T-120	Slippery Elm	<i>Ulmus rubra</i>	36	Good	Located within forested area
T-121	Red Maple	<i>Acer rubrum</i>	34	Good	Located within forested area
T-122	Tulip Poplar	<i>Liriodendron tulipifera</i>	39	Good	Located within forested area
T-123	Tulip Poplar	<i>Liriodendron tulipifera</i>	27	Good	Located within forested area
T-124	Tulip Poplar	<i>Liriodendron tulipifera</i>	32	Good	Located within forested area
T-125	Mockernut Hickory	<i>Carya tomentosa</i>	28	Good	Located within forested area
T-126	Tulip Poplar	<i>Liriodendron tulipifera</i>	29	Good	Located within forested area
T-127	Red Maple	<i>Acer rubrum</i>	41	Good	Located within forested area
T-128	Tulip Poplar	<i>Liriodendron tulipifera</i>	28	Good	Located within forested area
T-129	Slippery Elm	<i>Ulmus rubra</i>	27	Good	Located within forested area
T-130	Tulip Poplar	<i>Liriodendron tulipifera</i>	33	Good	Located within forested area
T-131	Tulip Poplar	<i>Liriodendron tulipifera</i>	24	Good	Located within forested area
T-132	Tulip Poplar	<i>Liriodendron tulipifera</i>	45	Good	Located within forested area
T-133	Tulip Poplar	<i>Liriodendron tulipifera</i>	44	Good	Located within forested area
T-134	Tulip Poplar	<i>Liriodendron tulipifera</i>	26	Good	Located within forested area
T-135	Tulip Poplar	<i>Liriodendron tulipifera</i>	26	Good	Located within forested area
T-136	Tulip Poplar	<i>Liriodendron tulipifera</i>	39	Good	Located within forested area
T-137	Tulip Poplar	<i>Liriodendron tulipifera</i>	39	Good	Located within forested area
T-138	Tulip Poplar	<i>Liriodendron tulipifera</i>	38	Good	Located within forested area
T-139	Tulip Poplar	<i>Liriodendron tulipifera</i>	29	Good	Located within forested area
T-140	Tulip Poplar	<i>Liriodendron tulipifera</i>	24	Good	Located within forested area
T-141	Tulip Poplar	<i>Liriodendron tulipifera</i>	35	Good	Located within forested area
T-142	Tulip Poplar	<i>Liriodendron tulipifera</i>	30	Good	Located within forested area
T-143	Mockernut Hickory	<i>Carya tomentosa</i>	25	Good	Located within forested area
T-144	Red Oak	<i>Quercus rubra</i>	37	Good	Located within forested area
T-145	Mockernut Hickory	<i>Carya tomentosa</i>	24	Good	Located within forested area
T-146	Tulip Poplar	<i>Liriodendron tulipifera</i>	39	Good	Located within forested area
T-147	White Pine	<i>Pinus strobus</i>	24	Good	Located within forested area
T-148	Tulip Poplar	<i>Liriodendron tulipifera</i>	35	Good	Located within forested area
T-149	Black Oak	<i>Quercus velutina</i>	41	Good	Located within forested area
T-150	White Oak	<i>Quercus alba</i>	24	Good	Located within forested area
T-151	White Oak	<i>Quercus alba</i>	31	Good	Located within forested area
T-152	White Oak	<i>Quercus alba</i>	24	Good	Located within forested area
T-153	White Oak	<i>Quercus alba</i>	27	Good	Located within forested area
T-154	White Oak	<i>Quercus alba</i>	25	Good	Located within forested area
T-155	White Oak	<i>Quercus alba</i>	28	Good	Located within forested area
T-156	Red Oak	<i>Quercus rubra</i>	39	Good	Located within forested area
T-157	White Oak	<i>Quercus alba</i>	28	Good	Located within forested area
T-158	White Oak	<i>Quercus alba</i>	34	Good	Located within forested area
T-159	Mockernut Hickory	<i>Carya tomentosa</i>	25	Good	Located within forested area
T-160	White Pine	<i>Pinus strobus</i>	25	Good	Located within forested area
T-161	American Beech	<i>Fagus grandifolia</i>	34	Good	Located within forested area
T-162	Tulip Poplar	<i>Liriodendron tulipifera</i>	40	Good	Located within forested area
T-163	Tulip Poplar	<i>Liriodendron tulipifera</i>	35	Good	Located within forested area
T-164	Tulip Poplar	<i>Liriodendron tulipifera</i>	33	Good	Located within forested area
T-165	Tulip Poplar	<i>Liriodendron tulipifera</i>	28	Good	Located within forested area
T-166	White Oak	<i>Quercus alba</i>	31	Good	Located within forested area
T-167	Tulip Poplar	<i>Liriodendron tulipifera</i>	25	Good	Located within forested area
T-168	Tulip Poplar	<i>Liriodendron tulipifera</i>	36	Good	Located within forested area
T-169	Tulip Poplar	<i>Liriodendron tulipifera</i>	29	Good	Located within forested area
T-170	American Sycamore	<i>Platanus occidentalis</i>	25	Good	Located within forested area
T-171	Tulip Poplar	<i>Liriodendron tulipifera</i>	25	Good	Located within forested area
T-172	Tulip Poplar	<i>Liriodendron tulipifera</i>	29	Good	Located within forested area
T-173	Tulip Poplar	<i>Liriodendron tulipifera</i>	36	Good	Located within forested area
T-174	Tulip Poplar	<i>Liriodendron tulipifera</i>	30	Good	Located within forested area
T-175	Tulip Poplar	<i>Liriodendron tulipifera</i>	26	Good	Located within forested area
T-176	Tulip Poplar	<i>Liriodendron tulipifera</i>	30	Good	Located within forested area
T-177	American Hornbeam	<i>Carpinus caroliniana</i>	32	Good	Located at edge of forested area adjacent to channelized stream
T-178	Tulip Poplar	<i>Liriodendron tulipifera</i>	29	Good	Located within forested area
T-179	American Sycamore	<i>Platanus occidentalis</i>	44	Good	Located within forested area
T-180	Tulip Poplar	<i>Liriodendron tulipifera</i>	35	Good	Located within forested area
T-181	White Pine	<i>Pinus strobus</i>	29	Good	Located within forested area
T-182	White Pine	<i>Pinus strobus</i>	25	Good	Located within forested area
T-183	White Pine	<i>Pinus strobus</i>	26	Good	Located within forested area
T-184	Tulip Poplar	<i>Liriodendron tulipifera</i>	35	Good	Located within forested area
T-187	White Pine	<i>Pinus strobus</i>	25	Good	Located within forested area
T-188	American Sycamore	<i>Platanus occidentalis</i>	35	Good	Located within forested area
T-189	American Sycamore	<i>Platanus occidentalis</i>	40	Good	Located within forested area
T-188	White Pine	<i>Pinus strobus</i>	28	Good	Located within forested area
T-189	Pin Oak	<i>Quercus palustris</i>	30	Good	Located within forested area

TAG NO.	COMMON NAME	BOTANICAL NAME	DBH	CONDITION	COMMENTS
T-190	White Pine	<i>Pinus strobus</i>	30	Good	Located in maintained lawn area
T-191	Sweet Cherry	<i>Prunus avium</i>	24	Good	Located in maintained lawn area
T-192	Sweet Cherry	<i>Prunus avium</i>	24	Good	Located in maintained lawn area
T-193	White Pine	<i>Pinus strobus</i>	31	Good	Located in maintained lawn area
T-195	White Pine	<i>Pinus strobus</i>	24	Good	Located in maintained lawn area
T-196	White Pine	<i>Pinus strobus</i>	26	Good	Located in maintained lawn area
T-197	Tulip Poplar	<i>Liriodendron tulipifera</i>	35	Good	Located in maintained lawn area
T-198	Tulip Poplar	<i>Liriodendron tulipifera</i>	35	Good	Located in maintained lawn area
T-199	Red Oak	<i>Quercus rubra</i>	39	Good	Located in maintained lawn area
T-200	Tulip Poplar	<i>Liriodendron tulipifera</i>	36	Good	Located in maintained lawn area
T-201	Red Oak	<i>Quercus rubra</i>	30	Good	Located in maintained lawn area
T-203	Red Oak	<i>Quercus rubra</i>	26	Good	Located within forested area
T-204	Red Maple	<i>Acer rubrum</i>	25	Good	Located within forested area
T-205	Black Locust	<i>Robinia pseudoacacia</i>	30	Fair	Located within forested area
T-217	Red Maple	<i>Acer rubrum</i>	26	Fair	Located in maintained lawn area
T-218	White Pine	<i>Pinus strobus</i>	31	Good	Located in maintained lawn area
T-219	Weeping Cherry	<i>Prunus subhirtella</i>	29	Good	Located in maintained lawn area
T-230	Red Maple	<i>Acer rubrum</i>	43	Good	Located in maintained lawn area
T-233	Silver Maple	<i>Acer saccharinum</i>	40	Good	Located in maintained lawn area
T-234	Tulip Poplar	<i>Liriodendron tulipifera</i>	47	Fair	Located within forested area
T-235	Tulip Poplar	<i>Liriodendron tulipifera</i>	28	Fair	Located within forested area
T-236	White Pine	<i>Pinus strobus</i>	26	Fair	Located in maintained lawn area
T-237	White Pine	<i>Pinus strobus</i>	24	Good	Located in maintained lawn area
T-243	White Mulberry	<i>Morus alba</i>	28	Fair	Located in maintained lawn area
T-244	White Pine	<i>Pinus strobus</i>	24	Fair	Located in maintained lawn area
T-245	White Pine	<i>Pinus strobus</i>	20	Fair	Located in maintained lawn area
T-246	White Pine	<i>Pinus strobus</i>	19	Fair	Located in maintained lawn area
T-251	Black Gum	<i>Nyssa sylvatica</i>	28	Good	Located within forested area
T-252	Red Oak	<i>Quercus rubra</i>	26	Good	Located within forested area
T-253	Tulip Poplar	<i>Liriodendron tulipifera</i>	39	Fair	Located within forested area
T-254	Tulip Poplar	<i>Liriodendron tulipifera</i>	38	Poor	Located within forested area
T-255	Tulip Poplar	<i>Liriodendron tulipifera</i>	29	Fair	Located within forested area
T-256	Black Cherry	<i>Prunus serotina</i>	28	Fair	Located within forested area
T-257	Tulip Poplar	<i>Liriodendron tulipifera</i>	30	Good	Located within forested area
T-258	Tulip Poplar	<i>Liriodendron tulipifera</i>	30	Good	Located within forested area
T-259	Tulip Poplar	<i>Liriodendron tulipifera</i>	24	Good	Located within forested area
T-260	Tulip Poplar	<i>Liriodendron tulipifera</i>	26	Good	Located within forested area
T-261	Sycamore	<i>Platanus occidentalis</i>	30	Good	Located within forested area
T-262	Tulip Poplar	<i>Liriodendron tulipifera</i>	39	Good	Located within forested area
T-263	Tulip Poplar	<i>Liriodendron tulipifera</i>	28	Good	Located within forested area
T-264	Black Walnut	<i>Juglans nigra</i>	39	Good	Located within forested area
T-265	Tulip Poplar	<i>Liriodendron tulipifera</i>	28	Good	Located within forested area
T-266	Tulip Poplar	<i>Liriodendron tulipifera</i>	38	Good	Located within forested area
T-267	Tulip Poplar	<i>Liriodendron tulipifera</i>	26	Good	Located within forested area
T-268	Tulip Poplar	<i>Liriodendron tulipifera</i>	25	Good	Located within forested area
T-269	Tulip Poplar	<i>Liriodendron tulipifera</i>	52	Good	Located within forested area
T-270	Tulip Poplar	<i>Liriodendron tulipifera</i>	43	Good	Located within forested area
T-271	Tulip Poplar	<i>Liriodendron tulipifera</i>	46	Good	Located within forested area
T-272	Tulip Poplar	<i>Liriodendron tulipifera</i>	38	Good	Located within forested area
T-273	Tulip Poplar	<i>Liriodendron tulipifera</i>	26	Good	Located within forested area
T-274	Tulip Poplar	<i>Liriodendron tulipifera</i>	27	Good	Located within forested area
T-275	Tulip Poplar	<i>Liriodendron tulipifera</i>	26	Good	Located within forested area
T-276	Tulip Poplar	<i>Liriodendron tulipifera</i>	32	Good	Located within forested area
T-277	Tulip Poplar	<i>Liriodendron tulipifera</i>	26	Good	Located within forested area
T-278	Tulip Poplar	<i>Liriodendron tulipifera</i>	25	Good	Located within forested area
T-279	Tulip Poplar	<i>Liriodendron tulipifera</i>	34	Good	Located within forested area
T-280	Tulip Poplar	<i>Liriodendron tulipifera</i>	71/48/24	Poor	71" 2 of 3 stems, 1 of 3 dead/gone
T-281	Red Oak	<i>Quercus rubra</i>	34	Good	Located within forested area
T-282	Tulip Poplar	<i>Liriodendron tulipifera</i>	25	Good	Located within forested area
T-283	Red Oak	<i>Quercus rubra</i>	40	Good	Located within forested area
T-284	Red Oak	<i>Quercus rubra</i>	27	Good	Located within forested area
T-285	Tulip Poplar	<i>Liriodendron tulipifera</i>	38	Good	Located within forested area
T-286	Tulip Poplar	<i>Liriodendron tulipifera</i>	28	Good	Located within forested area
T-287	Tulip Poplar	<i>Liriodendron tulipifera</i>	43	Fair	Located within forested area
T-288	Tulip Poplar	<i>Liriodendron tulipifera</i>	27	Fair	Located within forested area
T-289	Tulip Poplar	<i>Liriodendron tulipifera</i>	26	Good	Located within forested area
T-290	Tulip Poplar	<i>Liriodendron tulipifera</i>	35	Good	Located within forested area
T-291	Tulip Poplar	<i>Liriodendron tulipifera</i>	29	Good	Located within forested area
T-292	Tulip Poplar	<i>Liriodendron tulipifera</i>	34	Good	Located within forested area
T-293	Tulip Poplar	<i>Liriodendron tulipifera</i>	36	Good	Located within forested area
T-294	Tulip Poplar	<i>Liriodendron tulipifera</i>	26	Good	Located within forested area
T-295	Tulip Poplar	<i>Liriodendron tulipifera</i>	25	Good	Located within forested area
T-296	Tulip Poplar	<i>Liriodendron tulipifera</i>	24	Good	Located within forested area
T-297	Tulip Poplar	<i>Liriodendron tulipifera</i>	28	Fair	Located within forested area
T-298	Tulip Poplar	<i>Liriodendron tulipifera</i>	34	Fair	Located within forested area
T-299	Southern Red Oak	<i>Quercus falcata</i>	41	Good	Located within forested area
T-300	Slippery Elm	<i>Ulmus rubra</i>	33	Fair	Located within forested area
T-301	Tulip Poplar	<i>Liriodendron tulipifera</i>	29	Good	Located within forested area
T-309	Red Maple	<i>Acer rubrum</i>	24	Good	Located in maintained lawn area
T-310	Red Oak	<i>Quercus rubra</i>	21	Good	Located in maintained lawn area
T-311	Red Oak	<i>Quercus rubra</i>	19	Good	Located in maintained lawn area
T-312	Red Oak	<i>Quercus rubra</i>	22	Good	Located in maintained lawn area
T-313	American Elm	<i>Ulmus americana</i>	14	Good	Located in maintained lawn area
T-314	American Elm	<i>Ulmus americana</i>	16	Good	Located in maintained lawn area
T-315	Cherry	<i>Prunus spp.</i>	3	Good	Located in maintained lawn area
T-316	Cherry	<i>Prunus spp.</i>	5	Good	Located in maintained lawn area
T-317	Cherry	<i>Prunus spp.</i>	4	Good	Located in maintained lawn area
T-318	White Pine	<i>Pinus strobus</i>	17	Good	Located in maintained lawn area
T-319	Red Maple	<i>Quercus rubra</i>	16	Good	Located in maintained lawn area
T-320	Red Oak	<i>Quercus rubra</i>	11	Good	Located in maintained lawn area
T-321	White Pine	<i>Pinus strobus</i>	17	Good	Located in maintained lawn area
T-322	Red Maple	<i>Acer rubrum</i>	16	Good	Located in maintained lawn area
T-323	Red Maple	<i>Acer rubrum</i>	15	Good	Located in maintained lawn area
T-324	Red Maple	<i>Acer rubrum</i>	12	Good	Located in maintained lawn area
T-325	Red Maple	<i>Acer rubrum</i>	17	Good	Located in maintained lawn area
T-326	Red Maple	<i>Acer rubrum</i>	15	Good	Located in maintained lawn area
T-327	Golden Rain Tree	<i>Koelerutera paniculata</i>	3	Poor	Located in maintained lawn area
T-328	Golden Rain Tree	<i>Koelerutera paniculata</i>	3	Poor	Located in maintained lawn area
T-329	Golden Rain Tree	<i>Koelerutera paniculata</i>	3	Poor	Located in maintained lawn area
T-330	Golden Rain Tree	<i>Koelerutera paniculata</i>	3	Poor	Located in maintained lawn area
T-331	Golden Rain Tree	<i>Koelerutera paniculata</i>	3	Poor	Located in maintained lawn area
T-332	American Sycamore	<i>Platanus occidentalis</i>	3	Good	Located in maintained lawn area
T-333	American Sycamore	<i>Platanus occidentalis</i>	7	Good	Located in maintained lawn area
T-334	American Sycamore	<i>Platanus occidentalis</i>	10	Good	Located in maintained lawn area
T-335	American Sycamore	<i>Platanus occidentalis</i>	7	Good	Located in maintained lawn area
T-336	American Sycamore	<i>Platanus occidentalis</i>	5	Good	Located in maintained lawn area
T-337	American Sycamore	<i>Platanus occidentalis</i>	5	Good	Located in maintained lawn area
T-338	American Sycamore	<i>Platanus occidentalis</i>	4	Poor	Located in maintained lawn area
T-339	American Sycamore	<i>Platanus occidentalis</i>	5	Good	Located in maintained lawn area
T-340	American Sycamore	<i>Platanus occidentalis</i>	4	Good	Located in maintained lawn area

A TREE SAVE PLAN WILL BE SUBMITTED FOR REVIEW AND APPROVAL AT THE TIME OF SEDIMENT CONTROL PERMIT APPLICATION.

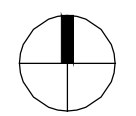
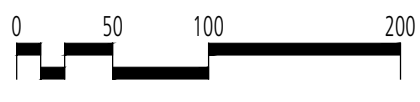
LEGEND

- ⊕ SPECIMEN / SIGNIFICANT TREE & CRITICAL ROOT ZONE
- SLOPE EQUAL TO OR GREATER THAN 25%
- SLOPE BETWEEN 15-25%
- STREAM
- - - STREAM BUFFER
- 100 YEAR FLOODPLAIN & BRL
- - - INDEX CONTOUR
- - - INTERMEDIATE CONTOUR
- SOIL TYPES
- WETLAND
- ⊕ SAMPLE PLOT POINT
- EXISTING FOREST COVER
- FOREST STAND BOUNDARY
- 13+ SLOPE ANALYSIS CROSS-SECTION
- EXISTING TRAIL
- STUDY AREA
- LOD LIMIT OF DISTURBANCE
- PROPOSED ROADWAY

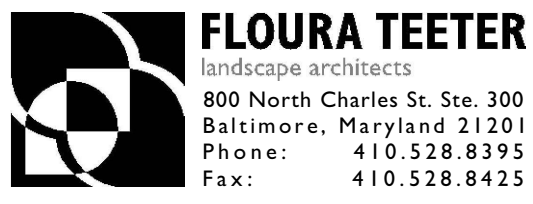


1 NATURAL RESOURCE INVENTORY / FOREST STAND DELINEATION PLAN
PLAN

SCALE: 1" = 100'-0"



PROFESSIONAL CERTIFICATION: I hereby certify that these documents were prepared or approved by me, and that I am a duly licensed professional landscape architect under the laws of the State of Maryland.
LICENSE NUMBER: 3385
EXPIRATION DATE: 01/22/2021



CAPITAL CRESCENT TRAIL
INTERSECTION IMPROVEMENTS AT LITTLE FALL PARKWAY
APPLICANT: MONTGOMERY NATIONAL CAPITAL PARKS AND PLANNING COMMISSION
PARCELS 709, 530, 330; WSSC GRID 208NW05
TAX MAP GRID HN121 GRID XX, XX
16 ELECTION DISTRICT, BETHESDA, MARYLAND

REVISION: DATE:
2019-03-11
2019-03-21

DRAWN: JB, AL
CHECKED: MM, JF

NATURAL RESOURCE
INVENTORY / FOREST STAND
DELINEATION PLAN
SNRI-42019124E

DATE: 2019-03-06
SCALE: 1" = 100'

FSD1.2

A TREE SAVE PLAN WILL BE SUBMITTED FOR REVIEW AND APPROVAL AT THE TIME OF SEDIMENT CONTROL PERMIT APPLICATION.

LEGEND

- ⊕

SPECIMEN / SIGNIFICANT TREE & CRITICAL ROOT ZONE
- SLOPE EQUAL TO OR GREATER THAN 25%
- SLOPE BETWEEN 15-25%
- STREAM
- - -

STREAM BUFFER
- ▨

100 YEAR FLOODPLAIN & BRL
- INDEX CONTOUR
- INTERMEDIATE CONTOUR
- SOIL TYPES
- ▨

WETLAND
- ⊕

SAMPLE PLOT POINT
- EXISTING FOREST COVER
- FOREST STAND BOUNDARY
- ⊕

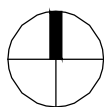
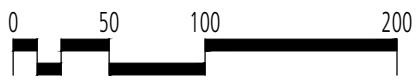
SLOPE ANALYSIS CROSS-SECTION
- EXISTING TRAIL
- ▨

STUDY AREA
- LOD
- PROPOSED ROADWAY

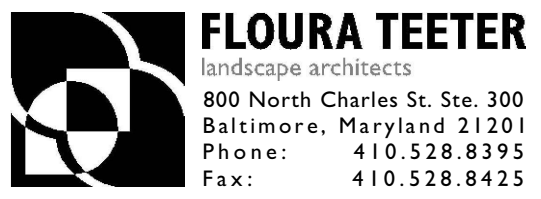


1 NATURAL RESOURCE INVENTORY / FOREST STAND DELINEATION PLAN
PLAN

SCALE: 1"= 100'-0"



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CAPITAL CRESCENT TRAIL
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PARCELS 709, 530, 330; WSSC GRID 208NW05
TAX MAP GRID HN121 GRID XX, XX
16 ELECTION DISTRICT, BETHESDA, MARYLAND

REVISION: DATE:
2019-03-11
2019-03-21

DRAWN: JB, AL
CHECKED: MM, JF

NATURAL RESOURCE INVENTORY / FOREST STAND DELINEATION PLAN
SNRI-42019124E

DATE: 2019-03-06
SCALE: 1" = 100'

FSD1.3

Appendix C

Stormwater Management Concept

STORMWATER MANAGEMENT CONCEPT PLAN

CAPITAL CRESCENT TRAIL CROSSING AT LITTLE FALLS PARKWAY
PROPOSED PEDESTRIAN CROSSING IMPROVEMENTS
FROM ARLINGTON RD TO HILLANDALE RD

LITTLE FALLS PARKWAY
BETHESDA, MD 20815

STORMWATER CONCEPT APPLICATION #: 284-594

GENERAL NOTES:

THE GOAL OF FOLLOWING DRAWINGS IS TO DEMONSTRATE THE PROPOSED TRAFFIC IMPROVEMENTS AT THE INTERSECTION OF THE CAPITAL CRESCENT TRAIL (CCT) AND SURROUNDING SIDEWALKS AND TRAILS MEET OR EXCEED THE STORMWATER MANAGEMENT REQUIREMENTS FOR MONTGOMERY COUNTY AND THE STATE OF MARYLAND. THE PROJECT IS PLANNING TO DISTURB APPROXIMATELY 1.6 AC. THE PROJECT PROPOSES TO REMOVE TWO TRAVEL LANES OF THE EXISTING LITTLE FALLS PARKWAY PAVEMENT, PORTIONS OF THE DRIVEWAY ENTRANCE TO THE NEARBY COMMUNITY POOL AND REPLACE THEM WITH NEW PAVED TRAILS, A NEW RIGHT TURN LANE ON NORTH BOUND LITTLE FALLS PARKWAY AT ARLINGTON ROAD, AND GRASS AREAS.

THE WORK SHOWN ON THESE PLANS WILL CAUSE NO CHANGE IN THE DRAINAGE PATTERN AND DOES NOT PROPOSE ADDITIONAL NET NEW IMPERVIOUS AREAS. MAJORITY OF THE WORK DEPICTED IN THE FOLLOWING PLANS IS REMOVAL OF EXISTING SOUTHBOUND TRAVELWAYS ALONG LITTLE FALLS PARKWAY AND REPLACING IT WITH LANDSCAPE AREAS.

PROJECT NARRATIVE:

THE CAPITAL CRESCENT TRAIL CROSSING AT LITTLE FALLS PARKWAY IS LOCATED SOUTH OF THE INTERSECTION OF ARLINGTON ROAD AND LITTLE FALLS PARKWAY. THE TRAIL CURRENTLY CROSSES THE PARKWAY OVER TWO 22-FT WIDE (4 LANES WIDE) ROADWAY SECTIONS AND A GRASS MEDIAN MAKING AN UNSAFE CROSSING PATH FOR PEDESTRIANS AND BICYCLES AT THIS INTERSECTION. IN ADDITION TO THE CROSSING, THERE IS NO DIRECT CONNECTION BETWEEN THE LITTLE FALLS TRAIL EAST OF HILLANDALE ROAD AND THE CAPITAL CRESCENT TRAIL. CURRENTLY, VEHICULAR TRAFFIC HAS BEEN REDUCED TO TWO LANES OF TRAFFIC THROUGH THE PROJECT SITE.

THE PURPOSE OF THIS WORK IS TO REMOVE THE TEMPORARY NATURE OF THE EXISTING CONDITIONS AND CREATE A PERMANENT SOLUTION FOR THE CCT CROSSING AT LITTLE FALLS PARKWAY. NORTHBOUND LITTLE FALLS PARKWAY WILL BE RESTRIPEDED SO THAT IT CAN SUPPORT BOTH NORTHBOUND AND SOUTHBOUND TRAFFIC. THE EXISTING SOUTHBOUND TRAVELWAYS WILL BE REMOVED AND CONVERTED INTO LANDSCAPED AREAS. A RAISED SPEED TABLE WILL BE INTRODUCED TO THE REROUTED TRAVELWAYS CREATING A NEW CROSSING FOR THE CCT. THE PROJECT WILL ALSO INTRODUCE PAVED PATHS ALONG BOTH SIDES OF LITTLE FALLS PARKWAY PROVIDING A NEW PAVED PATH FOR BICYCLES AND PEDESTRIANS CONNECTING TO THE TRAIL.

THIS WORK ALSO PROVIDES AN 8-FT TRAIL CONNECTOR FROM LITTLE FALLS TRAIL TO THE CAPITAL CRESCENT TRAIL ALONG THE EAST SIDE OF HILLANDALE ROAD. A NEW RAISED SPEED TABLE IS INTRODUCED TO HILLANDALE ROAD FOR TRAIL USERS TO ACCESS MONTGOMERY COUNTY PARK'S BETHESDA COMMUNITY POOL. THE PROJECT ALSO PROPOSES TO REPLACE AN EXISTING PAVED TRAIL BETWEEN THE CCT PARKING LOT LOCATED AT THE NORTHWEST INTERSECTION OF LITTLE FALLS PARKWAY AND ARLINGTON ROAD WITH AN ACCESSIBLE PAVED PATH.

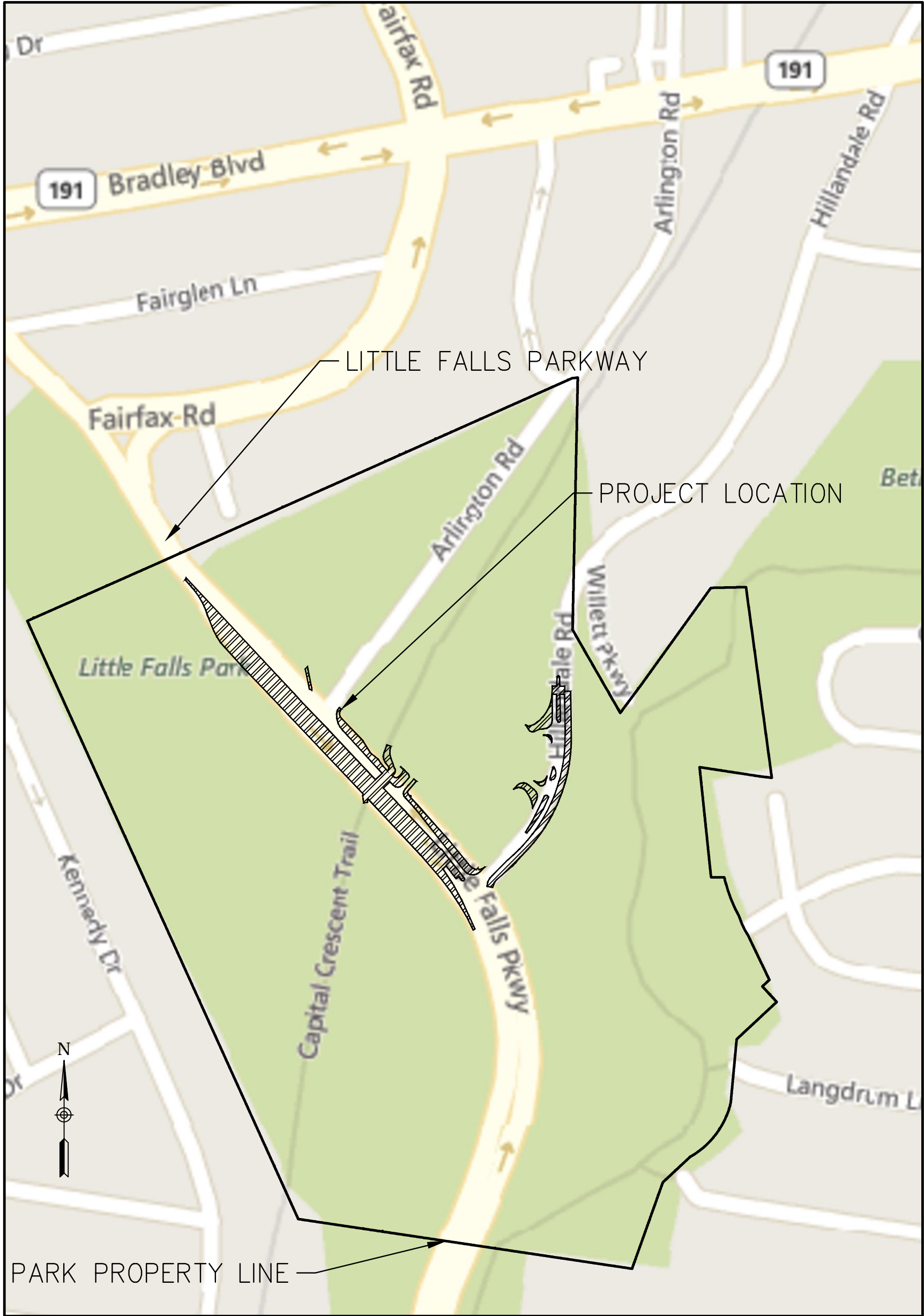
PROPOSED WATER RESOURCES IMPROVEMENTS:

STORMWATER MANAGEMENT STRATEGY FOR THE PROJECT IS TO PROVIDE FULL TREATMENT OF THE ENVIRONMENTAL SITE DESIGN VOLUME (ESDv). FULL ESDv TREATMENT WILL SATISFY THE GROUND WATER RECHARGE, WATER QUALITY, AND CHANNEL PROTECTION REQUIREMENTS PER MDE STORMWATER DESIGN MANUAL.

THE MAIN IMPROVEMENTS FOR THE SITE ARE ACHIEVED BY REMOVING MORE THAN 50% OF THE EXISTING IMPERVIOUS COVER AND INSTALLING NEW GRASS AREAS THROUGH STRATEGIC SITE DESIGN.

ALL POSSIBLE NON-STRUCTURAL ESD PRACTICES SUCH AS NON-ROOFTOP DISCONNECT, AND DISCHARGE TO NATURAL CONSERVATION AREAS HAVE BEEN INVESTIGATED FOR THE GIVEN PROJECT TO THE EXTENT PRACTICABLE TO ACHIEVE THE STORMWATER MANAGEMENT STRATEGY.

A BIO-SWALE IS PROPOSED TO PROVIDE TREATMENT FOR IMPERVIOUS AREAS WITHIN THE PROJECT LIMIT. IN ADDITION TO THE CONCEPT, WE HAVE PROVIDED AN ALTERNATIVE CONCEPT DESIGN SINCE THERE ARE NO INFORMATION ON HAND REGARDING THE GROUND WATER DEPTH AND EXISTING SUBSURFACE CONDITIONS. SHOULD THE GROUND WATER BECOMES AN ISSUE FOR INSTALLATION OF A BIO-SWALE, TWO CONCRETE BIO-RETENTION PLANTER STRUCTURES ARE PROPOSED AS AN ALTERNATIVE TO PROVIDE THE REQUIRED TREATMENT FOR THE PROJECT.



VICINITY MAP

SCALE: 1"=250'

INDEX OF DRAWINGS

NUMBER	DRAWING NUMBER	SHEET TITLE
1	G-001	COVER SHEET
2	CV-001	NATURAL RESOURCES MAP
3	CV-100	EXISTING CONDITIONS PLAN
4	CD-100	CONCEPT DEMOLITION PLAN
5	C-101	CONCEPT SITE PLAN
6	C-102	CONCEPT SITE PLAN (ALTERNATIVE)
7	CG-101	CONCEPT GRADING PLAN
8	CG-101	CONCEPT GRADING PLAN (ALTERNATIVE)
9	C-200	SITE IMPERVIOUSNESS AND SOIL MAP
10	C-201	SITE ESDv AREA MAP
11	C-202	CONCEPT STORMWATER MANAGEMENT
12	C-203	CONCEPT STORMWATER MANAGEMENT (ALTERNATIVE)
13	C-204	BIO-RETENTION PLANTER BOX DETAILS

SWM Concept Summary Table	
SM#	284-594
Type of Concept	Concept/ Site Development
MNCP&PC	Capital Crescent Trail
Property Address	Little Falls Parkway
Property Legal Description	OATLAND ETC
Tract Size	55.8 ac.
Total Concept Project Area	1.6 ac.
Total Modified LOD	0.59 ac.
Zoning	R-60
Watershed and Stream Class	Willet Branch Use-I
RPA	No
100-YR Flood	No
Redevelopment or New Development	Redevelopment
Target PE / Proposed PE	1.1 / 1.1
Target ESDv / Proposed ESDv	1276 / 1281
ESD Measures	Bio-Swale (See Alternative)
Structural Storage Required / Provided	See Alternative
Structural Measures	See Alternative
Waiver Requested/QL/QN/Both	N/A
Other Waivers	N/A
WSSC Grid	208NW05
Water/Sewer Categories	W-1 / S-1
Other Information	

Proposed Site ESDv Summary Chart									
Facility Designation	Type	Contributing Drainage Area (SF)	Contributing Impervious Area (SF)	Rv	min. ESDv (CF)	max. ESDv (CF)	Upstream ESDv Treatment (CF)	ESDv Achieved (CF)	Required ESDv: 1276 CF
3S-1	Bio-Swale	15960	8980	0.56	740	1924	0	1281	Provided ESDv: 1281 CF
Total							1281		
Alternative Desing Site ESDv Summary Chart									
3R-01	Bio-Retention	13450	6890	0.51	573	1489	0	593	Provided ESDv (Alternative): 1482 CF
3R-02	Bio-Retention	16125	10848	0.66	881	2290	0	889	
Total							1482		

COVER SHEET

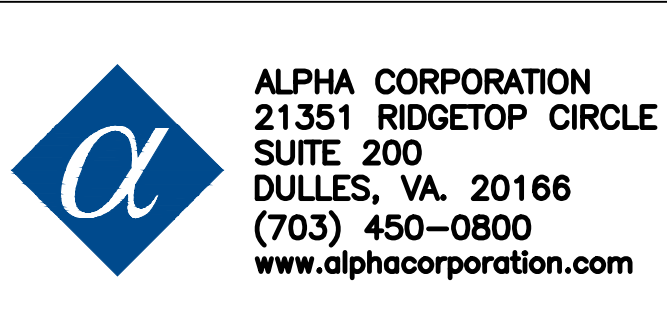


NO.	REVISION	BY	APP'D	DATE	DESIGNED BY: S. ABDSHARIFABADI	DATE 03/27/2019
					DRAWN BY: S. ABDSHARIFABADI	DATE 03/27/2019
					CHECKED BY: D. QUINN	DATE 03/27/2019
					DRAWING NO.:	G-001
					Approved: _____	
					Chief, Traffic Engineering and Operations	
					Reviewed: _____	
					Manager, Transportation Systems Engineering	
					Reviewed: _____	
					Manager, Traffic Control and Lighting Engineering	
					Recommended: _____	
					Engineer, Transportation Systems Engineering	

DEPARTMENT OF TRANSPORTATION
DIVISION OF OPERATIONS
MONTGOMERY COUNTY, MARYLAND

CAPITAL CRESCENT TRAIL CROSSING
AT LITTLE FALLS PARKWAY
PED CROSSING IMPROVEMENT PLAN
FROM ARLINGTON RD TO
HILLANDALE RD

SCALE:





LEGEND

FLEXIBLE BOLLARD
TREES & TREE CANOPY LINE
SOIL DIVIDE
WETLAND BOUNDARY
STREAM BUFFER
MAJOR CONTOUR
MINOR CONTOUR
APPROXIMATE PROJECT LIMIT
STREAM CENTER LINE
TRAFFIC LIGHT

100-YR FLOOD PLAIN
STEEP SLOPES 25
STEEP SLOPES 15-25

- NATURAL RESOURCES NOTES:**
1. THE BACKGROUND INFORMATION HAS BEEN OBTAINED FROM THE COUNTY GIS. NO FIELD SURVEY WAS CONDUCTED.
 2. PROJECT IS LOCATED WITHIN THE WILLET BRANCH WATERSHED.
 3. THE PROJECT LIMIT IS LOCATED OUTSIDE THE 100-YR FLOODPLAIN.
 4. THE WORK WILL NOT IMPACT ANY OF THE NATURAL RESOURCES SHOWN ON THIS DRAWING.
 5. THE WORK SHOWN ON SUBSEQUENT DRAWINGS WILL IMPROVE THE DRAINAGE CONDITIONS WITHIN THE PROJECT LIMIT BY REMOVING EXISTING SECTIONS OF LITTLE FALLS PARKWAY AND THE COMMUNITY POOL ENTRANCE AND INTRODUCING NEW PERVIOUS COVER RESULTING IN A NET REDUCTION OF IMPERVIOUS SURFACES.



**THE MARYLAND-NATIONAL CAPITAL
Park and Planning Commission**

SABRA & ASSOCIATES, INC.

ALPHA CORPORATION
21351 RIDGETOP CIRCLE
SUITE 200
DULLES, VA. 20166
(703) 450-0800
www.alphacorporation.com

NO.	REVISION	BY	APP'D	DATE

DESIGNED BY: S. ABDSHARIFABADI	DATE 03/27/2019
DRAWN BY: S. ABDSHARIFABADI	DATE 03/27/2019
CHECKED BY: D. QUINN	DATE 03/27/2019
DRAWING NO.: CV-001	

Approved: _____
Chief, Traffic Engineering and Operations

Reviewed: _____
Manager, Transportation Systems Engineering

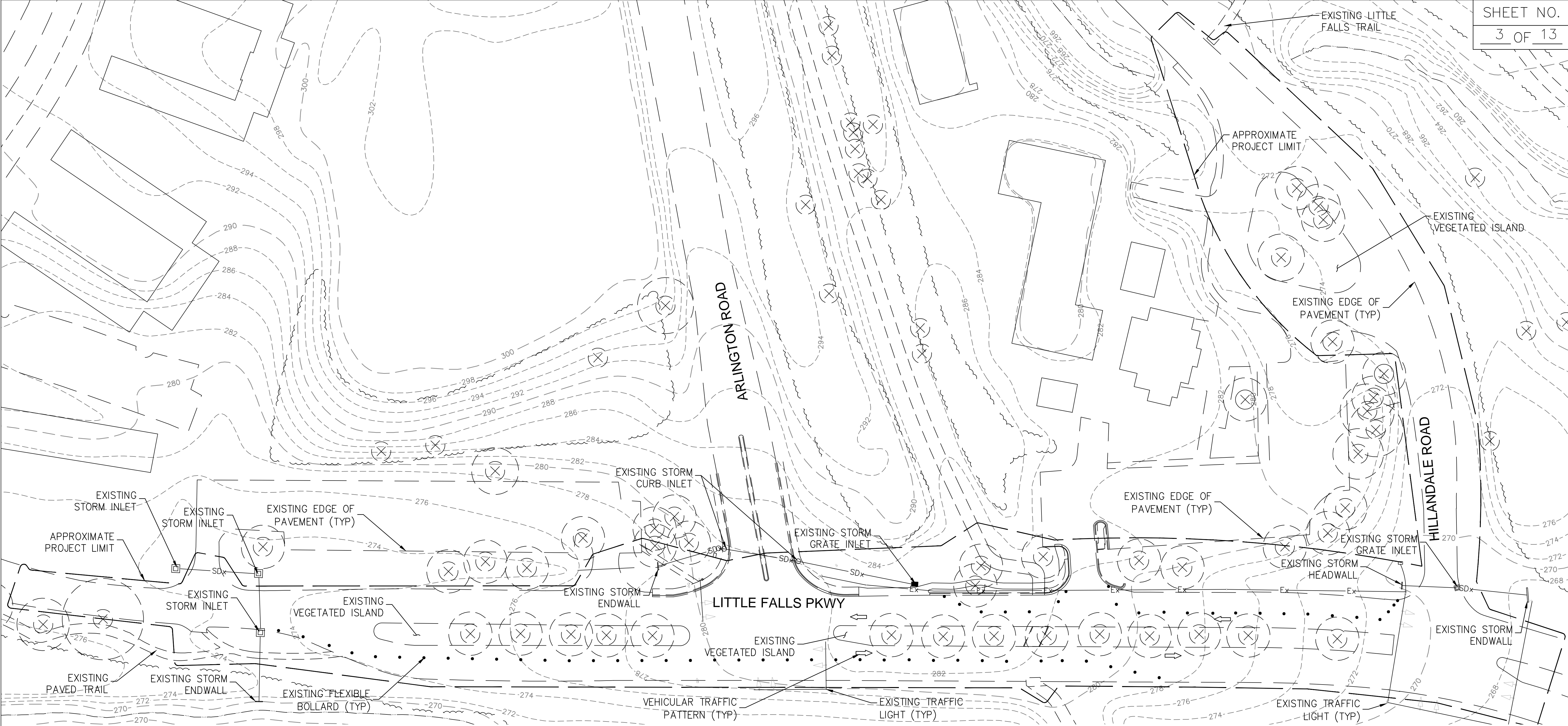
Reviewed: _____
Manager, Traffic Control and Lighting Engineering

Recommended: _____
Engineer, Transportation Systems Engineering

DEPARTMENT OF TRANSPORTATION
DIVISION OF OPERATIONS
MONTGOMERY COUNTY, MARYLAND

**CAPITAL CRESCENT TRAIL CROSSING
AT LITTLE FALLS PARKWAY
PED CROSSING IMPROVEMENT PLAN
FROM ARLINGTON RD TO
HILLDALE RD**

SCALE: 1" = 60'



SHEET NO.
3 OF 13

LEGEND

FLEXIBLE BOLLARD

EXISTING TREES AND CANOPY LINE

EXISTING MAJOR CONTOUR

EXISTING MINOR CONTOUR

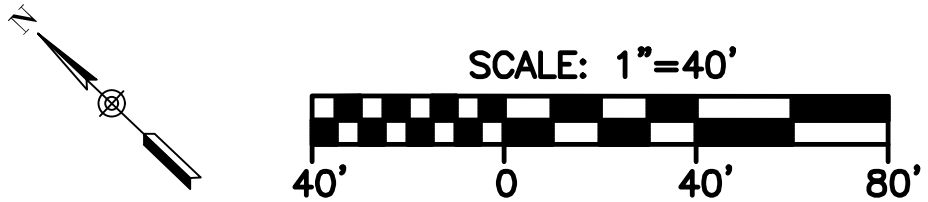
EXISTING TRAFFIC LIGHT

APPROXIMATE PROJECT LIMIT

- EXISTING CONDITIONS NOTES:**
- 1. THE BACKGROUND INFORMATION IS OBTAINED FROM THE COUNTY GIS.
 - 2. LITTLE FALLS PARKWAY CURRENTLY HAS TWO 22-FT TRAVEL WAYS IN EACH DIRECTION WITH A GRASS MEDIAN SEPARATING THE TWO PATHWAYS.
 - 3. THE SOUTHBOUND TRAVELWAY OF LITTLE FALLS PARKWAY HAS BEEN CONVERTED INTO ONE VEHICLE TRAVEL LANE.



EXISTING CONDITION PLAN



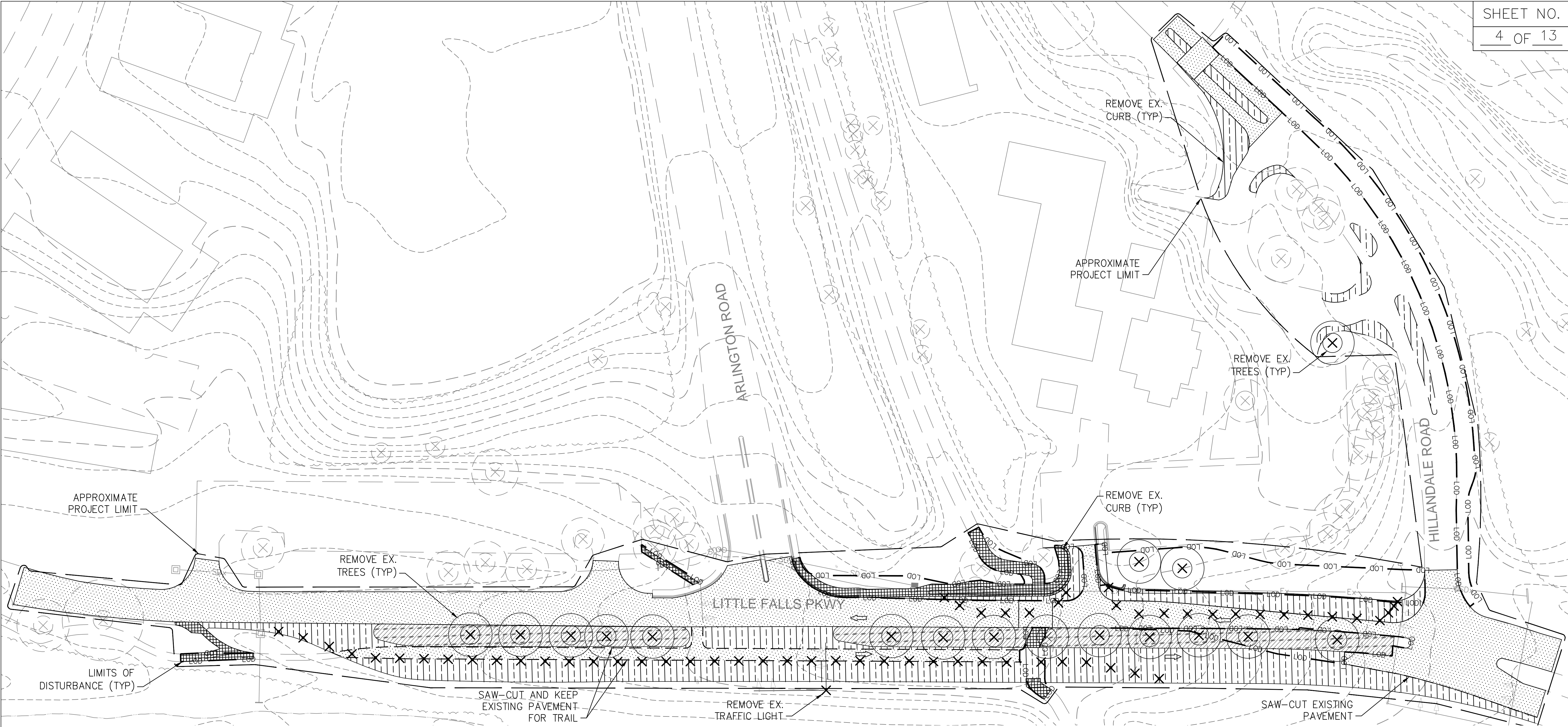
NO.	REVISION	BY	APP'D	DATE	DESIGNED BY: S. ABDSHARIFABADI	DATE 03/27/2019	DEPARTMENT OF TRANSPORTATION DIVISION OF OPERATIONS MONTGOMERY COUNTY, MARYLAND
					DRAWN BY: S. ABDSHARIFABADI	DATE 03/27/2019	
					CHECKED BY: D. QUINN	DATE 03/27/2019	
					DRAWING NO.:	CV-100	
					Approved: _____ Chief, Traffic Engineering and Operations		CAPITAL CRESCENT TRAIL CROSSING AT LITTLE FALLS PARKWAY PED CROSSING IMPROVEMENT PLAN FROM ARLINGTON RD TO HILLDALE RD
					Reviewed: _____ Manager, Transportation Systems Engineering		
					Reviewed: _____ Manager, Traffic Control and Lighting Engineering		
					Recommended: _____ Engineer, Transportation Systems Engineering		
					SCALE: 1" = 40'		

SCALE: 1" = 40'

PLOTTED: \$DATE\$
FILE: \$FILE\$



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LEGEND

REMOVE FEATURE

REMOVE FULL DEPTH ASPHALT

REMOVE PEDESTRIAN PATHS

EXISTING VEGETATED ISLANDS

MILL 3-IN SURFACE MATERIAL

APPROXIMATE PROJECT LIMIT

LIMITS OF DISTURBANCE

SAWCUT

- DEMOLITION PLAN NOTES:**
- MILLING SHALL NOT EXPOSE SUB-BASE LAYER OF THE EXISTING ASPHALT.
 - THE LIMITS OF DISTURBANCE IS ONLY FOR PLACES WHERE; (1) NEW IMPERVIOUS COVER IS INTRODUCED, AND (2) FULL DEPTH OF ASPHALT IS REMOVED AND REPAVED. SEE ADDITIONAL NARRATIVE ON SHEET C-201.TOTAL LIMITS OF DISTURBANCE WITHIN THE PROJECT IS 0.59 AC.



DEMOLITION PLAN



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					CHECKED BY: D. QUINN	DATE 03/27/2019
					DRAWING NO.:	CD-101
					Approved: _____ Chief, Traffic Engineering and Operations	
					Reviewed: _____ Manager, Transportation Systems Engineering	
					Reviewed: _____ Manager, Traffic Control and Lighting Engineering	
					Recommended: _____ Engineer, Transportation Systems Engineering	

DEPARTMENT OF TRANSPORTATION
DIVISION OF OPERATIONS
MONTGOMERY COUNTY, MARYLAND

CAPITAL CRESCENT TRAIL CROSSING
AT LITTLE FALLS PARKWAY
PED CROSSING IMPROVEMENT PLAN
FROM ARLINGTON RD TO
HILLDALE RD

SCALE: 1" = 40'

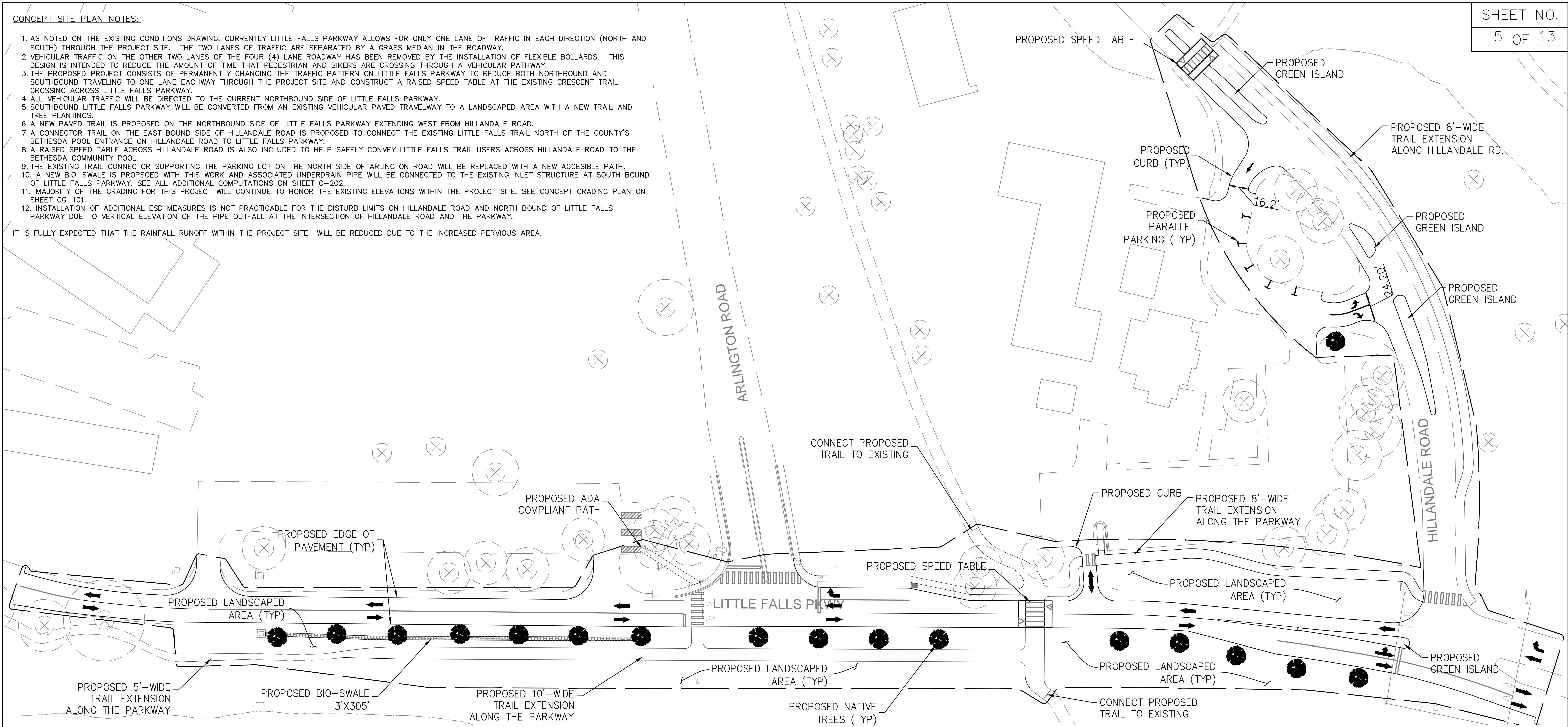


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CONCEPT SITE PLAN NOTES:

1. AS NOTED ON THE EXISTING CONDITIONS DRAWING, CURRENTLY LITTLE FALLS PARKWAY ALLOWS FOR ONLY ONE LANE OF TRAFFIC IN EACH DIRECTION (NORTH AND SOUTH) THROUGH THE PROJECT SITE. THE TWO LANES OF TRAFFIC ARE SEPARATED BY A GRASS MEDIAN IN THE ROADWAY.
2. VEHICULAR TRAFFIC ON THE OTHER TWO LANES OF THE FOUR (4) LANE ROADWAY HAS BEEN REMOVED BY THE INSTALLATION OF FLEXIBLE BOLLARDS. THIS DESIGN IS INTENDED TO REDUCE THE AMOUNT OF TIME THAT PEDESTRIAN AND BIKERS ARE CROSSING THROUGH A VEHICULAR PATHWAY.
3. THE PROPOSED PROJECT CONSISTS OF PERMANENTLY CHANGING THE TRAFFIC PATTERN ON LITTLE FALLS PARKWAY TO REDUCE BOTH NORTHBOUND AND SOUTHBOUND TRAVELING TO ONE LANE EACHWAY THROUGH THE PROJECT SITE AND CONSTRUCT A RAISED SPEED TABLE AT THE EXISTING CRESCENT TRAIL CROSSING ACROSS LITTLE FALLS PARKWAY.
4. ALL VEHICULAR TRAFFIC WILL BE DIRECTED TO THE CURRENT NORTHBOUND SIDE OF LITTLE FALLS PARKWAY.
5. SOUTHBOUND LITTLE FALLS PARKWAY WILL BE CONVERTED FROM AN EXISTING VEHICULAR PAVED TRAVELWAY TO A LANDSCAPED AREA WITH A NEW TRAIL AND TREE PLANTINGS.
6. A NEW PAVED TRAIL IS PROPOSED ON THE NORTHBOUND SIDE OF LITTLE FALLS PARKWAY EXTENDING WEST FROM HILLDALE ROAD.
7. A CONNECTOR TRAIL ON THE EAST BOUND SIDE OF HILLDALE ROAD IS PROPOSED TO CONNECT THE EXISTING LITTLE FALLS TRAIL NORTH OF THE COUNTY'S BETHESDA POOL ENTRANCE ON HILLDALE ROAD TO LITTLE FALLS PARKWAY.
8. A RAISED SPEED TABLE ACROSS HILLDALE ROAD IS ALSO INCLUDED TO HELP SAFELY CONVEY LITTLE FALLS TRAIL USERS ACROSS HILLDALE ROAD TO THE BETHESDA COMMUNITY POOL.
9. THE EXISTING TRAIL CONNECTOR SUPPORTING THE PARKING LOT ON THE NORTH SIDE OF ARLINGTON ROAD WILL BE REPLACED WITH A NEW ACCESIBLE PATH.
10. A NEW BIO-SWALE IS PROPOSED WITH THIS WORK AND ASSOCIATED UNDERDRAIN PIPE WILL BE CONNECTED TO THE EXISTING INLET STRUCTURE AT SOUTH BOUND OF LITTLE FALLS PARKWAY. SEE ALL ADDITIONAL COMPUTATIONS ON SHEET C-202.
11. MAJORITY OF THE GRADING FOR THIS PROJECT WILL CONTINUE TO HONOR THE EXISTING ELEVATIONS WITHIN THE PROJECT SITE. SEE CONCEPT GRADING PLAN ON SHEET CG-101.
12. INSTALLATION OF ADDITIONAL ESD MEASURES IS NOT PRACTICABLE FOR THE DISTURB LIMITS ON HILLDALE ROAD AND NORTH BOUND OF LITTLE FALLS PARKWAY DUE TO VERTICAL ELEVATION OF THE PIPE OUTFALL AT THE INTERSECTION OF HILLDALE ROAD AND THE PARKWAY.

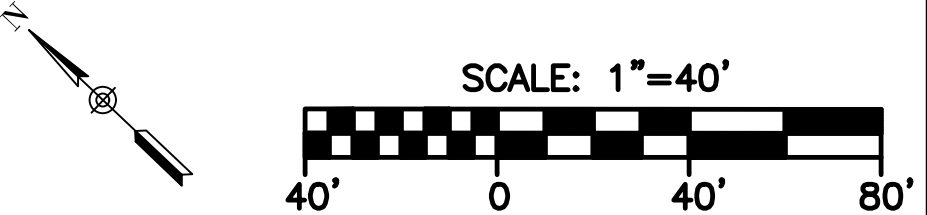
IT IS FULLY EXPECTED THAT THE RAINFALL RUNOFF WITHIN THE PROJECT SITE WILL BE REDUCED DUE TO THE INCREASED PERVIOUS AREA.



LEGEND

- PROPOSED NATIVE TREE
- APPROXIMATE PROJECT LIMIT
- PAINTED DIRECTION ARROWS
- PROPOSED BIO-SWALE

CONCEPT SITE PLAN



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					DRAWING NO.:	C-101
					Approved: _____ Chief, Traffic Engineering and Operations	
					Reviewed: _____ Manager, Transportation Systems Engineering	
					Reviewed: _____ Manager, Traffic Control and Lighting Engineering	
					Recommended: _____ Engineer, Transportation Systems Engineering	

DEPARTMENT OF TRANSPORTATION
DIVISION OF OPERATIONS
MONTGOMERY COUNTY, MARYLAND

CAPITAL CRESCENT TRAIL CROSSING
AT LITTLE FALLS PARKWAY
PED CROSSING IMPROVEMENT PLAN
FROM ARLINGTON RD TO
HILLDALE RD

SCALE: 1" = 40'

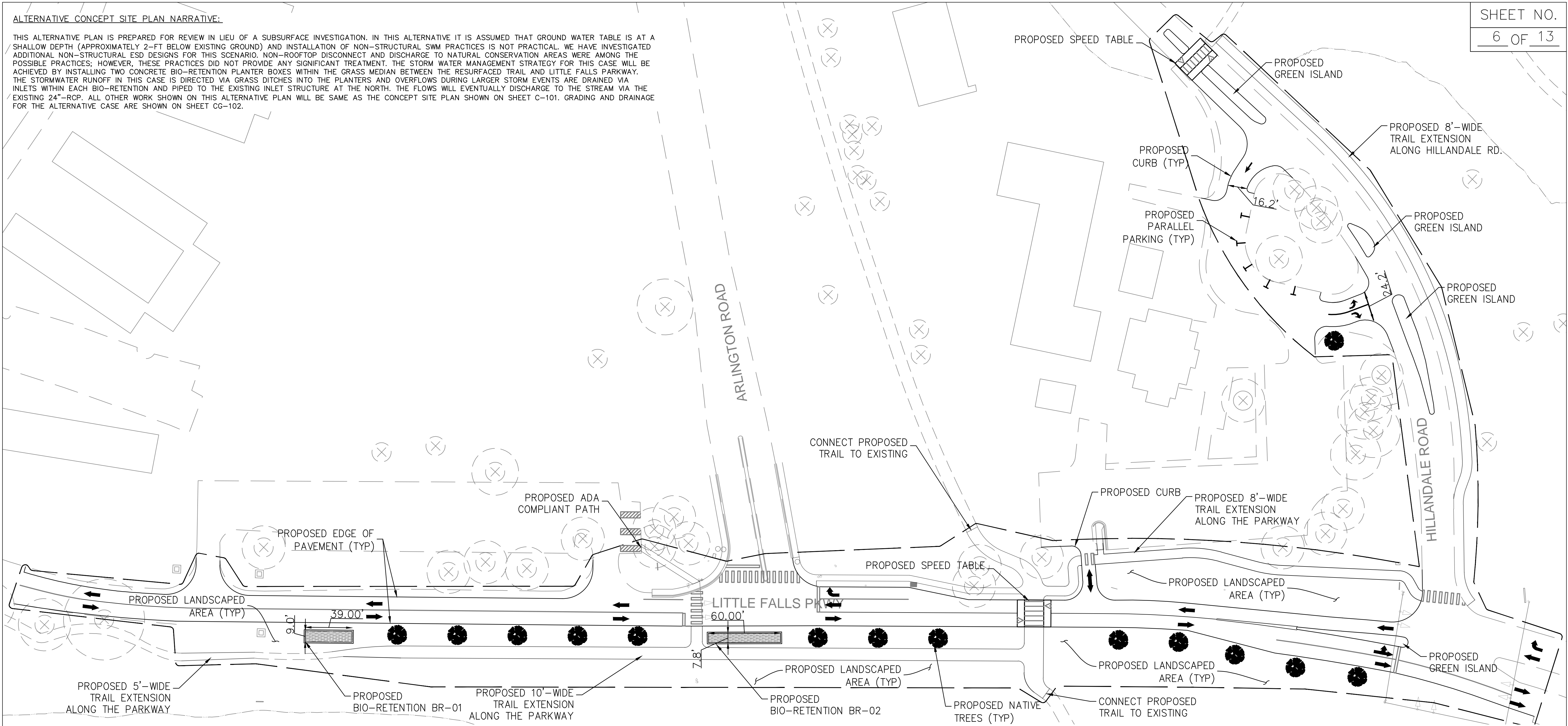


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ALTERNATIVE CONCEPT SITE PLAN NARRATIVE:

THIS ALTERNATIVE PLAN IS PREPARED FOR REVIEW IN LIEU OF A SUBSURFACE INVESTIGATION. IN THIS ALTERNATIVE IT IS ASSUMED THAT GROUND WATER TABLE IS AT A SHALLOW DEPTH (APPROXIMATELY 2-FT BELOW EXISTING GROUND) AND INSTALLATION OF NON-STRUCTURAL SWM PRACTICES IS NOT PRACTICAL. WE HAVE INVESTIGATED ADDITIONAL NON-STRUCTURAL ESD DESIGNS FOR THIS SCENARIO. NON-ROOFTOP DISCONNECT AND DISCHARGE TO NATURAL CONSERVATION AREAS WERE AMONG THE POSSIBLE PRACTICES; HOWEVER, THESE PRACTICES DID NOT PROVIDE ANY SIGNIFICANT TREATMENT. THE STORM WATER MANAGEMENT STRATEGY FOR THIS CASE WILL BE ACHIEVED BY INSTALLING TWO CONCRETE BIO-RETENTION PLANTER BOXES WITHIN THE GRASS MEDIAN BETWEEN THE RESURFACED TRAIL AND LITTLE FALLS PARKWAY. THE STORMWATER RUNOFF IN THIS CASE IS DIRECTED VIA GRASS DITCHES INTO THE PLANTERS AND OVERFLOWS DURING LARGER STORM EVENTS ARE DRAINED VIA INLETS WITHIN EACH BIO-RETENTION AND PIPED TO THE EXISTING INLET STRUCTURE AT THE NORTH. THE FLOWS WILL EVENTUALLY DISCHARGE TO THE STREAM VIA THE EXISTING 24"-RCP. ALL OTHER WORK SHOWN ON THIS ALTERNATIVE PLAN WILL BE SAME AS THE CONCEPT SITE PLAN SHOWN ON SHEET C-101. GRADING AND DRAINAGE FOR THE ALTERNATIVE CASE ARE SHOWN ON SHEET CG-102.

SHEET NO.
6 OF 13



LEGEND

- PROPOSED NATIVE TREE
- APPROXIMATE PROJECT LIMIT
- PAINTED DIRECTION ARROWS
- PROPOSED BIO-RETENTIONS

CONCEPT SITE PLAN (ALTERNATIVE)



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					CHECKED BY: D. QUINN	DATE 03/27/2019
					DRAWING NO.:	C-102
					Approved: _____ Chief, Traffic Engineering and Operations	
					Reviewed: _____ Manager, Transportation Systems Engineering	
					Reviewed: _____ Manager, Traffic Control and Lighting Engineering	
					Recommended: _____ Engineer, Transportation Systems Engineering	

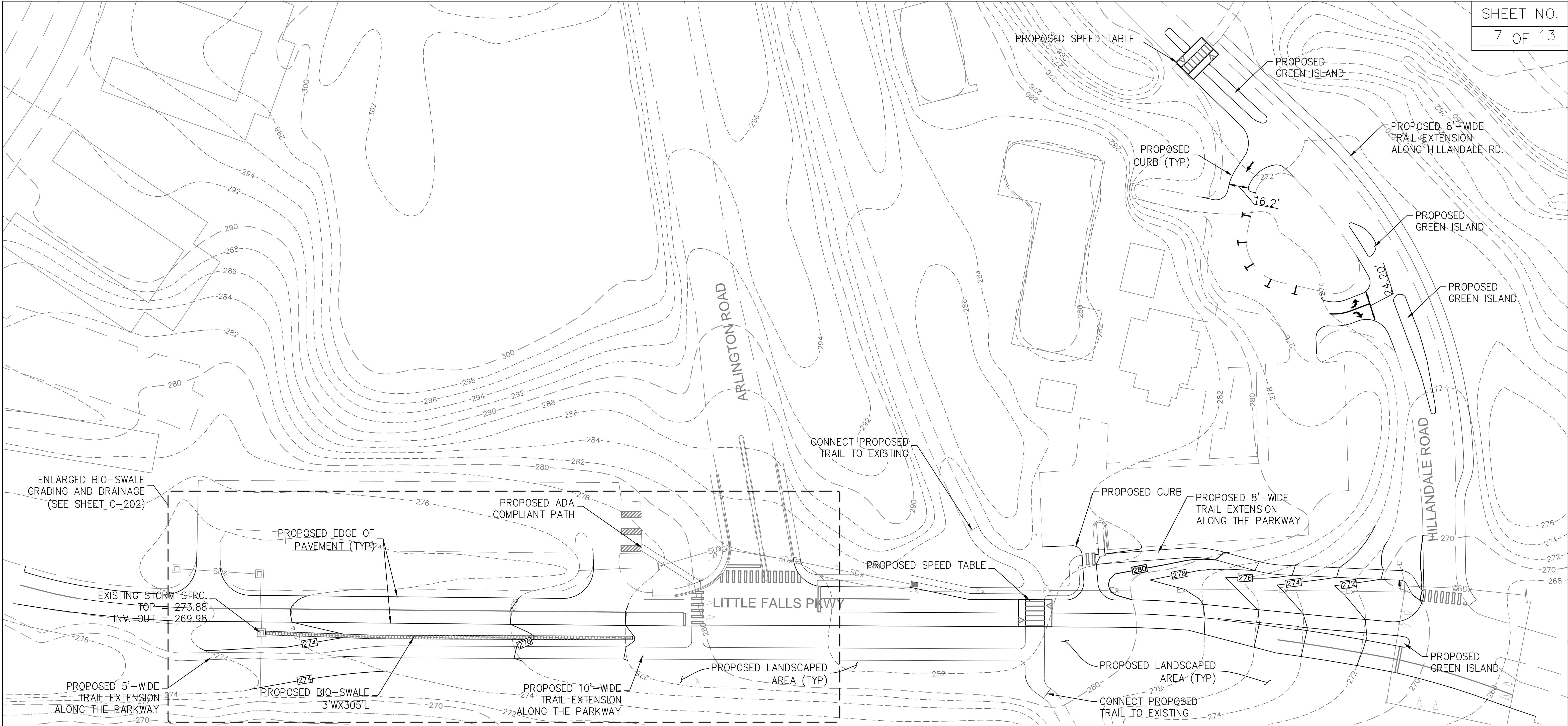
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DIVISION OF OPERATIONS
MONTGOMERY COUNTY, MARYLAND

CAPITAL CRESCENT TRAIL CROSSING
AT LITTLE FALLS PARKWAY
PED CROSSING IMPROVEMENT PLAN
FROM ARLINGTON RD TO
HILLDALE RD



SCALE: 1" = 40'



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LEGEND

PROPOSED MAJOR CONTOUR	 280
PROPOSED MINOR CONTOUR	 274

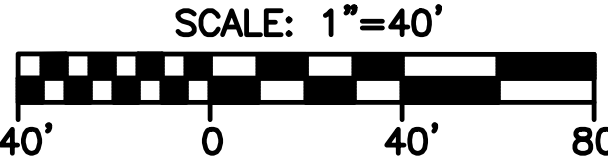
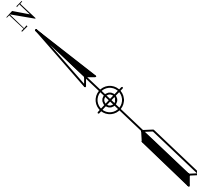
CONCEPT GRADING PLAN NOTES:

1. ALL EXISTING ELEVATIONS ARE OBTAINED FROM THE COUNTY GIS.
2. THE PROPOSED GRADING ON NORTH BOUND OF THE PARKWAY, BETWEEN HILLDALE ROAD AND BETHESDA COMMUNITY POOL ENTRANCE ARE SOLELY TO IMPROVE THE DRAINAGE CONDITIONS OF THE EXISTING GRASSED SWALE.
3. FOR BIO-SWALE 1 ENLARGEMENT GRADING AND DRAINAGE SEE SHEET C-202.
4. ALL OTHER GRADING PROPOSED WITH THE PROJECT FOR THE NEW TRAILS AND THE CONVERSION OF SOUTHBOUND LITTLE FALLS PARKWAY WILL CONTINUE TO HONOR EXISTING DRAINAGE PATTERNS WITHIN THE PROJECT SITE.
5. NEW UNDER DRAINS, CLEANOUTS AND SECTIONS FOR THE BIO-SWALE ARE SHOWN ON SHEET C-202.

IT IS FULLY EXPECTED THAT THE RAINFALL RUNOFF WITHIN THE PROJECT SITE WILL BE REDUCED DUE TO THE INCREASED PERVIOUS AREA.



CONCEPT GRADING PLAN



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					Reviewed: _____ Manager, Transportation Systems Engineering	
					Reviewed: _____ Manager, Traffic Control and Lighting Engineering	
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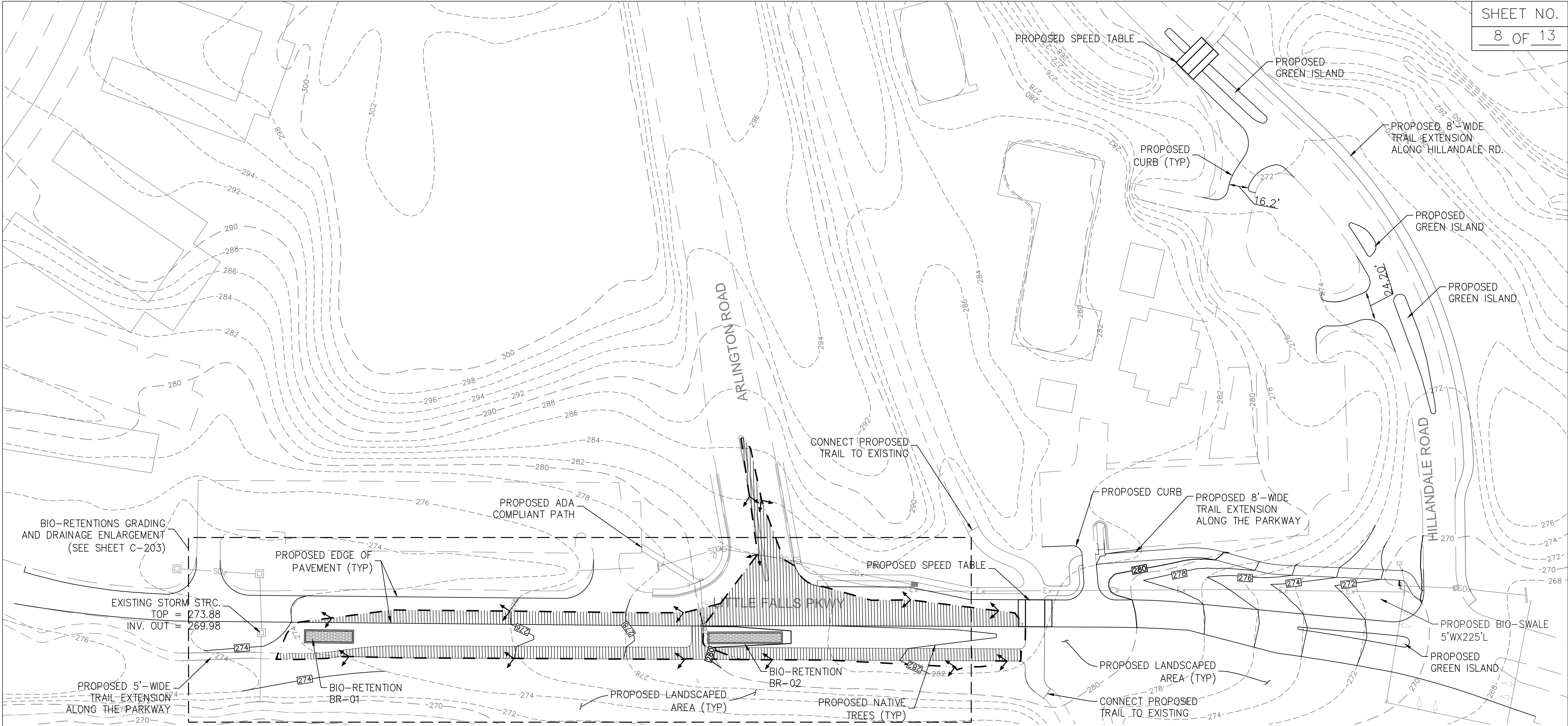
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LEGEND

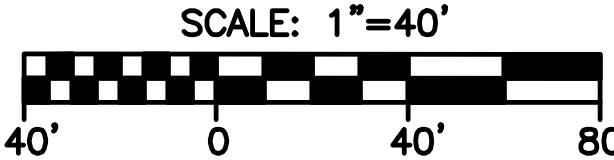
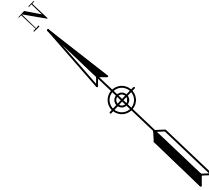
PROPOSED MAJOR CONTOUR	
PROPOSED MINOR CONTOUR	
DRAINAGE DIVIDE	
TREATED IMPERVIOUS COVER	

ALTERNATIVE CONCEPT GRADING PLAN NOTES:

1. ALL EXISTING ELEVATIONS ARE OBTAINED FROM THE COUNTY GIS.
 2. FOR BIO-RETENTION ENLARGEMENTS GRADING AND DRAINAGE SEE SHEET C-203.
 3. ALL OTHER GRADING PROPOSED WITH THE PROJECT FOR THE NEW TRAILS AND THE CONVERSION OF SOUTHBOUND LITTLE FALLS PARKWAY WILL CONTINUE TO HONOR EXISTING DRAINAGE PATTERNS WITHIN THE PROJECT SITE.
 4. NEW UNDER DRAINS, CLEANOUTS AND STORM DRAIN NETWORK FOR THE BIO-RETENTION PLANTER BOXES ARE SHOWN ON SHEET C-203.
- IT IS FULLY EXPECTED THAT THE RAINFALL RUNOFF WITHIN THE PROJECT SITE WILL BE REDUCED DUE TO THE INCREASED PERVIOUS AREAS.



CONCEPT SITE GRADING PLAN (ALTERNATIVE)

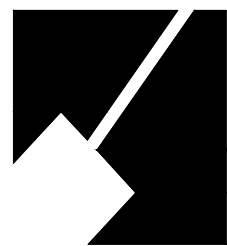


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					CHECKED BY: D. QUINN	DATE 03/27/2019
					DRAWING NO.:	CG-102
					Approved: _____	
					Chief, Traffic Engineering and Operations	
					Reviewed: _____	
					Manager, Transportation Systems Engineering	
					Reviewed: _____	
					Manager, Traffic Control and Lighting Engineering	
					Recommended: _____	
					Engineer, Transportation Systems Engineering	

DEPARTMENT OF TRANSPORTATION
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AT LITTLE FALLS PARKWAY
PED CROSSING IMPROVEMENT PLAN
FROM ARLINGTON RD TO
HILLDALE RD

SCALE: 1" = 40'



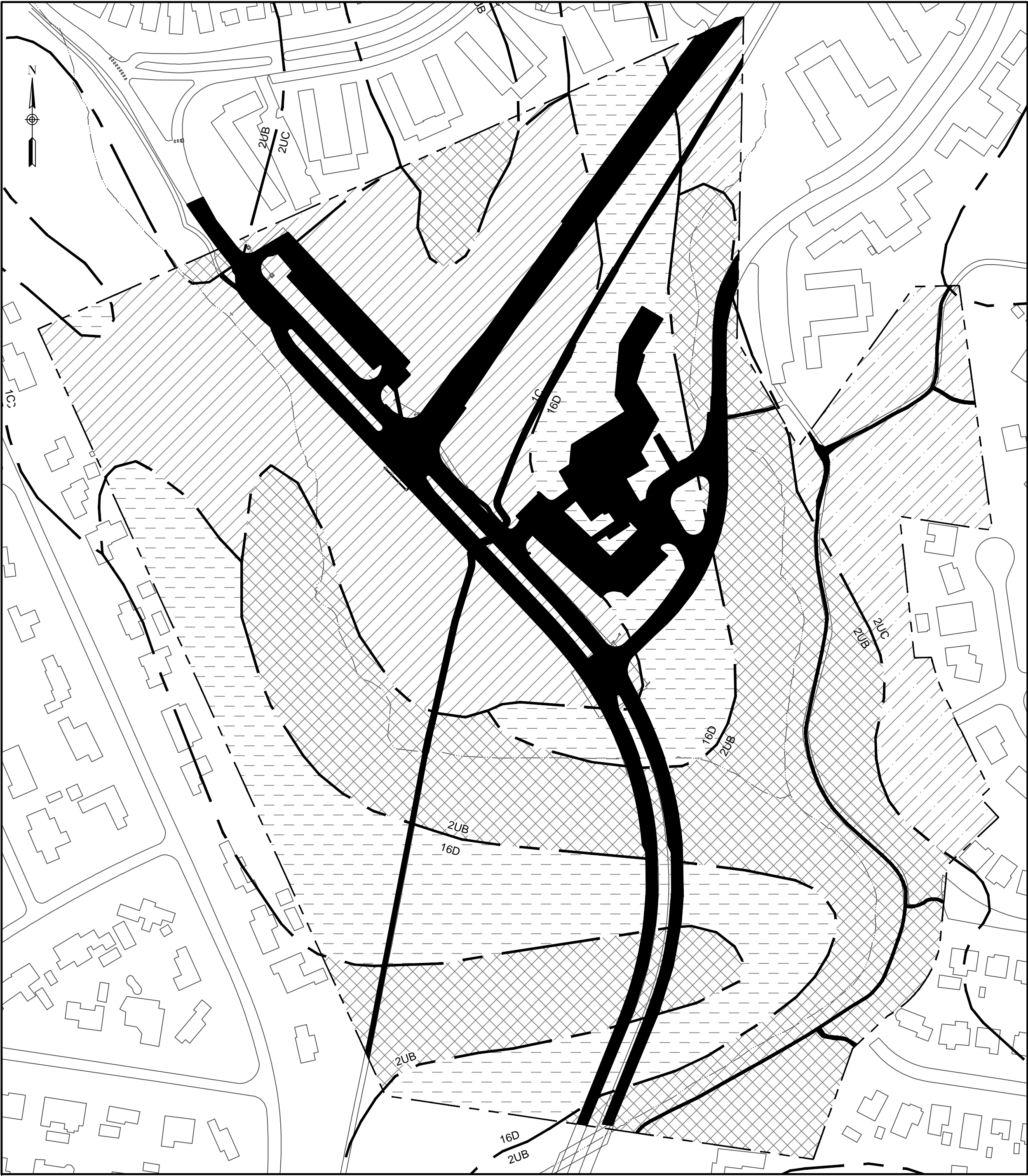
THE MARYLAND-NATIONAL CAPITAL
Park and Planning Commission



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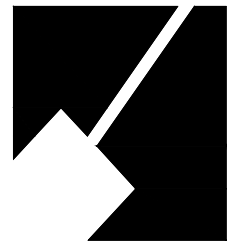
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LEGEND	
PROPERTY LIMITS	---
SOIL DIVIDE	---
EXISTING IMPERVIOUS COVER	█
SOIL 1C	▨
SOIL 2UB	▩
SOIL 2UC	▨
SOIL 16D	---

SITE IMPERVIOUSNESS AND SOIL MAP NOTES:

- THE BACKGROUND INFORMATION AND SOIL TYPES ARE OBTAINED FROM THE COUNTY GIS.
- PERTAINING TO MONTGOMERY COUNTY WRTP-5, THE ENTIRE SITE PROPERTY WAS USED FOR DETERMINING THE P_e VALUE FOR THIS PROJECT (SEE TABLE ON THIS SHEET). THE SITE PROPERTY INCLUDES AREAS OWNED BY THE MARYLAND NATIONAL CAPITAL PARK AND PLANNING COMMISSION (MNCPPC), PORTIONS OF RIGHT OF WAY OF ADJOINING STREETS AND THE BETHESDA COMMUNITY POOL.
- SEE ADDITIONAL COMPUTATIONS FOR ESDv ON SHEET C-201.



THE MARYLAND-NATIONAL CAPITAL
Park and Planning Commission



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PROPERTY SOIL CHART					PROPERTY PE COMPUTATIONS				
MAP UNIT SYMBO	HSG RATING	AREA		% HSG	IMPERVIOUS COVER		I	PE	WEIGHTED PE
		(SF)	(AC)		(SF)	(AC)			
1C	B	658720	15.12	27%	132113	3.03	20%	1.2	0.3
2UB	D	834688	19.16	34%	55686	1.28	7%	1.0	0.3
2UC	D	250537	5.75	10%	24920	0.57	10%	1.0	0.1
16D	C	686319	15.76	28%	104519	2.40	15%	1.0	0.3
	Total	2430579	55.80	100%	317238	7.28	-	-	1.1

Hydrologic Soil Group B										
%I	RCN*	$P_e = 1"$	1.2"	1.4"	1.6"	1.8"	2.0"	2.2"	2.4"	2.6"
0%	61	12								
5%	63									
10%	65									
15%	67	55								
20%	68	60	55	55						
25%	70	64	61	58						
30%	72	65	62	59	55					
35%	74	66	63	60	56					
40%	75	66	63	60	56					
45%	78	68	66	62	58					
50%	80	70	67	64	60					
55%	81	71	68	65	61	55				
60%	83	73	70	67	63	58				
65%	85	75	72	69	65	60	55			
70%	87	77	74	71	67	62	57			
75%	89	79	76	73	69	65	59			
80%	91	81	78	75	71	66	61			
85%	92	82	79	76	72	67	62	55		
90%	94	84	81	78	74	70	65	59	55	
95%	96	87	84	81	77	73	69	63	57	
100%	98	89	86	83	80	76	72	66	59	55

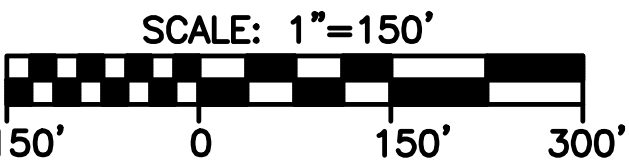
Hydrologic Soil Group D										
%I	RCN*	$P_e = 1"$	1.2"	1.4"	1.6"	1.8"	2.0"	2.2"	2.4"	2.6"
0%	80									
5%	81									
10%	82									
15%	83									
20%	84	77								
25%	85	78								
30%	86	78	77	77						
35%	86	79	78	78						
40%	87	82	81	79	77					
45%	88	82	81	79	78					
50%	89	83	82	80	78					
55%	90	84	82	80	78					
60%	91	85	83	81	78					
65%	92	85	83	81	78					
70%	93	86	84	81	78					
75%	94	86	84	81	78					
80%	94	86	84	82	79					
85%	95	86	84	82	79					
90%	96	87	84	82	79	77				
95%	97	88	85	82	80	78				
100%	98	89	86	83	80	78	77			

Table 5.3 Runoff Curve Number Reductions used for Environmental Site Design (continued)

Hydrologic Soil Group C										
%I	RCN*	$P_e = 1"$	1.2"	1.4"	1.6"	1.8"	2.0"	2.2"	2.4"	2.6"
0%	74									
5%	75									
10%	76									
15%	78									
20%	79	70								
25%	80	72	70	70						
30%	81	73	72	71						
35%	82	74	73	72	70					
40%	84	77	75	73	71					
45%	85	78	76	74	71					
50%	86	78	76	74	71					
55%	86	78	76	74	71	70				
60%	88	80	78	76	73	71				
65%	90	82	80	77	75	72				
70%	91	82	80	78	75	72				
75%	92	83	81	79	75	72				
80%	93	84	82	79	76	72				
85%	94	85	82	79	76	72				
90%	95	86	83	80	77	73	70			
95%	97	88	85	82	79	75	71			
100%	98	89	86	83	80	76	72	70		

█ C_p , Addressed (RCN = Woods in Good Condition)

█ RCN Applied to C_p Calculations



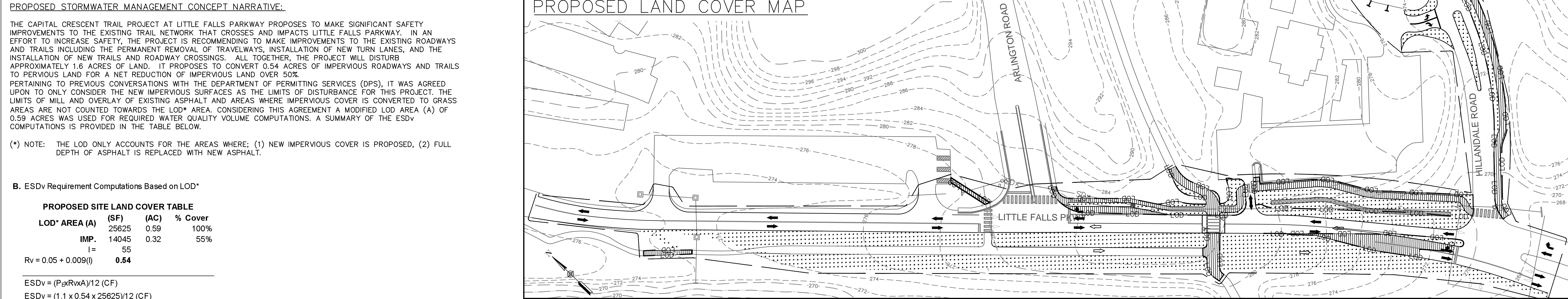
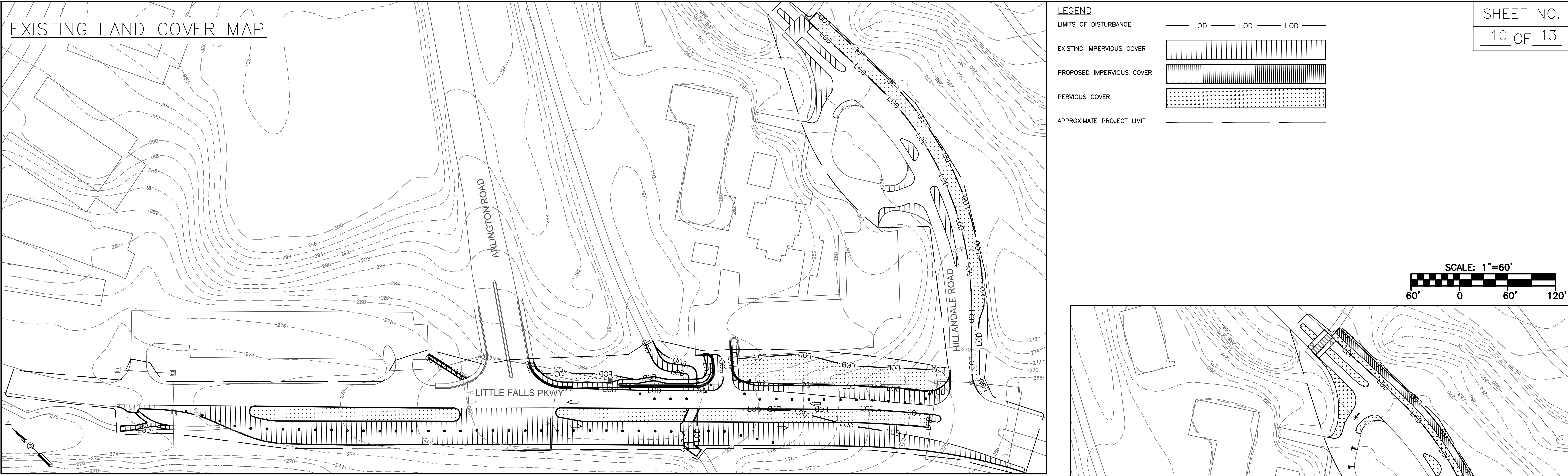
SITE IMPERVIOUSNESS AND SOIL MAP

NO.	REVISION	BY	APP'D	DATE	DESIGNED BY: S. ABDSHARIFABADI	DATE 03/27/2019
					DRAWN BY: S. ABDSHARIFABADI	DATE 03/27/2019
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					Approved: _____ Chief, Traffic Engineering and Operations	
					Reviewed: _____ Manager, Transportation Systems Engineering	
					Reviewed: _____ Manager, Traffic Control and Lighting Engineering	
					Recommended: _____ Engineer, Transportation Systems Engineering	

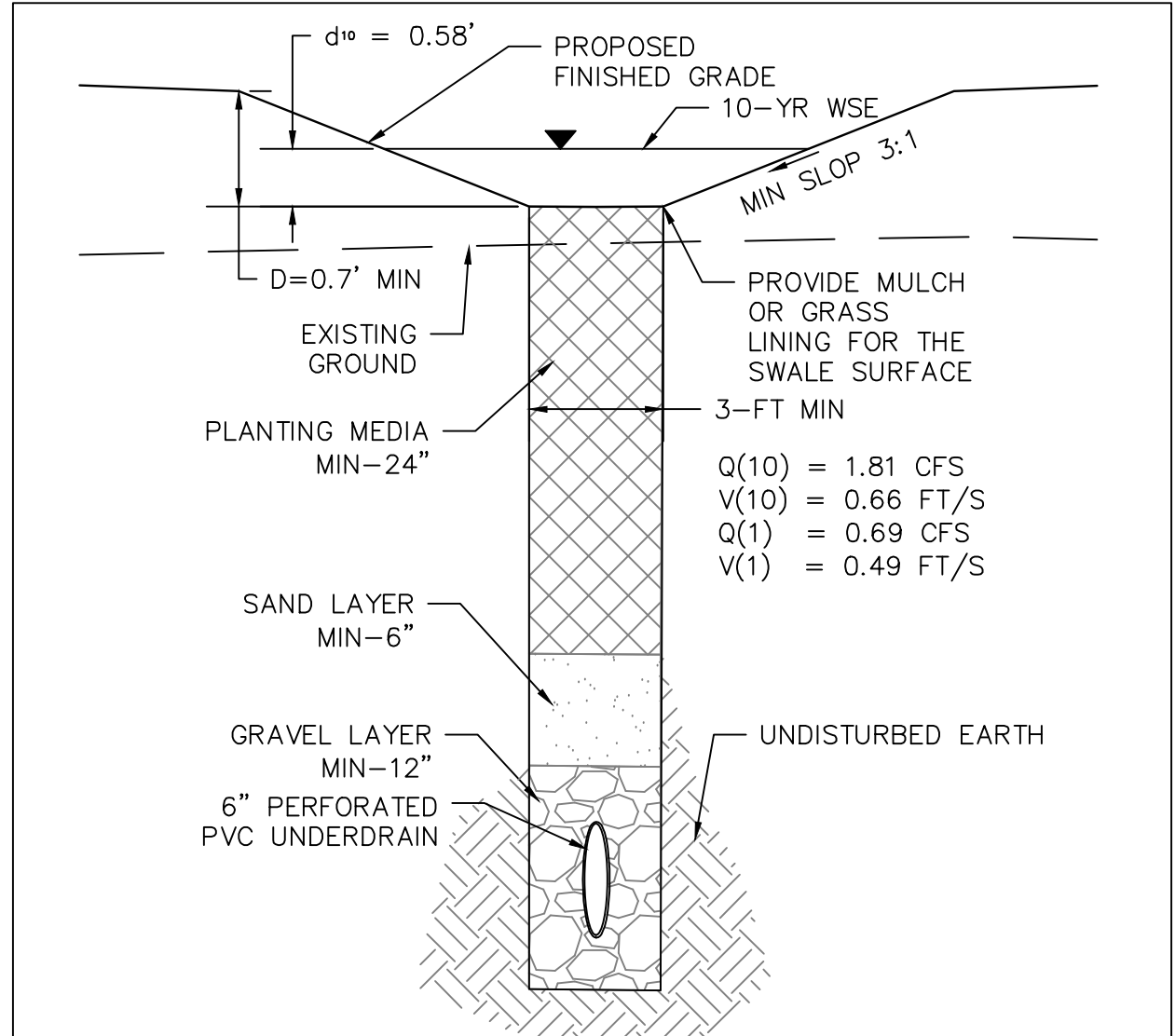
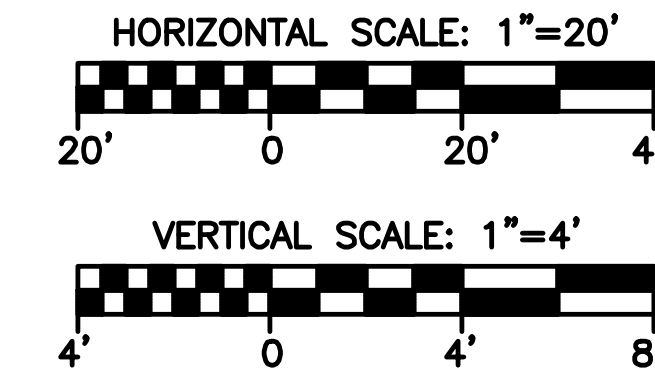
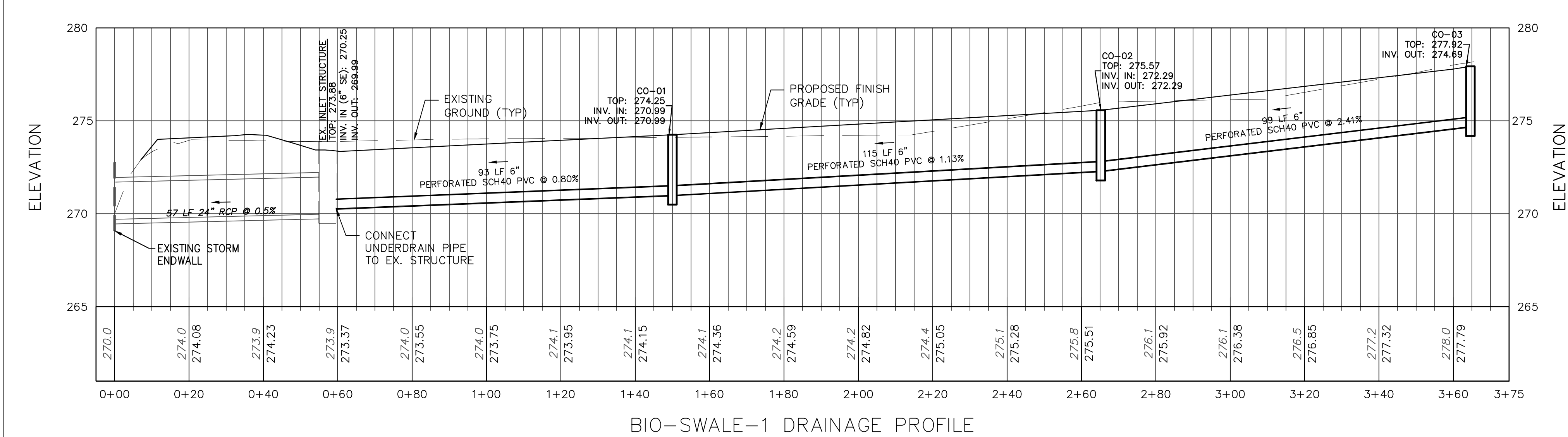
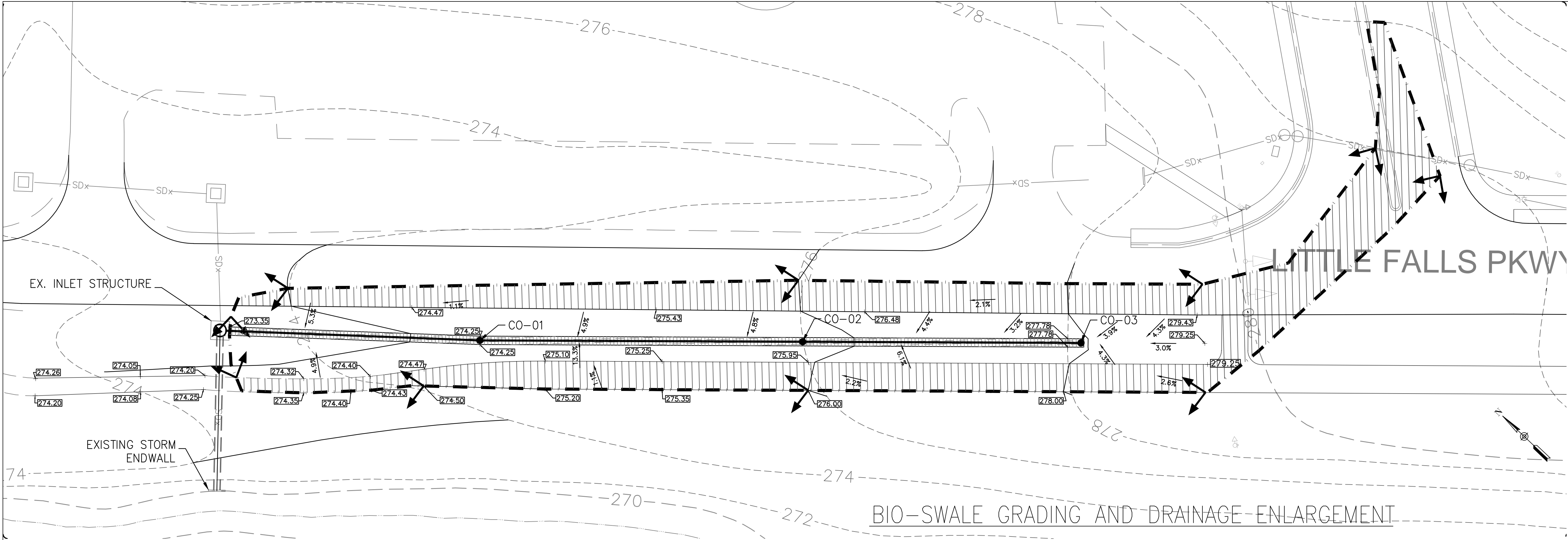
DEPARTMENT OF TRANSPORTATION
DIVISION OF OPERATIONS
MONTGOMERY COUNTY, MARYLAND

CAPITAL CRESCENT TRAIL CROSSING
AT LITTLE FALLS PARKWAY
PED CROSSING IMPROVEMENT PLAN
FROM ARLINGTON RD TO
HILLDALE RD

SCALE: 1" = 150'



B. ESDv Requirement Computations Based on LOD*		PROPOSED SITE LAND COVER TABLE	
LOD* AREA (A)	(SF)	(AC)	% Cover
IMP.	14045	0.32	55%
I=	55		
Rv = 0.05 + 0.009(I)	0.54		
ESDv = (P _x R _v A)/12 (CF)			
ESDv = (1.1 x 0.54 x 25625)/12 (CF)			
ESDv		1276 CF	
SITE ESDv AREA MAP			
NO.		REVISION	
		BY	
		APP'D	
		DATE	
		DESIGNED BY: S. ABDSHARIFABADI	
		DATE 03/27/2019	
		DRAWN BY: S. ABDSHARIFABADI	
		DATE 03/27/2019	
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		DATE 03/27/2019	
		DRAWING NO.: C-201	
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		Chief, Traffic Engineering and Operations	
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		Manager, Transportation Systems Engineering	
		Reviewed: _____	
		Manager, Traffic Control and Lighting Engineering	
		Recommended: _____	
		Engineer, Transportation Systems Engineering	
		DEPARTMENT OF TRANSPORTATION	
		DIVISION OF OPERATIONS	
		MONTGOMERY COUNTY, MARYLAND	
		CAPITAL CRESCENT TRAIL CROSSING	
		AT LITTLE FALLS PARKWAY	
		PED CROSSING IMPROVEMENT PLAN	
		FROM ARLINGTON RD TO	
		HILLDALE RD	
		SCALE: 1" = 60'	



NOTE: OWNER IS RESPONSIBLE FOR ALL MAINTENANCE AND REGULAR INSPECTIONS.

Bio Swale-1 (Proposed Parraller to SB-Little Falls Parkway Before Arlington Rd.)			
Impervious Area		8980	
Total DA to Bio-Swale		15960	
I		56%	
Rv		0.56	
Pe (MAX)		2.6	
Pe (MIN)		1.0	
ESDv (MAX)		1924	
ESDv (MIN)		740	
Proposed Area (SF)		915	
Proposee Planting Medium Depth (Ft.)		2	
Gravel Layer Depth (Ft.)		1	
Sand Layer Depth (Ft.)		0.5	
Void Ratio for Sand, Gravel, and Planting Medium		0.4	
Required ESDv (CF)		1276	
ESDv (provided in the filter Media) (CF)		1281	
ESDv Met		Yes	



CONCEPT STORMWATER MANAGEMENT

NO.	REVISION	BY	APP'D	DATE	DESIGNED BY: S. ABDSHARIFABADI	DATE 03/27/2019
					DRAWN BY: S. ABDSHARIFABADI	DATE 03/27/2019
					CHECKED BY: D. QUINN	DATE 03/27/2019
					DRAWING NO.:	C-202
					Approved: _____	
					Chief, Traffic Engineering and Operations	
					Reviewed: _____	
					Manager, Transportation Systems Engineering	
					Reviewed: _____	
					Manager, Traffic Control and Lighting Engineering	
					Recommended: _____	
					Engineer, Transportation Systems Engineering	

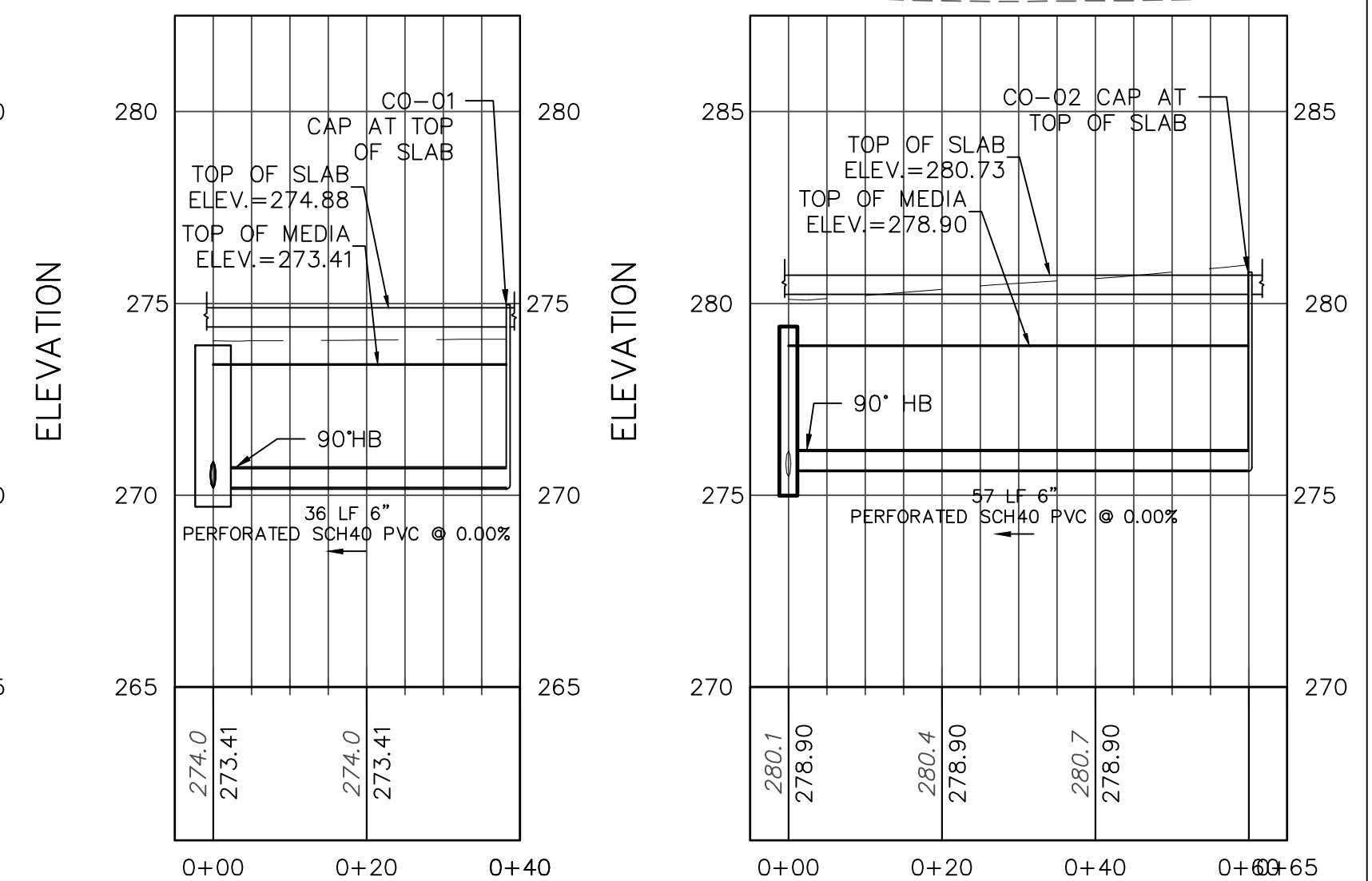
DEPARTMENT OF TRANSPORTATION
DIVISION OF OPERATIONS
MONTGOMERY COUNTY, MARYLAND

CAPITAL CRESCENT TRAIL CROSSING
AT LITTLE FALLS PARKWAY
PED CROSSING IMPROVEMENT PLAN
FROM ARLINGTON RD TO
HILLANDALE RD

SCALE: AS SHOWN



ALPHA CORPORATION
21351 RIDGETOP CIRCLE
SUITE 200
DULLES, VA. 20166
(703) 450-0800
www.alphacorporation.com

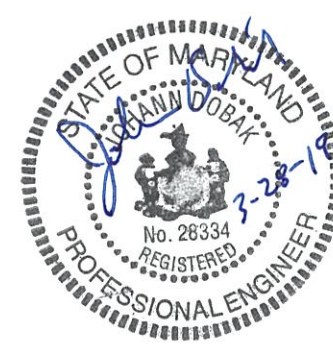


BR-02 DRAINAGE PROFILE

LEGEND

PROPOSED SPOT ELEVATION ???

NOTE: OWNER IS RESPONSIBLE FOR ALL MAINTENANCE AND REGULAR INSPECTIONS.



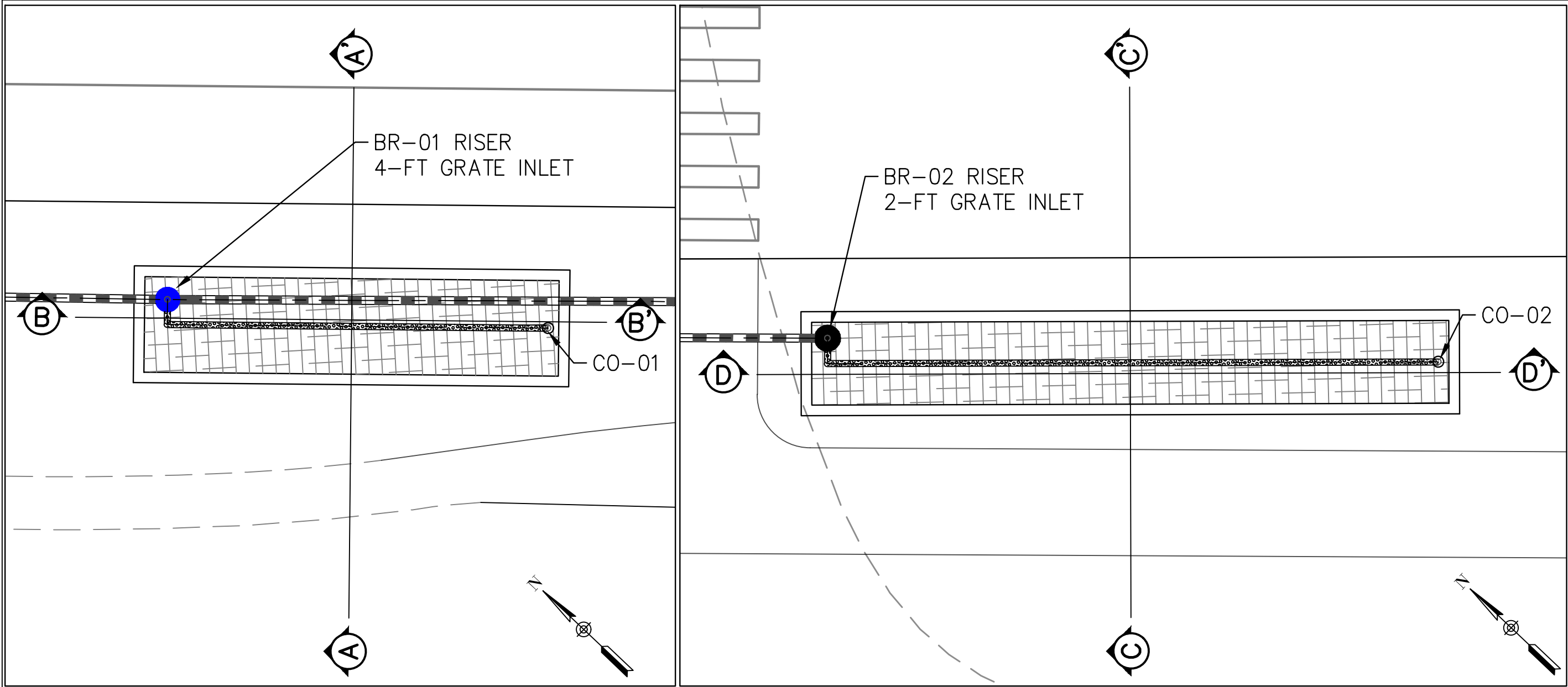
SABRA & ASSOCIATES, INC.



DEPARTMENT OF TRANSPORTATION
DIVISION OF OPERATIONS
MONTGOMERY COUNTY, MARYLAND

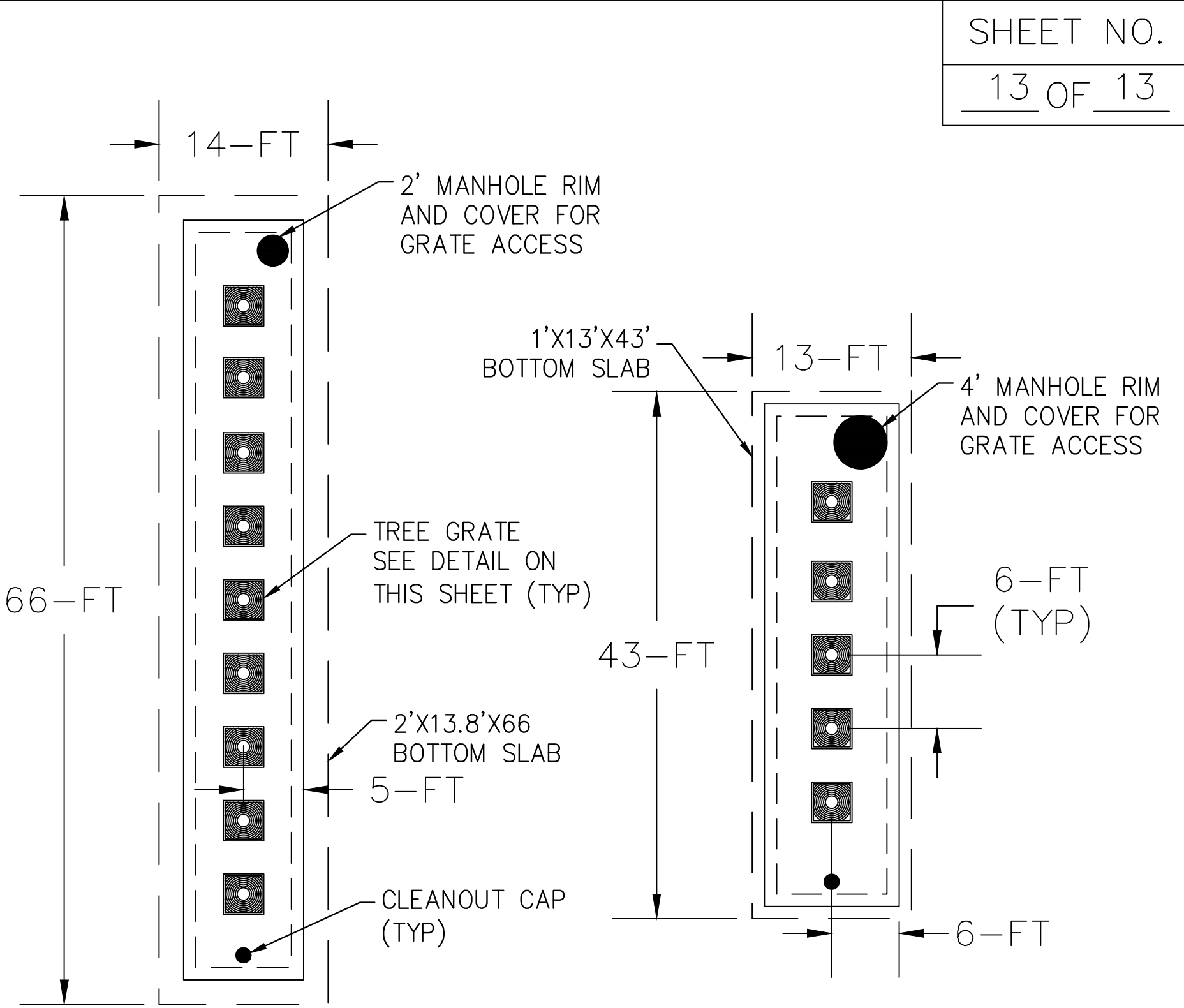
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PLOTTED: \$DATETIME\$
FILE: \$FILE\$

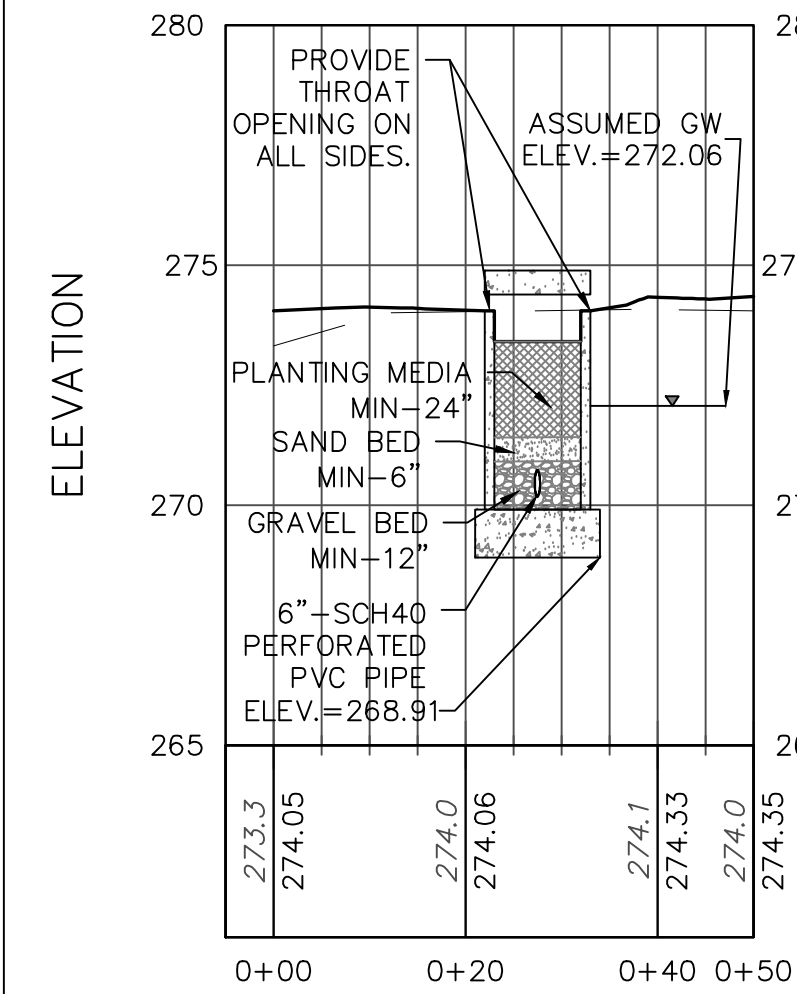


BIO-RETENTION ENLARGEMENT 1"=10'

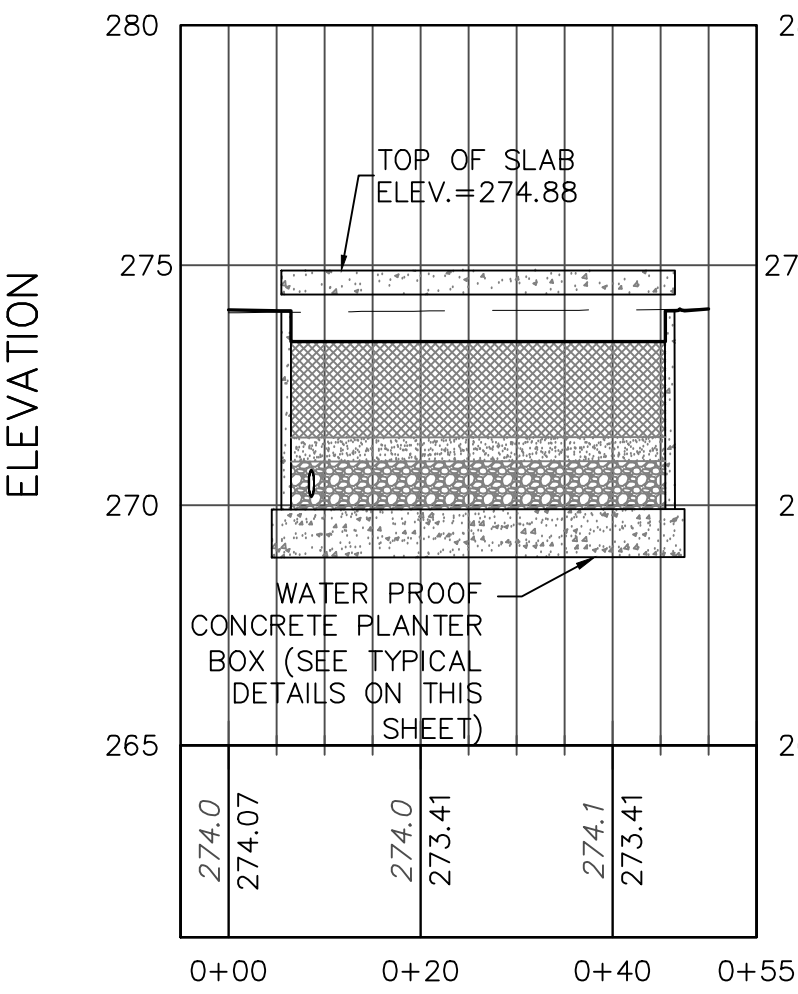
STRUCTURE INFORMATION		
DIMENSIONS	BIO-RETENTION BR-01	BIO-RETENTION BR-02
PLANTER BOX SIZES (FT)		
WALL THICKNESS	1	1
WIDTH	9	7.8
LENGTH	39	60
TOP SLAB SIZES (FT)		
THICKNESS	1	1
WIDTH	11	9.8
LENGTH	41	62
BASE SLAB SIZES (FT)		
THICKNESS	1	2
WIDTH	13	13.8
LENGTH	43	66
CONCRETE VOLUME (CF)	1361	2897
WEIGHT GRATES AND COVER (LBS.)	800	900
WEIGHT OF STRUCTURE (LBS.)	204950	435480
BACKFILL AND SLIDING RESISTANCE FORCES		
ASSUMED BACKFILL γ_{SUB}	70	70
BURIED STRUCTURAL PERIMETER (FT)	104	144
P (LBS./FT)	306	306
SLIDING RESISTANCE (LBS.)	9558	13197
WEIGHT SOIL (LBS.)	31374	66856
BOX BOTTOM ELEV.	268.9	273.4
ASSUMED GW ELEV.	272.1	278.6
SUBMERGED DEPTH	3.1	5.2
BUOYANCY ANALYSIS RESULTS		
TOTAL RESISTING FORCES (LB.)	245882	515533
BUOYANT FORCE (LB.)	88649	197154
F.S. MIN 2.0	2.8	2.6



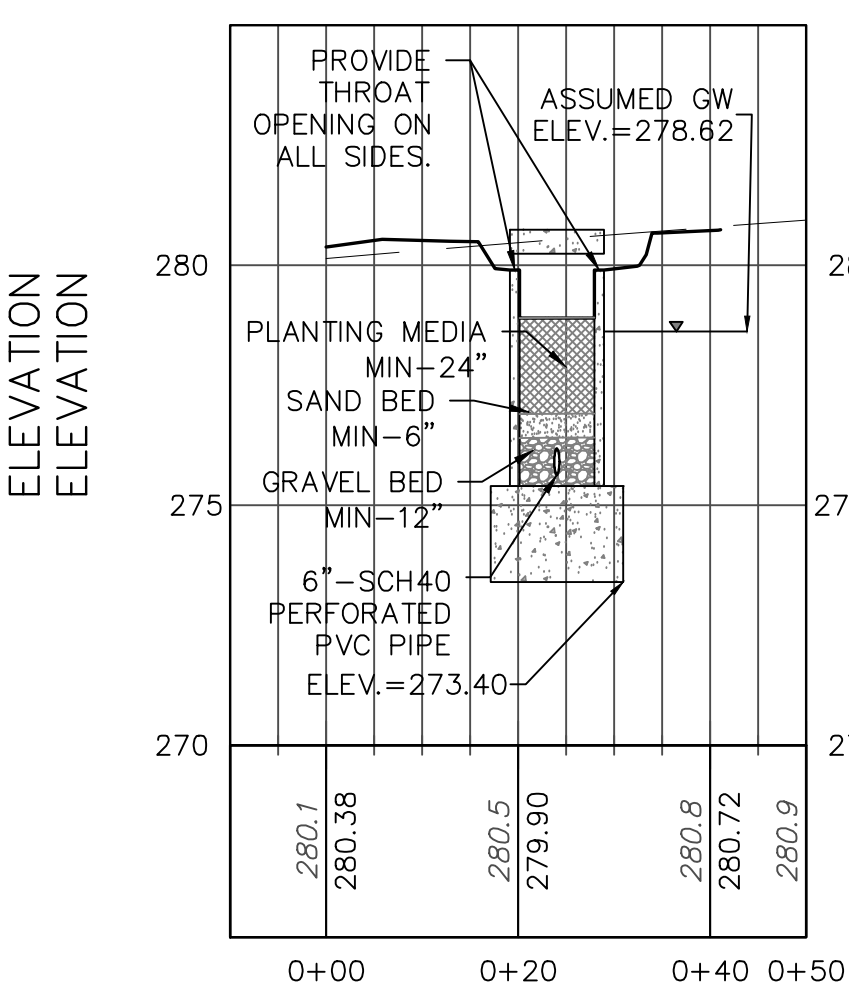
BIO-RETENTION PLAN DETAIL 1"=10'



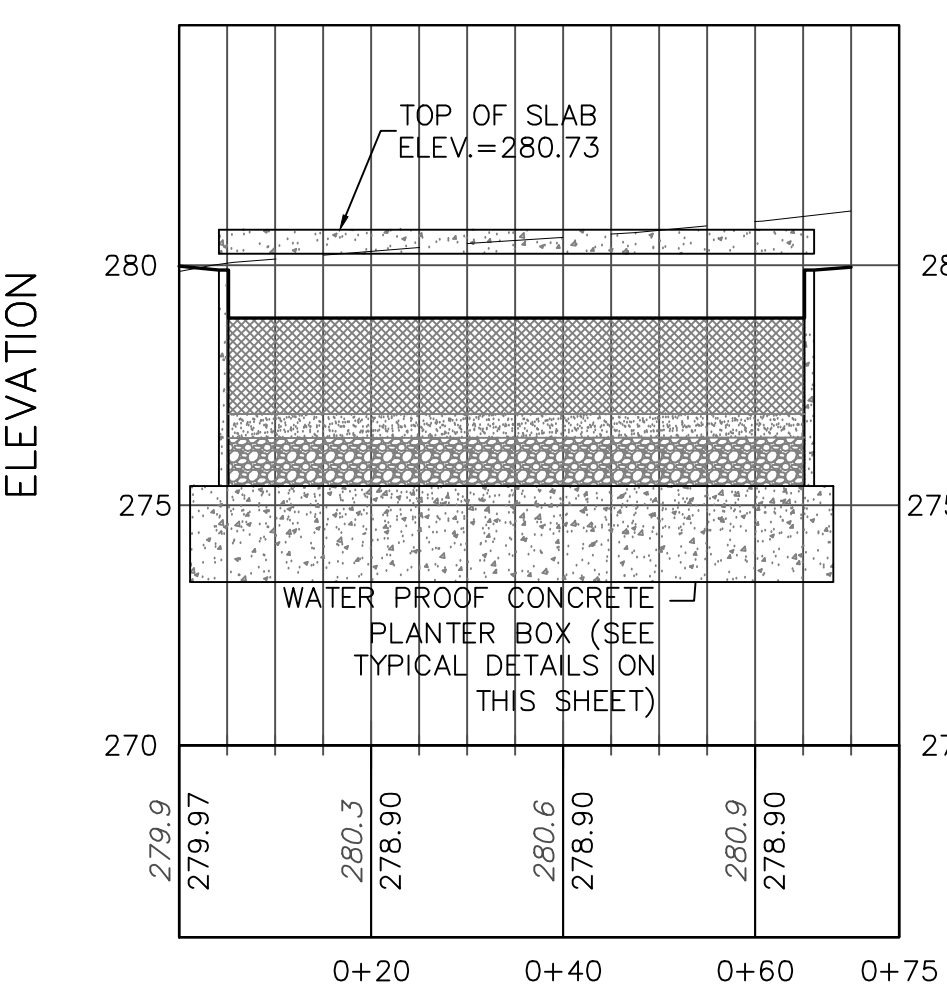
BR-01 SECTION A-A'



BR-01 SECTION B-B'



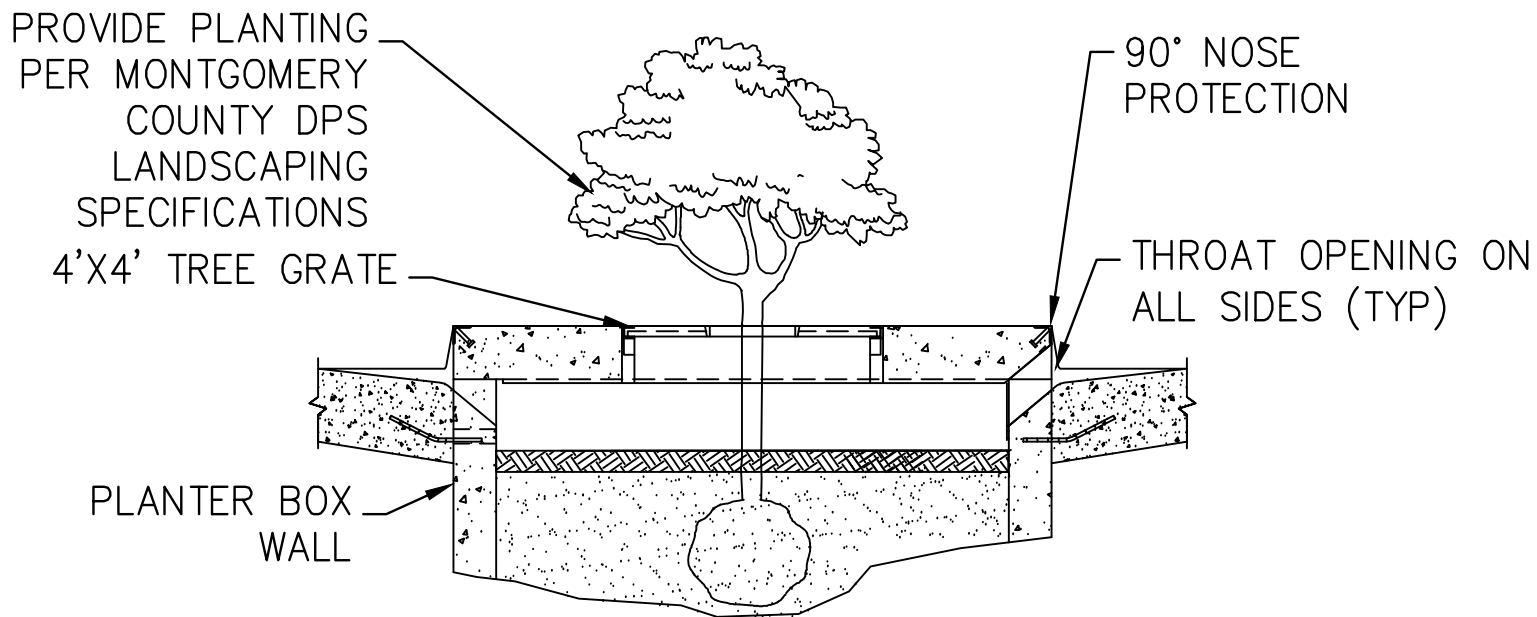
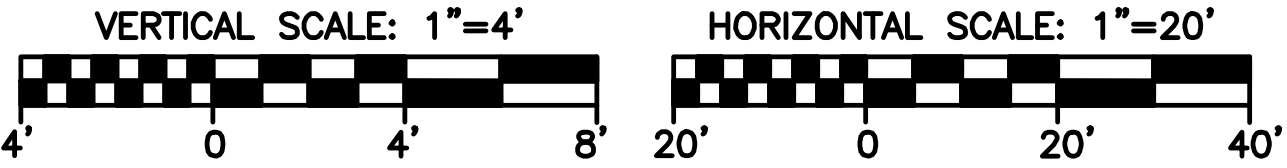
BR-02 SECTION C-C'



BR-02 SECTION D-D'

NOTE: OWNER IS RESPONSIBLE FOR ALL MAINTENANCE AND REGULAR INSPECTIONS.

BIO-RETENTION SECTION PROFILES



BIO-RETENTION TREE GRATE SECTION N.T.S

BIO-RETENTION PLANTER BOX DETAILS



NO.	REVISION	BY	APP'D	DATE	DESIGNED BY: S. ABDSHARIFABADI	DATE 03/27/2019
					DRAWN BY: S. ABDSHARIFABADI	DATE 03/27/2019
					CHECKED BY: D. QUINN	DATE 03/27/2019
					DRAWING NO.:	C-204
					Approved: _____	
					Chief, Traffic Engineering and Operations	
					Reviewed: _____	
					Manager, Transportation Systems Engineering	
					Reviewed: _____	
					Manager, Traffic Control and Lighting Engineering	
					Recommended: _____	
					Engineer, Transportation Systems Engineering	

DEPARTMENT OF TRANSPORTATION
DIVISION OF OPERATIONS
MONTGOMERY COUNTY, MARYLAND

CAPITAL CRESCENT TRAIL CROSSING
AT LITTLE FALLS PARKWAY
PED CROSSING IMPROVEMENT PLAN
FROM ARLINGTON RD TO
HILLANDALE RD

SCALE: AS SHOWN



Appendix D

Cost Estimates

- Recommended Facility Plan (Based on Alternate A: Road Diet)
- Alternate B: Trail Reorientation to Arlington Road Signal
- Alternate C: Pedestrian Bridge

APPENDIX D COST ESTIMATES

CAPITAL CRESCENT TRAIL - RECOMMENDED FACILITY PLAN - 30% DESIGN						
ITEM	ITEM CODE	DESCRIPTION	UNIT	ESTIMATED QUANTITY	UNIT PRICE	EXTENDED PRICE
CATEGORY 1 - PRELIMINARY						
		MOBILIZATION AND MAINTENANCE OF TRAFFIC	LS	1	\$59,000.00	\$59,000
		EROSION AND SEDIMENT CONTROLS	LS	1	\$36,500.00	\$36,500
CATEGORY 2 - GRADING						
		CLASS 1 EXCAVATION	CY	400	\$35.00	\$14,000
		REMOVAL OF EXISTING COMBINATION CURB & GUTTER	LF	650	\$15.00	\$9,750
		REMOVAL OF EXISTING PAVEMENT	CY	3,450	\$25.00	\$86,250
		TREE CLEARING AND DEMOLITION	SY	25	\$125.00	\$3,125
CATEGORY 3 - DRAINAGE						
ROAD/TRAIL DRAINAGE, LFP CULVERT REPLACEMENT, POOL TRENCH DRAIN REPLACEMENT; % OF CAT. 2,4,5,6					15%	\$39,030
		STORMWATER MANAGEMENT - BIOSWALE ~1300 CF	LS	1	\$49,520.00	\$49,520
CATEGORY 4 - STRUCTURES						
	N/A	N/A	-			\$0
CATEGORY 5 - PAVING						
		5 INCH WHITE REFLECTIVE THERMOPLASTIC PAVEMENT MARKING	LF	200	\$3.50	\$700
		5 INCH YELLOW REFLECTIVE THERMOPLASTIC PAVEMENT MARKING	LF	2,000	\$3.50	\$7,000
		24 INCH WHITE REFLECTIVE THERMOPLASTIC PAVEMENT MARKING	LF	570	\$20.00	\$11,400
		2 INCH SUPERPAVE ASPHALT MIX FOR SURFACE	TON	200	\$150.00	\$30,000
		2" SURFACE MILLING	SY	3,450	\$6.00	\$20,700
		4 INCH GRADED AGGREGATE BASE COURSE	SY	1,100	\$15.00	\$16,500
		TRAFFIC CALMING: HILLANDALE ROAD NARROWING, ALL INCLUSIVE	LUMP	1	\$200,000.00	\$200,000
CATEGORY 6 - SHOULDER						
		ASPHALT SPEED HUMP	Lump	2	\$5,000.00	\$10,000
		ADA RAMP	EA	8	\$1,250.00	\$10,000
		TYPE A CURB AND GUTTER	LF	1,300	\$25.00	\$32,500
		5 INCH CONCRETE SIDEWALK	SF	800	\$8.00	\$6,400
		Partial Guardrail Removal & Disposal	Lump	1	\$5,000.00	\$5,000
CATEGORY 7 - LANDSCAPING						
ESTIMATED LIMIT OF DISTURBANCE AND FOREST IMPACTS; FIXED					-	\$52,250
CATEGORY 8 - TRAFFIC						
		APS PEDESTRIAN SIGNAL ASSEMBLY	EA	6	\$8,000.00	\$48,000

APPENDIX D COST ESTIMATES

		RELOCATE SIGNAL POLES AND MAST ARMS	LUMP	1	\$150,000.00	\$150,000
		SQUARE PERFORATED TUBULAR STEEL SIGN POST	EA	10	\$100.00	\$1,000
		SQUARE PERFORATED TUBULAR STEEL ANCHOR BASES	EA	10	\$100.00	\$1,000
		SHEET ALUMINUM SIGNS	SF	65	\$50.00	\$3,250
		REMOVE EXISTING GROUND MOUNTED SIGNS AND SUPPORTS	SF	125	\$10.00	\$1,250
		RELOCATE EXISTING GROUND MOUNTED SIGNS AND SUPPORTS	SF	25	\$35.00	\$875
CONSTRUCTION SUBTOTAL:						\$905,000
GENERAL CONTRACTOR OVERHEAD AND PROFIT					8%	\$72,400
CONSTRUCTION CONTINGENCY					15%	\$135,750
CONSTRUCTION TOTAL :						\$1,113,150
Detailed (100%) Design and Permitting						\$181,000
Staff Construction Management and Inspections						\$39,000
Staff Chargebacks for Detailed Design						\$44,500
				Total Project Cost in 2019 Dollars		\$1,378,000

APPENDIX D COST ESTIMATES

CAPITAL CRESCENT TRAIL - ALTERNATE B: DIVERT TRAIL TO ARLINGTON ROAD SIGNAL - CONCEPT

ITEM	ITEM CODE	DESCRIPTION	UNIT	ESTIMATED QUANTITY	UNIT PRICE	EXTENDED PRICE
CATEGORY 1 - PRELIMINARY						
MOBILIZATION, SEDIMENT CONTROLS, MAINTENANCE OF TRAFFIC				% OF CAT. 2, 5, 6	20%	\$110,370
CATEGORY 2 - GRADING						
		CLASS 1 EXCAVATION	CY	400	\$35.00	\$14,000
		REMOVAL OF EXISTING COMBINATION CURB & GUTTER	LF	650	\$15.00	\$9,750
		REMOVAL OF EXISTING PAVEMENT	CY	4,000	\$25.00	\$100,000
		TREE CLEARING AND DEMOLITION	SY	800	\$125.00	\$100,000
CATEGORY 3 - DRAINAGE						
ROAD/TRAIL DRAINAGE, LFP CULVERT REPLACEMENT, POOL TRENCH DRAIN REPLACEMENT; % OF CAT. 2,4,5,6					15%	\$52,778
		STORMWATER MANAGEMENT - BIOSWALES	LS	1	\$90,000.00	\$90,000
CATEGORY 4 - STRUCTURES						
	N/A	N/A	-			\$0
CATEGORY 5 - PAVING						
		5 INCH WHITE REFLECTIVE THERMOPLASTIC PAVEMENT MARKING	LF	200	\$3.50	\$700
		5 INCH YELLOW REFLECTIVE THERMOPLASTIC PAVEMENT MARKING	LF	2,000	\$3.50	\$7,000
		24 INCH WHITE REFLECTIVE THERMOPLASTIC PAVEMENT MARKING	LF	570	\$20.00	\$11,400
		2 INCH SUPERPAVE ASPHALT MIX FOR SURFACE	TON	350	\$150.00	\$52,500
		2" Surface Milling	SY	3,600	\$6.00	\$21,600
		4 INCH GRADED AGGREGATE BASE COURSE	SY	2,900	\$15.00	\$43,500
		TRAFFIC CALMING: HILLANDALE ROAD NARROWING, ALL INCLUSIVE	LUMP	1	\$200,000.00	\$200,000
CATEGORY 6 - SHOULDER						
		ASPHALT SPEED HUMP	Lump	1	\$5,000.00	\$5,000
		ADA RAMP	EA	8	\$1,250.00	\$10,000
		TYPE A CURB AND GUTTER	LF	2,600	\$25.00	\$65,000
		5 INCH CONCRETE SIDEWALK	SF	800	\$8.00	\$6,400
		Partial Guardrail Removal & Disposal	Lump	1	\$5,000.00	\$5,000
		Stone Walls or Barriers (Divert to intersection)	LF	200	\$200.00	\$40,000
CATEGORY 7 - LANDSCAPING						
ESTIMATED LIMIT OF DISTURBANCE AND FOREST IMPACTS; FIXED					-	\$150,000
CATEGORY 8 - TRAFFIC						
		APS PEDESTRIAN SIGNAL ASSEMBLY	EA	8	\$8,000.00	\$64,000

APPENDIX D COST ESTIMATES

		RELOCATE SIGNAL POLES AND MAST ARMS	LUMP	1	\$210,000.00	\$210,000
		SQUARE PERFORATED TUBULAR STEEL SIGN POST	EA	10	\$100.00	\$1,000
		SQUARE PERFORATED TUBULAR STEEL ANCHOR BASES	EA	10	\$100.00	\$1,000
		SHEET ALUMINUM SIGNS	SF	65	\$50.00	\$3,250
		REMOVE EXISTING GROUND MOUNTED SIGNS AND SUPPORTS	SF	125	\$10.00	\$1,250
		RELOCATE EXISTING GROUND MOUNTED SIGNS AND SUPPORTS	SF	25	\$35.00	\$875
CONSTRUCTION SUBTOTAL:						\$1,376,373
GENERAL CONTRACTOR OVERHEAD AND PROFIT					8%	\$110,110
CONSTRUCTION CONTINGENCY					15%	\$206,456
CONSTRUCTION TOTAL :						\$1,692,938
Detailed (100%) Design and Permitting						\$195,445
Staff Construction Management and Inspections						\$50,788
Staff Chargebacks for Detailed Design						\$48,861
				Total Project Cost in 2019 Dollars		\$1,990,000

APPENDIX D COST ESTIMATES

CAPITAL CRESCENT TRAIL - ALTERNATE C: INSTALL PEDESTRIAN BRIDGE - CONCEPT						
ITEM	ITEM CODE	DESCRIPTION	UNIT	ESTIMATED QUANTITY	UNIT PRICE	EXTENDED PRICE
CATEGORY 1 - PRELIMINARY						
EROSION CONTROL, SEDIMENT CONTROLS, MAINTENANCE OF TRAFFIC, TEMP. TRAIL DIVERT				% OF CAT. 2, 5, 6	28%	\$320,925
CATEGORY 2 - GRADING						
		CLASS 1 EXCAVATION	CY	400	\$35.00	\$14,000
		REMOVAL OF EXISTING COMBINATION CURB & GUTTER	LF	650	\$15.00	\$9,750
		REMOVAL OF EXISTING PAVEMENT	CY	3,450	\$25.00	\$86,250
		TREE CLEARING AND DEMOLITION	SY	2,400	\$125.00	\$300,000
CATEGORY 3 - DRAINAGE						
/RAMP DRAINAGE, LFP CULVERT REPLACEMENT, POOL TRENCH DRAIN REPLACEMENT; % OF CAT. 2,4,5,6					15%	\$142,088
		STORMWATER MANAGEMENT - BIOSWALES AND BIORETENTIONS	LS	1	\$150,000.00	\$150,000
CATEGORY 4 - STRUCTURES						
		PEDESTRIAN BRIDGE -TRAFFIC RATED	SF	2,000	\$300.00	\$600,000
		RETAINING WALL AND BRIDGE RAMPS	SF	13,000	\$120.00	\$1,560,000
		TRAFFIC RATED CONCRETE BRIDGE ABUTMENTS	EA	2	\$100,000.00	\$200,000
CATEGORY 5 - PAVING						
		5 INCH WHITE REFLECTIVE THERMOPLASTIC PAVEMENT MARKING	LF	200	\$3.50	\$700
		5 INCH YELLOW REFLECTIVE THERMOPLASTIC PAVEMENT MARKING	LF	2,000	\$3.50	\$7,000
		24 INCH WHITE REFLECTIVE THERMOPLASTIC PAVEMENT MARKING	LF	570	\$20.00	\$11,400
		2 INCH SUPERPAVE ASPHALT MIX FOR SURFACE	TON	500	\$150.00	\$75,000
		2" SURFACE MILLING	SY	4,500	\$6.00	\$27,000
		4 INCH GRADED AGGREGATE BASE COURSE	SY	3,500	\$15.00	\$52,500
		TRAFFIC CALMING: HILLANDALE ROAD NARROWING, ALL INCLUSIVE	LUMP	1	\$200,000.00	\$200,000
CATEGORY 6 - SHOULDER						
		ASPHALT SPEED HUMP	Lump	1	\$5,000.00	\$5,000
		ADA RAMP	EA	12	\$1,250.00	\$15,000
		TYPE A CURB AND GUTTER	LF	1,300	\$40.00	\$52,000
		5 INCH CONCRETE SIDEWALK	SF	800	\$8.00	\$6,400
		Partial Guardrail Removal & Disposal	Lump	1	\$5,000.00	\$5,000
CATEGORY 7 - LANDSCAPING						

APPENDIX D COST ESTIMATES

ESTIMATED LIMIT OF DISTURBANCE, WETLAND MITIGATION, STREAM REMEDIATION, AND FOREST IMPACT						\$375,000
CATEGORY 8 - TRAFFIC						
		APS PEDESTRIAN SIGNAL ASSEMBLY	EA	10	\$8,000.00	\$80,000
		RELOCATE SIGNAL POLES AND MAST ARMS	LUMP	1	\$150,000.00	\$150,000
		SQUARE PERFORATED TUBULAR STEEL SIGN POST	EA	10	\$100.00	\$1,000
		SQUARE PERFORATED TUBULAR STEEL ANCHOR BASES	EA	10	\$100.00	\$1,000
		SHEET ALUMINUM SIGNS	SF	65	\$50.00	\$3,250
		REMOVE EXISTING GROUND MOUNTED SIGNS AND SUPPORTS	SF	125	\$10.00	\$1,250
		RELOCATE EXISTING GROUND MOUNTED SIGNS AND SUPPORTS	SF	25	\$35.00	\$875
CONSTRUCTION SUBTOTAL:						\$4,131,463
GENERAL CONTRACTOR OVERHEAD AND PROFIT					8%	\$330,517
CONSTRUCTION CONTINGENCY					15%	\$619,719
CONSTRUCTION TOTAL :						\$5,081,699
Detailed (100%) Design and Permitting						\$475,118
Staff Construction Management and Inspections						\$127,042
Staff Chargebacks for Detailed Design						\$104,526
Total Project Cost in 2019 Dollars						\$5,790,000

Appendix E

Traffic Engineering Study Data

This Appendix contains study data in raw tabulated form. Please refer to the included Memorandum or Facility Plan narrative for summaries and additional information.

- Memorandum: Transportation and Trail User Safety Impacts Alternatives Analysis and Recommendations for Capital Crescent Trail Crossing at Little Falls Parkway
- VISSIM Traffic Model Summary Data
- Capital Crescent Trail Little Falls Parkway crash records through March 2019
- Little Falls Parkway SPOT Speed Study
- Kennedy Drive Cut-through Study
- Dorset Avenue Pre- and Post-Road Diet Traffic Counts
- Pre-Diet Traffic Counts: Arlington Road, Hillandale Road, Little Falls Parkway
- Post-Diet Traffic Counts: Arlington Road, Hillandale Road, Little Falls Parkway

SABRA & ASSOCIATES

To: Mr. Andrew Tsai, PE, Project Manager
M-NCPPC Montgomery Parks
9500 Brunett Avenue
Silver Spring, MD 20901

From: Bryon White, PE, PTOE
Josh Coulson, EIT

Date: February 12, 2019

Re: Transportation and Trail User Safety Impacts Alternatives Analysis and Recommendations for the Capital Crescent Trail crossing at Little Falls Parkway

1. Introduction

The Capital Crescent Trail (CCT) runs from Bethesda, Maryland to the District of Columbia and is the most popular shared-use trail in Montgomery County, ranging from 2,000 to 5,000 users a day. The trail is used for a wide range of recreational activities as well as by commuters (on foot and bicycle). The CCT crosses Little Falls Parkway (LFP) at an uncontrolled mid-block at-grade crossing. Little Falls Parkway varies between 2 and 4 lanes wide and was originally built to provide access to Park amenities and Little Falls Stream Valley Park. In the area of the CCT crossing, Little Falls Parkway is 4 lanes wide.



Figure 1: Vicinity Map

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The CCT crossing at Little Falls Parkway has historically been the site of frequent conflicts between trail users and motorists. The four-lane cross-section on Little Falls Parkway, prior to the interim road diet, allowed for the multiple-lane threat scenario – a recognized traffic safety problem occurring along road sections that are more than two lanes wide and have an uncontrolled crossing (no traffic signal or stop sign). A multiple-lane threat crash involves a driver stopping in one lane on a multilane road to permit pedestrians/cyclists to cross, and an adjacent oncoming vehicle (in the same direction) striking the pedestrian who is crossing in front of the stopped vehicle. This crash is due to the initial stopped vehicle occluding the view of the second advancing vehicle who subsequently fails to yield. The figure below illustrates the multiple-threat crash conflict.

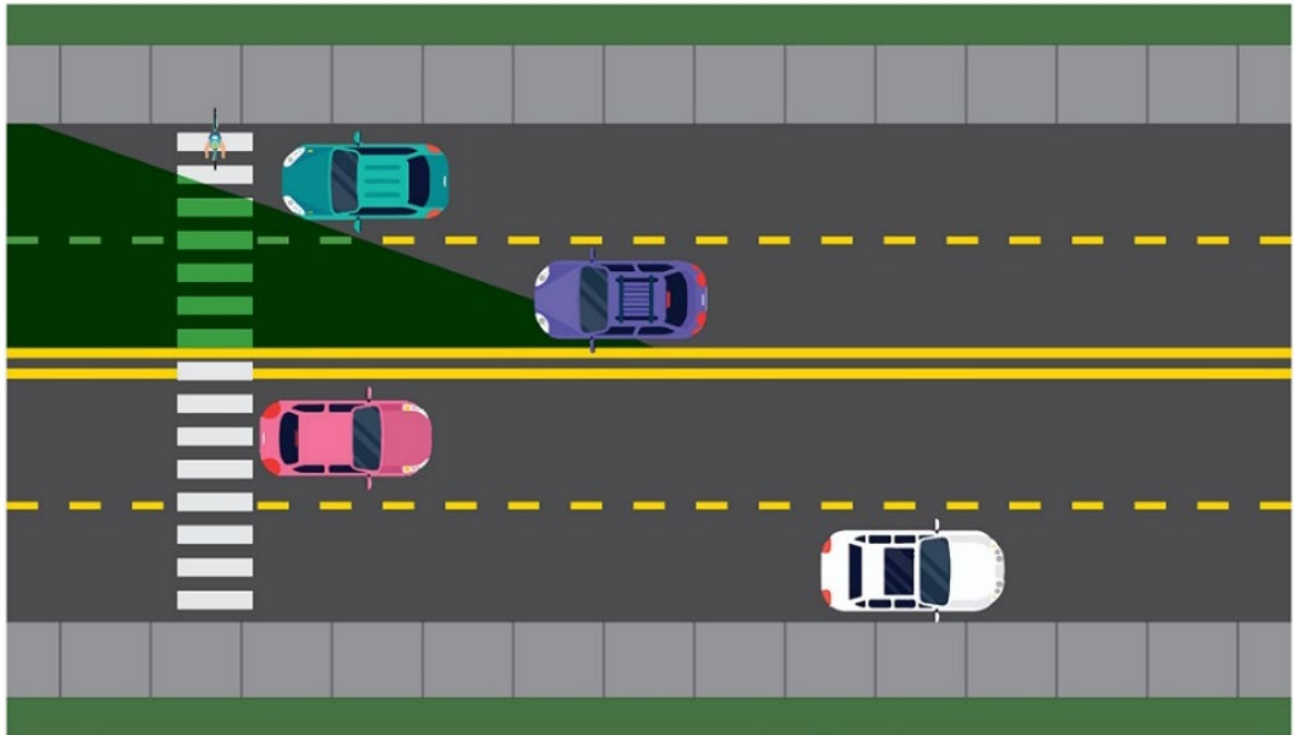


Figure 2: Pedestrian at midblock crosswalk is obscured by stopped car (blue). Adjacent traffic (purple) cannot see.

In the 2-year period from 2014 to 2016, the CCT crossing at LFP experienced 12 conflicts, with a conflict defined here as an incident that generated a police report. Among these 12 conflicts was a fatality involving a bicyclist in the “multiple-lane threat” scenario described above.



2. Interim Safety Improvement: Road Diet

Montgomery County is the first suburban County in the United States to commit to [Vision Zero](#): a national initiative to eliminate pedestrian and bicyclist traffic fatalities and conflicts, while increasing safe, healthy, and equitable mobility for all pedestrians.

In January of 2017, Montgomery Parks implemented a Vision Zero-based interim road diet along Little Falls Parkway, north of Hillandale Road, to address the multi-lane threat at the Capital Crescent Trail. The interim road diet condition reduced the cross-section of Little Falls Parkway from four-lanes to two-lanes, between Arlington Road and Hillandale Road, by closing the outside travel lane in each direction approaching the trail crossing. The travel lanes were closed off with chains and flexible bollards. The speed limit was reduced to 25 mph. The grass median dividing the northbound and southbound travel lanes remained.

2.1 Road Diet Safety Observations

The interim road diet eliminated the multiple-lane threat condition, while simultaneously slowing¹ traffic along Little Falls Parkway through the CCT crossing. These factors resulted in an overall crash reduction in this segment by 58%, from 12 in the two-year prior to the road diet down to 5 in the two years post-road diet. The number of pedestrian/bike crashes was reduced 50% from 6 (including one fatal) to 3, with no fatalities.

Multiple observations of interactions between motorists and trail users at the modified CCT crossing of Little Falls Parkway showed extremely high levels of drivers yielding to trail users in the crosswalk. Corresponding pedestrian/cyclist delay (or waiting time to cross) was effectively zero seconds.

Conclusion: By eliminating the multi-lane threat, reducing vehicle speeds and increasing motorist and trail user awareness, the road diet has been effective in improving safety for trail users. Significant reduction in conflicts and crashes from 12 in the two-year prior to the road diet down to 5 in the two years post-road diet. "Severity" of conflicts appears reduced due to slower vehicle speeds. Compliance of drivers yielding to trail users is very high and wait times for trail users are very low.

2.2 Road Diet Traffic Volume Observations

To observe how the interim road diet impacted traffic patterns, vehicle counts were taken 1 month prior to the installation of the interim road diet (in the first week of December 2016) and then again in May of 2017, four months after the installation of the interim road diet. The traffic counts were taken on Arlington Road, Hillandale Road and Little Falls Parkway near the CCT crossing. Seven-day counts were conducted and the average of Tuesday, Wednesday, and Thursday was used for comparing a typical weekday AM/PM peak hour before and after the interim improvement. The traffic counts provide an objective evaluation of how traffic patterns changed since the road diet. The pre- and post-road diet installation traffic volumes are shown in the figure below.

¹ Spot Speed Counts were conducted for the northbound and southbound segments of Little Falls Parkway from Arlington Road to Hillandale Road. Northbound average speed through the segment was 14 mph, while southbound was 19 mph.

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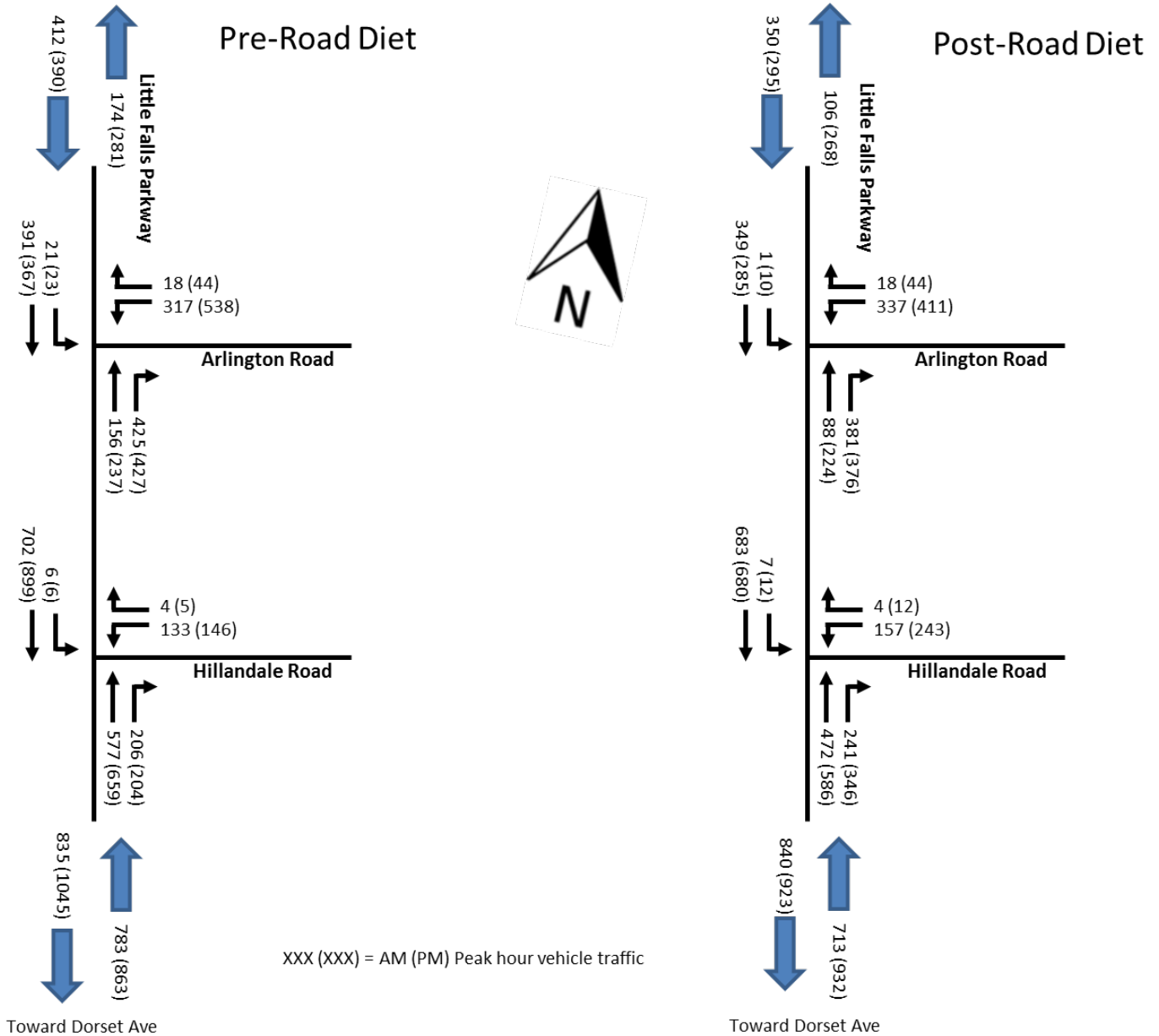


Figure 3: Peak Hour Counts along LFP, Arlington Road, and Hillandale Road before and after the road diet installation

Figure 3 shows that Little Falls Parkway carries the most traffic south of Hillandale (toward and coming from Dorset Ave). A comparison of the before and after volumes (see Table 1) at this location shows that about 60 fewer cars per hour traverse Little Falls Parkway through the collective Hillandale/CCT/Arlington intersections. This represents about a 4% drop in traffic in the AM peak period and a 2.8% drop in traffic in the PM peak period.

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Table 1: Comparison of before and After Volumes along LFP

Direction	Before Road Diet		After Road Diet		Change in Traffic	
	AM	PM	AM	PM	AM	PM
Southbound	835	1045	840	923	5	-122
Northbound	783	863	713	932	-70	69
<i>Total</i>	<i>1618</i>	<i>1908</i>	<i>1553</i>	<i>1855</i>	<i>-65</i>	<i>-53</i>
					<i>-4.0%</i>	<i>-2.8%</i>

Conclusion: The traffic volume counts show that Little Falls Parkway is effectively processing the same amount of traffic before and after the interim road diet. Note that the average 3.4% drop in traffic processed by the collective intersections could entirely be discounted by the fact that the “before” counts were conducted during early December and the “after” counts were taken in mid-May when the CCT is more heavily used.

2.3 Road Diet Traffic Diversion and Impacts to Adjacent Streets

There are several local roadways that could act as alternate routes for traffic that may wish to divert from Little Falls Parkway: Arlington Road, Dorset Ave, Kennedy Drive, and Hillandale Road

Arlington Road

Traffic volume counts show that Arlington Road has experienced a decrease in traffic after implementation of the interim road diet. DOT is currently conducting a road diet traffic study for Arlington Road independently of this project.

Dorset Avenue

A 48-hour traffic count was conducted on Dorset Ave in Somerset in the Spring of 2018 when County schools were in session to see if there was noticeable diversion of traffic away from Little Falls Pkwy. While no “before” count was conducted on Dorset Ave as part of this project, the Town of Somerset collected AM/PM peak period traffic data for vehicles entering the Town on Dorset Ave in 2015. As shown in the following table, Dorset Ave has not received any new cut-thru traffic, as overall traffic was shown to be currently *lower* than in 2015.

Table 2: Peak Period traffic on Dorset Ave entering Somerset

Dorset Ave	2015, Town of Somerset	2018 (by Sabra)
Eastbound AM Peak Period (7 to 9 AM)	444	240
Eastbound PM Peak Period (4 to 6 PM)	337	290

Conclusion: We find no adverse impacts to Dorset Avenue from the road diet on Little Falls Parkway.

Kennedy Drive

An origin-destination study was performed on Kennedy Drive after the installation of the interim road diet condition to quantify cut-through traffic through the Kenwood neighborhood. Using time-stamped video, traffic entering Kenwood at the Kennedy Drive/Bradley Blvd intersection was counted simultaneously with traffic exiting Kenwood at Dorset Avenue/Little Falls Parkway and at Brookside Drive/River Road. Traffic that entered the Kenwood neighborhood at Kennedy Drive and then exited at one of the other two exit points within 10 minutes was deemed to be a cut-through trip. The results of the study indicated that 5 vehicles during the



morning peak hour and 3 vehicles during the evening peak hour satisfied the criteria for cut-through traffic. The following table shows the results from the origin-destination study on Kennedy Drive, including the average speed of cut-through traffic.

Table 3: Kennedy Drive Cut-Through Traffic Study

Peak Hour	Entering Vehicles	Exiting Vehicles	Inbound Kennedy Drive Traffic that is cutting-through Kenwood	Average Speed of Cut-through motorist
Morning	21	50	5	20 mph
Evening	57	51	3	22 mph

The low volume of cut-through traffic on Kennedy is likely due to 3 factors: 1) AM peak hour turn restrictions from Bradley onto southbound Kennedy; 2) Kennedy Drive is only about 20 feet wide, not including on-street parking; 3) the traffic signal at Brookside/River Road is 2 ½ minutes long, meaning that any time saved by cutting through to Brookside is lost waiting to cross River Road. Additionally, northbound Kennedy Ave traffic (which is southbound only for most of its length) can only egress to eastbound Bradley Boulevard. These turn restrictions, along with the partial one-way operations of Kennedy Drive, make it an inconvenient and slower cut-through for drivers that wish to use it to bypass Little Falls Parkway, as the data indicates.

These low cut-through volumes are well below the minimum County thresholds for further study and implementation of additional traffic access restrictions. Per Montgomery County Executive Regulation No. 17-94AM *Through Traffic Volume Access Restrictions in Residential Areas*, more than cut-through 100 vehicles per hour are needed to trigger further restrictions for a residential street like Kennedy Drive.

Conclusion: We find no adverse impacts to Kennedy Drive and the surrounding Kenwood Community residential roadways from the road diet on Little Falls Parkway.

Hillandale Road

Based on the before and after traffic counts, Hillandale Road has received extra traffic due to the interim road diet. Peak hour AM traffic has increased along Hillandale Road by 24 cars (from 137 to 161 cars per hour); while PM peak hour traffic increased by 104 cars (151 to 255 cars per hour).

Parks has received citizen complaints of speeding vehicles on Hillandale Road. DOT conducted a SPOT speed study in January 2019, from 1 to 2 PM, just north of the Willett Parkway and Hillandale intersection which showed an 85th percentile speed of 30 mph (25 mph speed limit).

Hillandale Road is M-NCPPC owned from approximately Willett Parkway south to River Road, and DOT owned from Willett Parkway north to River Road. The DOT owned portion of Hillandale Road includes the Kenwood Forest II condominium community, with on-street parking. DOT is assessing whether traffic calming measures are warranted within the DOT portion of Hillandale Road. Traffic calming measures should be studied and implemented on the Park portion of Hillandale Road as part of this overall project.

Conclusion: Traffic volumes on Hillandale Road have increased after the road diet. We recommend traffic calming techniques be employed on Hillandale Road between Little Falls Parkway and Willett Parkway (the Park owned portion of the road), and traffic calming measures be studied by DOT for the DOT owned portion from Willett Parkway to Bradley Boulevard.



2.4 Road Diet Traffic Observations

Afternoon peak commuting hour observations of Little Falls Parkway, Arlington Road, and Hillandale Road were conducted in early May and in early October when the weather was clear and sunny and trail traffic was heavy. The afternoon peak hour (5:00 to 6:00 PM) reflects the highest hourly traffic volume along Little Falls Parkway. Observations were documented with video and are as follows:

- Northbound traffic on Little Falls Parkway (between Arlington and Hillandale) was observed to have an average speed of 14 mph, while southbound traffic had an average speed of 19 mph. The difference between northbound and southbound speeds is likely due to the uphill grade on Little Falls Parkway for northbound traffic and the downhill grade for southbound traffic.
- Arlington Road had the longest traffic queues of any approach, with drivers waiting to turn left onto southbound Little Falls Parkway varied in number from 15 cars to 0 cars. On several occasions during the peak hour, when Arlington Road received a green light, there were no cars queued up waiting to turn. This is likely due to the fact that the large majority of traffic on Arlington Road originates from the Bethesda CBD using southbound Arlington Road, and traversing through Bradley Blvd; the traffic signal at that location has a cycle length that is twice as long as the one at Arlington/ Little Falls Parkway, which means that the Bradley Boulevard signal effectively meters traffic arriving at the Arlington/ Little Falls Parkway intersection. For every platoon of traffic that comes from downtown Bethesda using Arlington, there are two traffic cycles (i.e. two green lights) available to process that platoon before another comes down.
- Only about 6 cars can stack in the southbound segment of Little Falls Parkway between Arlington and the CCT crossing. These cars occasionally spilled back into the intersection toward Arlington Road, blocking northbound Little Falls Parkway traffic momentarily until the trail cleared.
- Northbound Little Falls Parkway queues approaching the CCT were observed to spill back beyond Hillandale/Little Falls Parkway intersection. However, cycle failure (where a queued-up vehicle cannot traverse the Hillandale intersection within a single green phase) was observed only once in the PM peak hour.
- Vehicles utilizing the secondary Bethesda Pool driveway along Little Falls Parkway to exit the Pool parking lot occasionally contribute to additional queuing and delay for vehicles on Little Falls Parkway. In addition, the right turn results in the vehicles immediately approaching the trail crossing with less reaction time. Closure or conversion of this secondary pool driveway to a maintenance only driveway is recommended.
- Northbound queues, both approaching Hillandale and approaching the CCT, take longer to disperse than southbound queues, because of the uphill grade
- No queuing was observed in the northbound Little Falls Parkway right turn lane approaching Hillandale.
- Very light southbound Little Falls Parkway queuing was observed at the approach to Arlington.
- The short cycle lengths (60 seconds between the beginning of successive green lights) along Little Falls Parkway appear to be the critical factor in allowing traffic to progress through while keeping queues to a minimum.
- No bike or pedestrian delay was observed at the CCT crossing.
- Vehicle compliance to pedestrians and cyclists approaching the trail was observed to be near 100%.

2.5 Road Diet Travel Time Changes

As indicated previously, based on the before and after traffic volumes (Figure 3), the intersections along Little Falls Parkway currently process roughly the same amount of traffic as before the road diet. In addition to comparisons of before and after traffic volumes, we also evaluated the changes in travel time based on the road



diet. To perform this travel time estimation, we developed a traffic model in VISSIM™ software that replicates existing conditions (with road diet) traffic, mirroring current observed speeds and measured travel time through the intersections; queue lengths at all approaches; vehicle volumes processed; and trail user volumes. We then modified the interim road-diet traffic model to recreate the Little Falls Parkway intersection layout *prior* to the road diet by adding back the removed travel lanes to create and simulate the original Pre-road diet condition. This pre-road diet condition model was then simulated multiple times to compare differences in travel time needed by the average vehicle to traverse all 3 intersections (Hillandale, CCT, and Arlington) pre- and post- Road Diet.

An average of 5 simulations between pre- and post- road diet showed that the Interim Road Diet only added about 7 seconds to the average trip through all 3 intersections. The small change in travel time delay is expected, since the intersections now process only slightly less traffic than before the road diet. The primary reason that the road diet still allows so much traffic to progress through Little Falls Parkway is because it only altered one traffic parameter – the amount of queuing space where vehicles can be stored, while waiting for pedestrians/cyclists to clear the CCT crossing. The road diet effectively distributes the prior storage areas in the two travel lanes in each direction into two separated storage areas (see Figure 4) on either side of the intersections. However, since only a few vehicles could be stored along Little Falls Parkway approaching the CCT prior to the road diet, shifting these queued-up vehicles doesn't significantly add to congestion and vehicle throughput through the area. In addition, the short traffic signal cycle lengths at both intersections mean that no approach sees very long red lights; queues don't have an opportunity to build up to unmanageable lengths.

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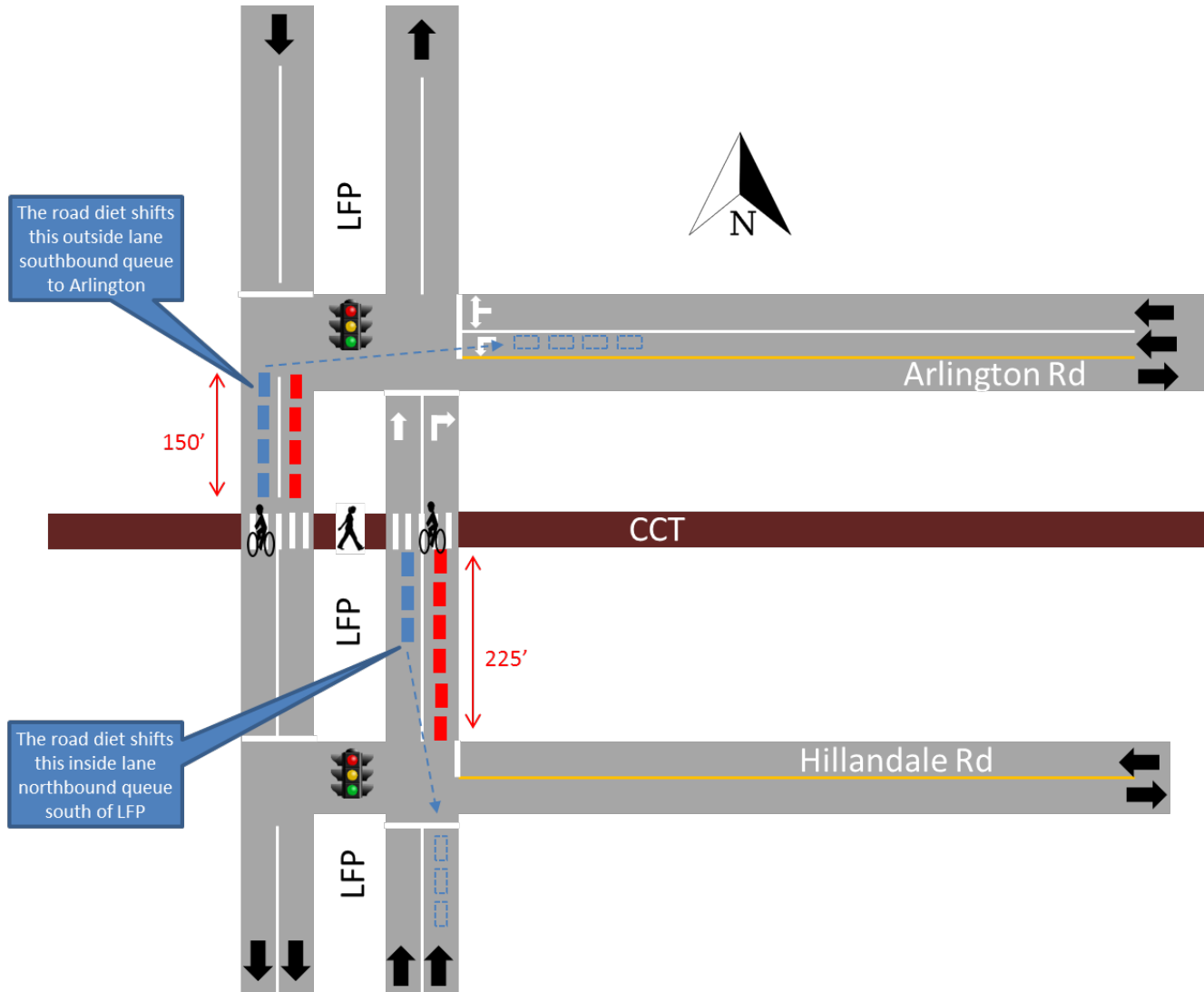


Figure 4: Representation of how the road diet shifts queued vehicles along LFP

As shown in Figure 4, *northbound* Little Falls Parkway did not have an even distribution of queued vehicles at approach to the CCT, pre-road diet. That is because northbound traffic's predominant destination is eastbound Arlington Road; accordingly, northbound traffic lined up largely in only *one* of the two available travel lanes in order to be able to efficiently get into the northbound right turn lane at Arlington Road. This can be seen in the screen capture from Google Street View from late 2016, pre-Road Diet (Figure 5). As a result of the low utilization of the inside northbound travel lane, the road diet had very little impact on overall northbound travel time along Little Falls Parkway.

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Figure 5: Prior to the Road Diet, the northbound traffic overwhelmingly favored one lane, as most drivers were turning right at Arlington Road toward Downtown Bethesda

Conclusion: The road diet shifts the queuing space where vehicles are “stored” as they travel through the intersection. The southbound queue is shifted to Arlington Road and the northbound queue is shifted to south of the Hillandale Road intersection. However, northbound travel times are not drastically affected because most of the northbound traffic before and after the road diet is utilizing the right lane only to turn onto Arlington Road. Overall, the road diet increased travel time on average by 7 seconds.



3. Preliminary Alternatives

Shortly after the interim road diet was implemented, Montgomery Parks contracted with Sabra & Associates to develop a long-term solution for eventual presentation and approval from the Montgomery County Planning Board.

In June of 2018, Montgomery Parks held a public presentation at Somerset Elementary, presenting 12 preliminary alternatives for public comment. These 12 alternatives centered around three distinct themes for addressing the CCT crossing at Little Falls:

- Relocating the trail to an existing controlled intersection along Little Falls Parkway
- Removing the trail/road conflict entirely by creating a trail underpass/overpass
- Formalizing and/or modifying the existing interim condition to maintain the current number of travel lanes and the existing trail crossing control.

Based on public input, cost, coordination with DOT, environmental impacts, and preliminary traffic modeling, several alternatives were removed from consideration and three (3) long-term alternatives were selected for further study and evaluated for their projected safety benefits, pedestrian and vehicular operational impacts, potential increases in cut-through traffic in adjacent neighborhoods, and estimated construction costs. The three remaining alternatives were refined variations from the original 12, with the primary modification for each consisting of the removal of the center median such that overall footprint of the roadway is narrowed considerably. The resulting two-lane roadway would then be of similar size and cross-section as other Parks roads, irrespective of the final chosen alternative. The 3 preliminary alternatives include:

- Alternative A: Permanent Road Diet with a speed table at the CCT Crossing.
- Alternative B: Relocating the CCT crossing to align with Arlington Road Intersection, with Little Falls Parkway remaining a two-lane facility.
- Alternative C: CCT bridge over Little Falls Parkway at the current location of the crossing, with Little Falls Parkway remaining a two-lane facility.

In October of 2018, the 3 preliminary alternatives were presented to the public at a joint meeting with the Montgomery County Department of Transportation. Renderings of each alternative are shown in the following figures:

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Figure 6: Alternative A - Permanent Road Diet with speed table looking north approaching the CCT (rendering by Flaura Teeter)



Figure 7: Alternative B – Realigned CCT to Arlington Road intersection, looking south towards Hillandale Road, two lane roadway (rendering by Flaura Teeter)

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Figure 8: Alternative C – Bridge over Little Falls Parkway, two lane roadway, looking north toward CCT (rendering by Floura Teeter)

4. Transportation and Safety Impacts Measured

Prior to the construction of the interim condition, traffic counts were taken in late Fall of 2016 along Hillandale Road, Arlington Road, and Little Falls Parkway; subsequently in May of 2017, traffic counts were conducted along the same roads to determine how the interim road diet restricted vehicle travel or diverted traffic to alternate streets. Along with multiple field visits, additional data collected included:

- Trail user delay experienced at the crossing of Little Falls Parkway under existing interim road diet;
- Observations of interactions between trail users and motorists at the CCT crossing;
- Before and after crash data for Little Falls Parkway between Arlington Road and Hillandale Road
- Dorset Road peak hour traffic counts, in Somerset;
- Average peak hour speed for vehicles along Little Falls Parkway between Hillandale and Arlington;
- Cut-through data collected along Kennedy Ave in Kenwood;
- Current traffic signal timing for Hillandale/Little Falls Parkway and Arlington/Little Falls Parkway

These data were used to determine and estimate impacts to:

- Trail user safety
- Trail user delay;
- Vehicle delay;
- Diverted traffic onto other local roads;
- Adjacent environmental features including forests, streams, water channels, and wetlands;

These metrics were then compared against each other for each alternative and for the Interim Road Diet Condition:

4.2 Alternatives A, B, and C



Alternative A – Permanent Road Diet

Under the permanent road diet alternative, the existing southbound travel way is removed completely, and the northbound travel way is converted to bi-directional traffic flow resulting in one travel lane in each direction with no median (refer to Figure 6). The single travel lane in each direction eliminates the *multiple-lane threat* potential, while the removal of the wide median provides a further safety benefit by simplifying the driver's decision making on when to yield right-of-way to trail users in the crossing or approaching the crossing. Under current conditions with the median in place, the decision to yield can be ambiguous with some drivers prematurely stopping for trail users crossing the opposing roadway, and other drivers waiting to stop until after trail users have crossed both the opposing roadway and median. A critical component of Alternative A is maintaining clear lines of sight between vehicles and pedestrians/cyclists approaching the CCT crossing, as well as maintaining reduced vehicle speeds.

Alternative A has minor impacts to some adjacent environmental features but the removal of existing impervious surfaces will result in a net environmental benefit.

Traffic volumes have increased on Hillandale Road after implementation of the interim road diet. DOT is assessing speed and traffic conditions along Hillandale Road. Traffic and speed calming measures are recommended on Hillandale Road between Willett Parkway and Little Falls Parkway in conjunction with the Little Falls Park Trail connector installation.

Results from the traffic analysis indicate that the permanent road diet alternative would result in negligible changes in traffic operations over the current interim road diet condition, and no additional diversions onto alternate routes are expected. Recommended total budgetary estimate (design and construction) is \$1.2 M, with minimal continuing operational and maintenance costs anticipated. This Alternative does not preclude the installation of a pedestrian bridge over Little Falls Parkway, or other trail and safety improvements if warranted in the future.

Alternative B – Reorienting the Trail to Arlington Signal

Alternative B proposes to reorient the CCT to the existing signalized intersection at Arlington Road and Little Falls Parkway and to control conflicts between trail users and motorists with the addition of a pedestrian-only signal phase at that intersection. The pedestrian only signal phase would provide red indications for all vehicular movements while trail users are given the walk indication. The northbound right-turn pocket from Little Falls Parkway to Arlington Road would remain, but a "no turn on red" restriction is highly recommended for this movement. Similar to Alternative A, the existing southbound travel way is removed completely, and the northbound travel way is converted to bi-directional traffic flow resulting in one travel lane in each direction with no median. The reduction in travel lanes and removal of the wide median reduces the crossing distance for trail users, further limiting their exposure to conflicts with vehicular traffic.

Alternative B has moderate impacts to adjacent environmental features. The re-configuration of the trail may result in minor tree loss, however, the removal of existing impervious surfaces is an environmental benefit.

Unlike all other alternatives, signalization of the trail crossing introduces delay to trail users (approximately 30 seconds on average), where they currently have none. Our analysis showed that the additional signal phase for trail users is also projected to increase travel times along the corridor for vehicular traffic by approximately 13 seconds *over pre-road diet conditions*. These increases in delay for all users may result in non-compliance in the form of violations of the "no right turn on red" restriction for vehicles and "jay walking" by trail users, potentially degrading the safety benefits of signalization. Based on the impact of the interim road diet on diverted traffic, it is estimated that up to 6% of all traffic that wishes to use this area of Little Falls Parkway will divert to alternate



routes – particularly Hillandale Road. DOT is assessing speed and traffic conditions along Hillandale Road. Traffic and speed calming measures are recommended on Hillandale Road between Willett Parkway and Little Falls Parkway in conjunction with the Little Falls Park Trail connector installation.

Restoring Little Falls Parkway to four lanes with this Alternative is perceived to be advantageous. However, traffic analysis showed that, while it would only perform marginally better operationally than the two-lane alignment, average vehicle travel time would still be about 6 seconds greater than the traffic conditions *prior* to the interim road diet. This is because the extra lane capacity provided with a four-lane section for this short segment of Little Falls Parkway would not make up for the additional traffic phase needed for trail users to cross Little Falls Parkway free from conflicting vehicles.

Recommended total budgetary estimate (design and construction) is \$1.9 M, with minimal continuing operational and maintenance costs anticipated.

Alternative C – Pedestrian Bridge

Alternative C proposes a grade-separated trail crossing, via a pedestrian bridge, over Little Falls Parkway. The grade separation would provide superior safety benefits over all other alternatives under consideration by eliminating conflict points between trail users and vehicular traffic on Little Falls Parkway. As in the other alternatives, the pedestrian bridge alternative would entail the removal of the southbound travel way entirely, with conversion of the northbound travel way to bi-directional flow resulting in one travel lane in each direction with no median. Results from the traffic analysis show an average reduction in vehicular travel times of three (3) seconds with this Alternative over pre-road diet conditions, with no delays for trail users. It is expected that vehicle traffic that has diverted to Hillandale under the interim road diet condition would return to using Little Falls Parkway under this Alternative.

Alternative C has significant impacts to adjacent environmental features. The pedestrian bridge would be 15 feet above Little Falls Parkway, requiring placement of substantial fill in sensitive natural areas and retaining walls for the ramps to the bridge. The ramps and retaining walls would block viewpoints of the adjacent stream valley. The ramps would be steeper than the current trail and may deter some less able-bodied users from using the bridge. The bridge abutments, ramps, and associated sidewalk connections would impact an existing mixed wetland forest stand as well as Willett Branch stream valley and buffer, thus requiring additional environmental mitigation.

Restoring Little Falls Parkway to four lanes requires a substantially longer bridge span, mitigation of significant environmental impacts to Willett Branch and adjacent forest stands, and much higher costs. Alternative C also has the longest design and construction timeframe, and it impacts Capital Crescent Trail users during construction, as temporary closure and detours would be required to construct the bridge.

Recommended total budgetary estimate (design and construction) is \$5 M with additional recurring costs to inspect and maintain the bridge.



5. Recommendation for Preferred Alternative

Installation of the interim road diet along Little Falls Parkway approaching the Capital Crescent Trail was quantitatively shown to significantly improve safety at the trail crossing while also minimizing adverse impacts to vehicular operations along the corridor. The interim road diet has eliminated the multi-lane threat, slowed vehicle speeds through this segment of Little Falls Parkway, and increased visibility between trail users and drivers. Before and after traffic count data revealed minimal reductions in vehicular throughput along Little Falls Parkway due to the road diet, with some traffic diverting from Arlington Road to Hillandale Road. The reason for the minimal change is two-fold: 1) the road diet occurs over a very short segment, resulting in a low amount of vehicle storage capacity shifted from Little Falls Parkway to beyond Arlington Road and past the Hillandale intersection; 2) the traffic signals along the segment of Little Falls Parkway have short 60-second cycle lengths, meaning there is minimal opportunity for lengthy queues develop due to vehicles waiting at a red light. No increases in cut-through traffic were counted along Dorset Avenue or Kennedy Drive (Kenwood community) adjacent to Little Falls Parkway, though increases in overall traffic volumes were observed along Hillandale Road.

Multiple observations showed high levels of vehicle compliance to stopping for pedestrians and cyclists at the CCT crossing. Consequently, trail user delay at the interim crossing was effectively zero.

Based upon the measured success of the interim road diet in increasing trail user safety, lower capital and ongoing maintenance costs, minimal additional adverse impacts in vehicular or pedestrian operations, and overall environmental impacts, the permanent road diet (Alternative A) is recommended as the preferred alternative to advance through to 30% design. Additional traffic calming is also recommended on Hillandale Road to discourage its use as an alternative to Arlington Road. The permanent road diet alternative also has the lowest projected construction costs of the three long-term solutions, maintains a trail orientation which would be least disruptive, allows for further safety features to be added in the future, and does not preclude a future pedestrian overpass bridge.

Recommendations include:

- Alternative A as the preferred alternative; this alternative is a permanent road diet consisting of one travel lane in each direction (two lanes total with no median) and an elevated speed table crossing for the CCT.
- Additional lighting at the CCT crossing of Little Falls Parkway.
- The secondary Bethesda Pool entrance on Little Falls Parkway be closed or modified to a maintenance-only entrance.
- Traffic calming along Hillandale Road (the M-NCPPC owned portion) between Willett Parkway and Little Falls Parkway, to include reduced width travel lanes, modifications to Bethesda Pool entrance, speed table crossing at the Little Falls Park Trail connection at the Pool entrance.
- Little Falls Park Trail connector along the east side of Hillandale Road crossing it at Little Falls Parkway and running along the north side of Little Falls Parkway to connect to the Capital Crescent Trail.
- Support of ongoing DOT traffic calming study along the DOT owned portion of Hillandale Road from Willett Parkway north to Bradley Boulevard.
- Support of ongoing DOT road diet study for Arlington Road.

Appendix E VISSIM Traffic Model Summary Data

#	Intersection	Movement	Alt 0 Existing Conditions (Before)						Alt 0 Existing Conditions (After)						Alt 1 Arlington Crossing						Alt 2 Road Diet & Speed Table						Alt 3 Pedestrian Bridge					
			Delay (veh/sec)		Level of Service		Average Queue Length (ft)		Delay (veh/sec)		Level of Service		Average Queue Length (ft)		Delay (veh/sec)		Level of Service		Average Queue Length (ft)		Delay (veh/sec)		Level of Service		Average Queue Length (ft)		Delay (veh/sec)		Level of Service		Average Queue Length (ft)	
			AM	PM	AM	PM	AM	PM	AM	PM	AM	PM	AM	PM	AM	PM	AM	PM	AM	PM	AM	PM	AM	PM	AM	PM	AM	PM	AM	PM	AM	PM
1	Little Falls Parkway at Arlington Road	Overall	9.6	10.2	A	B	-	-	11.3	11.5	B	B	-	-	24.9	28.2	C	C	-	-	12.3	14.2	B	B	-	-	10.6	11.1	B	B	-	-
		WBL	10.4	11.4	B	B	10	18	13.5	15.3	B	B	20	28	31.3	40.6	C	D	76	128	14.7	18.6	B	B	34	54	12.0	13.1	B	B	28	38
		WBR	5.3	8.1	A	A	9	18	3.2	4.5	A	A	20	28	27.5	35.8	C	D	79	133	11.1	15.9	B	B	35	56	9.1	11.4	A	B	29	39
		NBT	14.1	14.1	B	B	11	19	14.7	14.2	B	B	7	17	26.9	28.6	C	C	22	45	13.1	13.8	B	B	5	15	14.0	14.8	B	B	5	16
		NBR	2.1	2.1	A	A	11	19	2.1	2.2	A	A	6	16	9.8	10.3	A	B	22	44	2.3	2.5	A	A	4	14	2.4	2.2	A	A	4	16
		SBL	16.8	16.8	B	B	21	20	23.3	15.9	C	B	32	24	63.3	98.7	E	F	71	50	20.2	25.1	C	C	40	35	18.4	21.5	B	C	32	25
		SBT	15.4	15.4	B	B	21	20	18.8	17.7	B	B	32	24	34.5	30.4	C	C	71	50	21.1	23.0	C	C	40	35	17.4	16.7	B	B	32	25
		Overall	6.5	6.7	A	A	-	-	7.5	11.1	A	B	-	-	8.0	11.2	A	B	-	-	7.8	11.4	A	B	-	-	7.9	11.0	A	B	-	-
2	Little Falls Parkway at Hillandale Road	WBL	33.4	34.6	C	C	23	27	35.0	37.1	D	D	29	52	35.3	37.4	D	D	30	52	35.3	37.3	D	D	30	52	35.4	37.3	D	D	30	52
		WBR	18.3	23.2	B	C	23	27	16.0	30.5	B	C	29	52	15.9	28.8	B	C	30	52	16.6	28.9	B	C	30	52	15.6	28.8	B	C	30	52
		NBT	4.7	4.8	A	A	11	12	6.0	9.3	A	A	9	23	4.8	7.4	A	A	9	21	5.0	8.7	A	A	9	24	4.6	7.1	A	A	9	20
		NBR	4.3	4.2	A	A	11	12	2.9	7.1	A	A	9	23	2.8	4.1	A	A	9	21	2.8	4.1	A	A	9	24	2.8	4.1	A	A	9	20
		SBL	14.3	9.5	B	A	7	10	9.3	15.8	A	B	8	11	10.0	17.1	A	B	25	40	11.5	14.5	B	B	20	34	11.5	14.5	B	B	22	36
		SBT	3.4	3.9	A	A	7	10	3.7	4.9	A	A	8	11	5.8	8.4	A	A	25	40	5.0	7.7	A	A	20	34	5.5	8.1	A	A	22	36
		Overall	3.8	3.8	A	A	-	-	6.3	8.1	A	A	-	-	Not Applicable						6.8	8.1	A	A	-	-	Not Applicable					
		NBT	5.1	4.4	A	A	6	8	6.8	10.2	A	B	16	44							6.9	8.0	A	A	14	27						
		SBT	2.7	3.3	A	A	5	8	5.9	6.2	A	A	24	24							6.8	8.3	A	A	24	31						
3	Little Falls Parkway at CCT	Trail	0.6	1.0	-	-	-	-	0.2	0.3	-	-	-	-	29.3	29.3	-	-	-	-	0.5	1.1	-	-	-	-	0.0	0.0	-	-	-	-

Note: Average Queue in Vissim is not calculated the same way as the average static queue in Synchro. While the latter only calculates queue length when a queue is present, Vissim calculates queue length continuously, even when an approach has no vehicles in it. Accordingly, the average queue reported above does not purport to show the typical queue length when queuing is observed

Appendix E VISSIM Traffic Model Summary Data

Travel Time (sec)												
Direction	#	Origin and Destination of travel through LFP Segment	Alt 0 Existing Conditions (Before)		Alt 0 Existing Conditions (After)		Alt 1 Arlington Crossing		Alt 2 Road Diet & Speed Table		Alt 3 Pedestrian Bridge	
			AM	PM	AM	PM	AM	PM	AM	PM	AM	PM
Southbound	1	Little Falls Pkwy to Little Fall Pkwy	62	63	68	68	78	76	73	80	64	65
	2	Arlington Rd to Little Falls Pkwy	55	58	61	64	76	90	65	72	56	59
Northbound	3	Little Falls Pkwy to Little Falls Pkwy	57	58	62	68	62	69	56	62	51	52
	4	Little Falls Pkwy to Arlington Rd	49	50	52	60	48	53	49	54	43	43

Speed (mph)												
Direction	#	Origin and Destination of travel through LFP Segment	Alt 0 Existing Conditions (After)		Alt 0 Existing Conditions (After)		Alt 1 Arlington Crossing		Alt 2 Road Diet & Speed Table		Alt 3 Pedestrian Bridge	
			AM	PM	AM	PM	AM	PM	AM	PM	AM	PM
Southbound	1	Little Falls Pkwy to Little Fall Pkwy	15	15	14	14	12	12	13	12	14	14
	2	Arlington Rd to Little Falls Pkwy	17	16	15	14	12	10	14	13	16	16
Northbound	3	Little Falls Pkwy to Little Falls Pkwy	16	16	15	13	15	13	16	15	18	18
	4	Little Falls Pkwy to Arlington Rd	19	18	18	15	19	17	19	17	22	21

Note: Change in travel time for each alternative was a weighted average, with higher volume movements given proportionally more weighting.

Appendix E Crash Records

Report Number	Local Case Num	Agency Na	ACRS Report Type	Crash Date/Time	Route Type	Road Name	Cross-Strec	Cross-Street Name	Off-Road D	Municipali	Related Ne	At Fault	Collision Type	Weather	Surface Co	Light	Traffic Control
HA24050002	15000026	Maryland-T	Injury Crash	1/10/2015 22:23	Government	LITTLE FALLS PKWY	County	ARLINGTON RD	N/A		UNKNOWN		SINGLE VEHICLE	CLEAR	DRY	DARK LIGHTS ON	TRAFFIC SIGNAL
HA23380002	15000059	Maryland-T	Property Damage Crash	1/24/2015 21:03	Government	LITTLE FALLS PKWY	County	ARLINGTON RD	N/A		DRIVER		SINGLE VEHICLE	CLEAR	WET	DARK NO LIGHTS	TRAFFIC SIGNAL
MCP2094005J	15050541	MONTGOM	Property Damage Crash	10/6/2015 6:25	Government	LITTLE FALLS PKWY	County	ARLINGTON RD	N/A		DRIVER		STRAIGHT MOVEMEN	CLEAR	DRY	DARK NO LIGHTS	TRAFFIC SIGNAL
HA22800007	15001176	MCPARK	Injury Crash	10/20/2015 8:46	Government	LITTLE FALLS PKWY	County	ARLINGTON RD	N/A		BICYCLIST	NONMOTORIST	STRAIGHT MOVEMEN	CLEAR	DRY	DAYLIGHT	STOP SIGN
MCP3011000K	16009277	Montgome	Property Damage Crash	2/24/2016 16:59	Government	LITTLE FALLS PKWY	County	ARLINGTON RD	N/A		DRIVER		SAME DIR REAR END	RAINING	WET	DAYLIGHT	TRAFFIC SIGNAL
HA23680008	16000440	Maryland-T	Injury Crash	4/16/2016 11:00	Government	LITTLE FALLS PKWY	Government	HILLANDALE RD	N/A		BICYCLIST	DRIVER	STRAIGHT MOVEMEN	CLEAR	DRY	DAYLIGHT	OTHER
HA2399000M	16000538	Maryland-T	Injury Crash	5/8/2016 13:18	Government	LITTLE FALLS PKWY	County	ARLINGTON RD	N/A		BICYCLIST	BOTH	OTHER	CLEAR	DRY	DAYLIGHT	N/A
HA22880003	16001149	Maryland-T	Injury Crash	9/8/2016 7:20		LITTLE FALLS PKWY		ARLINGTON ROAD	N/A		BICYCLIST	DRIVER	STRAIGHT MOVEMEN	N/A	DRY	DAYLIGHT	YIELD SIGN
HA22880004	16001255	Maryland-T	Property Damage Crash	10/5/2016 15:10		LITTLE FALLS PKWY		ARLINGTON ROAD	N/A		DRIVER		SINGLE VEHICLE	N/A	DRY	DAYLIGHT	YIELD SIGN
MCP2559001G	16053346	Montgome	Fatal Crash	10/17/2016 11:28	Government	LITTLE FALLS PKWY	County	ARLINGTON RD	N/A		BICYCLIST	UNKNOWN	STRAIGHT MOVEMEN	CLEAR	DRY	DAYLIGHT	OTHER
MCP3010001H	16055750	Montgome	Injury Crash	10/29/2016 13:54	Government	LITTLE FALLS PKWY	County	ARLINGTON RD	N/A		DRIVER		SINGLE VEHICLE	CLEAR	DRY	DAYLIGHT	TRAFFIC SIGNAL
HA2399000Q	16001370	Maryland-T	Injury Crash	11/7/2016 8:02	Government	LITTLE FALLS PKWY	County	ARLINGTON RD	N/A		BICYCLIST	DRIVER	STRAIGHT MOVEMEN	CLEAR	DRY	DAYLIGHT	N/A
HA22880008	17000323	Maryland-T	Property Damage Crash	3/25/2017 14:30		LITTLE FALLS PKWY		HILLANDALE ROAD	N/A		DRIVER		SAME DIR REAR END	CLEAR	DRY	DAYLIGHT	N/A
HA23990016	18000178	Maryland-T	Injury Crash	2/19/2018 13:34	Government	LITTLE FALLS PKWY	County	ARLINGTON RD	N/A		BICYCLIST	UNKNOWN	STRAIGHT MOVEMEN	RAINING	WET	DAYLIGHT	WARNING SIGN
HA22870008	18000563	Maryland-T	Injury Crash	5/17/2018 10:59	Government	LITTLE FALLS PKWY	Government	HILLANDALE RD	N/A		DRIVER		SINGLE VEHICLE	RAINING	WET	DAYLIGHT	N/A
HA2372000X	18001064	Maryland-T	Injury Crash	8/13/2018 11:19	Government	LITTLE FALLS PKWY	County	ARLINGTON RD	N/A		PEDESTRIA	NONMOTORIST	OTHER	CLEAR	DRY	DAYLIGHT	STOP SIGN
MCP30020030	180061734	Montgome	Injury Crash	12/10/2018 17:20	Government	LITTLE FALLS PKWY	County	ARLINGTON RD	N/A		PEDESTRIA	NONMOTORIST	OTHER	CLEAR	DRY	DARK LIGHTS ON	NO CONTROLS

Previous Condition
6 driver crashes
6 bike crashes
Over 2 Years

POST INTERIM CONDITION
2 driver crashes
3 bike/ped crashes
Over 2 Years

Appendix E Little Falls Parkway SPOT Speed Study

Spot Speed S Weather: Warm, sunny

8-May-18

Location: Segment of LFP, through the CCT crossing

ID	Speed NB (mph)	Speed SB (mph)
1	17	18
2	17	23
3	17	18
4	23	23
5	17	23
6	14	23
7	17	31
8	17	23
9	17	23
10	17	18

17.2

22.3

Appendix E Kennedy Dr Cut Through Study

Intersection: Kenneday at Bradley		Time: 8AM to 9AM				Average Speed through Kenwood
IN at KENNEDY	OUT of KENNEDY	TIME	Vehicle descriptors	Time Exiting Kenwood	Exiting Location	
	black SUV	8:01:28 AM				
	silver SUV	8:02:17 AM				
	black SUV	8:03:08 AM	jeep? Running boards			
	black sedan	8:04:03 AM	4 dr			
	black SUV	8:04:43 AM	Edge/ or Lexus			
white SUV		8:05:44 AM	Jeep, roof rack			
	silver SUV	8:06:18 AM	sun roof. no racks.			
silver SUV		8:07:13 AM	mercedes. Large sun roof, running boards	8:09:45	SB through brookside/River	20.6 mph
	silver sedan	8:08:22 AM	giant black sun/moon roof			
	black SUV	8:09:23 AM	no moonroof. Silver racks			
	blue/silver sedan	8:09:29 AM	moonroof. 4 drs			
burgundy sedan	silver minivan	8:09:48 AM	black racks. Boxy vehicle			
		8:09:57 AM	moonroof. 4 drs			
	silver/white sedan	8:11:23 AM	moonroof. 4 drs			
	silver SUV	8:11:28 AM	large boxy, two moonroofs, racks			
	silver/white sedan	8:11:32 AM	moonroof. 4 drs, huge rear window			
black SUV		8:12:10 AM	boxy			
	Silver SUV	8:12:51 AM	entire roof is a moonroof			
	burgundy SUV	8:13:20 AM	no racks, matte color			
white VAN		8:13:55 AM	no markings			
	black SUV	8:14:37 AM	no rack or moonroof			
	silver SUV	8:14:42 AM	racks and cross racks. No moonroof			
	silver/white SUV	8:14:48 AM	Small rectangular moonroof			
	school bus	8:15:42 AM				
gray SUV		8:16:08 AM	boxy. Small rectangular moonroof			
	dark grey SUV	8:17:45 AM	silver racks			
	white SUV	8:18:16 AM	moonroof black racks			
black SUV		8:18:16 AM	silver racks			
	grey SUV	8:19:00 AM	tall. Boxy. Giant moonroof			
	white range rover	8:19:36 AM	5th wheel on back			
	white SUV	8:19:46 AM	no moonroof or racks			
	silver suv	8:20:05 AM	rack and crossracks			
dark blue / black crossover		8:20:05 AM	curvy roof line			
	white pickup	8:23:33 AM	bed. No cab			
dark blue / black crossover		8:23:33 AM	silver racks			
silver/tan sedan		8:23:49 AM	large back window			
	silver/white SUV	8:24:37 AM	boxy. Small rectangular moonroof			
	silver sedan	8:25:20 AM	entire roof is moonroof			
black sporty sedan		8:26:07 AM	2 dr ragtop			
dark blue / black SUV		8:26:28 AM	silver racks	8:29:06 AM	EB through Dorset/LFP	15.9 mph
	white SUV	8:26:50 AM	2 moonroofs			
	silver-blue minivan	8:27:01 AM	small moonroof			
	black SUV	8:28:40 AM	silver racks. No moonroof			
lightblue SUV		8:29:37 AM	moonroof	8:33:08 AM	SB through brookside/River	14.8 mph
	black sedan	8:32:48 AM	moonroof 4 drs			
black/grey matte Jeep		8:33:54 AM				
silver pickup		8:34:04 AM	work boxes in bed			
red pickup		8:38:10 AM	with cab and bed cabin			
	grey SUV	8:38:22 AM	rack and crossrack. Moonroof			
	silver SUV	8:39:00 AM	boxy. Small rectangular moonroof. Rack and crossrack			
	black suv	8:40:14 AM	large. Silver racks			
	black suv	8:40:20 AM	tall. Boxy. No racks			
work dump truck and trailer		8:40:52 AM				
	white SUV	8:43:33 AM	black rack and cross racks			
black sporty sedan		8:43:54 AM	4 doors			
	large white SUV	8:44:19 AM	rack, crossrack, moonroof			
	black SUV	8:45:15 AM	large. Silver racks. No moonroof			
red minivan		8:46:05 AM	red? Racks	8:48:15 AM	SB Left at Brookside/River	24.1
	grey black sedan	8:46:50 AM	4 drs moonroof			
	white SUV	8:51:59 AM	rack, crossrack, moonroof			
	black SUV	8:52:47 AM	no racks. Moonroof			
grey sedan		8:52:53 AM	4 doors	8:55:11 AM	SB Through at Brookside/River	22.7
	white SUV	8:53:10 AM	moonroof			
	silver sedan	8:53:28 AM	moonroof			
grey pickup extended cab		8:54:05 AM	black bed			
	black sedan	8:54:22 AM	moonroof . 4 drs			
	black sedan	8:57:39 AM	moonroof			
	black SUV	8:57:49 AM	boxy . Tall. Racks.			
	grey minivan	8:58:01 AM	small moon roof			
	silver SUV	8:58:03 AM	boxy . Tall. Racks.crossracks			
	white work truck	8:58:41 AM	ladders on it			
Total inbound 21	Total outbound 50	Number of Cut-through Vehicles from 8AM to 9AM 5			Average cut-through Speed 19.6	

Appendix E Kennedy Dr Cut Through Study

Intersection: Kenneday at Bradley		Time: 5PM to 6PM				
IN at KENNEDY	OUT of KENNEDY	TIME	Vehicle descriptors	Time Exiting Kenwood	Exiting Location	Average Speed through Kenwood
silver/tan ford edge		17:00:49	moonroof			
blue/silver SUV		17:01:39	moonroof. Racks			
	red corvette	17:01:52				
	dark blue sedan	17:01:52	4 drs			
silver tan crossover		17:02:20	sunroof, rack, crossrack			
	white ford edge	17:02:26	moonroof			
dark blue mini cooper		17:03:16				
	whitepickup	17:04:08	extended cab			
black convertible		17:04:42	top down			
	white SUV	17:06:05	large sunroof. Racks			
black sedan		17:06:06	4 drs			
dark grey SUV		17:07:02	silver racks			
blue/silver sedan		17:07:10	2dr			
blue/silver sedan		17:07:23	moonroof 4 dr			
	mahogany sedan	17:07:30	4 dr			
blue/black SUV		17:07:41	silver racks			
	silver SUV	17:08:31	rack. Cross rack.boxy			
	white fed ex van	17:09:49				
	grey SUV	17:10:42	no moonroof			
silver chevy tahoe		17:11:20				
	white sedan	17:11:36	2dr			
white SUV		17:13:02	moonroof, rear spoiler			
	silver/blue minivan	17:13:46	black racks			
dark grey SUV		17:14:46	boxy. Racks, cross racks			
dark grey chevy tahoe		17:15:02	silver racks			
white SUV		17:15:16	moon roof, racks, cross racks			
	grey sedan	17:15:45	4 drs			
dark blue truck		17:16:21	4 drs. Cab			
white pickup		17:16:27	small cab			
	burgundy SUB	17:16:27	silver racks			
silver/blue crossover		17:16:46				
	white boxy SUV	17:17:27				
	silver boxy SUV	17:17:31	large moon roof and siver racks			
	grey SUV	17:17:46	Ford Edge			
black SUV		17:18:08	racks and cross racks			
black SUV		17:18:47	silver racks	17:20:39 EB through Dorset/LFP		22.5 mph
silver sedan		17:19:11	4 dr. large sunroof			
	white sedan	17:20:20	4 dr no sunroof			
	small white SUV	17:20:25	boxy. Large moonroof			
silver sedan		17:20:53	2 door. Early model			
	grey boxy SUV	17:21:11	black racks			
	red ford edge	17:21:57	black racks			
	grey 4dr sedan	17:22:06	moonroof			
black matte jeep		17:22:17	hard top			
	white work pickup	17:22:27				
	silver SUV	17:22:34	black racks. Moonroof. Wraparound rear window			
black SUV		17:22:47				
	white SUV	17:23:10	lawn trailer			
grey SUV		17:23:23	rack and cross rack			
black/blue sedan		17:24:45	4 drs			
	white sedan	17:25:07	4 drs			
	black SUV	17:25:27	boxy black racks			
	lawn care truck	17:25:39	with trailer			
black sedan		17:25:42				
dark blue subaru outback		17:26:36				
black/blue sedan		17:27:10	4 dr. mercedes			
grey SUV		17:27:22	cross racks. Crome lower body			
	silver sedan	17:28:32	black roof			
white SUV		17:28:59	no moonroof. Wraparound rear window			
	white SUV	17:30:05	large moonroof. Wraparound rear window			
light blue sedan		17:30:22				
silver/tan SUV		17:30:48	rack cross rack			
white SUV		17:31:01	boxy. No moonroof or racks			
	silver sedan	17:31:51	4 drs. No moonroof			
silver SUV		17:32:32	silver racks. Spoiler			
	green/gray SUV	17:32:42	black racks			
	white SUV	17:33:10	rack cross rack			
	white minivan	17:34:37	no moonroof or racks			
burgundy crossover		17:34:52	chrome trim			
grey SUV		17:35:34	silver racks			
	grey sedan	17:35:40	4 dr. moonroof			
	fruit delivery truck	17:37:16				
	grey sedan	17:37:48	4 dr. no moonroof			
	white prius	17:37:50	moonroof			
blue SUV		17:41:15	silver racks			
dark blue SUV		17:42:45	silver racks	17:45:05 EB through Dorset/LFP		18.0 mph
dark blue SUV		17:42:50	silver racks. Running boards			
light blue/white SUV		17:42:55	rack and cross rack			
	white SUV	17:42:55	large moon roof			
grey SUV		17:44:17	boxy. No moonroof or racks			
large black SUV		17:44:45	silver racks			
grey sedan		17:44:50	4 drs moonroof			
black sporty sedan		17:44:56	4 dr			
	large black SUV	17:45:52	silver rack and cross rack			
	large grey SUV	17:45:59	silver rack			
	dark purple small SUV	17:46:19	no racks			
	red SUV	17:46:40	rack and cross rack. Spare wheel			
	silver/tan ford edge	17:46:53				
	large dark blue SUV	17:46:58	boxy. Silver racks			
large grey SUV		17:47:41	black racks and cross rack			
silver SUV		17:48:22	silver racks and moonroof			
	black SUV	17:49:10	silver racks and moonroof			
white sedan		17:49:17	2 drs. No moonroof			
	black prius	17:49:50				
	silver sporty sedan	17:49:53	large moonroof			
silver sporty sedan		17:50:03	no moonroof. 2 drds			
	white boxy large SUV	17:51:07	racks. Cross rack			
white sedan		17:51:33	moonroof			
silver SUV		17:52:08	cross racks. Moonroof			
	white sedan	17:52:14	moonroof. 4 drs			
black/blue sedan		17:52:50	4 drs			
black sedan		17:56:18	4 drs			
dark blue station wagon		17:56:23				
white SUV		17:56:43	cross racks			
	dark blue SUV	17:58:13	silver racks. Boxy			
white SUV		17:58:52	running board	18:01:00 SB through brookside/River		24.3 mph
silver blue SUV		17:59:30				
	silver SUV	17:59:31	black racks. Moonroof			
Total inbound		Total outbound		Number of Cut-through Vehicles from 5PM to 6PM		Average cut-through Speed
57		51		3		21.6

Appendix E Kennedy Dr Cut Through Study

Cut Through Traffic from Kennedy/Bradley Intersection to either Dorset/LFP or Brookside/River

Peak Hour	Entering Traffic	Exiting Traffic	Inbound Traffic that is cut-through (leaves neighborhood in <10min)	Average Speed of Cut-through motorist
AM Peak Hour	21	50	5	20mph
PM Peak Hour	57	51	3	22mph

Note: Inbound traffic into the Kenwood Neighborhood is restricted in the AM peak period

Cameras placed at Kennedy/Bradley; LFP/Dorset; and River/Brookside intersections

APPENDIX E DORSET AVE TRAFFIC COUNTS

AM

Dorset and Little Falls

FROM DORSET ON KENWOOD SIDE

**ON THE KENWOOD SIDE, FROM DORSET (LEAVING KENWOOD, CROSSING
LITTLE FALLS AND ENTERING SOMERSET ON DORSET)**

ONLY THE THRU MARKINGS ARE RELEVANT TO SOMERSET.

8 PAGES

Dorset & Little Falls 4/1/15 Cars exiting Kenwood on
 (Kenwood St) APPENDIX E DORSET AVE TRAFFIC COUNTS Dorset.

LOCATION:

COUNTED BY:

Birdie P Pieczenik

APPROACH:

TIME	LICENSE PLATE	COLOR	DIRECTION	TIME	LICENSE PLATE	COLOR	DIRECTION
7:00	829 RA	Red	(L) T R		040	Bl	(L) T R
	194 RA	Bl	(L) T R		023	Bl	(L) T R
	809	Gray	(L) T R		227	W	(L) T R
	627	B	(L) T R		^{not md} 019?	W	(L) T R
7:05	088 DE	Gray	(L) T R		9-- Gray	Gray	L (T) R
	1VS?	Bl	(L) (T) R	7:20	996	W	L T (R)
	2HDVA	Truck	(L) T R		058	Gr	(L) T R
	330	Red	L T (R)		519 DC		L T (R)
	254	Bl	(L) T R		076	Bl	L T (R)
	360	Gr	L T (R)		969	Bl	L T (R)
	677	Tr	(L) T R		V14	Gray	L (T) R
	845	Bl	L T (R)	7:25	W7	2. Bl	(L) T R
	N16	Gr	L T (R)		361	Bl	L T (R)
7:10?	618	Wh	L (T) R		627	Blue	L (T) R
	D30	Gr	L T (R)		309	Bl	L T (R)
	922	Gr	L (T) R		151	Bl	L T (R)
	024	Bl	L (T) R		F88	Bl	L T (R)
	248	Bl	L (T) R		081 VA	Gr.	L (T) R
	204	Bl	L (T) R		001	Gr.	(L) T R

← normally right
 but
 water
 main
 break

* Counter is standing on
 Kenwood Side. Only the (T)
 Should be counted.

38
 276

LOCATION:
COUNTED BY:

Dorset + Little Falls
Kenwood Side

APPENDIX E DORSET AVE TRAFFIC COUNTS

APPROACH:

Cars exiting
Kenwood on
DORSET

9/10/15
(2)

TIME	LICENSE PLATE	COLOR	DIRECTION	TIME	LICENSE PLATE	COLOR	DIRECTION
	093	Wh	L T (R)		546	Gr	L T (R)
7:30	1 M 4	Gr.	L T (R)	7:40	115	BK	(L) T R
	661	Gr	(L) T R		617	Tr Bl	(L) T R
	899	W	L T (R)			BK	L T (R)
	L 92	?	L (T) R		605	Gr.	L (T) R
	671	BK	L T (R)		6M6	Tr whole	L T (R)
	610	BK	L T (R)		098	Gr	(L) T R
7:35	367	W	(L) T R		398	Blue	L T (R)
	532	BK	(L) T R		299	Speeding cutly thro	L (T) R
	M 48	BK	L T (R)	7:45	558	Gr.	(L) T R
	497	W	L (T) R		YTF	Gr.	(L) T R
	2	Black	L (T) R		050	BK	L T (R)
	774	BK	L T (R)		915	BK	(L) T R
	592	Gr	L T (R)		042	Blue	L T (R)
	218	Gr	L T (R)		937 VA	Red	(L) T R
	360	BK	L (T) R		472	Black	(L) T R
	030	Wh	L (T) R	7:50	745	BK	L T (R)
	~	BK	L T (R)		178	W	(L) T R
	144	BK	L T (R)		508	Gr.	(L) T R

Lots of bikes! Kids + adults
ONLY count the Ts.

9/16/15

APPENDIX E DORSET AVE TRAFFIC COUNTS

Cars exiting Kenwood
Dorset (p. 3)LOCATION: Dorset + Little Falls
COUNTED BY: Kenwood Side

Birdie

APPROACH: Dorset + Little Falls
Kenwood Side

TIME	LICENSE PLATE	COLOR	DIRECTION	TIME	LICENSE PLATE	COLOR	DIRECTION
7:50 (cont)	589	Gr.	L T (R)		218	Wh	(L) T R
	677	Blk	L T (R)	8:05	380	Gr	L T (R)
	495	Blk	L T (R)		633	Blk Truck	L (T) R
	794	Blk	L (T) R		672	Blk	L (T) R
	583	Tr-Red	L (T) R		646	Gray	(L) T R
7:55	385	Gr.	(L) T R		05T	Blk	(L) T R
	509	Gr	(L) T R		972	Blk	(L) T R
	282	Blk	(L) T (R)		483	Gray	L (T) R
	281	Gr	(L) T R		885	Blk	L (T) R
	?	Truck	(L) T R		?	Gray	L T (R)
	016	Gray	(L) T R	8:10	207	Gray	(L) T R
	420	Blk	L (T) R		741	Blk	L (T) R
8:00	311	Gray	L (T) R		800	Blk	L (T) R
	122	Brown	L T (R)		669	Blk	L (T) R
	371	Br	L (T) R		655	Blk	(L) T R
	R75	Blk	(L) T R		400	Blk	(L) T R
	631	Blk	(L) T R		631	Gray	(L) T R
	841	Green	(L) T R		441	Blk (?)	L (T) R
	659	Gray	(L) T R		213	Gr.	(L) T R

Hands cold but heart (+ body) warm!

ONLY COUNT (T)

APPENDIX E DORSET AVE TRAFFIC COUNTS

9/14/15 Cars exiting Kenwood on Dorset (P.4)

LOCATION: Dorset + Little Falls
COUNTED BY: Kenwood side (Burdie)

APPROACH: Dorset + Little Falls (Burdie)

TIME	LICENSE PLATE	COLOR	DIRECTION	TIME	LICENSE PLATE	COLOR	DIRECTION
8:15	035	Gr.	L (T) R		475	Gray	L (T) R
	686	White	L (T) R		80X	Bluish	L (T) R
	112	Gray	L (T) R		291	Brown	L (T) R
	597	Blk	L (T) R		28K	Red ^{lost people}	L (T) R
	808	Wh	L (T) R		091	Red	L (T) R
	564	Blk	L (T) R		867	Gray	L (T) R
	659	Blk	L (T) R		743	Gray	L (T) R
	844	Wh	L (T) R		194	Gray	L (T) R
	1NA	W	L (T) R		229	Brown	L (T) R
	76T	Gray	L (T) R	8:25	164	Gray	L (T) R
	2	Bl	L (T) R		228	Wh	L (T) R
	9M9	Gr	L (T) R		002	Gr.	L (T) R
	391	Gr	L (T) R		RKR	Gray	L (T) R
	030	Brown	L (T) R		225	Wh	L (T) R
	191	Blue	L (T) R	8:30	454	Gr.	L (T) R
	640	Blk	L (T) R		2	Wh	L (T) R
8:20	459	Gr	L (T) R		659	Gr	L (T) R
	P90	Gr	L (T) R		853	Wh ^{truck}	L (T) R
		Blk	L (T) R		5M7	Blue	L (T) R

APPENDIX E DORSET AVE TRAFFIC COUNTS

Cars exiting Kenwood
Dorset + Little Falls
Kenwood Side
p.5

Dorset + Little Falls
Kenwood Side

LOCATION:

COUNTED BY

AM

APPROACH:

Birdie

TIME	LICENSE PLATE	COLOR	DIRECTION	TIME	LICENSE PLATE	COLOR	DIRECTION
8:30	383	Blk	L (T) R		X35	Gray	L (T) R
	109	Gray	L (T) R		338	Blk	L (T) R
	677	Blk	L (T) R		2	Gray	L (T) R
	6818	Blk	L (T) R	8:40	750	Blk	L (T) R
	5717	Blk	L (T) R		811	Gray	L (T) R
	844	Blk	L (T) R		760	Blk	L (T) R
	378	Blk	L (T) R		287	Gray	L (T) R
	443	wh	L (T) R		1M8	Gray	L (T) R
8:35	765	Blk	L (T) R		342	Wh (?)	L (T) R
	869	Wh	L (T) R		195	Wh	L (T) R
	191	Blk	L (T) R		297	Red	L (T) R
	1570	Blk	L (T) R		647	White	L (T) R
	584	Blk	L (T) R		073	Blue	L (T) R
	079	Blk	L (T) R		077	Blk	L (T) R
	136	Blk	L (T) R		9M9	Blk	L (T) R
	257	Blk	L (T) R	8:45	088	Blk	L (T) R
	NIA	Blk	L (T) R		BZR	Blk	L (T) R
	S31	Blk	L (T) R		553	Gray	L (T) R
	Q24	Blk (?)	L (T) R		520	Brown	L (T) R

When I said it was a Somerset study, she asked if it was a good idea if she went to the Somerset. Only 2 questions from drivers are: why let it be asked why it was OK with it. Feels creepy to write names.

ONLY COUNT (T)

APPENDIX E DORSET AVE TRAFFIC COUNTS

9/16/15

CARS exiting Kenwood on Dorset
Dorset + Little Falls
Kenwood Side

P6

AM
LOCATION: Dorset + Little Falls
COUNTED BY: Kenwood Side (Birdie)

Birdie APPROACH:

TIME	LICENSE PLATE	COLOR	DIRECTION	TIME	LICENSE PLATE	COLOR	DIRECTION
8:45 (cont)	106	Gray	(L) T R	9:00	426	Wh	L T (R)
	842	Gray	L (T) R		528	Blk	(L) T R
8:50	622	Truck	(L) T R		912	Gray	L T (R)
	755	Blk	(L) T R		C71	Blue	L (T) R
	374	Red	(L) T R		351	Blue	L T (R)
	206	Wh	L (T) R		367	Wh	(L) T R
	876	Gray	L (T) R		940	Wh	(L) T R
8:55	58M	Gray	(L) T R		404	Trk	L T R
	3M8	Blk	(L) T R		552	?	L (T) R
	02P	Red	L (T) R		582	Red	(L) T R
	518	Blue	L T (R)		5M5	Gray	(L) T R
	791	Gray	(L) T R		258	Blk	L (T) R
	588	Blk	(L) T R		596	Wh	(L) T R
	?	Red	L (T) R		386	Blk	L (T) R
	2M3	Blk	(L) T R	9:05	297	Blk	L (T) R
	670	Blk	L T (R)		653	Gray	L (T) R
	978	Wh	(L) T R		332	Blk	(L) T R
	088	Gray	(L) T R		911	Tr	(L) T R
9:10			L T R		829	Gray	(L) T R

Another nervous person

was thanked by several Kenwood residents (who might have contributed their time!)

ONLY COUNT T

9/16/15

APPENDIX E DORSET AVE TRAFFIC COUNTS

CARS Exiting Kenwood
 Dorset + Little Falls
 Kenwood Side Dorset (p.7)

LOCATION:

COUNTED BY:

APPROACH:

TIME	LICENSE PLATE	COLOR	DIRECTION	TIME	LICENSE PLATE	COLOR	DIRECTION
9:05 (cont)	929	wh	L (T) R	9:15 (cont)	212 654	Blk	(L) T R
	818	Red	L T R		037	Red	L (T) R
	642	Blk	L (T) R		068	Blue	(L) T R
	?	Gray	(L) T R		506	Gray	L (T) R
	2	Red	(L) T R		K19	Blue	(L) T R
	656	Gray	(L) T R		677	Gray	L (T) R
	206	Blk	(L) T R	9:20	681 Michigan	Blk	(L) T R
	ATT	Gray	L T (R)		118	Blk	L (T) R
	002	Blk	(L) T R		042	Gray	L T (R)
9:10	W83	Blk	(L) T R		255	Gray	L T (R)
	328	Blk	L T (R)		9??	White	L T (R)
	932	Blue	L (T) R		144	Gray	L (T) R
	692	Blk	L T (R)		263	Blue	(L) T R
	7m1	?	L (T) R		600	Blue	L (T) R
	118-	Truck	L T (R)		020	Gray	L (T) R
	811	Blk	(L) T R	9:25	746	Blk	(L) T R
	234	Truck	(L) T R		815	Blk	(L) T R
	761	Orange	L (T) R		F36	Gray	(L) T R
9:15	664	Blk	(L) T R		039	White	(L) T R

ONLY COUNT

(T)

APPENDIX E DORSET AVE TRAFFIC COUNTS

9/16/15

Dorset + Little Falls
Kenwood Side

Cars exiting
Kenwood on Dorset P.8

LOCATION:

COUNTED BY:

Birdie APPROACH:

TIME	LICENSE PLATE	COLOR	DIRECTION	TIME	LICENSE PLATE	COLOR	DIRECTION
9:25 (cont)	838	Gray	L (T) R				L T R
	840	Blk	L (T) R				L T R
	594	Green	(L) T R				L T R
	405	Gray	(L) T R				L T R
	WC 1	Blk	(L) T R				L T R
	020	White	(L) T R				L T R
	105	Gray	L (T) R				L T R
	427	Blk	L (T) R				L T R
	2.	Red	(L) T R				L T R
	775	Blk	L (T) R				L T R
		Gray	(L) T R				L T R
		Blue	(L) T R				L T R
			L T R				L T R
			L T R				L T R
			L T R				L T R
			L T R				L T R
			L T R				L T R
			L T R				L T R
			L T R				L T R

ONLY COUNT

(T)

APPENDIX E DORSET AVE TRAFFIC COUNTS

AM

Dorset and Little Falls

From Little Falls, L

4 pages

APPENDIX E DORSET AVE TRAFFIC COUNTS

9/16/15

1

Dorset & Little Falls

From Little Falls (Southbound)
L to Dorset to Somerset

LOCATION:

COUNTED BY:

Dabney Goold

APPROACH:

TIME	LICENSE PLATE	COLOR	DIRECTION	TIME	LICENSE PLATE	COLOR	DIRECTION
7:05	295	white ^C	(L) T R	8:00	504	white	(L) T R
7:15	276	Gray	(L) T R		2 m'i	Brown	(L) T R
	007	Black	(L) T R	8:05		Gray	(L) T R
	426	Gray	(L) T R		158	white ^C	(L) T R
	715	Brown	(L) T R		DC 161	white	(L) T R
7:20	531	Black	(L) T R		527	Gray	(L) T R
	636	Gray	(L) T R		R27	Brown	(L) T R
7:30	202	white ^C	(L) T R	8:10	182	Gray	(L) T R
	409	Black	(L) T R		229	Gray	(L) T R
7:40	548	Black	(L) T R		2 m'7	white	(L) T R
7:45	770	Gray	(L) T R		031	Brown	(L) T R
	193	Black	(L) T R		844?	white	(L) T R
	942	Gray	(L) T R	8:15	918	Dark Blue	(L) T R
	362	Gray	(L) T R		137	Gray	(L) T R
7:50	200	white	(L) T R		527	Gray	(L) T R
7:55	75 B	Black	(L) T R		150	white ^C Black	(L) T R
8:00	080	Black	(L) T R		903	Black	(L) T R
	167	Gray	(L) T R		828	Black	(L) T R
			L T R				L T R

at 7:55 & 8:15 one red car & one white van (c) made U-turn at light. might have changed mind about coming into Somerset.

APPENDIX E DORSET AVE TRAFFIC COUNTS

LOCATION:

COUNTED BY:

APPROACH:

TIME	LICENSE PLATE	COLOR	DIRECTION	TIME	LICENSE PLATE	COLOR	DIRECTION
8:15	712	Gray	(L) T R	8:35	340	Black	(L) T R
8:20	E44	Black	(L) T R		502	White	(L) T R
	959	Blue	(L) T R		673	Red	(L) T R
	008	Gray	(L) T R		452	Black	(L) T R
	456	Gray	(L) T R		326	Black	(L) T R
	778	White	(L) T R		108	Brown	(L) T R
8:25	400	Gray	(L) T R		727	gray	(L) T R
	527	Blue	(L) T R		250	White	(L) T R
	034	Gray Brown	(L) T R		N69	Black	(L) T R
	221	black	(L) T R		950	gray	(L) T R
	197	Black	(L) T R		914	Black	(L) T R
	041	Gray	(L) T R		^{DIP} 084	Black	(L) T R
8:30	BC0	Gray	(L) T R	8:45	^{DIP} 417	Brown (gray)	(L) T R
	745	Gray	(L) T R		659	gray	(L) T R
	50F	Gray	(L) T R		155	Black	(L) T R
	154	Gray	(L) T R		083	Gray	(L) T R
8:35	252	Gray	(L) T R		E09	White	(L) T R
	734	Black	(L) T R		900	Gray	(L) T R
8:35	247	White	(L) T R		168	Blue	(L) T R

8:45 552 Gray (L)
 009 Bl. (L)
 101 Gray (L)
 101 ~~Bl~~ Gray

Somerset Side 9/16/15

APPENDIX E DORSET AVE TRAFFIC COUNTS

LOCATION:

COUNTED BY:

Dorset / Little Falls

APPROACH:

Little Falls

③

TIME	LICENSE PLATE	COLOR	DIRECTION	TIME	LICENSE PLATE	COLOR	DIRECTION
AM	834	gray	Ⓛ T R	9:00	BAH	gray	Ⓛ T R
	1M6	Black	Ⓛ T R		E94	Black	Ⓛ T R
	146	Gray	Ⓛ T R		412	^{dark blue} Black	Ⓛ T R
8:50	985	gray	Ⓛ T R	9:05	803	Black Blue	Ⓛ T R
	002	Black	Ⓛ T R		3M7	red	Ⓛ T R
	E12	gray	Ⓛ T R		170	white	Ⓛ T R
	098	blue	Ⓛ T R		W98	gray	Ⓛ T R
	579	Black	Ⓛ T R		849	Black	Ⓛ T R
8:55	587	gray	Ⓛ T R		871	gray	Ⓛ T R
	418	black	Ⓛ T R		R38	gray	Ⓛ T R
	266	Gray	Ⓛ T R		466	gray	Ⓛ T R
	728	black	Ⓛ T R		**		
	950	Black	Ⓛ T R		933	white	Ⓛ T R
	825	gray	Ⓛ T R		120	red	Ⓛ T R
	327	gray	Ⓛ T R		VA 015	red	Ⓛ T R
	264	Black	Ⓛ T R		996	Brown	Ⓛ T R
9:00	467	Black	Ⓛ T R	9:10	27C	^{* ambulance} white	Ⓛ T R
	Y48	gray	Ⓛ T R	9:20	212	gray	Ⓛ T R
VA	80K	white	Ⓛ T R		585	white	Ⓛ T R

* ambulance
 ** I listed another 466 gray car here, but it must have been a mistake, erased it.

Southern Side

Dorset / Little Falls

9/16/15
E TRAFFIC COUNTS
Little Falls

APPROACH:

4

APPENDIX E DORSET AVE TRAFFIC COUNTS

AM

Dorset and Little Falls

From Little Falls, R

3 pages

9/16/15

Dorset & Little Falls
(Somerset side)

APPENDIX E DORSET AVE TRAFFIC COUNTS

From Little Falls (Northbound)
To Somerset

LOCATION:

COUNTED BY:

Maura Mahoney

APPROACH:

R @ Dorset → Somerset

TIME	LICENSE PLATE	COLOR	DIRECTION	TIME	LICENSE PLATE	COLOR	DIRECTION
7:00	399	white	L T (R)	7:50	Va 256	grey	L T (R)
7:05	Va 503	black	L T (R)	7:55	N30	grey	L T (R)
7:10			L T (R)		M46	silver	L T (R)
7:15	silver → 829		L T (R)		DC 571	silver	L T (R)
	Red → 984		L T (R)	8:00	L29	white/silver	L T (R)
7:20	green → Va 644		L T (R)		C 143	white	L T (R)
7:25	wh → Va 035		L T (R)		167	grey	L T (R)
	grey → 974		L T (R)			Maroon	L T (R)
7:30			L T (R)		OM9	black	L T (R)
7:35	135	dark grey	L T (R)		DC 904	grey	L T (R)
7:40	DC 278	white	L T (R)	8:05	DC 832	black	L T (R)
	MD 567	silver	L T (R)		801	black	L T (R)
	293	brown	L T (R)	8:10	023	black	L T (R)
	492	silver	L T (R)		950	gray	L T (R)
	425	black	L T (R)		Va 70P	blue	L T (R)
7:45	492	white	L T (R)		330	black	L T (R)
	925	white	L T (R)		DC 425	black	L T (R)
	516	black	L T (R)				L T (R)
	169	black	L T (R)				L T (R)

gray

Somerset SIDE

9/16/15

APPENDIX E DORSET AVE TRAFFIC COUNTS

(2)

LOCATION:
COUNTED BY

Dorset & Little Falls

APPROACH:

Little Falls

TIME	LICENSE PLATE	COLOR	DIRECTION	TIME	LICENSE PLATE	COLOR	DIRECTION
8:10			L T (R)		D1 106	silver	L T (R)
8:15	DC 71	white	L T (R)	8:40	DC 019	white	L T (R)
	885	blue	L T (R)		Va 018	white	L T (R)
	C 976	white	L T (R)		701	black	L T (R)
	586	silver	L T (R)	8:45	MCPS bus 327	yellow	L T (R)
	879	black	L T (R)		C 838	red	L T (R)
	A93	white/silver	L T (R)		Del 015	white	L T (R)
8:20	849	silver	L T (R)		DC 225	white silver	L T (R)
8:25	752	red	L T (R)		221	black	L T (R)
	C 921	white	L T (R)		Va 31K	silver	L T (R)
	VA 122	black	L T (R)		166	silver	L T (R)
	WEST VA 460	black	L T (R)	8:50	MCPS bus 560	yellow	L T (R)
8:30	Va 619	silver/gray	L T (R)		454	blue	L T (R)
8:35	DC 463	white	L T (R)	8:55	BLH	blue	L T (R)
	Fla PUI	white	L T (R)	9:00	269	white silver	L T (R)
	V 870	gray	L T (R)	9:05			L T (R)
	K55	red	L T (R)	9:10	MCPS bus 015	yellow	L T (R)
	DC 340	black	L T (R)				L T (R)
	345	silver	L T (R)				L T (R)

9/16/15
NDIX E DORSET AVE

Little Falls

[illegible]

PM

Dorset and Little Falls

From Dorset on Kenwood Side

These are cars leaving Kenwood on Dorset, crossing Little Falls and entering Somerset on Dorset.

Only the Thru traffic is relevant to Somerset.

7 pages

APPENDIX E DORSET AVE TRAFFIC COUNTS

LOCATION:

COUNTED BY:

Dorset & Little Falls (Kenwood Side) Ruth LIS 9/16/15 Dorset (Eastbound) 1
 Kenwood Side
 APPROACH: LTR @ LITTLE FALLS

TIME	LICENSE PLATE	COLOR	DIRECTION	TIME	LICENSE PLATE	COLOR	DIRECTION
PM							
4	524 NC	R	L (T) R	8:45	EXM 844 GRY		L (T) R
	170 DC	GRY	L (T) R	4:15	309	WH	L (T) R
	^{MD} 3MI	Wh	L (T) R		378	Gold	L (T) R
	^{SUV} Jeep	BL	L (T) R		109	BL	L (T) R
	E83	GRY	L (T) R		668	GRY	L (T) R
	187	GRY	L (T) R		18T	BL	L (T) R
	³⁰⁶ 444	R	L (T) R		^{DC} 229	GRY	L (T) R
4:05	W50	Gold	L (T) R		790	BL	L (T) R
	R50	BL	L (T) R		P14	GRY	L (T) R
	483	GRY	L (T) R	4:20	064	GRY	L (T) R
	944	GRY WH	L (T) R		M03	R	L (T) R
	4M8	GRY	L (T) R		579	BL	L (T) R
	5M4	BL	L (T) R	25	F36	BL	L (T) R
4:30		BLU	L (T) R		458	WH	L (T) R
	235 VA	GOLD	L (T) R		401	BL	L (T) R
	X86	MULTI	L (T) R		569	BL	L (T) R
	587	GRY DC	L (T) R		027	GRY	L (T) R
	? 183 DC	TXI DC	L (T) R		262	R	L (T) R
	215	BRW	L (T) R		JRK 516		L (T) R

R = Red
 WH = WHITE
 GRY = GRAY
 BRW = BROWN
 BL = BLACK
 BLU = BLUE
 GOLD = Beige + Gold
 MULTI

APPENDIX E DORSET AVE TRAFFIC COUNTS

LOCATION:

COUNTED BY:

APPROACH:

TIME	LICENSE PLATE	COLOR	DIRECTION	TIME	LICENSE PLATE	COLOR	DIRECTION
4:10	549	GRY	L T R		240	BLU	L T R
4:30	NTJ	GRY	L T R		C72	GRY	L T R
		WH	L T R		675	GRY	L T R
DC	803	BL	L T R		397	GRY	L T R
	382	GRY	L T R		584	BLU	L T R
DC	811	GRY	L T R		889	BL	L T R
	164	PL	L T R	445	726	COM VAN WH	L T R
		GRN	L T R		300	GLD	L T R
	207	BL	L T R		295	WH VAN	L T R
	058	GRY	L T R		853	R	L T R
4:35	638	R TRK	L T R		C83	GRY	L T R
	050	GRY	L T R		455	GRN	L T R
	647	BL	L T R		157	BL	L T R
	6M4	GRY	L T R	DC	644	BL	L T R
DC	430	BL	L T R		095	BLU	L T R
(24)	X86	MULTI	L T R		656	TRK VAN	L T R
	BEW	BRW	L T R		527	BLU	L T R
4:40	971	GRY	L T R	D	C47	WH	L T R
	565	GRY	L T R		112	R	L T R

APPENDIX E DORSET AVE TRAFFIC COUNTS

LOCATION:

COUNTED BY:

APPROACH:

Dorset / Little Falls

9/16/15

From Dorset
Kenswood Side

TIME	LICENSE PLATE	COLOR	DIRECTION	TIME	LICENSE PLATE	COLOR	DIRECTION
PM 4:40	MD	GOLD	L (T) R		267	GRY	(L) T R
	MD	WH ^{VN}	L T (R)		642	BLK	(L) T R
450	500	BL	L (T) R		662	GRY	(L) T R
	AWE	WH	(L) T R		467	GRY	(L) T R
	708	GRY	(L) T R	505	538	GRY	(L) T R
	2M4	BL	L (T) R		575	GRY	L (T) R
		WH	(L) T R			WH	L (T) R
	078	TXI	(L) T R		293	BL	(L) T R
	542	GRY	(L) T R		575	GRN	(L) T R
	307	R	(L) T R		758	BL	L (T) R
	BH6	WH	L (T) R	DC	951	GRY	(L) T R
	035	BL	L (T) R	DC	219	BL	L T (R)
	867	BL	(L) T R		234	BLU	L T (R)
455	FLA	WH	L (T) R		536	GRY	(L) T R
	6M5	GRN	L (T) R			JUNK TRK	L (T) R
	222	BL	(L) T R			GRY ^{SUV}	L (T) R
	143	TRK ^{BL}	(L) T R		636	GRY	(L) T R
500	606	BLU	L (T) R		743	GRY	L (T) R
	363	WH	(L) T R	510			L T R

LOCATION:

COUNTED BY:

APPENDIX E DORSET AVE TRAFFIC COUNTS

APPROACH:

Dorset/Little Falls

9/16/15

From Dorset
Renwood Side

4

TIME	LICENSE PLATE	COLOR	DIRECTION	TIME	LICENSE PLATE	COLOR	DIRECTION
5 ¹⁰	295	GOLD	L (T) R		727	GRY	(L) T R
	791	GRY	(L) T R		980	MULTI WH	(L) T R
	044	BL	L (T) R	5 ²⁵	000	R	L (T) R
	730	R	L (T) R	DC	716	GRY	L (T) R
	FUN	GRY	(L) T R		524	GRY	(L) T R
	DC 920	GRY	L T (R)	VA	430	GOLD	L T (R)
5 ¹⁵	451	MULTI BL	L (T) R		018	TXI	L (T) R
	944	BL	L (T) R		977	GOLD	L (T) R
	913	GRY	L (T) R		132		L T R
	009	GOLD	L (T) R	VA	644	BL	L (T) R
	4M8	GRY	(L) T R		411	BL	(L) T R
	658	BL	L T (R)		045	GRY	(L) T R
5 ²⁰	967	GRY	(L) T R	5 ³⁰	627	GRY	L T (R)
	671	GRY	L (T) R		VA 135	BL	L T (R)
	076	GRY	L (T) R		165	BL	(L) T R
	978	WH	L (T) R		543	WH	L (T) R
	876	FRY	L (T) R		249	GRY	(L) T R
	418	GRY	(L) T R		283	BL	(L) T R
	841	GRN	(L) T R		005	BL	L (T) R

APPENDIX E DORSET AVE TRAFFIC COUNTS

Dorset / Little Falls 9/11/15 From
Kenwood Side Dorset (5)

LOCATION:

COUNTED BY:

APPROACH:

TIME	LICENSE PLATE	COLOR	DIRECTION	TIME	LICENSE PLATE	COLOR	DIRECTION
535	DEL 088	GLD	(L) TR R		643	GRY	(L) T R
	DC 890	II	L T (R)		614	BL	L (T) R
	076	BL	L T (R)		705	WH	L T (R)
	427	BLU	L (T) R		922	GRY	(L) T R
	PC	GRY	(L) T (R)		543	WH	L T (R)
	608	BL	L (T) R		213	GRY	(L) T R
	215	BL	(L) T R		212	GRY	L T (R)
	356	R	(L) T R		750	BL	L TR R
	716	GRY	(L) T R		47M	BL	(L) T R
		BLU	(L) T R	550	587	GRY	L (T) R
		? L	(L) T R		958	GRY	L (T) R
540	310	GRY	(L) T R		833	BL	(L) T R
	615	R	(L) T R		361	R	(L) T R
	912	R	(L) T R		NJ 4AV	TRK WH	L (T) R
		WH	L (T) R		OHIO 109	GLD	L (T) R
	134	R	(L) TR R		813	BLU	L T (R)
	VA 219	WH	L T (R)		022		L T (R)
	VA 20X	GLD	L T (R)	555	DC 842	GRY	L (T) R
545	BNG	BL	(L) T R		507	BL	L (T) R

WH

T

APPENDIX E DORSET AVE TRAFFIC COUNTS

Dorset / Little Falls 11/15 From Dorset
 Kenwood Side

LOCATION:

COUNTED BY:

APPROACH:

TIME	LICENSE PLATE	COLOR	DIRECTION	TIME	LICENSE PLATE	COLOR	DIRECTION
5:45	427	BLK	L (T) R		722	GLD	L (T) R
	868	R	(L) T R	6:0			L T R
DL	836	BL	L (T) R		640	BLK	L T (R)
	741	BL	L (T) R	DL	803	GRY	L (T) R
	858	GRY	L (T) R		300	R	L (T) R
	051	GLD	L T (R)		436	GRY	(L) T R
	MTH	GRY	L (T) R	DL	109	GRY	(L) T R
6:00	782	GRY GLD	L (T) R		247	GOLD	(L) T R
	040	BL	(L) T R	DL	198	BL	L (T) R
	207	BL	L (T) R		328	BL	L T (R)
	N97	R	(L) T R		WCH	GRY	L (T) R
	224	BL	(L) T R		084	BLK	L (T) R
	850	GRY BLK	L (T) R	DL		WH	L (T) R
6:05	445	GRY	(L) T R		MD ^{MINI VAN}	WH	L (T) R
	A24	BLU	L (T) R		682	BLU	(L) T R
	236	GRY	(L) T R		774	GRN	L (T) R
	597	GRY	L (T) R		154	BL	(L) T R
	049	GRY	L (T) R	6:15	743	GRY	(L) T R
	844	BL	(L) T R		658	WT	(L) T R

JMC GRY (L)

Dorset / Little Falls 9/11/15 From Dorset Kenwood Side. (7)

COUNTED BY:

FFIC COUNTS

9/11/15 From Dorset (7)
Kenwood Side.

[illegible]

APPENDIX E DORSET AVE TRAFFIC COUNTS

PM

Dorset and Little Falls

From Little Falls, L

3 pages

Dorset & Little Falls

APPENDIX E DORSET AVE TRAFFIC COUNTS

From Little Falls (Southbound)
L @ Dorset to Somerset

LOCATION:

COUNTED BY:

Dabney Goold

APPROACH:

TIME	LICENSE PLATE	COLOR	DIRECTION	TIME	LICENSE PLATE	COLOR	DIRECTION
4:00	202	gray	(L) T R	4:40	692	gray	(L) T R
	0 turn	black			527	black	(L) T R
	816	gray	(L) T R	4:45	003	red	(L) T R
	503	black	(L) T R		638	gray	(L) T R
4:05	247	blue	(L) T R		1787	gray	(L) T R
	009	blue	(L) T R		—	gray	(L) T R
	971	beige brown	(L) T R	4:50	124	red	(L) T R
	032	black	(L) T R		309	gray	(L) T R
4:10	463	gray	(L) T R		051	Black	(L) T R
	671	white	(L) T R		W60	white	(L) T R
4:15	276	gray	(L) T R		517	red	(L) T R
	920	black	(L) T R	4:55	976	Black	(L) T R
4:20	801	red	(L) T R		216	gray	(L) T R
4:25	579	beige brown	(L) T R	5:00	065	black	(L) T R
4:30	198	black dark blue	(L) T R	5:05	827	gray green	(L) T R
	815	gray	(L) T R		* 950	white	(L) T R
	876	gray	(L) T R		* 200	white	(L) T R
	063	black	(L) T R		bc? 1154	Black	(L) T R
4:35	0 turn	green			866	Black	(L) T R

* bc? plate in back

APPENDIX E DORSET AVE TRAFFIC COUNTS

LOCATION:

COUNTED BY:

APPROACH:

Dorset / Little Falls 1/15 From Little Falls (2)

TIME	LICENSE PLATE	COLOR	DIRECTION	TIME	LICENSE PLATE	COLOR	DIRECTION
5:05	aturn			5:30 cont.	104	gray	(L) T R
	590	gray	(L) T R		906 ^{DC}	white	(L) T R
5:10	996	Black	(L) T R		431	black	(L) T R
5:15	APY	blue	(L) T R		875	black	(L) T R
	493	gray/ brown	(L) T R	5:35	922	gray blue	(L) T R
	254	gray	(L) T R	5:40	457	red	(L) T R
	201	Black	(L) T R		433	black	(L) T R
	885	gray	(L) T R		266	gray	(L) T R
	052	gray	(L) T R		753	black	(L) T R
	175	gray	(L) T R	5:45	aturn		
	770	gray	(L) T R		AZA	gray	(L) T R
	182	gray	(L) T R		680	black	(L) T R
	863	gray beige	(L) T R		500	gray	(L) T R
	771	gray	(L) T R		228	black	(L) T R
	6M5	black	(L) T R	4:50	BKP	white off	(L) T R
	927	black	(L) T R		434	gray	(L) T R
	059	gray	(L) T R		734 VA?	black	(L) T R
5:30	608 VA	white	(L) T R	4:55 PA	917	gray	(L) T R
	518	blue	(L) T R		531	dark blue	(L) T R

APPENDIX E DORSET AVE TRAFFIC COUNTS

Dorset / Little Falls 9/16/15 From Little Falls (3)

LOCATION:

COUNTED BY:

APPROACH:

TIME	LICENSE PLATE	COLOR	DIRECTION	TIME	LICENSE PLATE	COLOR	DIRECTION
5:55	792	gray	L T R				L T R
	095	white	L T R				L T R
	117	black	L T R				L T R
	787	white	L T R				L T R
	692	gray	L T R				L T R
6:00	909	white	L T R				L T R
6:05	DC 140	gray	L T R				L T R
	172	black	L T R				L T R
	N, York 730	white	L T R				L T R
	V57	black	L T R				L T R
6:10	340	white	L T R				L T R
	772	black	L T R				L T R
	408	gray	L T R				L T R
	772	black	L T R				L T R
	673	black	L T R				L T R
	650	white	L T R				L T R
	570	gray	L T R				L T R
	073	white	L T R				L T R
			L T R				L T R

APPENDIX E DORSET AVE TRAFFIC COUNTS

PM

Dorset and Little Falls, R

5 pages

APPENDIX E DORSET AVE TRAFFIC COUNTS

Dorset + Little Falls (Somerset + Side) 9/14/15

*From Little Falls (Northbound)
R@ Dorset to Somerset*

①

LOCATION:

COUNTED BY: C / NZIA FURLANE / 10

APPROACH:

TIME	LICENSE PLATE	COLOR	DIRECTION	TIME	LICENSE PLATE	COLOR	DIRECTION
4:00 PM	023	blu	L T R				L T <u>R</u>
	0	blu	L T R				L T <u>R</u>
	815	Black	L T R				L T <u>R</u>
	3MI	White	L T R				L T <u>R</u>
	282	Green	L T R				L T <u>R</u>
	0	C	L T R				L T <u>R</u>
	163	White	L T R				L T <u>R</u>
	032	Black	L T R				L T <u>R</u>
	478	Green	L T R				L T <u>R</u>
	008	Blue	L T R				L T <u>R</u>
	271	Grey	L T R				L T <u>R</u>
	175	Brown	L T R				L T <u>R</u>
	312	Grey	L T R				L T <u>R</u>
	718	Brown	L T R				L T <u>R</u>
4:10	821	Grey	L T R				L T <u>R</u>
	848	Grey/wh	L T R				L T <u>R</u>
	233	Black	L T R				L T <u>R</u>
	NAP	Black	L T R				L T <u>R</u>
4:15			L T R				L T <u>R</u>

Dorset / LIND FOLLS

APPENDIX E DORSET AVE TRAFFIC COUNTS

From L/H/e FOLLS (northbound)
R@ Dorset to Somerset

LOCATION:

COUNTED BY:

C. NEILA FURLONG

APPROACH:

TIME	LICENSE PLATE	COLOR	DIRECTION	TIME	LICENSE PLATE	COLOR	DIRECTION
4:15	/		L T R			←	L T R
4:20	846	Black	L T R				L T R
	444	White	L T R				L T R
	460	Grey	L T R				L T R
	178	Black	L T R				L T R
	534	Black	L T R				L T R
4:25	877	Black	L T R				L T R
	468	Red	L T R				L T R
	702	White	L T R				L T R
	461	White	L T R				L T R
	548	Black	L T R				L T R
4:30	408	Red	L T R				L T R
4:35	OLV	Blue	L T R				L T R
	647	White	L T R				L T R
	682	Green	L T R				L T R
4:40	847	Blue	L T R				L T R
4:45	112	Brown	L T R				L T R
	838	Black	L T R				L T R
	242	Black	L T R				L T R

462 Black

314 Red

Dorset / Little Falls
Somerset - 51010

APPENDIX E DORSET AVE TRAFFIC COUNTS

4/16/15

From Little Falls (North) to Dorset
Re Dorset to Somerset

LOCATION:

COUNTED BY: C / NZIA FURLANETTO

APPROACH:

TIME	LICENSE PLATE	COLOR	DIRECTION	TIME	LICENSE PLATE	COLOR	DIRECTION
4:45	162	Black	L T R			←	L T R
	314	Black	L T R				L T R
	6M0	Grey	L T R				L T R
4:50	555	Blue/Black	L T R				L T R
4:55	444	Gold	L T R				L T R
	718	Blue	L T R			←	L T R
	861	Light Blue	L T R				L T R
	CUP	Silver/Grey	L T R				L T R
5:00	881	Black	L T R				L T R
	160	Red	L T R			←	L T R
	8711	Dark Grey	L T R				L T R
		Pick-up not C	L T R				L T R
5:05	1NA	Black	L T R				L T R
	343	Black	L T R				L T R
	681	Blue	L T R			←	L T R
	522	White/Grey	L T R				L T R
	164	Black	L T R			←	L T R
5:10	056	Silver	L T R				L T R
	846	Dark Grey	L T R				L T R

APPENDIX E DORSET AVE TRAFFIC COUNTS

9/16/15

4

DORSET - L.H.Q. FOLLS (Somerset side)

from Little Falls (Northbound)
R@ Dorset to Somerset

LOCATION:

COUNTED BY: C. N. Z. A. FURLANETTO

APPROACH:

TIME	LICENSE PLATE	COLOR	DIRECTION	TIME	LICENSE PLATE	COLOR	DIRECTION
5:15	867	Black	L T R			←	L T R
	602	Grey/Silver	L T R				L T R
5:20	510	Black	L T R			←	L T R
		Red	L T R				L T R
	183	Dark Grey	L T R				L T R
5:25	440	Grey	L T R			←	L T R
	709	Gold	L T R				L T R
5:30	0M4	Black	L T R			←	L T R
	844	White	L T R				L T R
	816	White	L T R				L T R
5:35	171	Dark Silver	L T R				L T R
5:40	812	Silver	L T R				L T R
	412	Silver/Green	L T R			←	L T R
	448	Silver	L T R				L T R
	870	White	L T R				L T R
5:45	880	White	L T R			←	L T R
	-	B	L T R				L T R
	873	White	L T R				L T R
5:50	826	Gold	L T R			←	L T R

LOCATION:

COUNTED BY:

Dorset/Little Falls (Somerset side)

APPENDIX E DORSET AVE TRAFFIC COUNTS

LITTLE FALLS

FROM

LITTLE FALLS

(northbound)

APPROACH:

Dorset to

Somerset

TIME	LICENSE PLATE	COLOR	DIRECTION	TIME	LICENSE PLATE	COLOR	DIRECTION
5:50	029	Silver	L T R			←	L T (R)
	662	Dark Grey	L T R				L T (R)
5:55	GT2	Blue	L T R				L T (R)
	245	Black	L T R				L T (R)
	33F	Grey	L T R				L T (R)
6:00	305	Grey	L T R				L T (R)
	850	Grey	L T R				L T (R)
	787	Blue	L T R				L T (R)
	688	Silver	L T R				L T (R)
	N60	Grey	L T R				L T (R)
6:05	351	White	L T R				L T (R)
	18F	Black	L T R				L T (R)
6:10	082	Black	L T R				L T (R)
	881	Blue	L T R				L T (R)
			L T R				L T R
			L T R				L T R
			L T R				L T R
			L T R				L T R
			L T R				L T R

APPENDIX E DORSET AVE TRAFFIC COUNTS

	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P
1				Sabra & Associates, Inc. 7055 Samuel Morse Drive, Suite 100 Columbia, MD 21046 1-443-741-3500										Page 1		
2																
3																
4	MONTGOMERY													Site Code: 000000000000		
5	SOMERSET													Station ID: 000000000000		
6														DORSET AVE. BTW. LITTLE FAL		
7																
8														Latitude: 0' 0.0000 Undefined		
9																
10		15-May-18														
11		Tue	EB	WB							Total					
12	12:00 AM		1	4							5					
13	01:00		0	0							0					
14	02:00		0	0							0					
15	03:00		0	0							0					
16	04:00		1	0							1					
17	05:00		2	5							7					
18	06:00		21	13							34					
19	07:00		77	65							142					
20	08:00		163	105							268					
21	09:00		91	73							164					
22	10:00		101	66							167					
23	11:00		104	76							180					
24	12:00 PM		124	80							204					
25	01:00		115	80							195					
26	02:00		104	82							186					
27	03:00		138	107							245					
28	04:00		130	119							249					
29	05:00		153	120							273					
30	06:00		175	107							282					
31	07:00		65	62							127					
32	08:00		49	29							78					
33	09:00		31	6							37					
34	10:00		11	8							19					
35	11:00		10	4							14					
36	Total		1666	1211							2877					
37	Percent		57.9%	42.1%												
38																
39	AM Peak		08:00	08:00							08:00					
40	Vol.		163	105							268					
41																
42	PM Peak		18:00	17:00							18:00					
43	Vol.		175	120							282					

APPENDIX E DORSET AVE TRAFFIC COUNTS

[illegible]

Appendix E Pre-Diet Traffic Counts: Arlington Road, Hillandale Road, Little Falls Parkway
ARLINGTON RD PRE ROAD DIET COUNTS

Volume

Start Date: 12/7/2016

Start Time: 12:00:00 AM

Location 1: ARLINGTON RD B/W LITTLE FALLS PKWY &

Location 2: KENWOOD FOREST LN

MONTGOMERY

BETHESDA

Number	Date	Time	NB L1	NB L2	SB L3	SB L4
1	12/7/2016	12:00 AM	3	3	1	0
2	12/7/2016	12:15 AM	2	4	3	1
3	12/7/2016	12:30 AM	1	0	3	1
4	12/7/2016	12:45 AM	0	4	2	0
5	12/7/2016	01:00 AM	0	2	0	0
6	12/7/2016	01:15 AM	0	0	4	0
7	12/7/2016	01:30 AM	0	1	0	0
8	12/7/2016	01:45 AM	0	0	1	0
9	12/7/2016	02:00 AM	0	0	0	0
10	12/7/2016	02:15 AM	0	1	0	0
11	12/7/2016	02:30 AM	2	0	1	0
12	12/7/2016	02:45 AM	0	0	0	1
13	12/7/2016	03:00 AM	0	0	1	0
14	12/7/2016	03:15 AM	0	1	1	0
15	12/7/2016	03:30 AM	0	0	1	0
16	12/7/2016	03:45 AM	0	1	1	0
17	12/7/2016	04:00 AM	0	0	1	0
18	12/7/2016	04:15 AM	0	1	2	0
19	12/7/2016	04:30 AM	0	0	2	1
20	12/7/2016	04:45 AM	0	4	5	1
21	12/7/2016	05:00 AM	0	2	6	0
22	12/7/2016	05:15 AM	2	2	7	3
23	12/7/2016	05:30 AM	9	2	7	0
24	12/7/2016	05:45 AM	13	5	8	4
25	12/7/2016	06:00 AM	3	5	10	7
26	12/7/2016	06:15 AM	6	5	14	3
27	12/7/2016	06:30 AM	14	12	21	18
28	12/7/2016	06:45 AM	13	13	27	19
29	12/7/2016	07:00 AM	10	20	41	29
30	12/7/2016	07:15 AM	33	23	58	35
31	12/7/2016	07:30 AM	38	31	45	43
32	12/7/2016	07:45 AM	60	36	46	34
33	12/7/2016	08:00 AM	54	46	64	45
34	12/7/2016	08:15 AM	65	59	37	33
35	12/7/2016	08:30 AM	48	56	48	36
36	12/7/2016	08:45 AM	74	61	49	33
37	12/7/2016	09:00 AM	59	40	44	27
38	12/7/2016	09:15 AM	56	47	41	32
39	12/7/2016	09:30 AM	35	36	49	38
40	12/7/2016	09:45 AM	46	33	39	21
41	12/7/2016	10:00 AM	45	36	43	31

Appendix E Pre-Diet Traffic Counts: Arlington Road, Hillandale Road, Little Falls Parkway
ARLINGTON RD PRE ROAD DIET COUNTS

42	12/7/2016	10:15 AM	56	32	52	18
43	12/7/2016	10:30 AM	46	32	56	33
44	12/7/2016	10:45 AM	42	35	68	33
45	12/7/2016	11:00 AM	43	33	52	36
46	12/7/2016	11:15 AM	43	35	58	32
47	12/7/2016	11:30 AM	55	42	59	43
48	12/7/2016	11:45 AM	50	44	52	35
49	12/7/2016	12:00 PM	49	36	84	22
50	12/7/2016	12:15 PM	39	52	62	28
51	12/7/2016	12:30 PM	54	35	54	29
52	12/7/2016	12:45 PM	35	36	49	28
53	12/7/2016	01:00 PM	39	43	70	27
54	12/7/2016	01:15 PM	39	43	56	36
55	12/7/2016	01:30 PM	44	33	64	33
56	12/7/2016	01:45 PM	35	39	57	27
57	12/7/2016	02:00 PM	34	32	80	30
58	12/7/2016	02:15 PM	33	41	62	30
59	12/7/2016	02:30 PM	38	42	65	31
60	12/7/2016	02:45 PM	46	45	69	33
61	12/7/2016	03:00 PM	41	47	64	41
62	12/7/2016	03:15 PM	53	44	75	47
63	12/7/2016	03:30 PM	43	44	61	31
64	12/7/2016	03:45 PM	44	46	68	39
65	12/7/2016	04:00 PM	56	45	94	52
66	12/7/2016	04:15 PM	58	61	89	52
67	12/7/2016	04:30 PM	47	47	92	66
68	12/7/2016	04:45 PM	50	43	96	61
69	12/7/2016	05:00 PM	53	33	105	55
70	12/7/2016	05:15 PM	60	37	93	59
71	12/7/2016	05:30 PM	54	56	97	61
72	12/7/2016	05:45 PM	50	49	95	60
73	12/7/2016	06:00 PM	56	39	71	45
74	12/7/2016	06:15 PM	44	39	91	48
75	12/7/2016	06:30 PM	57	42	70	43
76	12/7/2016	06:45 PM	44	41	71	37
77	12/7/2016	07:00 PM	30	35	49	37
78	12/7/2016	07:15 PM	42	38	55	24
79	12/7/2016	07:30 PM	32	37	45	15
80	12/7/2016	07:45 PM	25	27	39	16
81	12/7/2016	08:00 PM	28	31	36	13
82	12/7/2016	08:15 PM	22	23	37	18
83	12/7/2016	08:30 PM	12	16	35	7
84	12/7/2016	08:45 PM	12	21	43	19
85	12/7/2016	09:00 PM	12	8	22	12
86	12/7/2016	09:15 PM	18	18	27	6
87	12/7/2016	09:30 PM	7	9	14	7
88	12/7/2016	09:45 PM	9	9	30	8
89	12/7/2016	10:00 PM	5	13	10	4
90	12/7/2016	10:15 PM	5	9	15	2
91	12/7/2016	10:30 PM	5	10	6	6
92	12/7/2016	10:45 PM	2	5	5	1
93	12/7/2016	11:00 PM	3	6	6	0

Appendix E Pre-Diet Traffic Counts: Arlington Road, Hillandale Road, Little Falls Parkway
ARLINGTON RD PRE ROAD DIET COUNTS

94	12/7/2016	11:15 PM	5	6	2	1
95	12/7/2016	11:30 PM	1	3	4	2
96	12/7/2016	11:45 PM	2	2	3	1
97	12/8/2016	12:00 AM	1	3	2	0
98	12/8/2016	12:15 AM	1	2	5	0
99	12/8/2016	12:30 AM	0	2	1	1
100	12/8/2016	12:45 AM	1	1	0	0
101	12/8/2016	01:00 AM	0	2	1	1
102	12/8/2016	01:15 AM	1	2	3	2
103	12/8/2016	01:30 AM	1	3	1	0
104	12/8/2016	01:45 AM	2	0	2	0
105	12/8/2016	02:00 AM	0	0	0	0
106	12/8/2016	02:15 AM	1	1	2	0
107	12/8/2016	02:30 AM	0	0	0	0
108	12/8/2016	02:45 AM	0	1	1	0
109	12/8/2016	03:00 AM	0	2	1	0
110	12/8/2016	03:15 AM	0	0	1	0
111	12/8/2016	03:30 AM	0	0	2	0
112	12/8/2016	03:45 AM	0	0	1	0
113	12/8/2016	04:00 AM	0	0	3	0
114	12/8/2016	04:15 AM	0	1	2	1
115	12/8/2016	04:30 AM	0	1	3	0
116	12/8/2016	04:45 AM	1	3	2	0
117	12/8/2016	05:00 AM	0	2	7	2
118	12/8/2016	05:15 AM	3	4	8	0
119	12/8/2016	05:30 AM	7	3	7	3
120	12/8/2016	05:45 AM	9	4	14	2
121	12/8/2016	06:00 AM	6	5	10	6
122	12/8/2016	06:15 AM	4	4	21	10
123	12/8/2016	06:30 AM	11	9	27	14
124	12/8/2016	06:45 AM	10	8	33	19
125	12/8/2016	07:00 AM	15	10	48	29
126	12/8/2016	07:15 AM	30	23	63	43
127	12/8/2016	07:30 AM	41	40	72	46
128	12/8/2016	07:45 AM	43	40	52	33
129	12/8/2016	08:00 AM	50	44	52	37
130	12/8/2016	08:15 AM	59	68	45	35
131	12/8/2016	08:30 AM	64	52	49	35
132	12/8/2016	08:45 AM	64	54	56	23
133	12/8/2016	09:00 AM	61	54	37	29
134	12/8/2016	09:15 AM	54	49	49	22
135	12/8/2016	09:30 AM	34	29	54	24
136	12/8/2016	09:45 AM	52	36	35	16
137	12/8/2016	10:00 AM	32	31	42	31
138	12/8/2016	10:15 AM	42	35	61	33
139	12/8/2016	10:30 AM	37	28	56	29
140	12/8/2016	10:45 AM	49	42	49	43
141	12/8/2016	11:00 AM	39	35	48	34
142	12/8/2016	11:15 AM	34	39	58	27
143	12/8/2016	11:30 AM	46	47	41	23
144	12/8/2016	11:45 AM	58	43	55	26
145	12/8/2016	12:00 PM	50	38	66	31

Appendix E Pre-Diet Traffic Counts: Arlington Road, Hillandale Road, Little Falls Parkway
ARLINGTON RD PRE ROAD DIET COUNTS

146	12/8/2016	12:15 PM	48	35	62	28
147	12/8/2016	12:30 PM	40	39	60	30
148	12/8/2016	12:45 PM	41	38	49	32
149	12/8/2016	01:00 PM	52	36	60	27
150	12/8/2016	01:15 PM	32	33	48	29
151	12/8/2016	01:30 PM	45	37	66	35
152	12/8/2016	01:45 PM	40	34	47	33
153	12/8/2016	02:00 PM	24	39	68	28
154	12/8/2016	02:15 PM	42	47	76	33
155	12/8/2016	02:30 PM	40	24	85	37
156	12/8/2016	02:45 PM	30	33	74	31
157	12/8/2016	03:00 PM	40	50	70	54
158	12/8/2016	03:15 PM	54	47	78	60
159	12/8/2016	03:30 PM	46	54	80	37
160	12/8/2016	03:45 PM	53	45	78	47
161	12/8/2016	04:00 PM	47	52	90	55
162	12/8/2016	04:15 PM	45	52	95	52
163	12/8/2016	04:30 PM	58	41	91	61
164	12/8/2016	04:45 PM	72	56	107	64
165	12/8/2016	05:00 PM	56	58	100	57
166	12/8/2016	05:15 PM	64	58	95	58
167	12/8/2016	05:30 PM	63	62	95	54
168	12/8/2016	05:45 PM	62	55	89	57
169	12/8/2016	06:00 PM	72	64	82	64
170	12/8/2016	06:15 PM	47	61	97	58
171	12/8/2016	06:30 PM	36	46	68	39
172	12/8/2016	06:45 PM	46	39	64	29
173	12/8/2016	07:00 PM	39	33	56	28
174	12/8/2016	07:15 PM	45	41	44	25
175	12/8/2016	07:30 PM	34	29	35	16
176	12/8/2016	07:45 PM	29	37	26	19
177	12/8/2016	08:00 PM	31	29	35	13
178	12/8/2016	08:15 PM	15	23	43	15
179	12/8/2016	08:30 PM	10	14	25	12
180	12/8/2016	08:45 PM	15	16	30	13
181	12/8/2016	09:00 PM	15	20	23	8
182	12/8/2016	09:15 PM	6	9	21	7
183	12/8/2016	09:30 PM	10	8	24	13
184	12/8/2016	09:45 PM	12	15	16	8
185	12/8/2016	10:00 PM	9	16	18	5
186	12/8/2016	10:15 PM	7	8	11	5
187	12/8/2016	10:30 PM	9	6	7	4
188	12/8/2016	10:45 PM	6	3	7	4
189	12/8/2016	11:00 PM	5	9	5	0
190	12/8/2016	11:15 PM	5	6	8	1
191	12/8/2016	11:30 PM	3	3	5	0
192	12/8/2016	11:45 PM	1	5	4	0
193	12/9/2016	12:00 AM	1	2	3	0
194	12/9/2016	12:15 AM	3	2	2	2
195	12/9/2016	12:30 AM	3	2	3	1
196	12/9/2016	12:45 AM	4	2	2	0
197	12/9/2016	01:00 AM	2	0	2	0

Appendix E Pre-Diet Traffic Counts: Arlington Road, Hillandale Road, Little Falls Parkway
ARLINGTON RD PRE ROAD DIET COUNTS

198	12/9/2016	01:15 AM	1	2	1	2
199	12/9/2016	01:30 AM	2	2	4	0
200	12/9/2016	01:45 AM	0	1	3	0
201	12/9/2016	02:00 AM	0	2	0	0
202	12/9/2016	02:15 AM	1	2	0	0
203	12/9/2016	02:30 AM	0	0	0	0
204	12/9/2016	02:45 AM	1	2	0	0
205	12/9/2016	03:00 AM	1	0	0	0
206	12/9/2016	03:15 AM	0	1	0	0
207	12/9/2016	03:30 AM	0	0	2	0
208	12/9/2016	03:45 AM	0	1	1	0
209	12/9/2016	04:00 AM	1	1	2	0
210	12/9/2016	04:15 AM	0	1	1	1
211	12/9/2016	04:30 AM	1	0	6	1
212	12/9/2016	04:45 AM	1	2	6	1
213	12/9/2016	05:00 AM	1	1	5	0
214	12/9/2016	05:15 AM	3	4	10	1
215	12/9/2016	05:30 AM	9	5	12	2
216	12/9/2016	05:45 AM	14	6	11	2
217	12/9/2016	06:00 AM	4	6	5	8
218	12/9/2016	06:15 AM	3	10	18	12
219	12/9/2016	06:30 AM	7	8	20	21
220	12/9/2016	06:45 AM	13	8	35	18
221	12/9/2016	07:00 AM	20	12	54	32
222	12/9/2016	07:15 AM	28	28	48	37
223	12/9/2016	07:30 AM	40	35	62	43
224	12/9/2016	07:45 AM	50	30	74	39
225	12/9/2016	08:00 AM	36	45	39	42
226	12/9/2016	08:15 AM	64	62	45	25
227	12/9/2016	08:30 AM	58	46	41	32
228	12/9/2016	08:45 AM	64	49	45	36
229	12/9/2016	09:00 AM	65	47	45	24
230	12/9/2016	09:15 AM	50	35	41	25
231	12/9/2016	09:30 AM	49	54	44	23
232	12/9/2016	09:45 AM	45	41	46	16
233	12/9/2016	10:00 AM	58	29	57	18
234	12/9/2016	10:15 AM	39	34	49	20
235	12/9/2016	10:30 AM	45	32	49	32
236	12/9/2016	10:45 AM	52	47	64	32
237	12/9/2016	11:00 AM	52	47	64	40
238	12/9/2016	11:15 AM	49	30	54	37
239	12/9/2016	11:30 AM	44	38	72	35
240	12/9/2016	11:45 AM	52	58	56	29
241	12/9/2016	12:00 PM	39	36	72	39
242	12/9/2016	12:15 PM	49	43	56	39
243	12/9/2016	12:30 PM	44	45	56	31
244	12/9/2016	12:45 PM	43	39	70	38
245	12/9/2016	01:00 PM	60	39	72	41
246	12/9/2016	01:15 PM	38	45	66	36
247	12/9/2016	01:30 PM	39	35	55	33
248	12/9/2016	01:45 PM	55	39	60	27
249	12/9/2016	02:00 PM	48	43	91	46

Appendix E Pre-Diet Traffic Counts: Arlington Road, Hillandale Road, Little Falls Parkway
ARLINGTON RD PRE ROAD DIET COUNTS

250	12/9/2016	02:15 PM	44	36	76	39
251	12/9/2016	02:30 PM	45	55	78	42
252	12/9/2016	02:45 PM	49	44	74	33
253	12/9/2016	03:00 PM	48	45	76	50
254	12/9/2016	03:15 PM	71	61	93	45
255	12/9/2016	03:30 PM	57	43	89	56
256	12/9/2016	03:45 PM	50	55	78	47
257	12/9/2016	04:00 PM	58	53	111	52
258	12/9/2016	04:15 PM	56	52	103	64
259	12/9/2016	04:30 PM	48	39	99	62
260	12/9/2016	04:45 PM	60	47	85	58
261	12/9/2016	05:00 PM	62	49	94	63
262	12/9/2016	05:15 PM	54	52	103	56
263	12/9/2016	05:30 PM	64	43	103	63
264	12/9/2016	05:45 PM	60	39	76	58
265	12/9/2016	06:00 PM	47	47	93	48
266	12/9/2016	06:15 PM	64	29	94	44
267	12/9/2016	06:30 PM	53	40	65	32
268	12/9/2016	06:45 PM	56	33	62	32
269	12/9/2016	07:00 PM	43	43	59	25
270	12/9/2016	07:15 PM	47	44	41	27
271	12/9/2016	07:30 PM	28	27	47	20
272	12/9/2016	07:45 PM	20	29	39	20
273	12/9/2016	08:00 PM	27	21	35	11
274	12/9/2016	08:15 PM	27	26	41	18
275	12/9/2016	08:30 PM	14	24	37	19
276	12/9/2016	08:45 PM	16	21	41	16
277	12/9/2016	09:00 PM	11	22	37	20
278	12/9/2016	09:15 PM	14	13	31	14
279	12/9/2016	09:30 PM	9	12	21	8
280	12/9/2016	09:45 PM	7	18	29	12
281	12/9/2016	10:00 PM	6	15	16	6
282	12/9/2016	10:15 PM	8	14	27	6
283	12/9/2016	10:30 PM	4	10	18	6
284	12/9/2016	10:45 PM	8	15	13	6
285	12/9/2016	11:00 PM	8	8	8	6
286	12/9/2016	11:15 PM	9	8	13	3
287	12/9/2016	11:30 PM	6	11	13	3
288	12/9/2016	11:45 PM	3	7	4	1
289	12/10/2016	12:00 AM	3	6	1	0
290	12/10/2016	12:15 AM	7	7	9	0
291	12/10/2016	12:30 AM	4	3	10	0
292	12/10/2016	12:45 AM	4	8	4	0
293	12/10/2016	01:00 AM	3	6	6	1
294	12/10/2016	01:15 AM	3	6	8	0
295	12/10/2016	01:30 AM	4	2	4	1
296	12/10/2016	01:45 AM	0	5	1	1
297	12/10/2016	02:00 AM	3	1	5	0
298	12/10/2016	02:15 AM	0	5	2	0
299	12/10/2016	02:30 AM	1	5	0	0
300	12/10/2016	02:45 AM	2	2	3	0
301	12/10/2016	03:00 AM	1	2	0	0

Appendix E Pre-Diet Traffic Counts: Arlington Road, Hillandale Road, Little Falls Parkway
ARLINGTON RD PRE ROAD DIET COUNTS

302	12/10/2016	03:15 AM	0	0	4	0
303	12/10/2016	03:30 AM	0	1	4	0
304	12/10/2016	03:45 AM	0	0	0	0
305	12/10/2016	04:00 AM	0	3	5	0
306	12/10/2016	04:15 AM	0	0	0	0
307	12/10/2016	04:30 AM	0	1	0	0
308	12/10/2016	04:45 AM	1	1	0	0
309	12/10/2016	05:00 AM	1	3	3	0
310	12/10/2016	05:15 AM	0	1	3	3
311	12/10/2016	05:30 AM	2	2	8	1
312	12/10/2016	05:45 AM	4	1	3	2
313	12/10/2016	06:00 AM	3	0	6	0
314	12/10/2016	06:15 AM	5	4	5	3
315	12/10/2016	06:30 AM	2	9	8	1
316	12/10/2016	06:45 AM	9	6	7	5
317	12/10/2016	07:00 AM	8	7	7	3
318	12/10/2016	07:15 AM	12	6	13	8
319	12/10/2016	07:30 AM	14	16	13	14
320	12/10/2016	07:45 AM	19	19	14	9
321	12/10/2016	08:00 AM	16	9	18	15
322	12/10/2016	08:15 AM	27	15	35	16
323	12/10/2016	08:30 AM	29	21	32	18
324	12/10/2016	08:45 AM	47	30	39	19
325	12/10/2016	09:00 AM	39	37	39	23
326	12/10/2016	09:15 AM	43	33	35	26
327	12/10/2016	09:30 AM	41	31	33	27
328	12/10/2016	09:45 AM	52	41	49	25
329	12/10/2016	10:00 AM	45	27	56	33
330	12/10/2016	10:15 AM	32	29	50	26
331	12/10/2016	10:30 AM	43	47	60	32
332	12/10/2016	10:45 AM	65	33	60	32
333	12/10/2016	11:00 AM	41	37	70	35
334	12/10/2016	11:15 AM	57	52	69	41
335	12/10/2016	11:30 AM	56	31	90	42
336	12/10/2016	11:45 AM	55	35	64	37
337	12/10/2016	12:00 PM	65	44	53	43
338	12/10/2016	12:15 PM	61	34	80	44
339	12/10/2016	12:30 PM	71	38	74	32
340	12/10/2016	12:45 PM	58	43	66	41
341	12/10/2016	01:00 PM	53	56	82	32
342	12/10/2016	01:15 PM	61	47	76	36
343	12/10/2016	01:30 PM	56	48	79	39
344	12/10/2016	01:45 PM	63	37	65	34
345	12/10/2016	02:00 PM	47	39	84	33
346	12/10/2016	02:15 PM	65	41	64	33
347	12/10/2016	02:30 PM	56	47	78	36
348	12/10/2016	02:45 PM	47	33	66	42
349	12/10/2016	03:00 PM	55	39	80	34
350	12/10/2016	03:15 PM	52	35	84	48
351	12/10/2016	03:30 PM	72	33	78	42
352	12/10/2016	03:45 PM	48	47	84	34
353	12/10/2016	04:00 PM	49	49	76	39

Appendix E Pre-Diet Traffic Counts: Arlington Road, Hillandale Road, Little Falls Parkway
ARLINGTON RD PRE ROAD DIET COUNTS

354	12/10/2016 04:15 PM	40	45	76	36
355	12/10/2016 04:30 PM	38	33	78	40
356	12/10/2016 04:45 PM	43	49	74	30
357	12/10/2016 05:00 PM	41	30	77	37
358	12/10/2016 05:15 PM	36	42	52	33
359	12/10/2016 05:30 PM	46	32	66	33
360	12/10/2016 05:45 PM	64	39	48	31
361	12/10/2016 06:00 PM	52	34	48	26
362	12/10/2016 06:15 PM	50	33	45	27
363	12/10/2016 06:30 PM	36	31	74	29
364	12/10/2016 06:45 PM	50	31	35	23
365	12/10/2016 07:00 PM	36	34	40	23
366	12/10/2016 07:15 PM	32	26	37	20
367	12/10/2016 07:30 PM	21	21	27	16
368	12/10/2016 07:45 PM	25	21	31	14
369	12/10/2016 08:00 PM	13	12	31	14
370	12/10/2016 08:15 PM	24	15	23	8
371	12/10/2016 08:30 PM	18	12	23	10
372	12/10/2016 08:45 PM	12	21	26	6
373	12/10/2016 09:00 PM	8	13	30	9
374	12/10/2016 09:15 PM	7	8	30	9
375	12/10/2016 09:30 PM	10	6	27	11
376	12/10/2016 09:45 PM	9	12	26	6
377	12/10/2016 10:00 PM	9	14	21	8
378	12/10/2016 10:15 PM	10	6	31	9
379	12/10/2016 10:30 PM	10	13	19	6
380	12/10/2016 10:45 PM	6	11	18	4
381	12/10/2016 11:00 PM	6	6	9	2
382	12/10/2016 11:15 PM	8	11	12	4
383	12/10/2016 11:30 PM	8	9	12	1
384	12/10/2016 11:45 PM	7	7	12	6
385	12/11/2016 12:00 AM	8	9	12	4
386	12/11/2016 12:15 AM	5	5	8	2
387	12/11/2016 12:30 AM	13	6	9	0
388	12/11/2016 12:45 AM	9	6	4	0
389	12/11/2016 01:00 AM	0	4	2	0
390	12/11/2016 01:15 AM	5	7	2	1
391	12/11/2016 01:30 AM	4	6	7	0
392	12/11/2016 01:45 AM	4	5	2	0
393	12/11/2016 02:00 AM	4	2	6	0
394	12/11/2016 02:15 AM	3	3	3	2
395	12/11/2016 02:30 AM	3	3	3	0
396	12/11/2016 02:45 AM	0	4	5	0
397	12/11/2016 03:00 AM	1	3	2	0
398	12/11/2016 03:15 AM	2	1	3	0
399	12/11/2016 03:30 AM	2	4	1	1
400	12/11/2016 03:45 AM	0	2	2	1
401	12/11/2016 04:00 AM	0	0	1	0
402	12/11/2016 04:15 AM	1	1	2	1
403	12/11/2016 04:30 AM	0	0	0	0
404	12/11/2016 04:45 AM	0	1	2	1
405	12/11/2016 05:00 AM	1	1	3	0

Appendix E Pre-Diet Traffic Counts: Arlington Road, Hillandale Road, Little Falls Parkway
ARLINGTON RD PRE ROAD DIET COUNTS

406	12/11/2016 05:15 AM	0	0	4	1
407	12/11/2016 05:30 AM	0	0	3	0
408	12/11/2016 05:45 AM	3	0	2	1
409	12/11/2016 06:00 AM	1	1	1	0
410	12/11/2016 06:15 AM	2	3	4	0
411	12/11/2016 06:30 AM	2	4	4	2
412	12/11/2016 06:45 AM	2	4	2	3
413	12/11/2016 07:00 AM	10	5	4	3
414	12/11/2016 07:15 AM	3	10	11	5
415	12/11/2016 07:30 AM	5	4	7	5
416	12/11/2016 07:45 AM	7	10	12	6
417	12/11/2016 08:00 AM	9	8	8	8
418	12/11/2016 08:15 AM	11	12	16	8
419	12/11/2016 08:30 AM	10	16	14	10
420	12/11/2016 08:45 AM	14	10	21	24
421	12/11/2016 09:00 AM	12	16	35	13
422	12/11/2016 09:15 AM	25	16	24	16
423	12/11/2016 09:30 AM	18	25	39	14
424	12/11/2016 09:45 AM	39	33	34	13
425	12/11/2016 10:00 AM	25	23	43	14
426	12/11/2016 10:15 AM	48	41	37	15
427	12/11/2016 10:30 AM	42	32	47	28
428	12/11/2016 10:45 AM	49	46	46	35
429	12/11/2016 11:00 AM	39	34	41	27
430	12/11/2016 11:15 AM	31	45	52	29
431	12/11/2016 11:30 AM	31	39	66	33
432	12/11/2016 11:45 AM	40	47	55	24
433	12/11/2016 12:00 PM	44	38	63	42
434	12/11/2016 12:15 PM	53	45	68	34
435	12/11/2016 12:30 PM	55	41	67	36
436	12/11/2016 12:45 PM	45	40	54	29
437	12/11/2016 01:00 PM	45	41	64	26
438	12/11/2016 01:15 PM	42	37	69	34
439	12/11/2016 01:30 PM	48	38	52	28
440	12/11/2016 01:45 PM	54	31	72	28
441	12/11/2016 02:00 PM	44	41	52	33
442	12/11/2016 02:15 PM	50	39	62	32
443	12/11/2016 02:30 PM	32	30	46	32
444	12/11/2016 02:45 PM	34	36	60	33
445	12/11/2016 03:00 PM	36	47	60	34
446	12/11/2016 03:15 PM	48	43	69	29
447	12/11/2016 03:30 PM	37	33	52	26
448	12/11/2016 03:45 PM	43	44	57	26
449	12/11/2016 04:00 PM	32	41	76	29
450	12/11/2016 04:15 PM	39	25	66	39
451	12/11/2016 04:30 PM	37	56	82	36
452	12/11/2016 04:45 PM	35	41	71	29
453	12/11/2016 05:00 PM	47	28	47	37
454	12/11/2016 05:15 PM	39	38	55	27
455	12/11/2016 05:30 PM	33	29	37	25
456	12/11/2016 05:45 PM	32	41	50	24
457	12/11/2016 06:00 PM	37	21	43	23

Appendix E Pre-Diet Traffic Counts: Arlington Road, Hillandale Road, Little Falls Parkway
ARLINGTON RD PRE ROAD DIET COUNTS

458	12/11/2016 06:15 PM	23	32	39	15
459	12/11/2016 06:30 PM	21	33	39	22
460	12/11/2016 06:45 PM	27	29	33	15
461	12/11/2016 07:00 PM	35	19	39	16
462	12/11/2016 07:15 PM	19	21	23	14
463	12/11/2016 07:30 PM	23	14	19	11
464	12/11/2016 07:45 PM	13	18	25	10
465	12/11/2016 08:00 PM	12	19	29	15
466	12/11/2016 08:15 PM	8	9	27	9
467	12/11/2016 08:30 PM	10	12	19	9
468	12/11/2016 08:45 PM	18	11	27	8
469	12/11/2016 09:00 PM	9	11	11	4
470	12/11/2016 09:15 PM	15	13	14	5
471	12/11/2016 09:30 PM	5	2	16	4
472	12/11/2016 09:45 PM	9	8	12	2
473	12/11/2016 10:00 PM	5	5	9	0
474	12/11/2016 10:15 PM	4	5	6	1
475	12/11/2016 10:30 PM	5	10	6	0
476	12/11/2016 10:45 PM	2	4	6	2
477	12/11/2016 11:00 PM	1	7	2	1
478	12/11/2016 11:15 PM	3	5	6	4
479	12/11/2016 11:30 PM	1	1	3	0
480	12/11/2016 11:45 PM	0	2	0	0
481	12/12/2016 12:00 AM	0	0	0	1
482	12/12/2016 12:15 AM	1	0	1	0
483	12/12/2016 12:30 AM	2	1	6	0
484	12/12/2016 12:45 AM	1	3	0	0
485	12/12/2016 01:00 AM	2	1	1	0
486	12/12/2016 01:15 AM	1	1	1	0
487	12/12/2016 01:30 AM	0	1	0	0
488	12/12/2016 01:45 AM	0	0	1	0
489	12/12/2016 02:00 AM	0	0	1	0
490	12/12/2016 02:15 AM	1	0	3	0
491	12/12/2016 02:30 AM	0	1	1	0
492	12/12/2016 02:45 AM	0	0	0	0
493	12/12/2016 03:00 AM	0	0	0	0
494	12/12/2016 03:15 AM	0	0	0	0
495	12/12/2016 03:30 AM	0	0	1	1
496	12/12/2016 03:45 AM	0	0	2	0
497	12/12/2016 04:00 AM	1	0	0	1
498	12/12/2016 04:15 AM	0	1	2	0
499	12/12/2016 04:30 AM	1	0	6	1
500	12/12/2016 04:45 AM	2	2	3	0
501	12/12/2016 05:00 AM	2	3	6	0
502	12/12/2016 05:15 AM	1	2	10	0
503	12/12/2016 05:30 AM	2	3	16	3
504	12/12/2016 05:45 AM	11	7	9	3
505	12/12/2016 06:00 AM	5	3	9	10
506	12/12/2016 06:15 AM	6	6	14	7
507	12/12/2016 06:30 AM	9	7	24	11
508	12/12/2016 06:45 AM	15	8	35	15
509	12/12/2016 07:00 AM	11	16	39	29

Appendix E Pre-Diet Traffic Counts: Arlington Road, Hillandale Road, Little Falls Parkway
ARLINGTON RD PRE ROAD DIET COUNTS

510	12/12/2016 07:15 AM	32	23	49	29
511	12/12/2016 07:30 AM	42	34	52	45
512	12/12/2016 07:45 AM	49	43	64	63
513	12/12/2016 08:00 AM	44	39	46	35
514	12/12/2016 08:15 AM	66	58	45	37
515	12/12/2016 08:30 AM	62	43	44	28
516	12/12/2016 08:45 AM	55	55	34	23
517	12/12/2016 09:00 AM	52	34	45	35
518	12/12/2016 09:15 AM	48	45	37	21
519	12/12/2016 09:30 AM	54	42	42	23
520	12/12/2016 09:45 AM	40	39	54	28
521	12/12/2016 10:00 AM	30	22	43	21
522	12/12/2016 10:15 AM	38	32	52	24
523	12/12/2016 10:30 AM	40	31	47	38
524	12/12/2016 10:45 AM	39	33	58	27
525	12/12/2016 11:00 AM	41	29	54	35
526	12/12/2016 11:15 AM	42	39	49	26
527	12/12/2016 11:30 AM	54	33	39	28
528	12/12/2016 11:45 AM	45	52	59	23
529	12/12/2016 12:00 PM	42	29	67	30
530	12/12/2016 12:15 PM	44	33	56	30
531	12/12/2016 12:30 PM	39	47	54	31
532	12/12/2016 12:45 PM	38	29	54	40
533	12/12/2016 01:00 PM	35	23	66	24
534	12/12/2016 01:15 PM	41	35	48	28
535	12/12/2016 01:30 PM	30	28	71	30
536	12/12/2016 01:45 PM	44	24	56	31
537	12/12/2016 02:00 PM	46	31	58	30
538	12/12/2016 02:15 PM	45	33	64	30
539	12/12/2016 02:30 PM	42	34	64	30
540	12/12/2016 02:45 PM	33	43	48	34
541	12/12/2016 03:00 PM	34	35	78	42
542	12/12/2016 03:15 PM	36	41	74	43
543	12/12/2016 03:30 PM	45	43	64	37
544	12/12/2016 03:45 PM	43	49	72	47
545	12/12/2016 04:00 PM	49	47	95	50
546	12/12/2016 04:15 PM	43	58	84	55
547	12/12/2016 04:30 PM	49	43	81	46
548	12/12/2016 04:45 PM	48	41	113	52
549	12/12/2016 05:00 PM	39	60	95	55
550	12/12/2016 05:15 PM	62	48	101	64
551	12/12/2016 05:30 PM	52	39	95	48
552	12/12/2016 05:45 PM	37	45	106	63
553	12/12/2016 06:00 PM	69	45	80	62
554	12/12/2016 06:15 PM	48	41	70	42
555	12/12/2016 06:30 PM	34	41	62	38
556	12/12/2016 06:45 PM	37	44	66	28
557	12/12/2016 07:00 PM	32	29	49	18
558	12/12/2016 07:15 PM	33	38	52	21
559	12/12/2016 07:30 PM	26	18	37	16
560	12/12/2016 07:45 PM	25	26	38	16
561	12/12/2016 08:00 PM	21	18	34	18

Appendix E Pre-Diet Traffic Counts: Arlington Road, Hillandale Road, Little Falls Parkway
ARLINGTON RD PRE ROAD DIET COUNTS

562	12/12/2016 08:15 PM	18	23	38	22
563	12/12/2016 08:30 PM	16	14	29	14
564	12/12/2016 08:45 PM	7	12	36	12
565	12/12/2016 09:00 PM	21	21	23	11
566	12/12/2016 09:15 PM	10	16	26	5
567	12/12/2016 09:30 PM	4	11	13	7
568	12/12/2016 09:45 PM	9	12	9	5
569	12/12/2016 10:00 PM	6	4	13	3
570	12/12/2016 10:15 PM	4	14	7	1
571	12/12/2016 10:30 PM	4	14	8	0
572	12/12/2016 10:45 PM	7	7	5	1
573	12/12/2016 11:00 PM	3	6	4	0
574	12/12/2016 11:15 PM	3	7	8	3
575	12/12/2016 11:30 PM	2	2	7	0
576	12/12/2016 11:45 PM	2	2	2	1
577	12/13/2016 12:00 AM	1	2	2	1
578	12/13/2016 12:15 AM	2	3	8	0
579	12/13/2016 12:30 AM	3	3	4	0
580	12/13/2016 12:45 AM	0	3	3	0
581	12/13/2016 01:00 AM	1	3	1	0
582	12/13/2016 01:15 AM	1	2	0	0
583	12/13/2016 01:30 AM	2	1	1	0
584	12/13/2016 01:45 AM	0	0	2	0
585	12/13/2016 02:00 AM	0	1	2	0
586	12/13/2016 02:15 AM	1	0	0	1
587	12/13/2016 02:30 AM	0	1	0	0
588	12/13/2016 02:45 AM	1	0	0	0
589	12/13/2016 03:00 AM	0	1	1	0
590	12/13/2016 03:15 AM	0	1	0	1
591	12/13/2016 03:30 AM	0	1	0	0
592	12/13/2016 03:45 AM	0	0	3	1
593	12/13/2016 04:00 AM	0	0	2	0
594	12/13/2016 04:15 AM	0	0	3	1
595	12/13/2016 04:30 AM	0	0	2	0
596	12/13/2016 04:45 AM	0	4	6	0
597	12/13/2016 05:00 AM	1	1	4	0
598	12/13/2016 05:15 AM	1	5	11	0
599	12/13/2016 05:30 AM	5	4	12	7
600	12/13/2016 05:45 AM	10	8	9	4
601	12/13/2016 06:00 AM	4	6	8	8
602	12/13/2016 06:15 AM	3	10	23	15
603	12/13/2016 06:30 AM	14	11	27	19
604	12/13/2016 06:45 AM	11	12	35	22
605	12/13/2016 07:00 AM	16	19	42	23
606	12/13/2016 07:15 AM	34	29	48	35
607	12/13/2016 07:30 AM	46	35	59	46
608	12/13/2016 07:45 AM	40	39	60	43
609	12/13/2016 08:00 AM	66	43	64	33
610	12/13/2016 08:15 AM	74	55	56	31
611	12/13/2016 08:30 AM	62	45	44	25
612	12/13/2016 08:45 AM	53	52	40	35
613	12/13/2016 09:00 AM	60	41	40	27

Appendix E Pre-Diet Traffic Counts: Arlington Road, Hillandale Road, Little Falls Parkway
ARLINGTON RD PRE ROAD DIET COUNTS

614	12/13/2016	09:15 AM	64	45	52	34
615	12/13/2016	09:30 AM	52	39	45	31
616	12/13/2016	09:45 AM	45	43	37	22
617	12/13/2016	10:00 AM	33	34	52	33
618	12/13/2016	10:15 AM	43	39	66	23
619	12/13/2016	10:30 AM	36	31	58	43
620	12/13/2016	10:45 AM	43	29	56	39
621	12/13/2016	11:00 AM	39	31	66	33
622	12/13/2016	11:15 AM	38	45	54	28
623	12/13/2016	11:30 AM	40	36	64	39
624	12/13/2016	11:45 AM	60	34	57	31
625	12/13/2016	12:00 PM	53	41	54	31
626	12/13/2016	12:15 PM	34	35	63	33
627	12/13/2016	12:30 PM	55	41	56	33
628	12/13/2016	12:45 PM	36	39	47	32
629	12/13/2016	01:00 PM	36	32	56	34
630	12/13/2016	01:15 PM	40	35	61	33
631	12/13/2016	01:30 PM	41	49	64	25
632	12/13/2016	01:45 PM	38	45	62	34
633	12/13/2016	02:00 PM	43	45	84	32
634	12/13/2016	02:15 PM	34	33	55	44
635	12/13/2016	02:30 PM	46	46	76	52
636	12/13/2016	02:45 PM	56	56	78	39
637	12/13/2016	03:00 PM	47	43	76	37
638	12/13/2016	03:15 PM	40	48	80	30
639	12/13/2016	03:30 PM	48	61	84	44
640	12/13/2016	03:45 PM	47	45	95	54
641	12/13/2016	04:00 PM	67	43	80	48
642	12/13/2016	04:15 PM	47	46	89	54
643	12/13/2016	04:30 PM	52	64	92	58
644	12/13/2016	04:45 PM	46	37	94	46
645	12/13/2016	05:00 PM	66	41	93	48
646	12/13/2016	05:15 PM	55	47	99	61
647	12/13/2016	05:30 PM	56	48	97	52
648	12/13/2016	05:45 PM	66	61	93	52
649	12/13/2016	06:00 PM	60	56	72	45
650	12/13/2016	06:15 PM	46	50	68	40
651	12/13/2016	06:30 PM	37	42	56	40
652	12/13/2016	06:45 PM	54	34	62	41
653	12/13/2016	07:00 PM	24	47	49	20
654	12/13/2016	07:15 PM	41	39	47	27
655	12/13/2016	07:30 PM	29	23	39	19
656	12/13/2016	07:45 PM	28	37	52	22
657	12/13/2016	08:00 PM	19	15	35	18
658	12/13/2016	08:15 PM	19	27	27	15
659	12/13/2016	08:30 PM	16	21	30	11
660	12/13/2016	08:45 PM	16	12	33	15
661	12/13/2016	09:00 PM	18	16	24	9
662	12/13/2016	09:15 PM	9	12	27	6
663	12/13/2016	09:30 PM	14	19	21	6
664	12/13/2016	09:45 PM	11	13	25	8
665	12/13/2016	10:00 PM	3	3	18	4

Appendix E Pre-Diet Traffic Counts: Arlington Road, Hillandale Road, Little Falls Parkway
ARLINGTON RD PRE ROAD DIET COUNTS

666	12/13/2016 10:15 PM	6	12	8	2
667	12/13/2016 10:30 PM	5	10	5	2
668	12/13/2016 10:45 PM	3	6	8	4
669	12/13/2016 11:00 PM	4	5	9	2
670	12/13/2016 11:15 PM	1	6	2	0
671	12/13/2016 11:30 PM	1	2	2	2
672	12/13/2016 11:45 PM	8	1	4	0

Appendix E Pre-Diet Traffic Counts: Arlington Road, Hillandale Road, Little Falls Parkway
HILLANDALE RD PRE ROAD DIET COUNTS

Volume

Start Date: 12/7/2016

Start Time: 12:00:00 AM

Location 1: HILLANDALE RD - 200FT N OF POOL ENT

Location 2:

MONTGOMERY

BETHESDA

Number	Date	Time	NB	SB
1	12/7/2016	12:00 AM	5	1
2	12/7/2016	12:15 AM	0	2
3	12/7/2016	12:30 AM	2	0
4	12/7/2016	12:45 AM	3	0
5	12/7/2016	01:00 AM	1	0
6	12/7/2016	01:15 AM	0	0
7	12/7/2016	01:30 AM	1	0
8	12/7/2016	01:45 AM	2	0
9	12/7/2016	02:00 AM	0	0
10	12/7/2016	02:15 AM	0	0
11	12/7/2016	02:30 AM	0	0
12	12/7/2016	02:45 AM	0	0
13	12/7/2016	03:00 AM	0	1
14	12/7/2016	03:15 AM	0	2
15	12/7/2016	03:30 AM	1	0
16	12/7/2016	03:45 AM	1	1
17	12/7/2016	04:00 AM	0	1
18	12/7/2016	04:15 AM	0	0
19	12/7/2016	04:30 AM	1	1
20	12/7/2016	04:45 AM	0	2
21	12/7/2016	05:00 AM	4	5
22	12/7/2016	05:15 AM	2	4
23	12/7/2016	05:30 AM	7	7
24	12/7/2016	05:45 AM	10	6
25	12/7/2016	06:00 AM	6	9
26	12/7/2016	06:15 AM	4	19
27	12/7/2016	06:30 AM	12	21
28	12/7/2016	06:45 AM	15	27
29	12/7/2016	07:00 AM	19	39
30	12/7/2016	07:15 AM	38	38
31	12/7/2016	07:30 AM	43	42
32	12/7/2016	07:45 AM	50	52
33	12/7/2016	08:00 AM	45	30
34	12/7/2016	08:15 AM	53	40
35	12/7/2016	08:30 AM	56	35
36	12/7/2016	08:45 AM	48	38
37	12/7/2016	09:00 AM	60	39
38	12/7/2016	09:15 AM	53	39
39	12/7/2016	09:30 AM	60	46
40	12/7/2016	09:45 AM	42	30
41	12/7/2016	10:00 AM	36	25

Appendix E Pre-Diet Traffic Counts: Arlington Road, Hillendale Road, Little Falls Parkway
HILLANDALE RD PRE ROAD DIET COUNTS

42	12/7/2016	10:15 AM	34	31
43	12/7/2016	10:30 AM	43	19
44	12/7/2016	10:45 AM	34	33
45	12/7/2016	11:00 AM	32	31
46	12/7/2016	11:15 AM	34	32
47	12/7/2016	11:30 AM	38	31
48	12/7/2016	11:45 AM	45	31
49	12/7/2016	12:00 PM	40	48
50	12/7/2016	12:15 PM	42	35
51	12/7/2016	12:30 PM	44	28
52	12/7/2016	12:45 PM	27	43
53	12/7/2016	01:00 PM	36	29
54	12/7/2016	01:15 PM	30	33
55	12/7/2016	01:30 PM	41	38
56	12/7/2016	01:45 PM	41	38
57	12/7/2016	02:00 PM	42	39
58	12/7/2016	02:15 PM	38	42
59	12/7/2016	02:30 PM	55	40
60	12/7/2016	02:45 PM	33	44
61	12/7/2016	03:00 PM	48	40
62	12/7/2016	03:15 PM	69	53
63	12/7/2016	03:30 PM	74	42
64	12/7/2016	03:45 PM	53	48
65	12/7/2016	04:00 PM	42	30
66	12/7/2016	04:15 PM	45	39
67	12/7/2016	04:30 PM	70	47
68	12/7/2016	04:45 PM	56	33
69	12/7/2016	05:00 PM	58	42
70	12/7/2016	05:15 PM	58	33
71	12/7/2016	05:30 PM	53	44
72	12/7/2016	05:45 PM	47	39
73	12/7/2016	06:00 PM	62	32
74	12/7/2016	06:15 PM	66	36
75	12/7/2016	06:30 PM	62	30
76	12/7/2016	06:45 PM	59	28
77	12/7/2016	07:00 PM	59	33
78	12/7/2016	07:15 PM	29	20
79	12/7/2016	07:30 PM	30	25
80	12/7/2016	07:45 PM	28	18
81	12/7/2016	08:00 PM	27	19
82	12/7/2016	08:15 PM	21	22
83	12/7/2016	08:30 PM	22	20
84	12/7/2016	08:45 PM	21	20
85	12/7/2016	09:00 PM	24	16
86	12/7/2016	09:15 PM	15	17
87	12/7/2016	09:30 PM	15	8
88	12/7/2016	09:45 PM	11	12
89	12/7/2016	10:00 PM	13	6
90	12/7/2016	10:15 PM	7	6
91	12/7/2016	10:30 PM	12	4
92	12/7/2016	10:45 PM	4	10
93	12/7/2016	11:00 PM	2	6

Appendix E Pre-Diet Traffic Counts: Arlington Road, Hillendale Road, Little Falls Parkway
HILLDALE RD PRE ROAD DIET COUNTS

94	12/7/2016	11:15 PM	6	6
95	12/7/2016	11:30 PM	0	2
96	12/7/2016	11:45 PM	3	1
97	12/8/2016	12:00 AM	4	2
98	12/8/2016	12:15 AM	4	0
99	12/8/2016	12:30 AM	4	0
100	12/8/2016	12:45 AM	1	1
101	12/8/2016	01:00 AM	2	0
102	12/8/2016	01:15 AM	0	2
103	12/8/2016	01:30 AM	1	1
104	12/8/2016	01:45 AM	0	0
105	12/8/2016	02:00 AM	0	0
106	12/8/2016	02:15 AM	0	0
107	12/8/2016	02:30 AM	0	0
108	12/8/2016	02:45 AM	0	0
109	12/8/2016	03:00 AM	0	0
110	12/8/2016	03:15 AM	0	2
111	12/8/2016	03:30 AM	0	0
112	12/8/2016	03:45 AM	0	2
113	12/8/2016	04:00 AM	1	0
114	12/8/2016	04:15 AM	1	2
115	12/8/2016	04:30 AM	1	1
116	12/8/2016	04:45 AM	1	3
117	12/8/2016	05:00 AM	1	1
118	12/8/2016	05:15 AM	6	4
119	12/8/2016	05:30 AM	6	5
120	12/8/2016	05:45 AM	10	4
121	12/8/2016	06:00 AM	4	12
122	12/8/2016	06:15 AM	6	16
123	12/8/2016	06:30 AM	4	19
124	12/8/2016	06:45 AM	18	32
125	12/8/2016	07:00 AM	30	29
126	12/8/2016	07:15 AM	34	40
127	12/8/2016	07:30 AM	41	42
128	12/8/2016	07:45 AM	44	62
129	12/8/2016	08:00 AM	48	25
130	12/8/2016	08:15 AM	58	32
131	12/8/2016	08:30 AM	48	42
132	12/8/2016	08:45 AM	57	38
133	12/8/2016	09:00 AM	58	28
134	12/8/2016	09:15 AM	45	36
135	12/8/2016	09:30 AM	49	30
136	12/8/2016	09:45 AM	30	37
137	12/8/2016	10:00 AM	38	32
138	12/8/2016	10:15 AM	24	30
139	12/8/2016	10:30 AM	28	31
140	12/8/2016	10:45 AM	31	32
141	12/8/2016	11:00 AM	31	29
142	12/8/2016	11:15 AM	39	25
143	12/8/2016	11:30 AM	55	25
144	12/8/2016	11:45 AM	35	37
145	12/8/2016	12:00 PM	33	45

Appendix E Pre-Diet Traffic Counts: Arlington Road, Hillendale Road, Little Falls Parkway
HILLANDALE RD PRE ROAD DIET COUNTS

146	12/8/2016	12:15 PM	44	27
147	12/8/2016	12:30 PM	35	34
148	12/8/2016	12:45 PM	38	29
149	12/8/2016	01:00 PM	42	29
150	12/8/2016	01:15 PM	32	30
151	12/8/2016	01:30 PM	44	27
152	12/8/2016	01:45 PM	38	38
153	12/8/2016	02:00 PM	54	21
154	12/8/2016	02:15 PM	41	33
155	12/8/2016	02:30 PM	48	33
156	12/8/2016	02:45 PM	40	39
157	12/8/2016	03:00 PM	64	34
158	12/8/2016	03:15 PM	72	48
159	12/8/2016	03:30 PM	76	38
160	12/8/2016	03:45 PM	58	42
161	12/8/2016	04:00 PM	63	26
162	12/8/2016	04:15 PM	62	39
163	12/8/2016	04:30 PM	69	35
164	12/8/2016	04:45 PM	54	39
165	12/8/2016	05:00 PM	66	37
166	12/8/2016	05:15 PM	62	31
167	12/8/2016	05:30 PM	45	33
168	12/8/2016	05:45 PM	45	35
169	12/8/2016	06:00 PM	53	38
170	12/8/2016	06:15 PM	63	31
171	12/8/2016	06:30 PM	42	26
172	12/8/2016	06:45 PM	52	29
173	12/8/2016	07:00 PM	52	41
174	12/8/2016	07:15 PM	44	17
175	12/8/2016	07:30 PM	35	19
176	12/8/2016	07:45 PM	31	28
177	12/8/2016	08:00 PM	26	21
178	12/8/2016	08:15 PM	20	8
179	12/8/2016	08:30 PM	14	17
180	12/8/2016	08:45 PM	19	11
181	12/8/2016	09:00 PM	23	25
182	12/8/2016	09:15 PM	17	9
183	12/8/2016	09:30 PM	15	14
184	12/8/2016	09:45 PM	17	15
185	12/8/2016	10:00 PM	13	8
186	12/8/2016	10:15 PM	9	8
187	12/8/2016	10:30 PM	5	9
188	12/8/2016	10:45 PM	9	7
189	12/8/2016	11:00 PM	6	2
190	12/8/2016	11:15 PM	5	4
191	12/8/2016	11:30 PM	3	4
192	12/8/2016	11:45 PM	6	3
193	12/9/2016	12:00 AM	2	0
194	12/9/2016	12:15 AM	6	8
195	12/9/2016	12:30 AM	3	4
196	12/9/2016	12:45 AM	1	1
197	12/9/2016	01:00 AM	1	0

Appendix E Pre-Diet Traffic Counts: Arlington Road, Hillandale Road, Little Falls Parkway
HILLANDALE RD PRE ROAD DIET COUNTS

198	12/9/2016	01:15 AM	0	0
199	12/9/2016	01:30 AM	1	0
200	12/9/2016	01:45 AM	1	0
201	12/9/2016	02:00 AM	0	0
202	12/9/2016	02:15 AM	0	1
203	12/9/2016	02:30 AM	0	0
204	12/9/2016	02:45 AM	3	2
205	12/9/2016	03:00 AM	0	1
206	12/9/2016	03:15 AM	1	1
207	12/9/2016	03:30 AM	0	0
208	12/9/2016	03:45 AM	4	1
209	12/9/2016	04:00 AM	1	1
210	12/9/2016	04:15 AM	0	1
211	12/9/2016	04:30 AM	1	0
212	12/9/2016	04:45 AM	0	5
213	12/9/2016	05:00 AM	4	1
214	12/9/2016	05:15 AM	3	5
215	12/9/2016	05:30 AM	5	9
216	12/9/2016	05:45 AM	13	7
217	12/9/2016	06:00 AM	5	5
218	12/9/2016	06:15 AM	6	19
219	12/9/2016	06:30 AM	14	13
220	12/9/2016	06:45 AM	11	34
221	12/9/2016	07:00 AM	17	30
222	12/9/2016	07:15 AM	42	38
223	12/9/2016	07:30 AM	30	44
224	12/9/2016	07:45 AM	36	59
225	12/9/2016	08:00 AM	55	42
226	12/9/2016	08:15 AM	66	22
227	12/9/2016	08:30 AM	56	31
228	12/9/2016	08:45 AM	66	24
229	12/9/2016	09:00 AM	45	43
230	12/9/2016	09:15 AM	43	42
231	12/9/2016	09:30 AM	37	36
232	12/9/2016	09:45 AM	37	29
233	12/9/2016	10:00 AM	34	28
234	12/9/2016	10:15 AM	26	23
235	12/9/2016	10:30 AM	44	24
236	12/9/2016	10:45 AM	35	27
237	12/9/2016	11:00 AM	43	31
238	12/9/2016	11:15 AM	36	28
239	12/9/2016	11:30 AM	42	27
240	12/9/2016	11:45 AM	42	34
241	12/9/2016	12:00 PM	52	34
242	12/9/2016	12:15 PM	37	31
243	12/9/2016	12:30 PM	37	33
244	12/9/2016	12:45 PM	40	46
245	12/9/2016	01:00 PM	43	38
246	12/9/2016	01:15 PM	40	21
247	12/9/2016	01:30 PM	27	29
248	12/9/2016	01:45 PM	47	27
249	12/9/2016	02:00 PM	56	33

Appendix E Pre-Diet Traffic Counts: Arlington Road, Hillandale Road, Little Falls Parkway
HILLANDALE RD PRE ROAD DIET COUNTS

250	12/9/2016	02:15 PM	50	33
251	12/9/2016	02:30 PM	72	44
252	12/9/2016	02:45 PM	56	47
253	12/9/2016	03:00 PM	55	32
254	12/9/2016	03:15 PM	67	51
255	12/9/2016	03:30 PM	73	49
256	12/9/2016	03:45 PM	69	39
257	12/9/2016	04:00 PM	56	28
258	12/9/2016	04:15 PM	62	36
259	12/9/2016	04:30 PM	62	36
260	12/9/2016	04:45 PM	58	48
261	12/9/2016	05:00 PM	60	42
262	12/9/2016	05:15 PM	58	38
263	12/9/2016	05:30 PM	58	51
264	12/9/2016	05:45 PM	60	42
265	12/9/2016	06:00 PM	56	34
266	12/9/2016	06:15 PM	61	36
267	12/9/2016	06:30 PM	55	41
268	12/9/2016	06:45 PM	36	20
269	12/9/2016	07:00 PM	48	26
270	12/9/2016	07:15 PM	37	26
271	12/9/2016	07:30 PM	44	25
272	12/9/2016	07:45 PM	30	24
273	12/9/2016	08:00 PM	31	21
274	12/9/2016	08:15 PM	19	21
275	12/9/2016	08:30 PM	17	22
276	12/9/2016	08:45 PM	17	17
277	12/9/2016	09:00 PM	17	19
278	12/9/2016	09:15 PM	20	16
279	12/9/2016	09:30 PM	17	14
280	12/9/2016	09:45 PM	16	11
281	12/9/2016	10:00 PM	12	9
282	12/9/2016	10:15 PM	18	10
283	12/9/2016	10:30 PM	16	11
284	12/9/2016	10:45 PM	14	12
285	12/9/2016	11:00 PM	10	15
286	12/9/2016	11:15 PM	11	4
287	12/9/2016	11:30 PM	12	7
288	12/9/2016	11:45 PM	5	4
289	12/10/2016	12:00 AM	6	3
290	12/10/2016	12:15 AM	7	3
291	12/10/2016	12:30 AM	2	3
292	12/10/2016	12:45 AM	10	6
293	12/10/2016	01:00 AM	3	1
294	12/10/2016	01:15 AM	4	1
295	12/10/2016	01:30 AM	0	1
296	12/10/2016	01:45 AM	3	0
297	12/10/2016	02:00 AM	3	0
298	12/10/2016	02:15 AM	1	0
299	12/10/2016	02:30 AM	2	1
300	12/10/2016	02:45 AM	1	1
301	12/10/2016	03:00 AM	3	2

Appendix E Pre-Diet Traffic Counts: Arlington Road, Hillendale Road, Little Falls Parkway
HILLANDALE RD PRE ROAD DIET COUNTS

302	12/10/2016	03:15 AM	0	0
303	12/10/2016	03:30 AM	0	1
304	12/10/2016	03:45 AM	2	1
305	12/10/2016	04:00 AM	0	0
306	12/10/2016	04:15 AM	0	0
307	12/10/2016	04:30 AM	1	0
308	12/10/2016	04:45 AM	0	2
309	12/10/2016	05:00 AM	1	1
310	12/10/2016	05:15 AM	0	1
311	12/10/2016	05:30 AM	3	1
312	12/10/2016	05:45 AM	1	2
313	12/10/2016	06:00 AM	5	2
314	12/10/2016	06:15 AM	2	6
315	12/10/2016	06:30 AM	5	7
316	12/10/2016	06:45 AM	7	10
317	12/10/2016	07:00 AM	5	6
318	12/10/2016	07:15 AM	5	11
319	12/10/2016	07:30 AM	18	16
320	12/10/2016	07:45 AM	17	18
321	12/10/2016	08:00 AM	17	12
322	12/10/2016	08:15 AM	12	19
323	12/10/2016	08:30 AM	22	24
324	12/10/2016	08:45 AM	24	47
325	12/10/2016	09:00 AM	46	34
326	12/10/2016	09:15 AM	37	32
327	12/10/2016	09:30 AM	36	34
328	12/10/2016	09:45 AM	35	31
329	12/10/2016	10:00 AM	46	27
330	12/10/2016	10:15 AM	37	38
331	12/10/2016	10:30 AM	42	43
332	12/10/2016	10:45 AM	54	42
333	12/10/2016	11:00 AM	49	42
334	12/10/2016	11:15 AM	51	30
335	12/10/2016	11:30 AM	38	35
336	12/10/2016	11:45 AM	62	34
337	12/10/2016	12:00 PM	68	48
338	12/10/2016	12:15 PM	72	47
339	12/10/2016	12:30 PM	58	32
340	12/10/2016	12:45 PM	54	37
341	12/10/2016	01:00 PM	61	30
342	12/10/2016	01:15 PM	58	41
343	12/10/2016	01:30 PM	50	43
344	12/10/2016	01:45 PM	46	34
345	12/10/2016	02:00 PM	58	53
346	12/10/2016	02:15 PM	52	31
347	12/10/2016	02:30 PM	42	37
348	12/10/2016	02:45 PM	61	52
349	12/10/2016	03:00 PM	63	41
350	12/10/2016	03:15 PM	39	39
351	12/10/2016	03:30 PM	51	41
352	12/10/2016	03:45 PM	55	39
353	12/10/2016	04:00 PM	43	47

Appendix E Pre-Diet Traffic Counts: Arlington Road, Hillandale Road, Little Falls Parkway
HILLANDALE RD PRE ROAD DIET COUNTS

354	12/10/2016	04:15 PM	47	30
355	12/10/2016	04:30 PM	53	28
356	12/10/2016	04:45 PM	42	43
357	12/10/2016	05:00 PM	37	34
358	12/10/2016	05:15 PM	44	22
359	12/10/2016	05:30 PM	38	28
360	12/10/2016	05:45 PM	40	33
361	12/10/2016	06:00 PM	42	40
362	12/10/2016	06:15 PM	46	27
363	12/10/2016	06:30 PM	41	24
364	12/10/2016	06:45 PM	24	22
365	12/10/2016	07:00 PM	37	25
366	12/10/2016	07:15 PM	21	20
367	12/10/2016	07:30 PM	19	21
368	12/10/2016	07:45 PM	19	19
369	12/10/2016	08:00 PM	19	23
370	12/10/2016	08:15 PM	21	12
371	12/10/2016	08:30 PM	13	15
372	12/10/2016	08:45 PM	20	8
373	12/10/2016	09:00 PM	13	10
374	12/10/2016	09:15 PM	13	21
375	12/10/2016	09:30 PM	16	14
376	12/10/2016	09:45 PM	11	22
377	12/10/2016	10:00 PM	10	16
378	12/10/2016	10:15 PM	14	12
379	12/10/2016	10:30 PM	14	5
380	12/10/2016	10:45 PM	16	11
381	12/10/2016	11:00 PM	14	6
382	12/10/2016	11:15 PM	7	6
383	12/10/2016	11:30 PM	15	4
384	12/10/2016	11:45 PM	6	4
385	12/11/2016	12:00 AM	14	8
386	12/11/2016	12:15 AM	6	7
387	12/11/2016	12:30 AM	5	4
388	12/11/2016	12:45 AM	6	1
389	12/11/2016	01:00 AM	5	3
390	12/11/2016	01:15 AM	1	3
391	12/11/2016	01:30 AM	4	3
392	12/11/2016	01:45 AM	4	1
393	12/11/2016	02:00 AM	3	1
394	12/11/2016	02:15 AM	2	1
395	12/11/2016	02:30 AM	2	1
396	12/11/2016	02:45 AM	1	0
397	12/11/2016	03:00 AM	2	4
398	12/11/2016	03:15 AM	1	0
399	12/11/2016	03:30 AM	1	1
400	12/11/2016	03:45 AM	1	0
401	12/11/2016	04:00 AM	0	1
402	12/11/2016	04:15 AM	0	0
403	12/11/2016	04:30 AM	2	3
404	12/11/2016	04:45 AM	1	2
405	12/11/2016	05:00 AM	0	1

Appendix E Pre-Diet Traffic Counts: Arlington Road, Hillendale Road, Little Falls Parkway
HILLANDALE RD PRE ROAD DIET COUNTS

406	12/11/2016	05:15 AM	3	1
407	12/11/2016	05:30 AM	0	3
408	12/11/2016	05:45 AM	1	3
409	12/11/2016	06:00 AM	4	6
410	12/11/2016	06:15 AM	3	2
411	12/11/2016	06:30 AM	2	7
412	12/11/2016	06:45 AM	6	5
413	12/11/2016	07:00 AM	4	2
414	12/11/2016	07:15 AM	4	8
415	12/11/2016	07:30 AM	10	6
416	12/11/2016	07:45 AM	2	13
417	12/11/2016	08:00 AM	12	5
418	12/11/2016	08:15 AM	14	17
419	12/11/2016	08:30 AM	10	19
420	12/11/2016	08:45 AM	33	16
421	12/11/2016	09:00 AM	22	21
422	12/11/2016	09:15 AM	24	17
423	12/11/2016	09:30 AM	25	27
424	12/11/2016	09:45 AM	21	24
425	12/11/2016	10:00 AM	35	27
426	12/11/2016	10:15 AM	29	21
427	12/11/2016	10:30 AM	29	25
428	12/11/2016	10:45 AM	35	30
429	12/11/2016	11:00 AM	33	33
430	12/11/2016	11:15 AM	31	26
431	12/11/2016	11:30 AM	53	33
432	12/11/2016	11:45 AM	51	39
433	12/11/2016	12:00 PM	42	30
434	12/11/2016	12:15 PM	44	19
435	12/11/2016	12:30 PM	42	32
436	12/11/2016	12:45 PM	39	38
437	12/11/2016	01:00 PM	47	30
438	12/11/2016	01:15 PM	47	24
439	12/11/2016	01:30 PM	41	34
440	12/11/2016	01:45 PM	31	31
441	12/11/2016	02:00 PM	41	30
442	12/11/2016	02:15 PM	35	37
443	12/11/2016	02:30 PM	52	29
444	12/11/2016	02:45 PM	50	36
445	12/11/2016	03:00 PM	33	33
446	12/11/2016	03:15 PM	24	25
447	12/11/2016	03:30 PM	37	30
448	12/11/2016	03:45 PM	35	33
449	12/11/2016	04:00 PM	26	42
450	12/11/2016	04:15 PM	35	36
451	12/11/2016	04:30 PM	29	34
452	12/11/2016	04:45 PM	33	27
453	12/11/2016	05:00 PM	43	31
454	12/11/2016	05:15 PM	26	30
455	12/11/2016	05:30 PM	36	21
456	12/11/2016	05:45 PM	35	31
457	12/11/2016	06:00 PM	42	27

Appendix E Pre-Diet Traffic Counts: Arlington Road, Hillendale Road, Little Falls Parkway
HILLANDALE RD PRE ROAD DIET COUNTS

458	12/11/2016	06:15 PM	24	28
459	12/11/2016	06:30 PM	25	23
460	12/11/2016	06:45 PM	20	34
461	12/11/2016	07:00 PM	26	27
462	12/11/2016	07:15 PM	14	18
463	12/11/2016	07:30 PM	31	6
464	12/11/2016	07:45 PM	21	6
465	12/11/2016	08:00 PM	17	14
466	12/11/2016	08:15 PM	12	8
467	12/11/2016	08:30 PM	17	17
468	12/11/2016	08:45 PM	8	6
469	12/11/2016	09:00 PM	25	14
470	12/11/2016	09:15 PM	32	8
471	12/11/2016	09:30 PM	14	9
472	12/11/2016	09:45 PM	8	6
473	12/11/2016	10:00 PM	9	5
474	12/11/2016	10:15 PM	6	4
475	12/11/2016	10:30 PM	1	5
476	12/11/2016	10:45 PM	7	4
477	12/11/2016	11:00 PM	3	1
478	12/11/2016	11:15 PM	3	1
479	12/11/2016	11:30 PM	2	0
480	12/11/2016	11:45 PM	3	6
481	12/12/2016	12:00 AM	2	0
482	12/12/2016	12:15 AM	4	0
483	12/12/2016	12:30 AM	2	2
484	12/12/2016	12:45 AM	0	1
485	12/12/2016	01:00 AM	1	2
486	12/12/2016	01:15 AM	3	1
487	12/12/2016	01:30 AM	1	1
488	12/12/2016	01:45 AM	0	0
489	12/12/2016	02:00 AM	1	0
490	12/12/2016	02:15 AM	1	0
491	12/12/2016	02:30 AM	0	0
492	12/12/2016	02:45 AM	0	0
493	12/12/2016	03:00 AM	0	0
494	12/12/2016	03:15 AM	0	0
495	12/12/2016	03:30 AM	0	0
496	12/12/2016	03:45 AM	0	1
497	12/12/2016	04:00 AM	0	0
498	12/12/2016	04:15 AM	0	2
499	12/12/2016	04:30 AM	0	1
500	12/12/2016	04:45 AM	0	7
501	12/12/2016	05:00 AM	4	2
502	12/12/2016	05:15 AM	2	3
503	12/12/2016	05:30 AM	6	7
504	12/12/2016	05:45 AM	9	6
505	12/12/2016	06:00 AM	7	8
506	12/12/2016	06:15 AM	8	11
507	12/12/2016	06:30 AM	9	21
508	12/12/2016	06:45 AM	18	34
509	12/12/2016	07:00 AM	22	36

Appendix E Pre-Diet Traffic Counts: Arlington Road, Hillendale Road, Little Falls Parkway
HILLANDALE RD PRE ROAD DIET COUNTS

510	12/12/2016	07:15 AM	40	37
511	12/12/2016	07:30 AM	21	44
512	12/12/2016	07:45 AM	47	55
513	12/12/2016	08:00 AM	51	38
514	12/12/2016	08:15 AM	56	24
515	12/12/2016	08:30 AM	47	33
516	12/12/2016	08:45 AM	47	32
517	12/12/2016	09:00 AM	55	27
518	12/12/2016	09:15 AM	30	29
519	12/12/2016	09:30 AM	36	38
520	12/12/2016	09:45 AM	29	23
521	12/12/2016	10:00 AM	41	23
522	12/12/2016	10:15 AM	38	35
523	12/12/2016	10:30 AM	39	31
524	12/12/2016	10:45 AM	35	18
525	12/12/2016	11:00 AM	34	25
526	12/12/2016	11:15 AM	27	22
527	12/12/2016	11:30 AM	31	24
528	12/12/2016	11:45 AM	32	20
529	12/12/2016	12:00 PM	35	36
530	12/12/2016	12:15 PM	39	25
531	12/12/2016	12:30 PM	38	29
532	12/12/2016	12:45 PM	32	38
533	12/12/2016	01:00 PM	30	30
534	12/12/2016	01:15 PM	33	27
535	12/12/2016	01:30 PM	35	21
536	12/12/2016	01:45 PM	45	35
537	12/12/2016	02:00 PM	41	28
538	12/12/2016	02:15 PM	42	26
539	12/12/2016	02:30 PM	50	40
540	12/12/2016	02:45 PM	41	44
541	12/12/2016	03:00 PM	48	35
542	12/12/2016	03:15 PM	55	42
543	12/12/2016	03:30 PM	74	32
544	12/12/2016	03:45 PM	61	41
545	12/12/2016	04:00 PM	56	37
546	12/12/2016	04:15 PM	48	39
547	12/12/2016	04:30 PM	61	38
548	12/12/2016	04:45 PM	53	41
549	12/12/2016	05:00 PM	46	29
550	12/12/2016	05:15 PM	55	49
551	12/12/2016	05:30 PM	50	35
552	12/12/2016	05:45 PM	48	41
553	12/12/2016	06:00 PM	47	37
554	12/12/2016	06:15 PM	57	35
555	12/12/2016	06:30 PM	38	33
556	12/12/2016	06:45 PM	36	34
557	12/12/2016	07:00 PM	34	34
558	12/12/2016	07:15 PM	37	21
559	12/12/2016	07:30 PM	26	19
560	12/12/2016	07:45 PM	30	22
561	12/12/2016	08:00 PM	28	31

Appendix E Pre-Diet Traffic Counts: Arlington Road, Hillandale Road, Little Falls Parkway
HILLANDALE RD PRE ROAD DIET COUNTS

562	12/12/2016	08:15 PM	25	24
563	12/12/2016	08:30 PM	20	15
564	12/12/2016	08:45 PM	12	13
565	12/12/2016	09:00 PM	17	7
566	12/12/2016	09:15 PM	14	20
567	12/12/2016	09:30 PM	7	10
568	12/12/2016	09:45 PM	8	7
569	12/12/2016	10:00 PM	15	7
570	12/12/2016	10:15 PM	9	4
571	12/12/2016	10:30 PM	6	4
572	12/12/2016	10:45 PM	3	4
573	12/12/2016	11:00 PM	5	3
574	12/12/2016	11:15 PM	6	4
575	12/12/2016	11:30 PM	2	2
576	12/12/2016	11:45 PM	2	4
577	12/13/2016	12:00 AM	0	0
578	12/13/2016	12:15 AM	1	0
579	12/13/2016	12:30 AM	3	0
580	12/13/2016	12:45 AM	1	1
581	12/13/2016	01:00 AM	1	2
582	12/13/2016	01:15 AM	2	1
583	12/13/2016	01:30 AM	1	0
584	12/13/2016	01:45 AM	0	0
585	12/13/2016	02:00 AM	0	0
586	12/13/2016	02:15 AM	2	0
587	12/13/2016	02:30 AM	0	0
588	12/13/2016	02:45 AM	0	2
589	12/13/2016	03:00 AM	0	0
590	12/13/2016	03:15 AM	0	0
591	12/13/2016	03:30 AM	0	0
592	12/13/2016	03:45 AM	1	0
593	12/13/2016	04:00 AM	0	1
594	12/13/2016	04:15 AM	0	1
595	12/13/2016	04:30 AM	2	1
596	12/13/2016	04:45 AM	0	4
597	12/13/2016	05:00 AM	6	2
598	12/13/2016	05:15 AM	3	6
599	12/13/2016	05:30 AM	6	4
600	12/13/2016	05:45 AM	6	5
601	12/13/2016	06:00 AM	7	8
602	12/13/2016	06:15 AM	8	11
603	12/13/2016	06:30 AM	9	19
604	12/13/2016	06:45 AM	16	30
605	12/13/2016	07:00 AM	23	27
606	12/13/2016	07:15 AM	39	37
607	12/13/2016	07:30 AM	36	47
608	12/13/2016	07:45 AM	45	72
609	12/13/2016	08:00 AM	60	35
610	12/13/2016	08:15 AM	50	28
611	12/13/2016	08:30 AM	51	36
612	12/13/2016	08:45 AM	61	33
613	12/13/2016	09:00 AM	62	24

Appendix E Pre-Diet Traffic Counts: Arlington Road, Hillendale Road, Little Falls Parkway
HILLANDALE RD PRE ROAD DIET COUNTS

614	12/13/2016	09:15 AM	38	36
615	12/13/2016	09:30 AM	43	22
616	12/13/2016	09:45 AM	43	20
617	12/13/2016	10:00 AM	34	22
618	12/13/2016	10:15 AM	30	27
619	12/13/2016	10:30 AM	43	31
620	12/13/2016	10:45 AM	26	36
621	12/13/2016	11:00 AM	45	35
622	12/13/2016	11:15 AM	40	35
623	12/13/2016	11:30 AM	31	34
624	12/13/2016	11:45 AM	41	36
625	12/13/2016	12:00 PM	56	38
626	12/13/2016	12:15 PM	38	37
627	12/13/2016	12:30 PM	37	20
628	12/13/2016	12:45 PM	33	26
629	12/13/2016	01:00 PM	42	29
630	12/13/2016	01:15 PM	38	36
631	12/13/2016	01:30 PM	30	34
632	12/13/2016	01:45 PM	22	39
633	12/13/2016	02:00 PM	38	39
634	12/13/2016	02:15 PM	35	30
635	12/13/2016	02:30 PM	58	45
636	12/13/2016	02:45 PM	64	55
637	12/13/2016	03:00 PM	50	44
638	12/13/2016	03:15 PM	71	44
639	12/13/2016	03:30 PM	74	47
640	12/13/2016	03:45 PM	60	48
641	12/13/2016	04:00 PM	52	39
642	12/13/2016	04:15 PM	42	50
643	12/13/2016	04:30 PM	52	46
644	12/13/2016	04:45 PM	62	41
645	12/13/2016	05:00 PM	46	43
646	12/13/2016	05:15 PM	49	36
647	12/13/2016	05:30 PM	64	48
648	12/13/2016	05:45 PM	49	48
649	12/13/2016	06:00 PM	44	37
650	12/13/2016	06:15 PM	61	28
651	12/13/2016	06:30 PM	35	41
652	12/13/2016	06:45 PM	45	30
653	12/13/2016	07:00 PM	49	22
654	12/13/2016	07:15 PM	42	36
655	12/13/2016	07:30 PM	33	12
656	12/13/2016	07:45 PM	23	21
657	12/13/2016	08:00 PM	27	17
658	12/13/2016	08:15 PM	23	12
659	12/13/2016	08:30 PM	10	22
660	12/13/2016	08:45 PM	22	24
661	12/13/2016	09:00 PM	26	10
662	12/13/2016	09:15 PM	13	13
663	12/13/2016	09:30 PM	11	11
664	12/13/2016	09:45 PM	13	8
665	12/13/2016	10:00 PM	7	12

Appendix E Pre-Diet Traffic Counts: Arlington Road, Hillandale Road, Little Falls Parkway
HILLANDALE RD PRE ROAD DIET COUNTS

666	12/13/2016	10:15 PM	8	6
667	12/13/2016	10:30 PM	13	1
668	12/13/2016	10:45 PM	5	6
669	12/13/2016	11:00 PM	5	5
670	12/13/2016	11:15 PM	5	1
671	12/13/2016	11:30 PM	1	1
672	12/13/2016	11:45 PM	3	2

Appendix E Pre-Diet Traffic Counts: Arlington Road, Hillandale Road, Little Falls Parkway
LITTLE FALLS PARKWAY PRE ROAD DIET COUNTS

Volume
Start Date: 12/7/2016
Start Time: 12:00:00 AM
Location 1: LITTLE FALLS PKWY B/W ARLGINTON RD &
Location 2: HILLANDALE RD

MONTGOMERY
BETHESDA

	Wednesday 12/7/2016				Thursday 12/8/2017				Friday 12/9/2017				Saturday 12/10/2017				Sunday 12/11/2017				Monday 12/12/2017				Tuesday 12/13/2018				Average Weekday Tu/Wed/Thur			
Time	NB L1	NB L2	SB L3	SB L4	NB L1	NB L2	SB L3	SB L4	NB L1	NB L2	SB L3	SB L4	NB L1	NB L2	SB L3	SB L4	NB L1	NB L2	SB L3	SB L4	NB L1	NB L2	SB L3	SB L4	NB L1	NB L2	SB L3	SB L4	NB L1	NB L2	SB L3	SB L4
12:00 AM	8	1	2	0	2	2	3	1	3	2	1	2	12	1	7	0	15	9	15	5	1	0	0	0	1	1	4	0	4	1	3	0
12:15 AM	4	3	1	1	3	2	3	0	4	6	3	2	15	4	2	2	13	2	16	7	0	1	0	2	3	1	2	2	1	3	2	1
12:30 AM	1	1	3	0	2	1	5	0	9	3	4	4	7	3	9	0	19	6	11	2	3	0	3	0	4	3	9	1	2	2	6	0
12:45 AM	3	5	4	0	2	0	2	1	5	2	4	1	13	4	10	3	16	3	12	2	4	2	4	0	4	0	2	2	3	2	3	1
01:00 AM	4	1	3	0	2	2	0	0	2	3	2	0	8	4	3	1	6	4	6	1	2	1	0	0	6	1	4	0	4	1	2	0
01:15 AM	0	1	0	0	3	1	1	1	3	0	3	0	11	3	6	4	14	1	5	1	1	1	1	0	2	3	1	0	2	2	1	0
01:30 AM	1	0	3	1	5	3	4	2	3	2	3	2	5	3	6	2	9	0	3	1	1	1	1	0	3	0	0	0	3	1	2	1
01:45 AM	0	0	0	0	1	1	2	0	1	0	4	0	8	5	3	1	12	3	6	0	0	1	1	0	0	0	0	1	0	0	1	0
02:00 AM	0	2	0	1	0	0	2	0	2	1	2	0	4	0	1	2	5	2	4	1	0	0	0	0	1	0	1	0	0	1	1	0
02:15 AM	0	1	0	0	3	0	1	0	1	2	0	0	5	0	6	0	6	2	4	2	2	1	1	0	1	0	2	1	1	0	1	0
02:30 AM	1	0	0	0	0	0	2	0	3	0	1	0	7	1	1	1	7	1	4	1	1	0	3	1	1	0	0	1	1	0	1	0
02:45 AM	0	0	1	0	1	0	0	0	1	1	0	0	6	2	1	0	4	1	3	2	0	0	1	1	1	0	0	0	1	0	0	0
03:00 AM	0	0	0	1	2	1	1	0	2	2	0	0	1	0	3	0	5	0	5	1	0	0	0	0	1	0	0	0	1	0	0	0
03:15 AM	1	0	1	0	0	1	1	0	1	0	1	0	0	0	0	0	4	0	2	0	0	0	0	0	1	0	1	0	1	0	1	0
03:30 AM	0	0	1	0	0	0	1	1	0	2	0	0	1	0	3	2	4	3	2	1	0	0	0	0	1	0	0	0	1	0	0	1
03:45 AM	1	0	2	0	0	0	2	0	2	0	2	0	2	0	4	0	2	0	1	0	0	1	1	0	0	0	0	0	0	0	1	0
04:00 AM	0	0	1	0	0	0	0	1	1	1	2	0	3	2	0	0	1	1	2	2	1	0	2	2	0	0	6	1	0	0	2	1
04:15 AM	1	1	1	0	1	0	4	1	1	0	1	0	0	1	5	3	2	1	1	0	1	0	1	0	2	2	3	0	1	1	3	0
04:30 AM	1	1	2	1	1	0	2	2	2	1	2	1	1	1	0	2	0	1	2	1	1	0	5	1	0	0	2	2	1	0	2	2
04:45 AM	4	0	3	2	4	4	4	0	3	0	5	3	2	0	0	1	1	3	2	0	4	0	4	4	4	0	1	3	4	1	3	2
05:00 AM	1	0	4	3	3	1	4	0	2	2	3	4	4	1	1	0	2	0	1	1	5	1	5	2	3	1	6	3	2	1	5	2
05:15 AM	7	0	6	4	6	2	5	6	7	2	6	1	1	0	1	2	0	0	1	2	4	0	2	6	7	1	6	1	7	1	6	4
05:30 AM	15	3	11	7	9	1	10	8	21	3	11	6	4	2	2	6	0	0	4	2	4	1	10	4	9	1	8	5	11	2	10	7
05:45 AM	17	3	8	8	15	0	12	5	18	3	8	10	5	2	4	8	3	0	2	2	18	3	14	12	16	3	12	14	16	2	11	9
06:00 AM	8	1	10	5	9	3	12	8	8	0	15	3	6	0	3	3	2	0	2	2	9	2	12	7	12	1	10	6	10	2	11	6
06:15 AM	14	4	26	10	9	2	18	9	12	3	12	9	8	0	7	4	6	1	0	2	14	3	16	12	14	2	18	12	12	3	21	10
06:30 AM	32	6	20	9	24	6	34	14	16	5	32	22	12	1	6	2	5	1	4	1	15	3	26	17	26	3	38	22	27	5	31	15
06:45 AM	20	7	36	28	23	10	40	26	19	5	37	23	14	1	12	3	5	2	5	6	22	7	39	20	26	11	48	24	23	9	41	26
07:00 AM	28	7	60	36	22	10	62	41	33	12	62	36	14	3	9	8	15	1	2	2	30	5	68	37	31	12	72	42	27	10	65	40
07:15 AM	59	14	99	66	53	18	94	70	53	12	96	72	20	6	12	8	11	0	5	9	57	16	91	64	58	22	91	56	57	18	95	64
07:30 AM	72	25	120	81	74	30	116	101	73	25	90	92	30	6	18	12	8	0	12	11	79	23	95	78	84	18	112	82	77	24	116	88
07:45 AM	100	31	92	82	77	35	120	106	78	32	124	90	34	6	26	17	16	2	9	6	88	32	110	96	82	34	106	102	86	33	106	97
08:00 AM	97	33	98	66	91	38	102	72	87	39	113	82	21	4	21	14	18	3	16	16	82	32	112	90	108	43	114	80	99	38	105	73
08:15 AM	108	48	124	92	108	47	94	70	114	40	81	68	43	8	25	26	21	5	16	7	112	36	98	74	111	32	119	74	109	42	112	79
08:30 AM	99	38	104	76	108	35	93	66	108	31	101	66	50	12	39	27	28	3	21	20	87	37	118	79	100	39	94	76	102	37	97	73
08:45 AM	124	36	94	90	119	52	105	79	110	30	92	70	63	14	47	33	22	6	34	22	112	23	86	72	102	28	80	60	115	39	93	76
09:00 AM	90	29	95	68	103	35	88	64	94	36	70	68	74	14	48	38	37	6	26	32	82	26	77	57	99	31	86	54	97	32	90	62
09:15 AM	97	28	78	52	96	34	64	52	78	22	63	42	64	20	59	44	31	10	44	22	88	28	82	62	114	34	73	44				

Appendix E Pre-Diet Traffic Counts: Arlington Road, Hillandale Road, Little Falls Parkway
LITTLE FALLS PARKWAY PRE ROAD DIET COUNTS

06:45 PM	80	50	112	77	86	46	92	56	94	36	92	60	76	23	96	52	48	14	56	26	80	37	86	50	82	32	88	56	83	43	97	63
07:00 PM	70	30	92	62	78	34	94	52	92	30	78	42	67	24	54	39	54	12	48	17	72	32	70	57	78	42	76	54	75	35	87	56
07:15 PM	73	37	74	47	78	36	70	48	87	25	77	46	60	31	74	42	39	5	43	22	56	30	56	28	69	37	72	28	73	37	72	41
07:30 PM	64	23	62	38	65	21	58	29	54	20	57	38	41	18	46	32	35	10	24	23	44	21	61	28	56	32	51	38	62	25	57	35
07:45 PM	47	24	48	20	56	35	48	28	44	20	56	22	38	10	38	20	29	15	28	16	48	21	46	34	53	29	54	26	52	29	50	25
08:00 PM	56	20	44	24	58	22	34	26	52	12	48	28	34	9	36	22	28	12	28	15	38	17	42	22	34	19	56	31	49	20	45	27
08:15 PM	35	16	42	21	35	17	40	20	47	21	42	15	33	9	45	18	20	12	38	20	37	16	44	16	46	18	54	22	39	17	45	21
08:30 PM	36	17	42	24	27	11	45	18	38	14	51	23	28	15	34	12	23	11	32	13	30	16	40	28	32	24	30	17	32	17	39	20
08:45 PM	34	14	44	10	28	11	29	21	39	12	50	20	34	12	22	16	26	5	28	10	21	16	44	17	27	15	44	16	30	13	39	16
09:00 PM	21	15	46	23	36	19	38	20	30	10	48	15	19	12	34	9	21	9	30	12	40	14	36	17	34	9	36	18	30	14	40	20
09:15 PM	30	18	32	16	18	11	28	9	28	11	54	16	18	7	40	20	25	5	12	9	20	8	31	11	19	18	34	11	22	16	31	12
09:30 PM	19	9	27	12	20	7	28	12	25	9	38	18	16	7	34	15	12	13	16	9	19	9	29	8	35	13	36	14	25	10	30	13
09:45 PM	19	14	23	7	26	6	26	16	22	14	22	15	22	12	33	15	14	9	19	3	20	12	18	7	16	12	25	12	20	11	25	12
10:00 PM	19	12	32	14	30	10	24	12	32	24	37	13	24	13	28	10	9	7	14	3	14	2	14	12	8	5	26	13	19	9	27	13
10:15 PM	17	6	14	5	15	3	18	7	19	12	23	12	17	9	37	12	12	3	5	6	16	6	14	6	15	3	22	4	16	4	18	5
10:30 PM	9	11	15	2	14	11	11	8	17	10	30	12	22	13	36	14	14	3	8	3	18	7	7	0	14	6	6	6	12	9	11	5
10:45 PM	7	5	9	4	12	3	10	6	21	9	20	12	20	10	26	11	5	4	7	3	15	4	8	4	8	4	6	4	9	4	8	5
11:00 PM	10	3	5	4	16	8	9	3	16	9	16	8	13	14	24	8	8	2	5	8	9	5	7	1	7	2	7	6	11	4	7	4
11:15 PM	11	11	8	1	11	6	7	2	16	10	12	6	20	8	16	5	8	2	3	4	10	2	5	2	5	6	11	2	9	8	9	2
11:30 PM	3	2	3	3	6	3	10	5	17	6	10	5	18	9	17	7	4	3	8	4	5	3	6	4	3	1	4	1	4	2	6	3
11:45 PM	7	9	4	1	7	2	5	1	10	4	18	7	14	16	20	7	0	3	2	2	7	2	8	2	9	4	2	2	8	5	4	1
	4752	1985	5349	3582	4839	2179	5365	3672	5165	2054	5571	3760	4409	1486	4459	2859	3484	1115	3304	2069	4423	1857	4982	3343	4811	2056	5308	3498	4801	2073	5341	3584
	15668			16055				16550					13213				9972				14605				15673				15799			

6874

8925

MCV Associates, Inc.

4605-C Pinecrest Office Park Dr
Alexandria, VA 22312
703-914-4850

Site Code: 0002

Station ID: N:023585

Arlington Road - Halfway Between
Little Falls Pkwy & Kenwood Forest Ln
Arlington Road - NB

Start Time	19-May-17 Fri	NB RT LN		NB LT LN		Combined		20-May Sat	NB RT LN		NB LT LN		Combined	
		A.M.	P.M.	A.M.	P.M.	A.M.	P.M.		A.M.	P.M.	A.M.	P.M.	A.M.	P.M.
12:00		2	42	4	56	6	98		2	38	6	38	8	76
12:15		1	36	4	34	5	70		3	30	6	55	9	85
12:30		2	41	7	52	9	93		3	47	10	38	13	85
12:45		3	32	2	46	5	78		2	51	4	45	6	96
01:00		1	33	3	53	4	86		3	51	6	43	9	94
01:15		1	27	2	42	3	69		2	45	5	46	7	91
01:30		0	30	1	30	1	60		1	42	2	50	3	92
01:45		0	35	2	36	2	71		0	45	3	40	3	85
02:00		1	30	2	36	3	66		0	37	2	40	2	77
02:15		0	27	1	38	1	65		0	36	2	40	2	76
02:30		2	34	1	39	3	73		0	31	1	39	1	70
02:45		0	36	1	57	1	93		1	36	5	44	6	80
03:00		0	37	1	42	1	79		1	52	4	59	5	111
03:15		0	35	0	54	0	89		3	40	3	49	6	89
03:30		0	42	1	54	1	96		0	43	1	42	1	85
03:45		1	42	0	40	1	82		0	25	1	34	1	59
04:00		0	22	0	42	0	64		0	38	1	48	1	86
04:15		0	38	1	51	1	89		1	41	1	48	2	89
04:30		0	50	1	47	1	97		0	44	1	45	1	89
04:45		1	37	2	46	3	83		0	30	3	29	3	59
05:00		1	51	1	55	2	106		0	33	0	31	0	64
05:15		0	33	2	51	2	84		1	29	1	34	2	63
05:30		3	31	4	47	7	78		0	28	1	41	1	69
05:45		5	30	4	54	9	84		0	29	0	37	0	66
06:00		11	43	13	43	24	86		1	28	5	29	6	57
06:15		14	36	19	53	33	89		2	42	3	33	5	75
06:30		5	50	11	47	16	97		2	25	5	39	7	64
06:45		4	29	9	37	13	66		4	26	6	25	10	51
07:00		11	36	14	44	25	80		4	31	11	25	15	56
07:15		13	28	11	38	24	66		6	24	9	32	15	56
07:30		16	43	30	34	46	77		8	19	9	27	17	46
07:45		26	29	27	32	53	61		10	26	16	28	26	54
08:00		32	32	48	30	80	62		16	22	18	27	34	49
08:15		31	25	54	23	85	48		13	15	20	18	33	33
08:30		43	14	48	25	91	39		17	21	25	20	42	41
08:45		36	9	61	22	97	31		14	14	25	18	39	32
09:00		54	7	57	15	111	22		35	17	45	12	80	29
09:15		37	14	41	19	78	33		35	9	29	14	64	23
09:30		48	8	44	14	92	22		23	13	26	5	49	18
09:45		28	11	35	22	63	33		36	10	45	13	81	23
10:00		35	4	39	15	74	19		29	8	29	15	58	23
10:15		31	9	27	14	58	23		33	3	38	8	71	11
10:30		34	4	25	9	59	13		46	10	51	11	97	21
10:45		32	5	28	12	60	17		36	10	28	16	64	26
11:00		32	10	41	16	73	26		43	8	34	10	77	18
11:15		18	4	42	12	60	16		38	2	31	8	69	10
11:30		35	15	36	16	71	31		44	7	38	9	82	16
11:45		37	9	41	18	78	27		23	2	47	9	70	11
Total		687	1325	848	1712	1535	3037		541	1313	662	1466	1203	2779
Day Total		2012		2560		4572			1854		2128		3982	
% Total		15.0%	29.0%	18.5%	37.4%				13.6%	33.0%	16.6%	36.8%		
Peak	-	08:45	04:15	08:15	02:45	08:15	04:15	-	10:30	00:30	09:45	02:45	10:15	00:45
Vol.	-	175	176	220	207	384	375	-	163	194	163	194	309	373
P.H.F.		0.810	0.863	0.902	0.908	0.865	0.884		0.886	0.951	0.799	0.822	0.796	0.971

MCV Associates, Inc.

4605-C Pinecrest Office Park Dr
Alexandria, VA 22312
703-914-4850

Site Code: 0002

Station ID: N:023585

Arlington Road - Halfway Between
Little Falls Pkwy & Kenwood Forest Ln
Arlington Road - NB

Start Time	21-May-17 Sun	NB RT LN		NB LT LN		Combined		22-May Mon	NB RT LN		NB LT LN		Combined	
		A.M.	P.M.	A.M.	P.M.	A.M.	P.M.		A.M.	P.M.	A.M.	P.M.	A.M.	P.M.
12:00		5	37	6	34	11	71		5	39	2	32	7	71
12:15		3	30	6	38	9	68		1	25	3	29	4	54
12:30		4	40	5	45	9	85		0	33	2	39	2	72
12:45		6	20	8	41	14	61		2	35	2	48	4	83
01:00		1	40	4	33	5	73		0	25	0	33	0	58
01:15		4	38	7	58	11	96		0	31	3	29	3	60
01:30		2	37	3	46	5	83		2	32	0	25	2	57
01:45		4	36	4	41	8	77		0	30	1	26	1	56
02:00		2	33	3	25	5	58		0	19	0	42	0	61
02:15		2	27	2	32	4	59		1	36	1	41	2	77
02:30		3	29	5	34	8	63		0	29	0	43	0	72
02:45		0	27	3	46	3	73		1	31	1	39	2	70
03:00		0	32	4	41	4	73		1	30	0	38	1	68
03:15		1	37	5	36	6	73		0	35	3	35	3	70
03:30		0	28	2	42	2	70		0	32	0	37	0	69
03:45		3	33	2	38	5	71		2	29	0	42	2	71
04:00		0	38	1	42	1	80		1	26	0	33	1	59
04:15		1	23	2	37	3	60		0	32	1	39	1	71
04:30		0	21	1	39	1	60		0	40	1	41	1	81
04:45		1	32	3	39	4	71		0	38	0	43	0	81
05:00		0	37	0	28	0	65		0	36	0	41	0	77
05:15		0	34	0	26	0	60		2	36	1	50	3	86
05:30		1	47	2	38	3	85		3	24	1	49	4	73
05:45		1	31	0	38	1	69		6	30	4	48	10	78
06:00		1	34	1	30	2	64		8	42	7	42	15	84
06:15		0	36	1	32	1	68		7	35	3	49	10	84
06:30		3	23	5	34	8	57		5	44	9	45	14	89
06:45		4	24	8	33	12	57		5	35	10	34	15	69
07:00		2	24	4	31	6	55		4	33	17	40	21	73
07:15		2	24	13	26	15	50		13	21	16	22	29	43
07:30		4	20	4	22	8	42		20	20	28	32	48	52
07:45		3	11	10	16	13	27		26	22	37	33	63	55
08:00		6	13	11	14	17	27		31	19	36	28	67	47
08:15		10	10	13	18	23	28		35	13	38	19	73	32
08:30		7	10	12	12	19	22		44	15	47	23	91	38
08:45		14	8	24	9	38	17		36	16	45	23	81	39
09:00		16	5	25	27	41	32		43	7	59	8	102	15
09:15		20	13	22	13	42	26		44	10	56	20	100	30
09:30		18	12	19	12	37	24		53	13	53	14	106	27
09:45		19	5	27	12	46	17		36	5	38	7	74	12
10:00		27	5	29	4	56	9		35	4	43	11	78	15
10:15		22	1	25	4	47	5		26	5	31	4	57	9
10:30		33	3	38	5	71	8		19	5	23	8	42	13
10:45		30	1	40	6	70	7		35	2	32	9	67	11
11:00		33	1	26	5	59	6		30	3	25	5	55	8
11:15		32	4	32	3	64	7		37	0	35	4	72	4
11:30		35	0	39	5	74	5		28	0	37	1	65	1
11:45		28	2	35	3	63	5		39	1	31	3	70	4
Total		413	1076	541	1293	954	2369		686	1123	782	1406	1468	2529
Day Total		1489		1834		3323			1809		2188		3997	
% Total		12.4%	32.4%	16.3%	38.9%				17.2%	28.1%	19.6%	35.2%		
Peak	-	10:45	01:00	10:45	00:45	10:45	01:00	-	08:45	06:00	08:45	05:15	08:45	05:45
Vol.	-	130	151	137	178	267	329	-	176	156	213	189	389	335
P.H.F.		0.929	0.944	0.856	0.767	0.902	0.857		0.830	0.886	0.903	0.945	0.917	0.941

MCV Associates, Inc.

4605-C Pinecrest Office Park Dr
Alexandria, VA 22312
703-914-4850

Site Code: 0002

Station ID: N:023585

Arlington Road - Halfway Between
Little Falls Pkwy & Kenwood Forest Ln
Arlington Road - NB

Start Time	23-May-17		NB RT LN		NB LT LN		Combined		24-May	NB RT LN		NB LT LN		Combined	
	Tue		A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	Wed	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.
12:00			1	43	0	38	1	81		1	29	3	50	4	79
12:15			0	27	3	33	3	60		1	39	1	50	2	89
12:30			2	33	2	29	4	62		3	35	2	48	5	83
12:45			1	35	2	42	3	77		1	31	2	39	3	70
01:00			1	30	2	40	3	70		0	43	0	26	0	69
01:15			1	32	0	35	1	67		2	22	2	39	4	61
01:30			1	31	0	44	1	75		1	44	1	32	2	76
01:45			0	35	3	41	3	76		0	31	0	37	0	68
02:00			0	29	0	49	0	78		0	26	0	44	0	70
02:15			0	36	3	38	3	74		0	23	0	41	0	64
02:30			0	22	0	40	0	62		0	33	2	46	2	79
02:45			0	24	1	41	1	65		0	39	0	47	0	86
03:00			1	42	0	48	1	90		1	34	4	51	5	85
03:15			0	28	1	47	1	75		0	49	3	42	3	91
03:30			0	36	0	44	0	80		0	39	0	39	0	78
03:45			0	39	0	48	0	87		1	38	0	46	1	84
04:00			0	37	0	58	0	95		0	33	0	45	0	78
04:15			0	44	1	42	1	86		0	40	0	47	0	87
04:30			0	43	0	42	0	85		0	32	2	50	2	82
04:45			0	43	1	61	1	104		0	46	1	64	1	110
05:00			1	44	3	45	4	89		0	39	2	39	2	78
05:15			1	35	2	61	3	96		0	39	1	48	1	87
05:30			4	55	3	36	7	91		2	52	4	55	6	107
05:45			5	43	7	48	12	91		9	41	3	56	12	97
06:00			4	63	4	36	8	99		6	47	12	44	18	91
06:15			7	32	7	36	14	68		4	51	8	50	12	101
06:30			7	48	10	50	17	98		5	44	8	48	13	92
06:45			12	33	11	37	23	70		3	40	12	48	15	88
07:00			10	36	20	35	30	71		15	39	20	56	35	95
07:15			19	45	17	56	36	101		8	31	20	48	28	79
07:30			16	35	20	40	36	75		28	41	21	31	49	72
07:45			27	30	32	32	59	62		19	23	38	35	57	58
08:00			27	36	47	34	74	70		42	19	36	35	78	54
08:15			47	30	47	27	94	57		42	10	48	29	90	39
08:30			54	15	59	17	113	32		48	21	64	28	112	49
08:45			44	15	51	16	95	31		48	6	53	14	101	20
09:00			48	12	48	12	96	24		42	10	52	13	94	23
09:15			42	10	48	26	90	36		37	15	54	23	91	38
09:30			45	11	37	9	82	20		41	4	47	13	88	17
09:45			21	7	42	17	63	24		42	10	30	17	72	27
10:00			28	7	27	12	55	19		28	8	26	9	54	17
10:15			26	8	30	9	56	17		28	6	31	10	59	16
10:30			30	4	38	8	68	12		37	2	34	9	71	11
10:45			42	6	28	13	70	19		28	4	36	7	64	11
11:00			33	1	41	3	74	4		36	5	27	7	63	12
11:15			40	4	39	7	79	11		37	5	46	6	83	11
11:30			24	1	36	2	60	3		30	3	31	4	61	7
11:45			31	3	35	11	66	14		37	4	36	4	73	8
Total			703	1358	808	1595	1511	2953		713	1325	823	1669	1536	2994
Day Total			2061		2403		4464			2038		2492		4530	
% Total			15.7%	30.4%	18.1%	35.7%				15.7%	29.2%	18.2%	36.8%		
Peak	-		08:15	05:15	08:30	04:30	08:15	04:45	-	08:00	05:30	08:30	04:00	08:30	05:30
Vol.	-		193	196	206	209	398	380	-	180	191	223	206	398	396
P.H.F.			0.894	0.778	0.873	0.857	0.881	0.913		0.938	0.918	0.871	0.805	0.888	0.925

Appendix E Post-Diet Traffic Counts: Arlington Road, Hillandale Road, Little Falls Parkway

MCV Associates, Inc.

4605-C Pinecrest Office Park Dr
Alexandria, VA 22312
703-914-4850

Page 1

Site Code: 0002

Station ID: N:023585

Arlington Road - Halfway Between
Little Falls Pkwy & Kenwood Forest Ln
Arlington Road - SB

Start Time	19-May-17 Fri	SB RT LN		SB LT LN		Combined		20-May Sat	SB RT LN		SB LT LN		Combined	
		A.M.	P.M.	A.M.	P.M.	A.M.	P.M.		A.M.	P.M.	A.M.	P.M.	A.M.	P.M.
12:00		0	19	3	81	3	100		6	22	8	90	14	112
12:15		0	10	8	73	8	83		0	21	8	59	8	80
12:30		0	15	7	70	7	85		1	25	5	70	6	95
12:45		0	11	3	55	3	66		0	17	9	65	9	82
01:00		0	19	5	63	5	82		0	22	9	84	9	106
01:15		1	16	2	65	3	81		0	22	6	82	6	104
01:30		0	24	0	65	0	89		0	36	5	92	5	128
01:45		1	20	2	67	3	87		0	32	5	88	5	120
02:00		0	18	2	70	2	88		0	29	2	84	2	113
02:15		1	16	1	64	2	80		0	19	2	72	2	91
02:30		0	19	1	76	1	95		0	21	4	80	4	101
02:45		0	16	0	76	0	92		0	19	3	64	3	83
03:00		0	23	0	84	0	107		0	31	4	74	4	105
03:15		0	11	0	94	0	105		0	18	1	73	1	91
03:30		0	16	2	92	2	108		0	18	2	73	2	91
03:45		0	18	0	104	0	122		0	16	1	64	1	80
04:00		0	19	2	91	2	110		0	17	2	66	2	83
04:15		0	25	1	88	1	113		0	16	1	83	1	99
04:30		0	28	1	109	1	137		0	30	2	84	2	114
04:45		1	20	1	111	2	131		0	21	2	82	2	103
05:00		1	32	3	130	4	162		0	12	3	84	3	96
05:15		0	20	4	80	4	100		0	23	4	62	4	85
05:30		0	21	10	98	10	119		0	14	5	66	5	80
05:45		2	17	24	87	26	104		0	13	5	71	5	84
06:00		12	25	31	107	43	132		0	14	2	65	2	79
06:15		3	18	17	105	20	123		0	18	5	54	5	72
06:30		2	27	20	96	22	123		1	16	7	61	8	77
06:45		4	18	15	82	19	100		0	13	7	51	7	64
07:00		12	11	29	68	41	79		1	16	8	41	9	57
07:15		8	14	50	68	58	82		1	9	15	58	16	67
07:30		11	16	53	57	64	73		0	8	16	41	16	49
07:45		11	11	63	62	74	73		2	6	20	36	22	42
08:00		5	15	65	45	70	60		6	11	19	45	25	56
08:15		9	8	64	39	73	47		2	8	16	24	18	32
08:30		8	7	70	43	78	50		3	5	29	39	32	44
08:45		11	6	73	41	84	47		6	13	34	32	40	45
09:00		13	10	69	32	82	42		17	9	43	48	60	57
09:15		16	10	47	40	63	50		13	9	46	32	59	41
09:30		11	9	49	39	60	48		11	9	41	24	52	33
09:45		13	12	74	37	87	49		12	4	55	36	67	40
10:00		18	14	49	49	67	63		18	8	57	28	75	36
10:15		10	3	57	20	67	23		14	6	37	25	51	31
10:30		11	2	62	26	73	28		18	6	56	20	74	26
10:45		9	7	64	40	73	47		13	10	50	32	63	42
11:00		13	10	70	22	83	32		19	3	61	21	80	24
11:15		17	10	72	27	89	37		22	3	70	15	92	18
11:30		26	3	67	17	93	20		24	5	74	24	98	29
11:45		16	4	63	7	79	11		16	3	75	9	91	12
Total		276	723	1375	3162	1651	3885		226	726	941	2673	1167	3399
Day Total		999		4537		5536			952		3614		4566	
% Total		5.0%	13.1%	24.8%	57.1%				4.9%	15.9%	20.6%	58.5%		
Peak	-	11:00	04:15	08:15	04:15	11:00	04:15	-	11:00	01:15	11:00	01:00	11:00	01:15
Vol.	-	72	105	276	438	344	543	-	81	119	280	346	361	465
P.H.F.		0.692	0.820	0.945	0.842	0.925	0.838		0.844	0.826	0.933	0.940	0.921	0.908

MCV Associates, Inc.

4605-C Pinecrest Office Park Dr
Alexandria, VA 22312
703-914-4850

Site Code: 0002

Station ID: N:023585

Arlington Road - Halfway Between
Little Falls Pkwy & Kenwood Forest Ln
Arlington Road - SB

Start Time	21-May-17 Sun	SB RT LN		SB LT LN		Combined		22-May Mon	SB RT LN		SB LT LN		Combined	
		A.M.	P.M.	A.M.	P.M.	A.M.	P.M.		A.M.	P.M.	A.M.	P.M.	A.M.	P.M.
12:00		3	17	10	76	13	93		1	19	4	77	5	96
12:15		1	18	16	68	17	86		0	9	4	73	4	82
12:30		0	12	14	55	14	67		2	17	2	57	4	74
12:45		2	13	10	66	12	79		0	15	2	61	2	76
01:00		0	17	8	62	8	79		0	10	3	67	3	77
01:15		0	16	4	64	4	80		0	12	1	60	1	72
01:30		0	19	3	70	3	89		0	14	0	63	0	77
01:45		0	16	5	59	5	75		0	9	0	59	0	68
02:00		0	24	2	82	2	106		0	16	0	63	0	79
02:15		0	17	3	53	3	70		0	13	0	56	0	69
02:30		0	15	2	74	2	89		0	17	1	79	1	96
02:45		0	14	1	56	1	70		0	13	0	63	0	76
03:00		0	21	3	66	3	87		0	17	0	77	0	94
03:15		0	14	0	80	0	94		0	12	0	71	0	83
03:30		0	12	0	80	0	92		0	13	0	83	0	96
03:45		0	12	4	80	4	92		0	24	0	77	0	101
04:00		0	25	1	82	1	107		0	17	0	68	0	85
04:15		0	15	0	61	0	76		0	9	2	67	2	76
04:30		0	12	3	63	3	75		0	13	2	98	2	111
04:45		1	16	1	52	2	68		0	11	2	83	2	94
05:00		0	18	1	63	1	81		0	15	3	95	3	110
05:15		0	20	2	80	2	100		0	15	5	82	5	97
05:30		0	16	5	81	5	97		0	18	5	112	5	130
05:45		0	9	4	58	4	67		0	19	5	98	5	117
06:00		1	12	4	66	5	78		0	18	9	115	9	133
06:15		1	24	8	63	9	87		0	20	9	88	9	108
06:30		0	9	7	54	7	63		2	25	11	100	13	125
06:45		0	15	6	43	6	58		2	10	28	85	30	95
07:00		1	12	8	53	9	65		3	7	22	65	25	72
07:15		1	5	7	44	8	49		6	12	35	66	41	78
07:30		0	12	12	38	12	50		8	14	52	56	60	70
07:45		1	8	10	33	11	41		8	10	70	52	78	62
08:00		0	7	18	36	18	43		10	8	82	39	92	47
08:15		5	9	14	36	19	45		13	9	91	39	104	48
08:30		7	8	22	48	29	56		11	7	76	43	87	50
08:45		2	8	14	36	16	44		13	7	54	32	67	39
09:00		7	8	27	31	34	39		17	7	56	35	73	42
09:15		3	5	30	34	33	39		18	5	52	29	70	34
09:30		5	5	32	25	37	30		12	4	53	19	65	23
09:45		10	4	36	20	46	24		15	6	67	23	82	29
10:00		10	5	38	23	48	28		6	6	61	22	67	28
10:15		8	3	27	14	35	17		7	3	59	16	66	19
10:30		7	3	53	17	60	20		7	3	78	26	85	29
10:45		6	2	52	6	58	8		10	0	60	16	70	16
11:00		9	3	59	7	68	10		9	1	76	11	85	12
11:15		16	1	51	5	67	6		12	1	44	12	56	13
11:30		13	0	56	8	69	8		16	1	54	7	70	8
11:45		23	1	58	3	81	4		11	2	59	7	70	9
Total		143	557	751	2374	894	2931		219	533	1299	2792	1518	3325
Day Total		700		3125		3825			752		4091		4843	
% Total		3.7%	14.6%	19.6%	62.1%				4.5%	11.0%	26.8%	57.7%		
Peak	-	11:00	01:30	11:00	03:15	11:00	03:15	-	09:00	05:45	07:45	05:30	07:45	05:30
Vol.	-	61	76	224	322	285	385	-	62	82	319	413	361	488
P.H.F.		0.663	0.792	0.949	0.982	0.880	0.900		0.861	0.820	0.876	0.898	0.868	0.917

MCV Associates, Inc.

4605-C Pinecrest Office Park Dr
Alexandria, VA 22312
703-914-4850

Site Code: 0002

Station ID: N:023585

Arlington Road - Halfway Between
Little Falls Pkwy & Kenwood Forest Ln
Arlington Road - SB

Start Time	23-May-17		SB RT LN		SB LT LN		Combined		24-May	SB RT LN		SB LT LN		Combined	
	Tue		A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	Wed	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.
12:00			0	12	3	67	3	79		1	13	6	79	7	92
12:15			0	8	4	71	4	79		0	18	1	69	1	87
12:30			0	12	1	81	1	93		0	16	3	77	3	93
12:45			0	17	0	72	0	89		0	13	2	69	2	82
01:00			0	18	0	64	0	82		1	14	2	76	3	90
01:15			0	9	4	71	4	80		0	13	1	79	1	92
01:30			0	15	2	69	2	84		0	20	3	77	3	97
01:45			0	15	2	51	2	66		0	16	0	61	0	77
02:00			0	22	0	67	0	89		0	17	2	72	2	89
02:15			0	18	1	66	1	84		0	19	1	61	1	80
02:30			0	14	0	77	0	91		0	17	1	85	1	102
02:45			1	8	0	72	1	80		0	14	0	76	0	90
03:00			0	21	0	89	0	110		0	16	0	74	0	90
03:15			0	11	0	84	0	95		0	11	0	77	0	88
03:30			0	22	0	91	0	113		0	13	1	91	1	104
03:45			0	16	4	68	4	84		0	16	0	76	0	92
04:00			0	21	2	76	2	97		0	23	0	88	0	111
04:15			0	17	0	97	0	114		0	19	3	87	3	106
04:30			0	18	2	130	2	148		0	16	3	109	3	125
04:45			0	22	1	106	1	128		0	24	1	102	1	126
05:00			0	17	3	99	3	116		0	14	1	115	1	129
05:15			0	14	7	97	7	111		0	26	3	98	3	124
05:30			0	25	5	132	5	157		2	21	13	116	15	137
05:45			1	14	1	98	2	112		1	10	5	98	6	108
06:00			0	29	13	104	13	133		0	19	8	100	8	119
06:15			1	13	7	85	8	98		0	16	9	84	9	100
06:30			5	18	13	98	18	116		2	19	13	84	15	103
06:45			1	16	31	97	32	113		1	17	17	82	18	99
07:00			7	19	34	110	41	129		9	9	34	63	43	72
07:15			7	19	42	79	49	98		8	8	44	74	52	82
07:30			6	15	69	66	75	81		6	7	59	54	65	61
07:45			8	11	81	45	89	56		11	6	72	70	83	76
08:00			7	7	74	47	81	54		5	8	88	55	93	63
08:15			13	6	92	38	105	44		13	6	79	42	92	48
08:30			11	10	66	43	77	53		13	6	57	52	70	58
08:45			9	7	79	51	88	58		13	6	75	34	88	40
09:00			7	4	67	39	74	43		9	14	57	44	66	58
09:15			7	3	57	32	64	35		9	12	65	27	74	39
09:30			10	11	61	36	71	47		6	5	55	38	61	43
09:45			13	2	54	34	67	36		13	3	56	35	69	38
10:00			15	8	44	18	59	26		10	4	69	25	79	29
10:15			11	2	59	23	70	25		2	3	63	19	65	22
10:30			14	2	54	17	68	19		16	2	71	12	87	14
10:45			15	4	57	12	72	16		13	3	56	16	69	19
11:00			10	0	65	11	75	11		15	4	80	6	95	10
11:15			14	0	66	5	80	5		10	1	70	8	80	9
11:30			18	0	57	7	75	7		18	1	68	5	86	6
11:45			13	0	72	3	85	3		12	1	84	5	96	6
Total			224	592	1356	3095	1580	3687		219	579	1401	3046	1620	3625
Day Total			816		4451		5267			798		4447		5245	
% Total			4.3%	11.2%	25.7%	58.8%				4.2%	11.0%	26.7%	58.1%		
Peak	-		10:45	05:15	07:30	04:45	07:45	05:15	-	10:45	04:45	11:00	04:45	11:00	04:45
Vol.	-		57	82	316	434	352	513	-	56	85	302	431	357	516
P.H.F.			0.792	0.707	0.859	0.822	0.838	0.817		0.778	0.817	0.899	0.929	0.930	0.942

Arlington Road - Halfway Between
Little Falls Pkwy & Kenwood Forest Ln
Arlington Road - SB

MCV Associates, Inc.

4605-C Pinecrest Office Park Dr
Alexandria, VA 22312
703-914-4850

Site Code: 0001

Station ID: N:023585
Hillandale Road - 200 Ft N of the
Entrance to Bethesda Pool
Hillandale Road - NB

Start Time	15-May-17		Tue		Wed		Thu		Fri		Sat		Sun		Average Da	
	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.
12:00	*	*	*	*	*	*	*	*	7	62	12	59	7	65	9	62
12:15	*	*	*	*	*	*	*	*	7	58	8	53	6	55	7	55
12:30	*	*	*	*	*	*	*	*	3	64	7	46	9	49	6	53
12:45	*	*	*	*	*	*	*	*	6	67	4	56	5	47	5	57
01:00	*	*	*	*	*	*	*	*	3	45	2	52	4	46	3	48
01:15	*	*	*	*	*	*	*	*	2	50	2	48	2	50	2	49
01:30	*	*	*	*	*	*	*	*	1	41	1	50	3	53	2	48
01:45	*	*	*	*	*	*	*	*	0	45	4	62	3	48	2	52
02:00	*	*	*	*	*	*	*	*	0	58	4	65	4	51	3	58
02:15	*	*	*	*	*	*	*	*	2	54	0	49	4	53	2	52
02:30	*	*	*	*	*	*	*	*	1	60	2	56	2	46	2	54
02:45	*	*	*	*	*	*	*	*	2	76	2	38	0	63	1	59
03:00	*	*	*	*	*	*	*	*	2	52	0	58	1	61	1	57
03:15	*	*	*	*	*	*	*	*	0	64	0	56	1	68	0	63
03:30	*	*	*	*	*	*	*	*	0	86	2	52	1	49	1	62
03:45	*	*	*	*	*	*	*	*	0	78	1	51	3	38	1	56
04:00	*	*	*	*	*	*	*	*	0	72	1	45	0	44	0	54
04:15	*	*	*	*	*	*	*	*	0	64	2	49	4	38	2	50
04:30	*	*	*	*	*	*	*	*	0	75	0	45	0	50	0	57
04:45	*	*	*	*	*	*	*	*	0	68	2	44	2	54	1	55
05:00	*	*	*	*	*	*	*	*	3	73	2	44	1	48	2	55
05:15	*	*	*	*	*	*	*	*	4	66	0	53	2	53	2	57
05:30	*	*	*	*	*	*	*	*	3	67	0	54	0	45	1	55
05:45	*	*	*	*	*	*	*	*	7	57	0	56	1	42	3	52
06:00	*	*	*	*	*	*	*	*	33	72	4	57	1	42	13	57
06:15	*	*	*	*	*	*	*	*	45	73	3	56	2	45	17	58
06:30	*	*	*	*	*	*	*	*	22	67	3	54	1	36	9	52
06:45	*	*	*	*	*	*	*	*	9	57	9	40	8	45	9	47
07:00	*	*	*	*	*	*	*	*	21	63	16	19	5	43	14	42
07:15	*	*	*	*	*	*	*	*	26	54	8	38	7	33	14	42
07:30	*	*	*	*	*	*	*	*	33	37	8	43	6	29	16	36
07:45	*	*	*	*	*	*	*	*	48	32	15	41	7	18	23	30
08:00	*	*	*	*	*	*	*	*	53	30	25	24	5	25	28	26
08:15	*	*	*	*	*	*	*	*	46	31	20	26	13	19	26	25
08:30	*	*	*	*	*	*	*	*	60	32	12	20	20	13	31	22
08:45	*	*	*	*	*	*	*	*	58	26	34	18	18	10	37	18
09:00	*	*	*	*	*	*	*	*	52	22	34	27	33	27	40	25
09:15	*	*	*	*	*	*	*	*	46	19	48	16	18	28	37	21
09:30	*	*	*	*	*	*	*	*	37	23	45	9	12	31	31	21
09:45	*	*	*	*	*	*	*	*	48	15	55	17	29	13	44	15
10:00	*	*	*	*	*	*	*	*	38	14	39	18	35	15	37	16
10:15	*	*	*	*	*	*	*	*	49	16	56	17	37	10	47	14
10:30	*	*	*	*	*	*	*	*	44	12	41	13	35	8	40	11
10:45	*	*	*	*	*	*	*	*	47	13	55	21	45	8	49	14
11:00	*	*	*	*	*	*	*	*	52	23	53	10	40	5	48	13
11:15	*	*	*	*	*	*	*	*	41	29	50	7	56	7	49	14
11:30	*	*	*	*	*	*	*	*	39	50	44	15	48	6	44	24
11:45	*	*	*	*	*	*	*	*	56	38	46	12	49	2	50	17
Total	0	0	0	0	0	0	0	0	1056	2350	781	1859	595	1734	811	1980
Day Total	0	0	0	0	0	0	0	0	3406	2640	2329	2791				
% Splits	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	31.0%	69.0%	29.6%	70.4%	25.5%	74.5%	29.1%	70.9%
Peak	-	-	-	-	-	-	-	-	08:00	03:15	10:15	01:45	11:00	02:45	11:00	02:45
Vol.	-	-	-	-	-	-	-	-	217	300	205	232	193	241	191	241
P.H.F.									0.904	0.872	0.915	0.892	0.862	0.886	0.955	0.956

MCV Associates, Inc.

4605-C Pinecrest Office Park Dr
Alexandria, VA 22312
703-914-4850

Site Code: 0001

Station ID: N:023585
Hillandale Road - 200 Ft N of the
Entrance to Bethesda Pool
Hillandale Road - NB

Start Time	22-May-17		Tue		Wed		Thu		Fri		Sat		Sun		Average Da	
	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.
12:00	1	43	1	49	0	67	3	50	*	*	*	*	*	*	1	52
12:15	3	51	4	55	2	61	1	39	*	*	*	*	*	*	2	52
12:30	3	37	2	35	2	61	5	72	*	*	*	*	*	*	3	51
12:45	2	43	3	50	1	56	1	67	*	*	*	*	*	*	2	54
01:00	1	47	2	47	0	49	2	62	*	*	*	*	*	*	1	51
01:15	4	39	3	41	3	43	4	53	*	*	*	*	*	*	4	44
01:30	1	34	3	31	0	31	0	47	*	*	*	*	*	*	1	36
01:45	1	46	4	46	1	51	1	48	*	*	*	*	*	*	2	48
02:00	0	36	0	56	1	47	0	50	*	*	*	*	*	*	0	47
02:15	0	43	1	58	1	54	1	50	*	*	*	*	*	*	1	51
02:30	1	58	0	53	0	57	0	49	*	*	*	*	*	*	0	54
02:45	0	65	0	51	0	64	0	66	*	*	*	*	*	*	0	62
03:00	0	56	0	61	0	58	0	46	*	*	*	*	*	*	0	55
03:15	0	48	0	57	0	58	0	56	*	*	*	*	*	*	0	55
03:30	0	73	0	83	0	79	0	60	*	*	*	*	*	*	0	74
03:45	0	96	0	94	0	100	0	87	*	*	*	*	*	*	0	94
04:00	0	52	2	68	0	84	1	49	*	*	*	*	*	*	1	63
04:15	1	60	0	64	2	69	2	63	*	*	*	*	*	*	1	64
04:30	0	71	0	72	1	58	0	55	*	*	*	*	*	*	0	64
04:45	2	62	4	79	1	83	0	68	*	*	*	*	*	*	2	73
05:00	1	58	2	72	0	62	2	57	*	*	*	*	*	*	1	62
05:15	3	54	2	60	2	62	4	73	*	*	*	*	*	*	3	62
05:30	2	60	4	67	1	65	1	85	*	*	*	*	*	*	2	69
05:45	5	65	4	64	9	62	5	53	*	*	*	*	*	*	6	61
06:00	7	58	7	73	6	76	6	72	*	*	*	*	*	*	6	70
06:15	6	59	8	79	5	88	8	64	*	*	*	*	*	*	7	72
06:30	8	74	9	87	6	68	6	49	*	*	*	*	*	*	7	70
06:45	8	51	8	52	8	71	1	46	*	*	*	*	*	*	6	55
07:00	13	54	11	46	15	63	21	53	*	*	*	*	*	*	15	54
07:15	17	54	17	62	17	61	17	48	*	*	*	*	*	*	17	56
07:30	23	30	31	47	28	41	29	50	*	*	*	*	*	*	28	42
07:45	28	29	42	42	29	45	32	48	*	*	*	*	*	*	33	41
08:00	43	24	52	26	62	25	53	21	*	*	*	*	*	*	52	24
08:15	53	26	60	30	51	39	54	25	*	*	*	*	*	*	54	30
08:30	43	14	57	29	59	33	54	22	*	*	*	*	*	*	53	24
08:45	49	19	59	23	57	21	58	17	*	*	*	*	*	*	56	20
09:00	56	23	65	26	56	20	61	19	*	*	*	*	*	*	60	22
09:15	64	11	66	22	61	22	63	23	*	*	*	*	*	*	64	20
09:30	46	16	52	16	64	16	58	20	*	*	*	*	*	*	55	17
09:45	52	13	54	16	42	11	67	13	*	*	*	*	*	*	54	13
10:00	39	15	41	7	53	13	48	11	*	*	*	*	*	*	45	12
10:15	39	9	37	8	40	13	50	7	*	*	*	*	*	*	42	9
10:30	31	12	40	7	62	7	39	13	*	*	*	*	*	*	43	10
10:45	44	9	41	12	45	17	43	12	*	*	*	*	*	*	43	12
11:00	35	7	36	17	45	6	31	17	*	*	*	*	*	*	37	12
11:15	42	3	46	6	38	4	40	7	*	*	*	*	*	*	42	5
11:30	43	5	55	7	54	4	43	9	*	*	*	*	*	*	49	6
11:45	56	6	73	2	56	3	40	9	*	*	*	*	*	*	56	5
Total	876	1918	1008	2155	986	2248	955	2080	0	0	0	0	0	0	957	2099
Day Total	2794		3163		3234		3035		0		0		0		3056	
% Splits	31.4 %	68.6 %	31.9 %	68.1 %	30.5 %	69.5 %	31.5 %	68.5 %	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	31.3 %	68.7%
Peak	09:00	03:30	08:30	03:30	08:45	03:30	09:00	04:45	-	-	-	-	-	-	08:45	03:30
Vol.	218	281	247	309	238	332	249	283	-	-	-	-	-	-	235	295
P.H.F.	0.852	0.732	0.936	0.822	0.930	0.830	0.929	0.813							0.918	0.785

ADT ADT 2,943 AADT 2,943

MCV Associates, Inc.

4605-C Pinecrest Office Park Dr
Alexandria, VA 22312
703-914-4850

Site Code: 0001

Station ID: N:023585
Hillandale Road - 200 Ft N of the
Entrance to Bethesda Pool
Hillandale Road - SB

Start Time	15-May-17		Tue		Wed		Thu		Fri		Sat		Sun		Average Da	
	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.
12:00	*	*	*	*	*	*	*	*	3	37	5	43	5	37	4	39
12:15	*	*	*	*	*	*	*	*	2	41	2	40	7	37	4	39
12:30	*	*	*	*	*	*	*	*	7	32	6	34	0	34	4	33
12:45	*	*	*	*	*	*	*	*	0	35	7	29	1	46	3	37
01:00	*	*	*	*	*	*	*	*	1	45	3	40	3	41	2	42
01:15	*	*	*	*	*	*	*	*	1	28	1	57	5	34	2	40
01:30	*	*	*	*	*	*	*	*	0	28	6	40	2	27	3	32
01:45	*	*	*	*	*	*	*	*	1	33	1	34	3	39	2	35
02:00	*	*	*	*	*	*	*	*	1	48	2	40	0	35	1	41
02:15	*	*	*	*	*	*	*	*	2	36	1	35	3	45	2	39
02:30	*	*	*	*	*	*	*	*	0	47	3	43	1	33	1	41
02:45	*	*	*	*	*	*	*	*	1	42	0	42	0	34	0	39
03:00	*	*	*	*	*	*	*	*	0	41	0	34	1	38	0	38
03:15	*	*	*	*	*	*	*	*	0	47	2	32	0	43	1	41
03:30	*	*	*	*	*	*	*	*	1	41	0	28	1	37	1	35
03:45	*	*	*	*	*	*	*	*	0	37	1	30	0	36	0	34
04:00	*	*	*	*	*	*	*	*	2	54	1	38	1	35	1	42
04:15	*	*	*	*	*	*	*	*	2	40	1	47	2	39	2	42
04:30	*	*	*	*	*	*	*	*	0	31	2	38	2	28	1	32
04:45	*	*	*	*	*	*	*	*	3	45	0	40	2	40	2	42
05:00	*	*	*	*	*	*	*	*	7	52	6	42	5	39	6	44
05:15	*	*	*	*	*	*	*	*	1	56	1	40	1	49	1	48
05:30	*	*	*	*	*	*	*	*	5	35	1	41	0	37	2	38
05:45	*	*	*	*	*	*	*	*	35	47	1	44	0	42	12	44
06:00	*	*	*	*	*	*	*	*	46	50	5	22	5	38	19	37
06:15	*	*	*	*	*	*	*	*	16	42	3	31	3	29	7	34
06:30	*	*	*	*	*	*	*	*	31	40	7	28	6	25	15	31
06:45	*	*	*	*	*	*	*	*	27	44	5	38	7	34	13	39
07:00	*	*	*	*	*	*	*	*	37	27	11	25	7	33	18	28
07:15	*	*	*	*	*	*	*	*	28	31	8	22	7	30	14	28
07:30	*	*	*	*	*	*	*	*	34	20	15	24	10	31	20	25
07:45	*	*	*	*	*	*	*	*	39	26	15	21	5	13	20	20
08:00	*	*	*	*	*	*	*	*	59	20	23	30	12	21	31	24
08:15	*	*	*	*	*	*	*	*	42	21	18	24	11	20	24	22
08:30	*	*	*	*	*	*	*	*	44	24	18	16	10	16	24	19
08:45	*	*	*	*	*	*	*	*	31	16	18	16	8	21	19	18
09:00	*	*	*	*	*	*	*	*	39	13	47	19	23	14	36	15
09:15	*	*	*	*	*	*	*	*	31	15	30	18	28	22	30	18
09:30	*	*	*	*	*	*	*	*	34	15	41	19	26	20	34	18
09:45	*	*	*	*	*	*	*	*	39	15	25	16	23	15	29	15
10:00	*	*	*	*	*	*	*	*	45	19	43	18	33	8	40	15
10:15	*	*	*	*	*	*	*	*	32	25	40	16	19	10	30	17
10:30	*	*	*	*	*	*	*	*	25	7	39	12	31	10	32	10
10:45	*	*	*	*	*	*	*	*	30	28	35	9	29	3	31	13
11:00	*	*	*	*	*	*	*	*	36	37	45	11	32	4	38	17
11:15	*	*	*	*	*	*	*	*	22	26	40	8	32	0	31	11
11:30	*	*	*	*	*	*	*	*	32	11	39	15	38	3	36	10
11:45	*	*	*	*	*	*	*	*	42	9	25	12	32	1	33	7
Total	0	0	0	0	0	0	0	0	916	1559	648	1401	482	1326	681	1428
Day Total	0	0	0	0	0	0	0	0	2475	2049	2049	1808	2109			
% Splits	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	37.0%	63.0%	31.6%	68.4%	26.7%	73.3%	32.3%	67.7%
Peak	-	-	-	-	-	-	-	-	07:45	05:00	10:15	01:00	11:00	05:00	11:00	05:00
Vol.	-	-	-	-	-	-	-	-	184	190	159	171	134	167	138	174
P.H.F.									0.780	0.848	0.883	0.750	0.882	0.852	0.908	0.906

MCV Associates, Inc.

4605-C Pinecrest Office Park Dr
Alexandria, VA 22312
703-914-4850

Site Code: 0001

Station ID: N:023585
Hillandale Road - 200 Ft N of the
Entrance to Bethesda Pool
Hillandale Road - SB

Start Time	22-May-17		Tue		Wed		Thu		Fri		Sat		Sun		Average Da	
	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.
12:00	1	36	3	43	2	42	3	34	*	*	*	*	*	*	2	39
12:15	3	36	2	32	3	50	5	40	*	*	*	*	*	*	3	40
12:30	1	44	2	49	1	41	4	34	*	*	*	*	*	*	2	42
12:45	1	29	2	45	1	38	0	32	*	*	*	*	*	*	1	36
01:00	2	40	0	30	0	23	1	43	*	*	*	*	*	*	1	34
01:15	0	31	0	37	0	39	1	31	*	*	*	*	*	*	0	34
01:30	1	37	0	46	0	48	1	36	*	*	*	*	*	*	0	42
01:45	0	28	1	29	1	37	0	39	*	*	*	*	*	*	0	33
02:00	1	42	1	41	0	51	1	34	*	*	*	*	*	*	1	42
02:15	0	32	0	45	1	34	1	33	*	*	*	*	*	*	0	36
02:30	0	40	0	38	0	47	1	34	*	*	*	*	*	*	0	40
02:45	0	33	0	52	0	50	1	33	*	*	*	*	*	*	0	42
03:00	1	48	0	31	0	53	0	44	*	*	*	*	*	*	0	44
03:15	0	38	0	36	0	43	0	32	*	*	*	*	*	*	0	37
03:30	1	41	0	42	0	46	0	41	*	*	*	*	*	*	0	42
03:45	0	36	0	39	1	31	1	38	*	*	*	*	*	*	0	36
04:00	3	39	3	42	1	40	3	44	*	*	*	*	*	*	2	41
04:15	1	39	2	48	2	40	1	58	*	*	*	*	*	*	2	46
04:30	0	44	0	58	2	40	2	47	*	*	*	*	*	*	1	47
04:45	1	47	0	31	3	55	3	35	*	*	*	*	*	*	2	42
05:00	6	50	5	44	3	55	1	26	*	*	*	*	*	*	4	44
05:15	3	36	4	62	4	45	4	53	*	*	*	*	*	*	4	49
05:30	4	42	4	49	6	65	2	55	*	*	*	*	*	*	4	53
05:45	6	40	4	57	8	53	5	44	*	*	*	*	*	*	6	48
06:00	8	47	7	68	5	55	2	46	*	*	*	*	*	*	6	54
06:15	10	44	14	61	12	52	12	72	*	*	*	*	*	*	12	57
06:30	12	40	18	25	22	44	17	43	*	*	*	*	*	*	17	38
06:45	26	44	29	46	32	39	25	45	*	*	*	*	*	*	28	44
07:00	41	41	32	31	37	42	31	38	*	*	*	*	*	*	35	38
07:15	27	35	32	38	26	30	34	24	*	*	*	*	*	*	30	32
07:30	43	31	43	32	40	25	42	28	*	*	*	*	*	*	42	29
07:45	44	31	35	27	41	33	42	22	*	*	*	*	*	*	40	28
08:00	60	21	68	22	63	24	56	15	*	*	*	*	*	*	62	20
08:15	26	16	38	34	49	24	35	16	*	*	*	*	*	*	37	22
08:30	34	28	33	22	31	26	36	28	*	*	*	*	*	*	34	26
08:45	49	21	34	20	56	19	38	8	*	*	*	*	*	*	44	17
09:00	30	18	27	16	37	26	33	28	*	*	*	*	*	*	32	22
09:15	30	9	45	15	33	13	28	15	*	*	*	*	*	*	34	13
09:30	34	14	35	19	29	15	31	12	*	*	*	*	*	*	32	15
09:45	34	11	25	12	30	8	36	9	*	*	*	*	*	*	31	10
10:00	31	14	34	8	35	13	30	15	*	*	*	*	*	*	32	12
10:15	22	5	39	12	29	7	33	7	*	*	*	*	*	*	31	8
10:30	31	8	33	10	29	4	38	11	*	*	*	*	*	*	33	8
10:45	33	3	42	11	34	11	44	7	*	*	*	*	*	*	38	8
11:00	35	2	40	3	39	3	29	5	*	*	*	*	*	*	36	3
11:15	36	1	31	4	30	6	45	4	*	*	*	*	*	*	36	4
11:30	27	2	39	2	30	3	32	3	*	*	*	*	*	*	32	2
11:45	45	3	38	1	37	3	41	6	*	*	*	*	*	*	40	3
Total	804	1417	844	1565	845	1591	831	1447	0	0	0	0	0	0	829	1502
Day Total	2221		2409		2436		2278		0		0		0		2331	
% Splits	36.2 %	63.8 %	35.0 %	65.0 %	34.7 %	65.3 %	36.5 %	63.5 %	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	35.6 %	64.4%
Peak Vol.	07:15 174	04:15 180	07:30 184	05:15 236	08:00 199	05:30 225	07:30 175	05:30 217	-	-	-	-	-	-	07:30 181	05:30 212
P.H.F.	0.725	0.900	0.676	0.868	0.790	0.865	0.781	0.753							0.730	0.930

ADT ADT 2,239 AADT 2,239

MCV Associates, Inc.

4605-C Pinecrest Office Park Dr
Alexandria, VA 22312
703-914-4850

Site Code: 0003

Station ID: N:023585
Little Falls Pkwy - Between
Arlington Road & Hillandale Rd
Little Falls Pkwy - EB

Start Time	15-May-17		Tue		Wed		Thu		Fri		Sat		Sun		Average Da	
	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.
12:00	*	*	*	*	*	*	*	*	4	124	16	159	23	123	14	135
12:15	*	*	*	*	*	*	*	*	9	110	14	123	23	128	15	120
12:30	*	*	*	*	*	*	*	*	8	122	7	128	19	88	11	113
12:45	*	*	*	*	*	*	*	*	6	113	14	126	14	114	11	118
01:00	*	*	*	*	*	*	*	*	6	112	11	141	13	112	10	122
01:15	*	*	*	*	*	*	*	*	2	125	11	155	7	113	7	131
01:30	*	*	*	*	*	*	*	*	2	126	5	174	5	127	4	142
01:45	*	*	*	*	*	*	*	*	2	134	10	173	10	105	7	137
02:00	*	*	*	*	*	*	*	*	3	119	3	167	5	127	4	138
02:15	*	*	*	*	*	*	*	*	3	115	5	134	5	105	4	118
02:30	*	*	*	*	*	*	*	*	1	146	5	139	3	118	3	134
02:45	*	*	*	*	*	*	*	*	0	133	4	142	3	98	2	124
03:00	*	*	*	*	*	*	*	*	0	153	4	160	4	120	3	144
03:15	*	*	*	*	*	*	*	*	0	173	2	132	1	133	1	146
03:30	*	*	*	*	*	*	*	*	2	167	3	130	0	137	2	145
03:45	*	*	*	*	*	*	*	*	0	212	1	106	4	130	2	149
04:00	*	*	*	*	*	*	*	*	2	191	3	123	1	131	2	148
04:15	*	*	*	*	*	*	*	*	2	191	2	139	0	106	1	145
04:30	*	*	*	*	*	*	*	*	2	171	3	163	5	106	3	147
04:45	*	*	*	*	*	*	*	*	5	217	2	175	2	96	3	163
05:00	*	*	*	*	*	*	*	*	5	216	5	148	2	126	4	163
05:15	*	*	*	*	*	*	*	*	6	133	4	145	3	141	4	140
05:30	*	*	*	*	*	*	*	*	15	160	5	141	6	135	9	145
05:45	*	*	*	*	*	*	*	*	40	164	5	130	6	102	17	132
06:00	*	*	*	*	*	*	*	*	52	188	6	121	5	114	21	141
06:15	*	*	*	*	*	*	*	*	22	173	8	113	10	123	13	136
06:30	*	*	*	*	*	*	*	*	40	161	13	112	11	86	21	120
06:45	*	*	*	*	*	*	*	*	30	161	13	97	8	87	17	115
07:00	*	*	*	*	*	*	*	*	52	109	12	91	12	79	25	93
07:15	*	*	*	*	*	*	*	*	97	108	25	99	14	64	45	90
07:30	*	*	*	*	*	*	*	*	147	104	31	67	22	70	67	80
07:45	*	*	*	*	*	*	*	*	134	105	35	63	19	60	63	76
08:00	*	*	*	*	*	*	*	*	153	78	44	73	28	54	75	68
08:15	*	*	*	*	*	*	*	*	144	65	31	52	26	56	67	58
08:30	*	*	*	*	*	*	*	*	132	55	49	62	34	70	72	62
08:45	*	*	*	*	*	*	*	*	166	66	55	52	34	52	85	57
09:00	*	*	*	*	*	*	*	*	144	53	71	73	44	44	86	57
09:15	*	*	*	*	*	*	*	*	102	62	80	48	62	47	81	52
09:30	*	*	*	*	*	*	*	*	106	60	93	58	52	37	84	52
09:45	*	*	*	*	*	*	*	*	134	59	87	55	75	29	99	48
10:00	*	*	*	*	*	*	*	*	108	76	105	47	74	36	96	53
10:15	*	*	*	*	*	*	*	*	102	32	73	42	63	23	79	32
10:30	*	*	*	*	*	*	*	*	116	36	108	36	77	26	100	33
10:45	*	*	*	*	*	*	*	*	119	64	104	51	89	14	104	43
11:00	*	*	*	*	*	*	*	*	114	43	110	32	98	13	107	29
11:15	*	*	*	*	*	*	*	*	130	45	110	21	99	10	113	25
11:30	*	*	*	*	*	*	*	*	128	22	118	35	96	14	114	24
11:45	*	*	*	*	*	*	*	*	118	17	132	24	114	8	121	16
Total	0	0	0	0	0	0	0	0	2715	5569	1657	4977	1330	4037	1898	4859
Day Total	0	0	0	0	0	0	0	0	8284	6634	6634	5367	5367	6757	6757	
% Splits	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	32.8%	67.2%	25.0%	75.0%	24.8%	75.2%	28.1%	71.9%
Peak	-	-	-	-	-	-	-	-	08:00	04:15	11:00	01:15	11:00	03:15	11:00	04:15
Vol.	-	-	-	-	-	-	-	-	595	795	470	669	407	531	455	618
P.H.F.									0.896	0.916	0.890	0.961	0.893	0.969	0.940	0.948

MCV Associates, Inc.

4605-C Pinecrest Office Park Dr
Alexandria, VA 22312
703-914-4850

Site Code: 0003

Station ID: N:023585
Little Falls Pkwy - Between
Arlington Road & Hillandale Rd
Little Falls Pkwy - EB

Start Time	22-May-17		Tue		Wed		Thu		Fri		Sat		Sun		Average Da	
	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.
12:00	6	113	5	120	8	133	7	127	*	*	*	*	*	*	6	123
12:15	9	108	5	110	2	129	2	116	*	*	*	*	*	*	4	116
12:30	2	98	3	138	4	131	6	126	*	*	*	*	*	*	4	123
12:45	2	99	0	116	6	120	5	97	*	*	*	*	*	*	3	108
01:00	3	108	0	116	2	139	2	95	*	*	*	*	*	*	2	114
01:15	3	97	4	116	3	132	3	119	*	*	*	*	*	*	3	116
01:30	0	99	3	112	4	137	2	107	*	*	*	*	*	*	2	114
01:45	0	106	2	113	0	116	0	127	*	*	*	*	*	*	0	116
02:00	0	104	0	131	2	122	0	127	*	*	*	*	*	*	0	121
02:15	1	89	1	142	1	109	1	119	*	*	*	*	*	*	1	115
02:30	1	123	1	118	1	143	0	119	*	*	*	*	*	*	1	126
02:45	1	109	0	118	1	125	2	116	*	*	*	*	*	*	1	117
03:00	0	143	0	135	0	132	0	124	*	*	*	*	*	*	0	134
03:15	0	129	0	146	1	133	2	141	*	*	*	*	*	*	1	137
03:30	0	149	1	151	1	162	3	132	*	*	*	*	*	*	1	148
03:45	0	153	4	133	0	139	2	158	*	*	*	*	*	*	2	146
04:00	0	124	2	143	0	154	1	152	*	*	*	*	*	*	1	143
04:15	2	119	3	176	5	165	1	140	*	*	*	*	*	*	3	150
04:30	2	159	3	213	2	169	5	189	*	*	*	*	*	*	3	182
04:45	2	153	2	177	3	188	1	165	*	*	*	*	*	*	2	171
05:00	6	161	5	162	2	174	5	142	*	*	*	*	*	*	4	160
05:15	7	133	10	175	5	183	8	165	*	*	*	*	*	*	8	164
05:30	6	176	9	225	17	198	11	204	*	*	*	*	*	*	11	201
05:45	18	167	10	205	21	171	21	166	*	*	*	*	*	*	18	177
06:00	13	169	20	196	13	159	6	194	*	*	*	*	*	*	13	180
06:15	16	142	14	163	14	150	12	188	*	*	*	*	*	*	14	161
06:30	21	150	26	167	26	154	23	183	*	*	*	*	*	*	24	164
06:45	48	119	47	140	38	142	48	179	*	*	*	*	*	*	45	145
07:00	51	107	75	176	69	110	53	148	*	*	*	*	*	*	62	135
07:15	81	99	82	128	92	113	90	119	*	*	*	*	*	*	86	115
07:30	122	83	159	110	138	83	126	106	*	*	*	*	*	*	136	96
07:45	171	80	171	77	173	97	176	76	*	*	*	*	*	*	173	82
08:00	178	68	164	71	183	81	181	87	*	*	*	*	*	*	176	77
08:15	168	59	189	58	158	55	155	61	*	*	*	*	*	*	168	58
08:30	157	65	175	68	164	71	159	69	*	*	*	*	*	*	164	68
08:45	160	51	175	75	187	54	166	51	*	*	*	*	*	*	172	58
09:00	159	55	172	50	139	64	147	51	*	*	*	*	*	*	154	55
09:15	138	43	115	46	143	50	137	58	*	*	*	*	*	*	133	49
09:30	117	33	117	56	118	57	122	47	*	*	*	*	*	*	118	48
09:45	141	33	128	44	132	44	138	34	*	*	*	*	*	*	135	39
10:00	116	33	107	33	146	39	113	29	*	*	*	*	*	*	120	34
10:15	100	29	102	32	126	31	126	29	*	*	*	*	*	*	114	30
10:30	125	30	104	18	129	20	139	28	*	*	*	*	*	*	124	24
10:45	103	31	114	18	131	19	129	25	*	*	*	*	*	*	119	23
11:00	128	13	103	18	150	16	136	19	*	*	*	*	*	*	129	16
11:15	78	16	116	7	144	12	128	11	*	*	*	*	*	*	116	12
11:30	99	12	108	7	124	8	125	16	*	*	*	*	*	*	114	11
11:45	91	10	118	3	147	7	134	6	*	*	*	*	*	*	122	6
Total	2652	4549	2774	5252	2975	5140	2859	5087	0	0	0	0	0	0	2812	5008
Day Total	7201		8026		8115		7946		0		0		0		7820	
% Splits	36.8%	63.2%	34.6%	65.4%	36.7%	63.3%	36.0%	64.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	36.0%	64.0%
Peak	07:45	05:30	08:15	05:15	08:00	04:45	07:45	05:30	-	-	-	-	-	-	07:45	05:15
Vol.	674	654	711	801	692	743	671	752	-	-	-	-	-	-	681	722
P.H.F.	0.947	0.929	0.940	0.890	0.925	0.938	0.927	0.922							0.967	0.898

ADT ADT 7,368 AADT 7,368

MCV Associates, Inc.

4605-C Pinecrest Office Park Dr
Alexandria, VA 22312
703-914-4850

Site Code: 0003

Station ID: N:023585
Little Falls Pkwy - Between
Arlington Road & Hillandale Rd
Little Falls Pkwy - WB

Start Time	15-May-17		Tue		Wed		Thu		Fri		Sat		Sun		Average Da	
	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.
12:00	*	*	*	*	*	*	*	*	10	116	7	90	17	95	11	100
12:15	*	*	*	*	*	*	*	*	9	95	10	114	21	85	13	98
12:30	*	*	*	*	*	*	*	*	13	127	14	108	15	111	14	115
12:45	*	*	*	*	*	*	*	*	6	114	8	126	16	80	10	107
01:00	*	*	*	*	*	*	*	*	7	106	10	115	10	90	9	104
01:15	*	*	*	*	*	*	*	*	2	107	8	117	13	114	8	113
01:30	*	*	*	*	*	*	*	*	4	74	5	105	8	99	6	93
01:45	*	*	*	*	*	*	*	*	1	99	3	109	10	98	5	102
02:00	*	*	*	*	*	*	*	*	3	93	4	93	10	82	6	89
02:15	*	*	*	*	*	*	*	*	0	101	4	107	6	79	3	96
02:30	*	*	*	*	*	*	*	*	3	106	4	89	10	90	6	95
02:45	*	*	*	*	*	*	*	*	2	142	7	117	4	90	4	116
03:00	*	*	*	*	*	*	*	*	1	120	7	116	3	98	4	111
03:15	*	*	*	*	*	*	*	*	1	116	4	124	7	92	4	111
03:30	*	*	*	*	*	*	*	*	0	120	2	101	2	85	1	102
03:45	*	*	*	*	*	*	*	*	2	128	2	79	5	89	3	99
04:00	*	*	*	*	*	*	*	*	2	132	2	107	1	96	2	112
04:15	*	*	*	*	*	*	*	*	0	147	3	118	6	83	3	116
04:30	*	*	*	*	*	*	*	*	1	163	4	101	2	74	2	113
04:45	*	*	*	*	*	*	*	*	3	137	2	82	4	98	3	106
05:00	*	*	*	*	*	*	*	*	2	151	1	79	0	94	1	108
05:15	*	*	*	*	*	*	*	*	3	132	1	83	0	83	1	99
05:30	*	*	*	*	*	*	*	*	7	124	1	86	3	91	4	100
05:45	*	*	*	*	*	*	*	*	12	142	1	88	1	84	5	105
06:00	*	*	*	*	*	*	*	*	30	132	9	66	2	81	14	93
06:15	*	*	*	*	*	*	*	*	28	133	7	98	3	87	13	106
06:30	*	*	*	*	*	*	*	*	20	138	10	80	10	67	13	95
06:45	*	*	*	*	*	*	*	*	14	109	9	68	11	74	11	84
07:00	*	*	*	*	*	*	*	*	30	109	16	69	7	67	18	82
07:15	*	*	*	*	*	*	*	*	25	95	16	72	17	64	19	77
07:30	*	*	*	*	*	*	*	*	55	100	21	66	11	50	29	72
07:45	*	*	*	*	*	*	*	*	80	81	29	65	13	32	41	59
08:00	*	*	*	*	*	*	*	*	114	72	43	59	21	41	59	57
08:15	*	*	*	*	*	*	*	*	120	61	42	40	25	40	62	47
08:30	*	*	*	*	*	*	*	*	114	56	54	49	26	34	65	46
08:45	*	*	*	*	*	*	*	*	117	40	50	42	44	25	70	36
09:00	*	*	*	*	*	*	*	*	130	36	94	45	49	44	91	42
09:15	*	*	*	*	*	*	*	*	110	37	80	34	49	30	80	34
09:30	*	*	*	*	*	*	*	*	102	31	66	27	40	38	69	32
09:45	*	*	*	*	*	*	*	*	74	51	94	32	57	18	75	34
10:00	*	*	*	*	*	*	*	*	96	33	74	30	73	19	81	27
10:15	*	*	*	*	*	*	*	*	73	36	93	17	58	11	75	21
10:30	*	*	*	*	*	*	*	*	68	32	99	37	86	11	84	27
10:45	*	*	*	*	*	*	*	*	82	23	95	31	81	11	86	22
11:00	*	*	*	*	*	*	*	*	92	37	97	30	67	11	85	26
11:15	*	*	*	*	*	*	*	*	82	20	85	21	83	9	83	17
11:30	*	*	*	*	*	*	*	*	88	41	98	22	85	13	90	25
11:45	*	*	*	*	*	*	*	*	100	31	97	16	88	7	95	18
Total	0	0	0	0	0	0	0	0	1938	4426	1492	3570	1180	3064	1536	3689
Day Total	0	0	0	0	0	0	0	0	6364	5062	4244	5225				
% Splits	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	30.5%	69.5%	29.5%	70.5%	27.8%	72.2%	29.4%	70.6%
Peak	-	-	-	-	-	-	-	-	08:15	04:15	10:15	00:30	11:00	01:00	11:00	04:00
Vol.	-	-	-	-	-	-	-	-	481	598	384	466	323	401	353	447
P.H.F.									0.925	0.917	0.970	0.925	0.918	0.879	0.929	0.963

MCV Associates, Inc.

4605-C Pinecrest Office Park Dr
Alexandria, VA 22312
703-914-4850

Site Code: 0003

Station ID: N:023585
Little Falls Pkwy - Between
Arlington Road & Hillandale Rd
Little Falls Pkwy - WB

Start Time	22-May-17		Tue		Wed		Thu		Fri		Sat		Sun		Average Da	
	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.
12:00	10	93	3	103	4	97	8	96	*	*	*	*	*	*	6	97
12:15	4	71	4	83	4	105	2	100	*	*	*	*	*	*	4	90
12:30	3	81	4	83	8	107	5	98	*	*	*	*	*	*	5	92
12:45	6	108	3	92	5	87	4	95	*	*	*	*	*	*	4	96
01:00	0	73	2	83	1	90	3	103	*	*	*	*	*	*	2	87
01:15	4	69	1	91	4	85	3	87	*	*	*	*	*	*	3	83
01:30	3	79	3	80	4	95	3	82	*	*	*	*	*	*	3	84
01:45	0	64	3	101	0	94	4	86	*	*	*	*	*	*	2	86
02:00	0	74	0	93	1	91	3	81	*	*	*	*	*	*	1	85
02:15	2	97	4	98	0	88	0	102	*	*	*	*	*	*	2	96
02:30	1	96	0	87	2	96	0	84	*	*	*	*	*	*	1	91
02:45	2	95	1	85	1	113	1	95	*	*	*	*	*	*	1	97
03:00	1	94	1	121	5	117	1	109	*	*	*	*	*	*	2	110
03:15	3	113	1	110	3	120	5	102	*	*	*	*	*	*	3	111
03:30	0	83	0	92	0	115	0	117	*	*	*	*	*	*	0	102
03:45	2	112	0	125	1	122	1	99	*	*	*	*	*	*	1	114
04:00	2	104	0	137	0	120	2	122	*	*	*	*	*	*	1	121
04:15	0	105	1	111	0	138	1	143	*	*	*	*	*	*	0	124
04:30	1	107	0	119	2	125	1	131	*	*	*	*	*	*	1	120
04:45	0	114	1	154	1	162	1	157	*	*	*	*	*	*	1	147
05:00	2	118	5	140	3	128	3	145	*	*	*	*	*	*	3	133
05:15	3	133	3	148	2	146	2	130	*	*	*	*	*	*	2	139
05:30	5	125	10	155	5	150	5	147	*	*	*	*	*	*	6	144
05:45	11	128	12	153	14	149	8	127	*	*	*	*	*	*	11	139
06:00	19	117	8	139	18	138	16	125	*	*	*	*	*	*	15	130
06:15	10	136	15	120	15	149	7	125	*	*	*	*	*	*	12	132
06:30	18	126	22	145	15	140	13	124	*	*	*	*	*	*	17	134
06:45	18	113	26	102	22	124	20	131	*	*	*	*	*	*	22	118
07:00	24	98	34	123	37	136	34	129	*	*	*	*	*	*	32	122
07:15	32	77	40	143	30	119	27	113	*	*	*	*	*	*	32	113
07:30	51	83	45	92	61	106	57	78	*	*	*	*	*	*	54	90
07:45	88	70	77	91	72	86	82	72	*	*	*	*	*	*	80	80
08:00	86	65	107	88	99	67	100	75	*	*	*	*	*	*	98	74
08:15	99	47	116	80	113	62	108	49	*	*	*	*	*	*	109	60
08:30	129	51	146	47	139	61	128	41	*	*	*	*	*	*	136	50
08:45	102	49	120	46	123	34	116	38	*	*	*	*	*	*	115	42
09:00	121	24	111	44	123	41	116	36	*	*	*	*	*	*	118	36
09:15	118	47	99	45	110	43	131	40	*	*	*	*	*	*	114	44
09:30	115	37	104	31	107	35	112	30	*	*	*	*	*	*	110	33
09:45	85	18	87	33	95	28	114	27	*	*	*	*	*	*	95	26
10:00	94	21	71	27	73	29	88	33	*	*	*	*	*	*	82	28
10:15	72	14	76	20	69	19	66	19	*	*	*	*	*	*	71	18
10:30	55	19	82	17	84	25	65	23	*	*	*	*	*	*	72	21
10:45	83	16	84	26	85	16	90	18	*	*	*	*	*	*	86	19
11:00	68	13	86	7	86	19	84	12	*	*	*	*	*	*	81	13
11:15	91	8	101	14	86	14	88	12	*	*	*	*	*	*	92	12
11:30	73	4	73	6	78	8	74	8	*	*	*	*	*	*	74	6
11:45	83	6	87	16	103	16	90	11	*	*	*	*	*	*	91	12
Total	1799	3595	1879	4146	1913	4255	1892	4007	0	0	0	0	0	0	1873	4001
Day Total	5394		6025		6168		5899		0		0		0		5874	
% Splits	33.4 %	66.6 %	31.2 %	68.8 %	31.0 %	69.0 %	32.1 %	67.9 %	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	31.9 %	68.1%
Peak	08:30	05:45	08:15	04:45	08:15	04:45	08:30	04:45	-	-	-	-	-	-	08:30	04:45
Vol.	470	507	493	597	498	586	491	579	-	-	-	-	-	-	483	563
P.H.F.	0.911	0.932	0.844	0.963	0.896	0.904	0.937	0.922							0.888	0.957

ADT ADT 5,594 AADT 5,594

MCV Associates INC

4605-C Pinecrest Office Park Dr
Alexandria, VA 22312

File Name : Little Falls Pkwy @ Arlington Rd

Site Code : 00000000

Start Date : 6/1/2017

Page No : 1

Groups Printed- Vehicles

	Little Falls Pkwy From North					Arlington Rd From East					Little Falls Pkwy From South					From West					
Start Time	Left	Thru	Right	U Turns	App. Total	Left	Thru	Right	U Turns	App. Total	Left	Thru	Right	U Turns	App. Total	Left	Thru	Right	U Turns	App. Total	Int. Total
07:00 AM	0	43	0	0	43	43	0	5	0	48	0	9	26	0	35	0	0	0	0	0	126
07:15 AM	0	58	0	0	58	60	0	1	0	61	0	8	53	0	61	0	0	0	0	0	180
07:30 AM	0	92	0	0	92	87	0	2	0	89	0	13	65	0	78	0	0	0	0	0	259
07:45 AM	0	88	0	0	88	88	0	5	0	93	0	23	76	0	99	0	0	0	0	0	280
Total	0	281	0	0	281	278	0	13	0	291	0	53	220	0	273	0	0	0	0	0	845
08:00 AM	0	84	0	0	84	104	0	9	0	113	0	25	76	0	101	0	0	0	0	0	298
08:15 AM	0	79	0	0	79	80	0	2	0	82	0	23	105	0	128	0	0	0	0	0	289
08:30 AM	1	92	0	0	93	70	0	5	0	75	0	19	103	0	122	0	0	0	0	0	290
08:45 AM	0	94	0	0	94	83	0	2	0	85	0	21	97	0	118	0	0	0	0	0	297
Total	1	349	0	0	350	337	0	18	0	355	0	88	381	0	469	0	0	0	0	0	1174
04:00 PM	2	55	0	3	60	113	0	0	0	113	0	55	93	0	148	0	0	0	0	0	321
04:15 PM	1	67	0	0	68	139	0	5	1	145	0	47	75	0	122	0	0	0	0	0	335
04:30 PM	4	46	0	0	50	116	0	8	0	124	0	54	93	0	147	0	0	0	0	0	321
04:45 PM	1	46	0	0	47	121	0	4	0	125	0	58	88	0	146	0	0	0	0	0	318
Total	8	214	0	3	225	489	0	17	1	507	0	214	349	0	563	0	0	0	0	0	1295
05:00 PM	0	71	0	2	73	115	0	4	0	119	0	55	89	0	144	0	0	0	0	0	336
05:15 PM	1	65	0	0	66	117	0	8	0	125	0	50	95	0	145	0	0	0	0	0	336
05:30 PM	7	71	0	0	78	96	0	10	0	106	0	60	93	0	153	0	0	0	0	0	337
05:45 PM	2	78	0	0	80	83	0	22	0	105	0	59	99	0	158	0	0	0	0	0	343
Total	10	285	0	2	297	411	0	44	0	455	0	224	376	0	600	0	0	0	0	0	1352
06:00 PM	1	58	0	0	59	103	0	11	0	114	0	40	88	0	128	0	0	0	0	0	301
06:15 PM	1	54	0	1	56	90	0	6	0	96	0	53	86	0	139	0	0	0	0	0	291
06:30 PM	2	59	0	0	61	89	0	7	0	96	0	41	76	0	117	0	0	0	0	0	274
06:45 PM	2	35	0	1	38	71	0	2	0	73	0	44	68	0	112	0	0	0	0	0	223
Total	6	206	0	2	214	353	0	26	0	379	0	178	318	0	496	0	0	0	0	0	1089
Grand Total	25	1335	0	7	1367	1868	0	118	1	1987	0	757	1644	0	2401	0	0	0	0	0	5755
Apprch %	1.8	97.7	0	0.5		94	0	5.9	0.1		0	31.5	68.5	0		0	0	0	0		
Total %	0.4	23.2	0	0.1	23.8	32.5	0	2.1	0	34.5	0	13.2	28.6	0	41.7	0	0	0	0	0	

MCV Associates INC

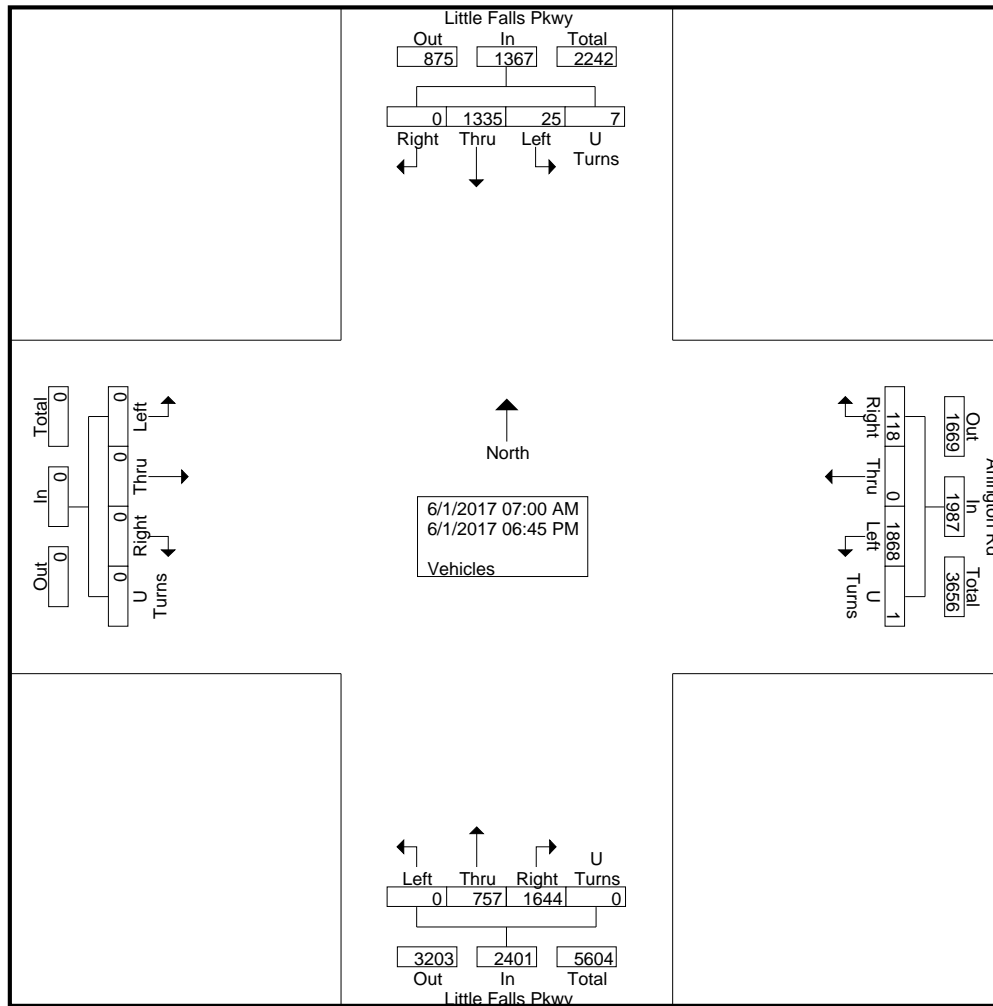
4605-C Pinecrest Office Park Dr
Alexandria, VA 22312

File Name : Little Falls Pkwy @ Arlington Rd

Site Code : 00000000

Start Date : 6/1/2017

Page No : 2



MCV Associates INC

4605-C Pinecrest Office Park Dr
Alexandria, VA 22312

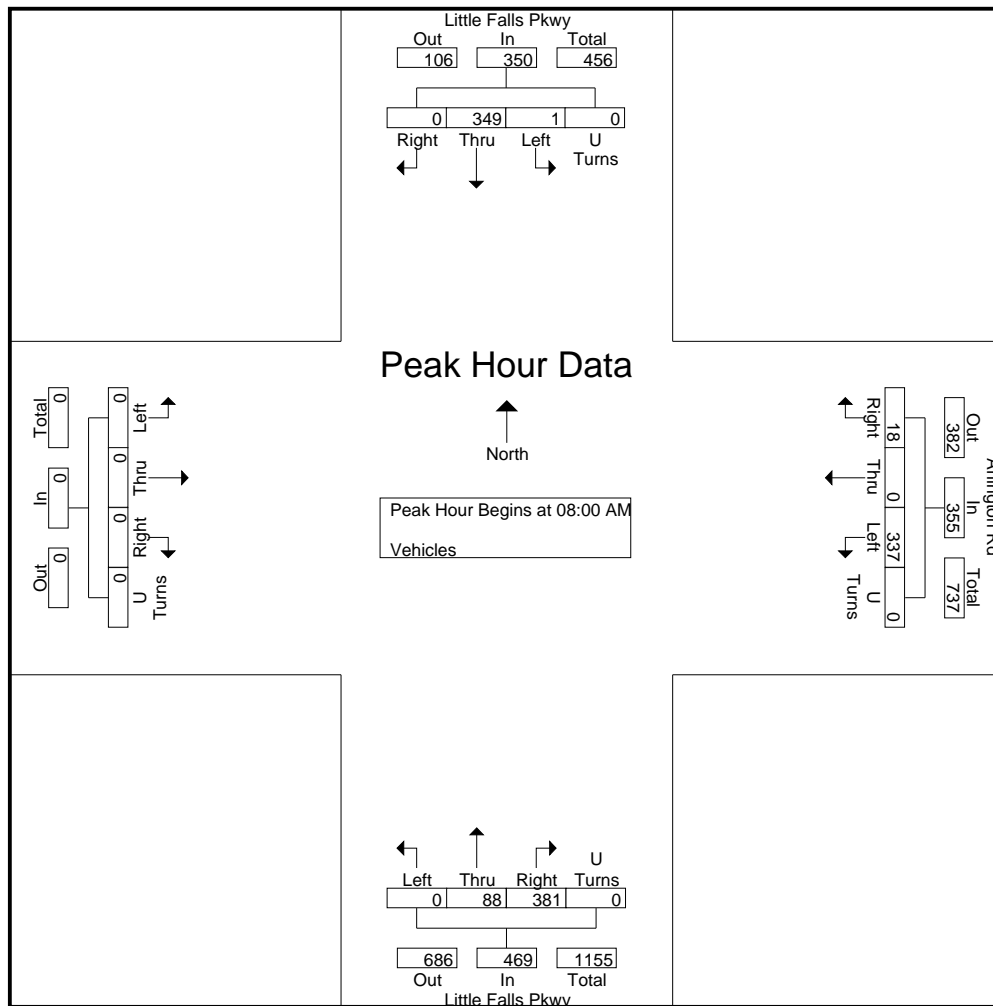
File Name : Little Falls Pkwy @ Arlington Rd

Site Code : 00000000

Start Date : 6/1/2017

Page No : 3

	Little Falls Pkwy From North					Arlington Rd From East					Little Falls Pkwy From South					From West					
Start Time	Left	Thru	Right	U Turns	App. Total	Left	Thru	Right	U Turns	App. Total	Left	Thru	Right	U Turns	App. Total	Left	Thru	Right	U Turns	App. Total	Int. Total
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 08:00 AM																					
08:00 AM	0	84	0	0	84	104	0	9	0	113	0	25	76	0	101	0	0	0	0	0	298
08:15 AM	0	79	0	0	79	80	0	2	0	82	0	23	105	0	128	0	0	0	0	0	289
08:30 AM	1	92	0	0	93	70	0	5	0	75	0	19	103	0	122	0	0	0	0	0	290
08:45 AM	0	94	0	0	94	83	0	2	0	85	0	21	97	0	118	0	0	0	0	0	297
Total Volume	1	349	0	0	350	337	0	18	0	355	0	88	381	0	469	0	0	0	0	0	1174
% App. Total	0.3	99.7	0	0		94.9	0	5.1	0		0	18.8	81.2	0		0	0	0	0	0	
PHF	.250	.928	.000	.000	.931	.810	.000	.500	.000	.785	.000	.880	.907	.000	.916	.000	.000	.000	.000	.000	.985



MCV Associates INC

4605-C Pinecrest Office Park Dr
Alexandria, VA 22312

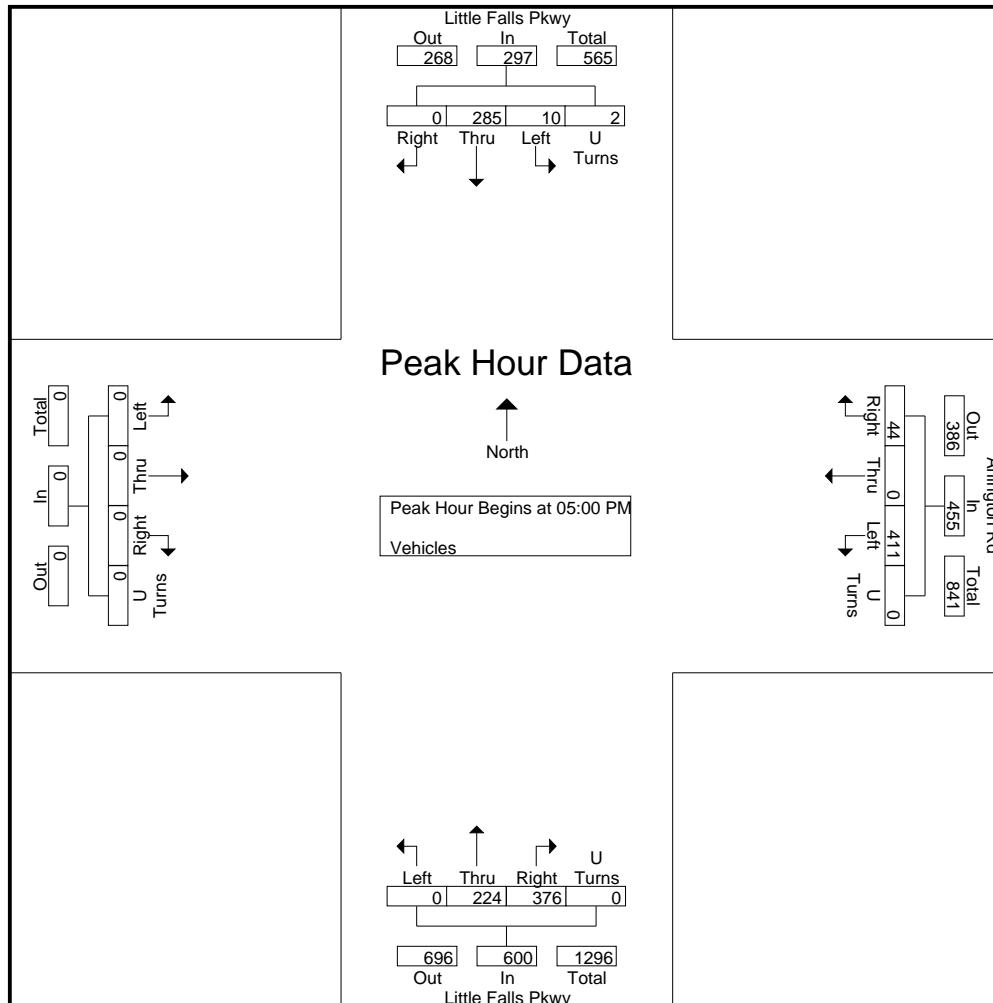
File Name : Little Falls Pkwy @ Arlington Rd

Site Code : 00000000

Start Date : 6/1/2017

Page No : 4

	Little Falls Pkwy From North					Arlington Rd From East					Little Falls Pkwy From South					From West					
Start Time	Left	Thru	Right	U Turns	App. Total	Left	Thru	Right	U Turns	App. Total	Left	Thru	Right	U Turns	App. Total	Left	Thru	Right	U Turns	App. Total	Int. Total
Peak Hour Analysis From 04:00 PM to 06:45 PM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 05:00 PM																					
05:00 PM	0	71	0	2	73	115	0	4	0	119	0	55	89	0	144	0	0	0	0	0	336
05:15 PM	1	65	0	0	66	117	0	8	0	125	0	50	95	0	145	0	0	0	0	0	336
05:30 PM	7	71	0	0	78	96	0	10	0	106	0	60	93	0	153	0	0	0	0	0	337
05:45 PM	2	78	0	0	80	83	0	22	0	105	0	59	99	0	158	0	0	0	0	0	343
Total Volume	10	285	0	2	297	411	0	44	0	455	0	224	376	0	600	0	0	0	0	0	1352
% App. Total	3.4	96	0	0.7		90.3	0	9.7	0		0	37.3	62.7	0		0	0	0	0		
PHF	.357	.913	.000	.250	.928	.878	.000	.500	.000	.910	.000	.933	.949	.000	.949	.000	.000	.000	.000	.000	.985



MCV Associates INC

4605-C Pinecrest Office Park Dr
Alexandria, VA 22312

File Name : Little Falls Pkwy at Hillandale Rd

Site Code : 00000000

Start Date : 6/1/2017

Page No : 1

Groups Printed- Vehicles

	Little Falls Pkwy From North					Hillandale Rd From East					Little Falls Pkwy From South					From West					
Start Time	Left	Thru	Right	U Turns	App. Total	Left	Thru	Right	U Turns	App. Total	Left	Thru	Right	U Turns	App. Total	Left	Thru	Right	U Turns	App. Total	Int. Total
07:00 AM	0	83	0	0	83	25	0	0	0	25	0	39	25	0	64	0	0	0	0	0	172
07:15 AM	0	116	0	0	116	56	0	1	0	57	0	60	35	0	95	0	0	0	0	0	268
07:30 AM	5	179	0	0	184	38	0	0	0	38	0	76	40	0	116	0	0	0	0	0	338
07:45 AM	1	165	0	0	166	62	0	5	0	67	0	95	60	0	155	0	0	0	0	0	388
Total	6	543	0	0	549	181	0	6	0	187	0	270	160	0	430	0	0	0	0	0	1166
08:00 AM	2	186	0	0	188	37	0	0	0	37	0	104	54	0	158	0	0	0	0	0	383
08:15 AM	1	160	0	1	162	39	0	2	0	41	0	127	65	0	192	0	0	0	0	0	395
08:30 AM	2	166	0	0	168	40	0	2	0	42	0	128	55	0	183	0	0	0	0	0	393
08:45 AM	2	171	0	1	174	41	0	0	0	41	0	113	67	0	180	0	0	0	0	0	395
Total	7	683	0	2	692	157	0	4	0	161	0	472	241	0	713	0	0	0	0	0	1566
04:00 PM	1	167	0	0	168	64	0	2	0	66	0	139	68	0	207	0	0	0	0	0	441
04:15 PM	4	198	0	0	202	61	0	3	0	64	0	120	77	0	197	0	0	0	0	0	463
04:30 PM	0	165	0	1	166	48	0	2	0	50	0	145	92	0	237	0	0	0	0	0	453
04:45 PM	3	158	0	1	162	55	0	0	0	55	0	145	70	0	215	0	0	0	0	0	432
Total	8	688	0	2	698	228	0	7	0	235	0	549	307	0	856	0	0	0	0	0	1789
05:00 PM	4	184	0	1	189	46	0	3	0	49	0	150	72	0	222	0	0	0	0	0	460
05:15 PM	0	181	0	1	182	52	0	3	0	55	0	145	83	0	228	0	0	0	0	0	465
05:30 PM	6	158	0	0	164	64	0	1	0	65	0	152	83	0	235	0	0	0	0	0	464
05:45 PM	2	157	0	0	159	81	0	5	0	86	0	139	108	0	247	0	0	0	0	0	492
Total	12	680	0	2	694	243	0	12	0	255	0	586	346	0	932	0	0	0	0	0	1881
06:00 PM	3	160	0	0	163	53	0	2	0	55	0	122	94	0	216	0	0	0	0	0	434
06:15 PM	6	140	0	0	146	47	0	3	0	50	0	142	83	0	225	0	0	0	0	0	421
06:30 PM	8	143	0	1	152	64	0	4	0	68	0	114	69	0	183	0	0	0	0	0	403
06:45 PM	2	111	0	0	113	53	0	0	0	53	0	116	79	0	195	0	0	0	0	0	361
Total	19	554	0	1	574	217	0	9	0	226	0	494	325	0	819	0	0	0	0	0	1619
Grand Total	52	3148	0	7	3207	1026	0	38	0	1064	0	2371	1379	0	3750	0	0	0	0	0	8021
Apprch %	1.6	98.2	0	0.2		96.4	0	3.6	0		0	63.2	36.8	0		0	0	0	0		
Total %	0.6	39.2	0	0.1	40	12.8	0	0.5	0	13.3	0	29.6	17.2	0	46.8	0	0	0	0	0	

MCV Associates INC

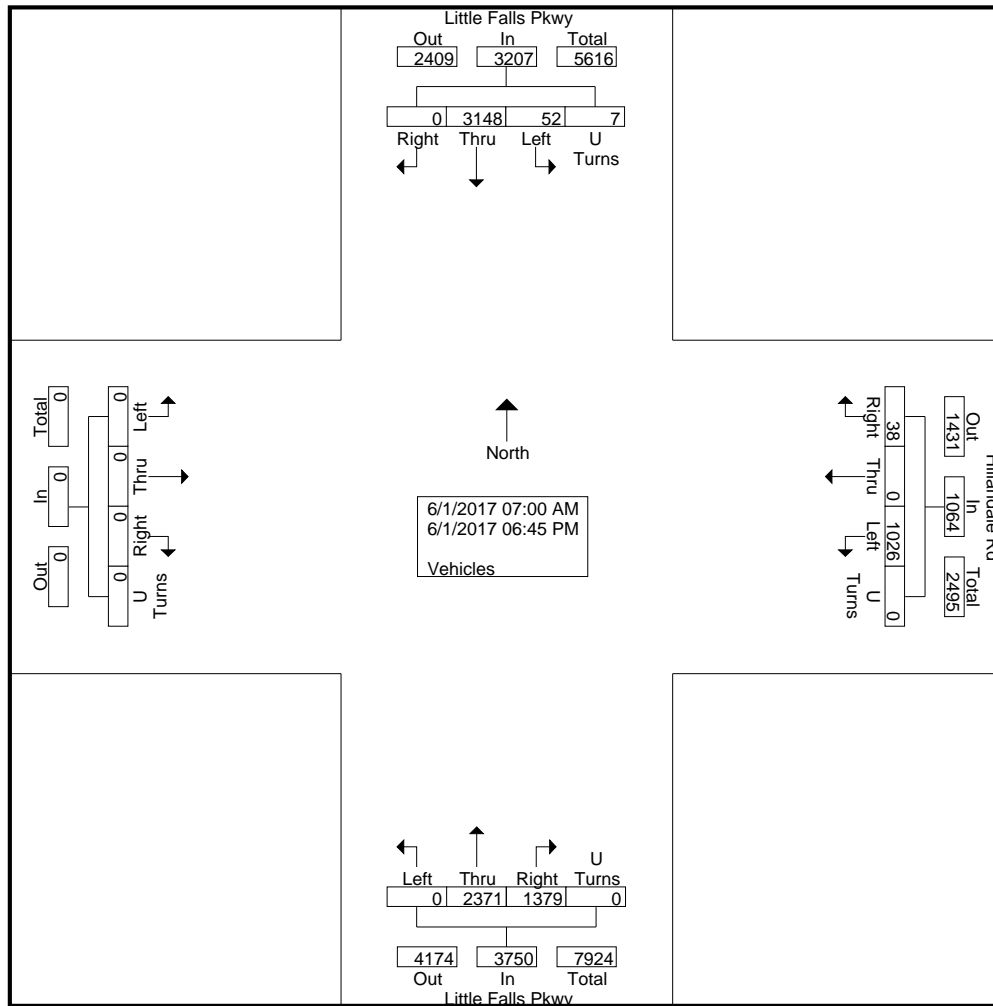
4605-C Pinecrest Office Park Dr
Alexandria, VA 22312

File Name : Little Falls Pkwy at Hillandale Rd

Site Code : 00000000

Start Date : 6/1/2017

Page No : 2



MCV Associates INC

4605-C Pinecrest Office Park Dr
Alexandria, VA 22312

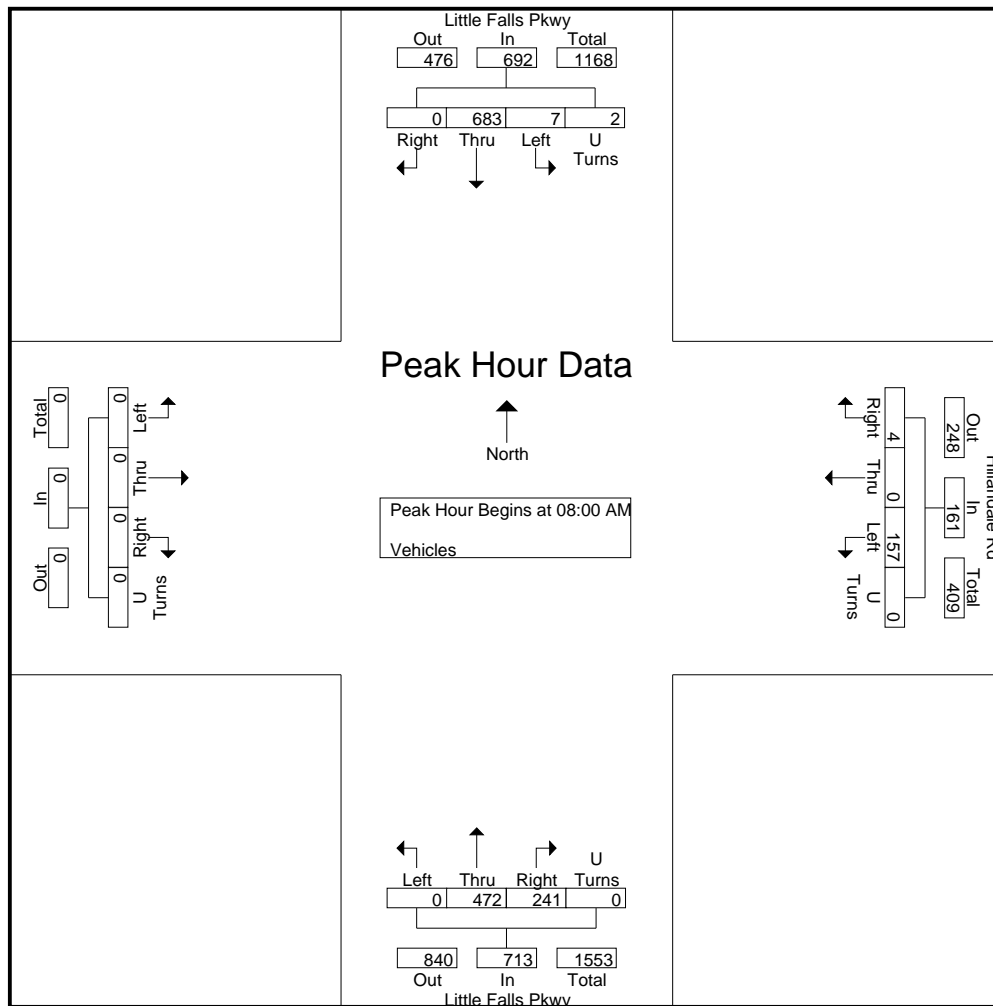
File Name : Little Falls Pkwy at Hillandale Rd

Site Code : 00000000

Start Date : 6/1/2017

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	Little Falls Pkwy From North					Hillandale Rd From East					Little Falls Pkwy From South					From West					
Start Time	Left	Thru	Right	U Turns	App. Total	Left	Thru	Right	U Turns	App. Total	Left	Thru	Right	U Turns	App. Total	Left	Thru	Right	U Turns	App. Total	Int. Total
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 08:00 AM																					
08:00 AM	2	186	0	0	188	37	0	0	0	37	0	104	54	0	158	0	0	0	0	0	383
08:15 AM	1	160	0	1	162	39	0	2	0	41	0	127	65	0	192	0	0	0	0	0	395
08:30 AM	2	166	0	0	168	40	0	2	0	42	0	128	55	0	183	0	0	0	0	0	393
08:45 AM	2	171	0	1	174	41	0	0	0	41	0	113	67	0	180	0	0	0	0	0	395
Total Volume	7	683	0	2	692	157	0	4	0	161	0	472	241	0	713	0	0	0	0	0	1566
% App. Total	1	98.7	0	0.3		97.5	0	2.5	0		0	66.2	33.8	0		0	0	0	0	0	
PHF	.875	.918	.000	.500	.920	.957	.000	.500	.000	.958	.000	.922	.899	.000	.928	.000	.000	.000	.000	.000	.991



MCV Associates INC

4605-C Pinecrest Office Park Dr
Alexandria, VA 22312

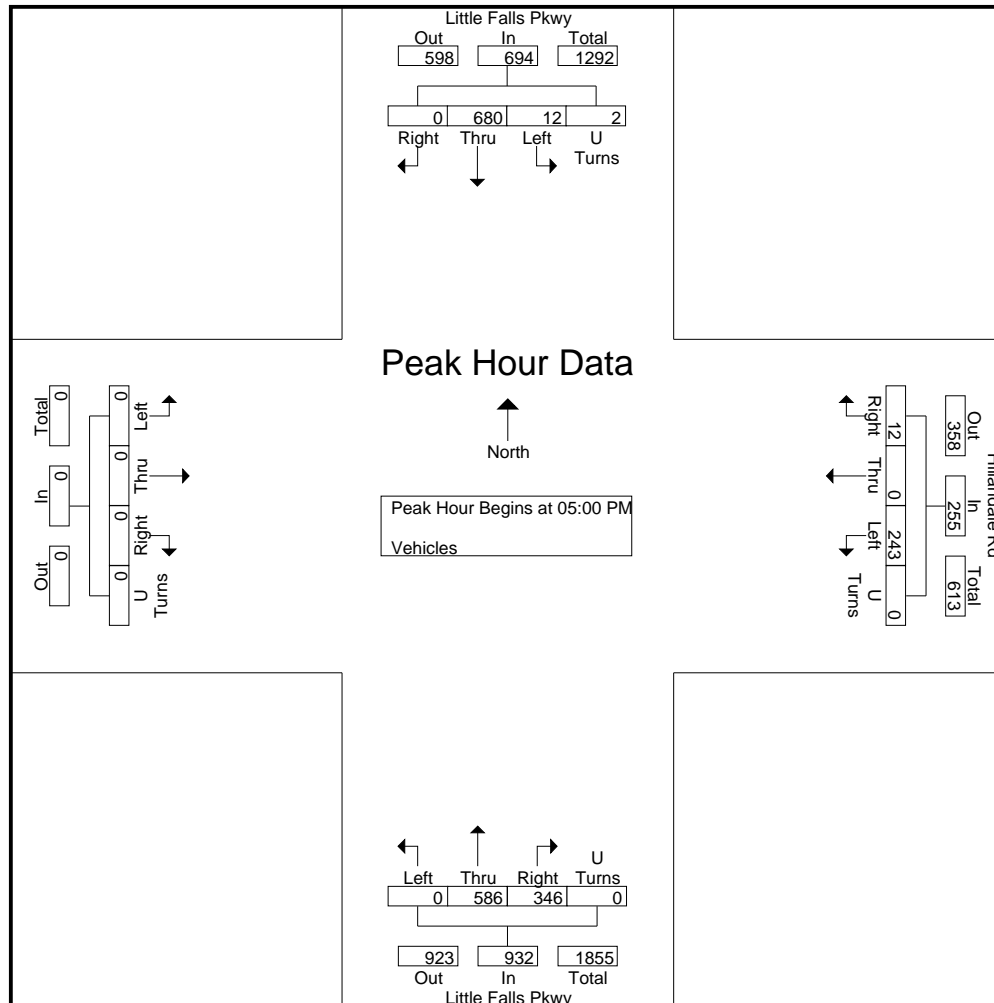
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Site Code : 00000000

Start Date : 6/1/2017

Page No : 4

	Little Falls Pkwy From North					Hillendale Rd From East					Little Falls Pkwy From South					From West					
Start Time	Left	Thru	Right	U Turns	App. Total	Left	Thru	Right	U Turns	App. Total	Left	Thru	Right	U Turns	App. Total	Left	Thru	Right	U Turns	App. Total	Int. Total
Peak Hour Analysis From 04:00 PM to 06:45 PM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 05:00 PM																					
05:00 PM	4	184	0	1	189	46	0	3	0	49	0	150	72	0	222	0	0	0	0	0	460
05:15 PM	0	181	0	1	182	52	0	3	0	55	0	145	83	0	228	0	0	0	0	0	465
05:30 PM	6	158	0	0	164	64	0	1	0	65	0	152	83	0	235	0	0	0	0	0	464
05:45 PM	2	157	0	0	159	81	0	5	0	86	0	139	108	0	247	0	0	0	0	0	492
Total Volume	12	680	0	2	694	243	0	12	0	255	0	586	346	0	932	0	0	0	0	0	1881
% App. Total	1.7	98	0	0.3		95.3	0	4.7	0		0	62.9	37.1	0		0	0	0	0		
PHF	.500	.924	.000	.500	.918	.750	.000	.600	.000	.741	.000	.964	.801	.000	.943	.000	.000	.000	.000	.000	.956



Appendix F

Concept Site Plan Sketches

- Alternate A: Permanent Road Diet
- Alternate A Rendering
- Alternate B: Trail Reorientation to Arlington Road Signal
- Alternate B Rendering
- Alternate C: Pedestrian Bridge
- Alternate C Rendering
- Concept “Ultimate” Site Plan

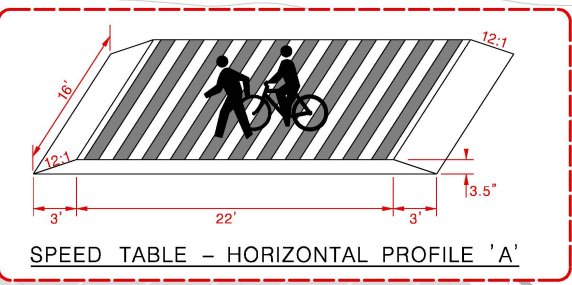
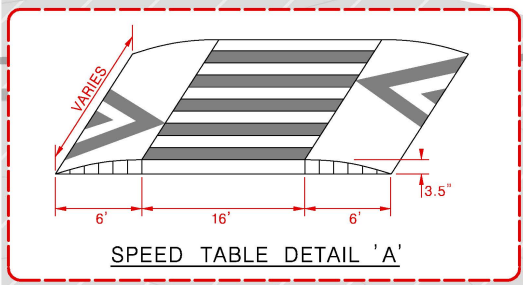
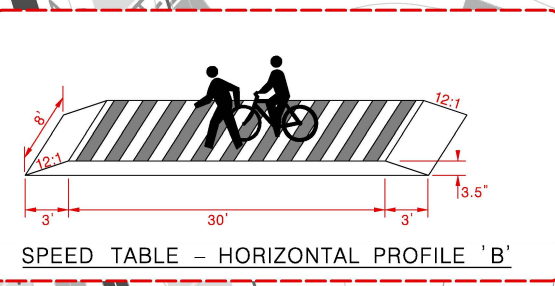
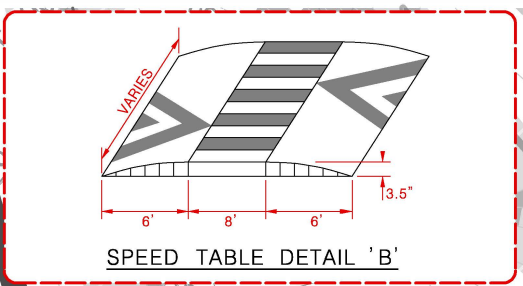
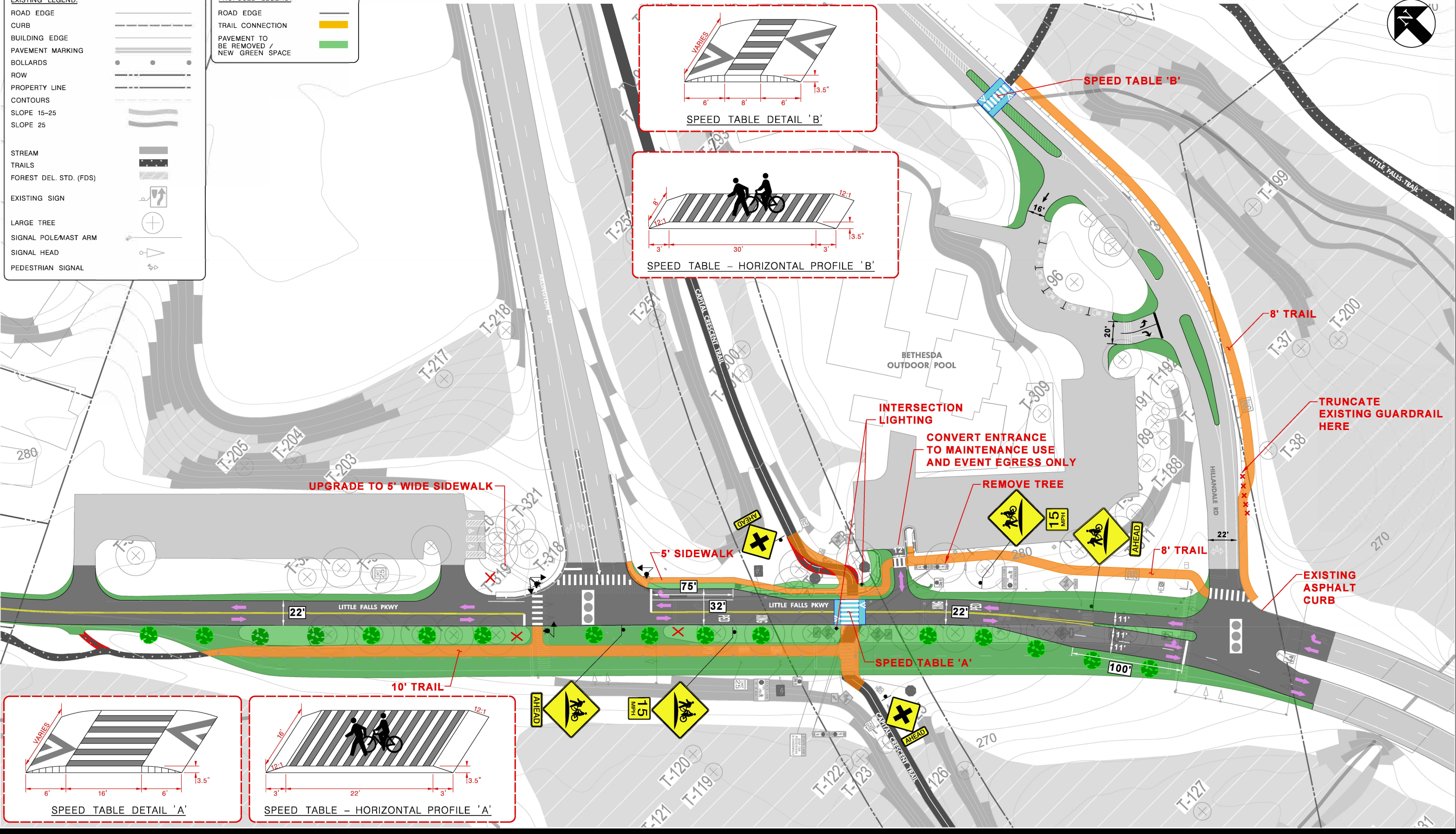
APPENDIX F CONCEPT SITE PLAN SKETCHES

EXISTING LEGEND:

- ROAD EDGE
- CURB
- BUILDING EDGE
- PAVEMENT MARKING
- BOLLARDS
- ROW
- PROPERTY LINE
- CONTOURS
- SLOPE 15-25
- SLOPE 25
- STREAM
- TRAILS
- FOREST DEL. STD. (FDS)
- EXISTING SIGN
- LARGE TREE
- SIGNAL POLE/MAST ARM
- SIGNAL HEAD
- PEDESTRIAN SIGNAL

PROPOSED LEGEND:

- ROAD EDGE
- TRAIL CONNECTION
- PAVEMENT TO BE REMOVED / NEW GREEN SPACE



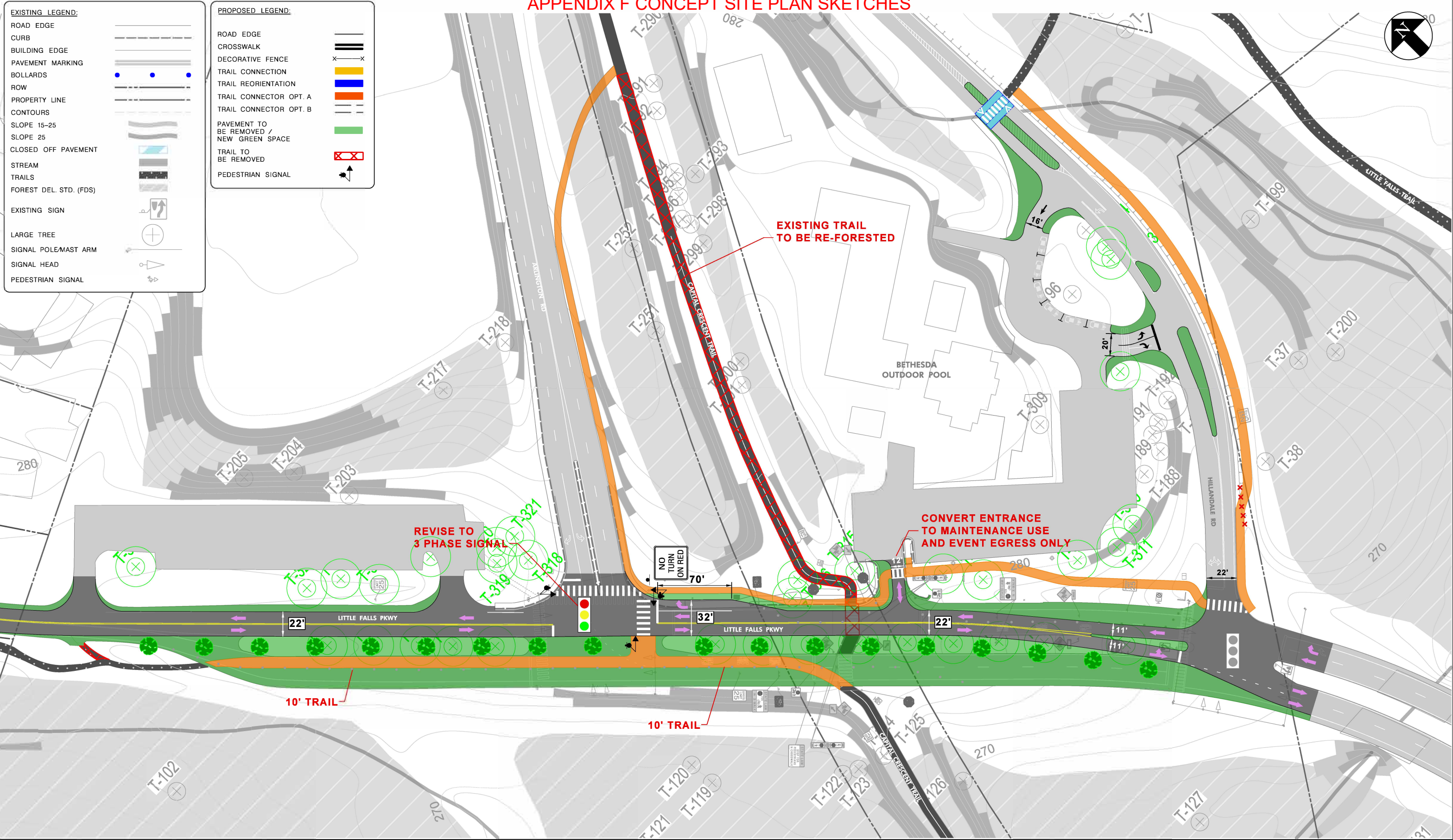
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APPENDIX F CONCEPT SITE PLAN SKETCHES

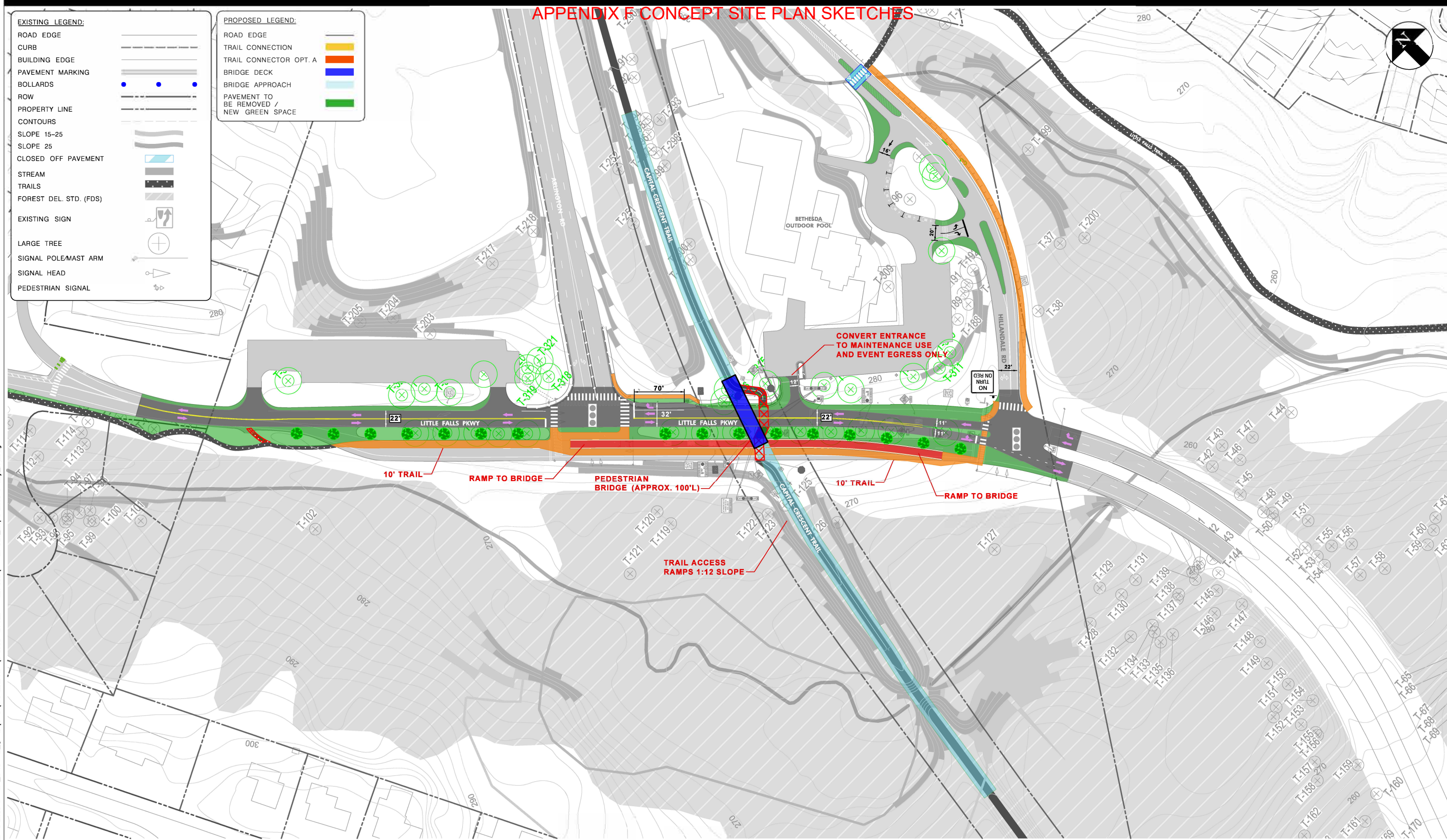
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CURB	---
BUILDING EDGE	---
PAVEMENT MARKING	---
BOLLARDS	• • •
ROW	---
PROPERTY LINE	---
CONTOURS	---
SLOPE 15-25	---
SLOPE 25	---
CLOSED OFF PAVEMENT	---
STREAM	---
TRAILS	---
FOREST DEL. STD. (FDS)	---
EXISTING SIGN	---
LARGE TREE	⊕
SIGNAL POLE/MAST ARM	---
SIGNAL HEAD	---
PEDESTRIAN SIGNAL	---

PROPOSED LEGEND:	
ROAD EDGE	---
CROSSWALK	---
DECORATIVE FENCE	X X
TRAIL CONNECTION	---
TRAIL REORIENTATION	---
TRAIL CONNECTOR OPT. A	---
TRAIL CONNECTOR OPT. B	---
PAVEMENT TO BE REMOVED / NEW GREEN SPACE	---
TRAIL TO BE REMOVED	---
PEDESTRIAN SIGNAL	---





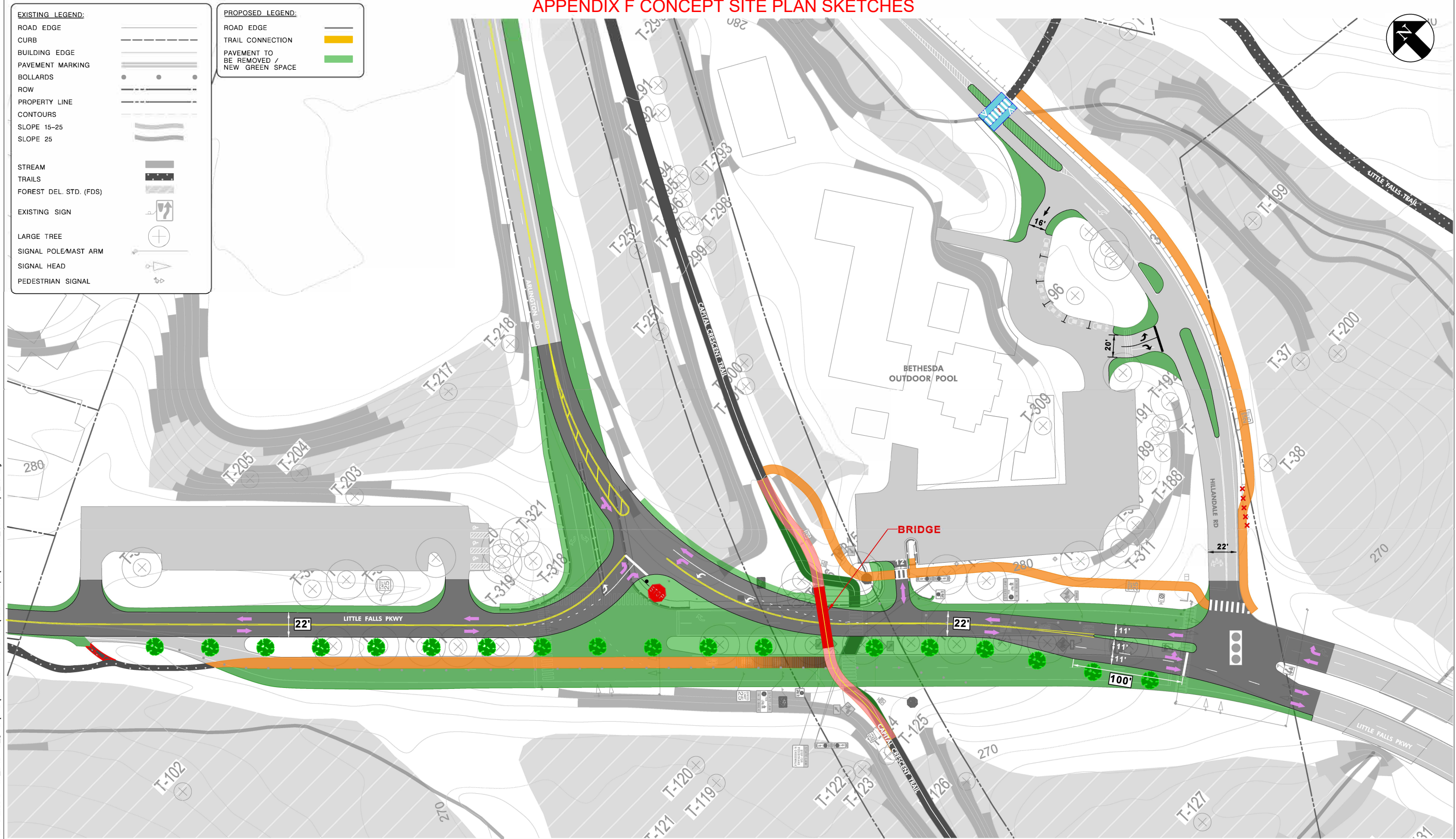
APPENDIX F CONCEPT SITE PLAN SKETCHES



APPENDIX F CONCEPT SITE PLAN SKETCHES



APPENDIX F CONCEPT SITE PLAN SKETCHES



Appendix G

Correspondence

Please note that correspondence sent directly to the Planning Board, or not sent to Montgomery Parks, may not be included in this Appendix.

- July 10, 2018 Coalition for the Capital Crescent Trail Letter
 - August 7, 2018 Westmoreland Citizen Association Feedback
 - August 14, 2018 Citizens Coordinating Committee on Friendship Heights Letter
 - August 15, 2018 Kenwood Citizens Association Letter
 - August 15, 2018 Washington Area Bicyclists Association Letter
 - October 23, 2018 Kenwood Forest II Letter
 - November 14, 2018 Resident and Community Associations Letter
 - December 11, 2018 Washington Area Bicyclists Association Letter
 - December 12, 2018 Representative Doris Matsui Letter
 - April 26, 2019 Montgomery County Department of Transportation Letter
-
- Open Town Hall Log
 - Email Correspondence

APPENDIX G CORRESPONDENCE



www.cctrail.org

July 12, 2018

P.O. Box 30703
Bethesda, MD 20824

Andrew Tsai, Project Manager
Montgomery County Parks
9500 Burnett Avenue
Silver Spring, MD 20901
by e-mail only: Andrew.Tsai@MontgomeryParks.org

Re: Coalition for the Capital Crescent Trail Position Paper -
Little Falls Parkway and CCT Intersection Improvements

Dear Mr. Tsai:

The Coalition for the Capital Crescent Trail (CCCT) is a non-profit volunteer citizens' group established in 1987 to promote the first class development of the 11.2 mile Rail-to-Trail conversion from Georgetown, D.C. to Bethesda and Silver Spring, MD, for multi-purpose, recreational use. The transformation from Georgetown to Bethesda from a disused single-track rail line to a first-class trail has been an impressive example of cooperation between civic groups and governments. The trail is possibly the most used trail in the county, with 516,974 pedestrian trips (or 1,416/day) and 324,971 (or 890/day) cyclist trips identified by the trail counter at nearby downtown Bethesda for all of 2017.

CCCT applauds the county's interest in improving how the Capital Crescent Trail and Little Falls Parkway intersect and appreciates its solicitation of comments from the public as to how this can be best accomplished. Here is CCCT's position on how to improve this intersection:

Our position is based on the following criteria:

1. Safety for all trail users and motorists;
2. A permanent solution that takes into account the fact that use of the trail and of the Parkway will likely increase due to the growth of downtown Bethesda - including the new Marriott headquarters, Purple Line users, and development at Westbard - all a short distance from downtown Bethesda along the trail. We feel it is important to plan for such long-term growth;
3. The environmental impact of any trail/road development, including minimal impacts on the nearby stream valleys;
4. The cost of suitable options; and
5. Minimizing the impact upon neighboring communities and parkway users, including traffic flows and traffic times.

APPENDIX G CORRESPONDENCE

Based on these criteria we believe that a bridge is the best long-term solution. It is the safest option because it eliminates any surface crossing of the Parkway by the Trail, thereby avoiding conflict between motorists and trail users. A bridge will best accommodate the increased usage of both the CCT and of Little Falls Parkway, as it will allow motorists to continue to use two lanes in each direction on Little Falls Parkway and eliminate any diversion for trail users. This further maximizes safety and minimizes delays for both motorists and trail users. Importantly, it also minimizes the impact of motor vehicles and bicyclists "detouring" into nearby residential areas as well as minimizing conflict with users of the Bethesda Pool. We think the marginal cost increase of a bridge over surface solutions may not be as great as feared when factoring in the savings of time and lessening of inconvenience. We prefer a bridge over a tunnel as a safer and probably less costly option. We therefore urge the County to take a close look at the costs and impacts (environmental, traffic and social) of a bridge.

We recognize, however, that such a permanent solution will take time to study and implement (it took many years to complete the bridge over River Road), and that a more immediate short-term remedy is needed in the interim. We believe that the best short-term solution to promote safety is to move the trail crossing of the Little Falls Parkway to the intersection of Arlington Road and the Parkway. We believe that this is the safest location for a surface crossing for all trail users and avoids situations where trail users either cross without a light or are tempted to avoid using a traffic light. It also requires the least diversion from the current trail path. Moreover, this alternative permits reopening the Parkway to two lanes in each direction and avoids an extra potential stop for motorists, thereby lessening motorists' temptation to travel through nearby residential neighborhoods. It also minimizes conflict with the users of the Bethesda Pool. We strongly believe that the rerouting of the trail should use a gradual curve on both sides of the Parkway (and avoid sharp 90 degree turns) and should widen the trail as it approaches the intersection from both sides so that there is adequate room for trail users to wait for the light to change. Consideration could also be given to separate bike and pedestrian lanes in this area. The details for the short-term recommendation are attached.

Respectfully submitted,



Ron Tripp, Chair,
Coalition for the Capital Crescent Trail

attachment - 1 page

cc: Andrew.Frank@MontgomeryParks.org

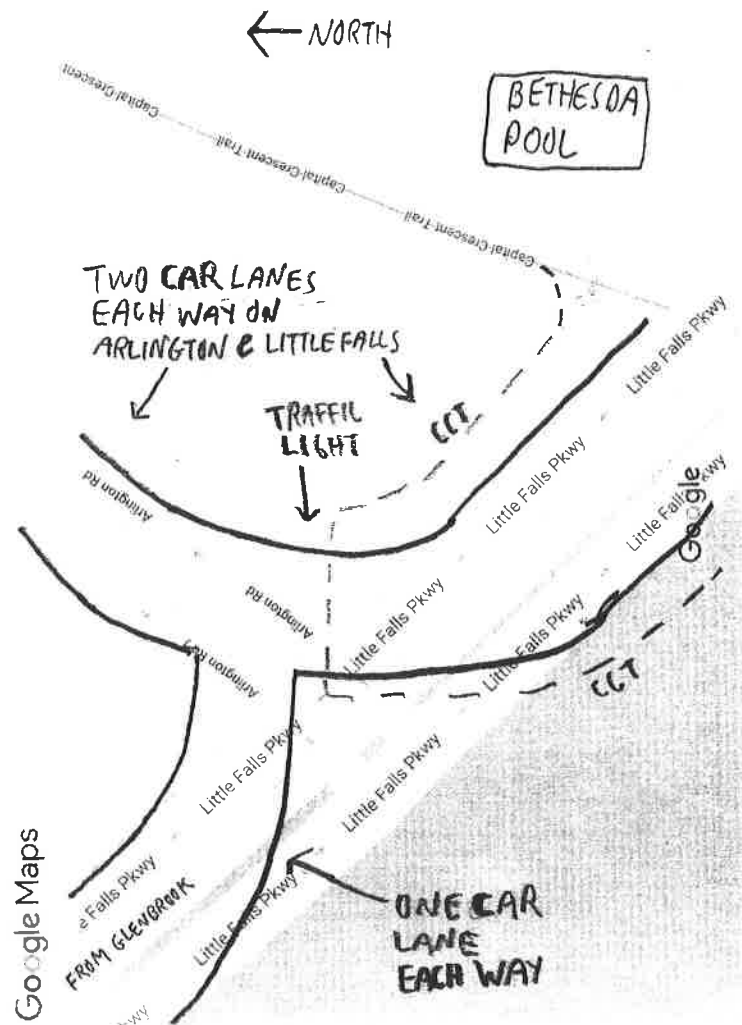
APPENDIX G CORRESPONDENCE

Short-term Recommendation - Arlington Road/Little Falls Parkway Intersection

The intersection is dangerous for pedestrians and cyclists to cross because only a minority of cars drive straight at the intersection; instead, the majority of the traffic makes a right turn from Little Falls onto Arlington going into Bethesda, and turn left from Arlington onto Little Falls outbound from Bethesda. Thus, a trail user who must cross Little Falls at this intersection must compete with cars turning onto the path of the crosswalk. We do not favor an all-way red light, because of the temptation of trail users to cross with Little Falls with the green light, and the frustration of drivers who must wait even when no trail users are present.

We instead propose a modification of the intersection so that Arlington/Little Falls is reconfigured as a through street and Glenbrook/Little Falls as a feeder street (with one lane in each direction) intersecting at a 90 degree angle to Arlington/Little Falls. In so doing, the trail users would cross when Glenbrook/Little Falls cars have the green light. A sketch of the intersection is below. The reconfiguration is similar to the redesign of the Fairfax and Clarendon Roads intersection (located about ½ mile north of Arlington and Little Falls). The actual work at the Fairfax/Clarendon intersection only took a few days, so we assume the cost would not be significant.

We again reemphasize that the redesigned intersection is only an interim solution - that a bridge for cyclists and pedestrians over Little Falls Parkway is the best long-term option.



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Preface:

Westmoreland Citizens Association (WCA) sent out a long email to members, soliciting input, and included the CCCT recommendation letter. WCA summarized the alternatives to members in this fashion:

alternative 1: control the trail crossing (redirect bike traffic to signals or add a signal and restore the Parkway to 4 lanes);

alternative 2: remove the conflict (close road sections or tunneling);

alternative 3: reduce the conflict potential (keep the reduced 2 lanes all the way to Dorset)

There are 25 WCA member responses below, received in the 2 days before the CCCFH meeting: 84% endorsed, in whole or in part, the recommendation of the Coalition for the Capital Crescent Trail. That is, to return car traffic to 4 lanes and the prior speed limits, move the trail to an existing light at the crossing, and longer term, build a bridge. Here are the member responses.

My husband bikes to work, but we also drive frequently to Bethesda. We strongly support one of the first two options – having bikes be diverted to a light, preferably the light at Arlington Road. It is much less costly than some of the other options, it provides more safety to bikers and walkers, and although it slows bikes somewhat, bikes are not supposed to be speeding on the Capital Crescent Trail anyway.

I strongly support controlling the trail crossing (redirect bike traffic to signals and restore the Parkway to 4 lanes). This is much less expensive and reasonable than the alternatives. The current traffic pattern is inconvenient and possibly dangerous. The larger number of auto drivers should not be inconvenienced by the small number of bicyclists (who don't comply with the bike speed limit) and don't want the inconvenience of a brief detour. Although the bicyclists are a strong lobby who I often support, in this case they should yield to the larger number of inconvenienced autos.

I am in favor of the first option which is to restore the 2 lanes of traffic (WCA – meaning 4 total) and either move the crossing which I don't believe bicyclists or pedestrians will honor..so I would put up a flashing light at current crossing.. a zebra crossing which is used very effectively all over the UK)

I agree with the use of the sidewalks and eventually a bridge. Traffic will increase when the Westbard development is begun so the two lane solution is really not viable. The current configuration is dangerous. I have seen someone going the wrong way from Arlington, and my neighbor's car stopped for a pedestrian was hit twice from behind.

Bikers and walkers should cross at the light short term, flyover long term. Current situation is not good for anyone.

Alternatives 1 and 2 seem to be the least costly, least complicated and possibly most effective solutions -- I would not support a tunnel under (safety concerns, water/environmental concerns, cost, complications) nor a bridge over (seems unnecessary)

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Yes, please press to have Little Falls Parkway restored to 4 lanes by temporarily moving the bicycle/pedestrian crossing to the light at Arlington Road and then building a permanent bridge over Little Falls Parkway (or a tunnel under it, whichever seems more cost effective and safe).

I feel very strongly about some of these options. Closing the road is not an option. Blocking it from Dorset is not either. Access to Bethesda for cars is shrinking with other reengineering of roads like Hillsdale/Leland. Little Falls and Arlington Blvd remain an important route. Cars are by far the most common mode of transport. I use the trail for biking, running and walking and still I believe that cars need to have access to a convenient route. Adding a light is fine but the two (at the trail and at Arlington) should be synchronized so that the traffic flows through. There would need to be light for trail users too so prevent accidents. Two lanes to Dorset solves nothing. It would make more congestion because the cars that use the lane for Hillsdale would no longer have access to the extra lane. It may seem like an easy solution for those living in that community, but it is not a solution for everyone else. Do NOT reduce the speed limit on Little Falls. Again, that would create congestion and people go faster anyway because it's logical to do so. DC gives us dozens of examples of how the lower speed limit creates congestion. I can't imagine how the tunnel would work. It seems it would be too close to the Arlington intersection. I'd really have to see some drawings related to this. Thanks for soliciting our input on this important matter. (Patricia Bonnard/Iain Shuker)

I think the temporary solution of moving the trail to the Arlington Rd stop light is excellent and reasonable. A 2 lane solution will benefit no one. The coalition letter is thoughtful and practical. Please thank them for us. Of course, an over-path bridge will be a good and speedier improvement, but that could take years. Meanwhile the current 2 lane solution is an accident waiting to happen if it hasn't already. I appreciate WCA for pushing this forward.

I agree that a bridge is the best long-term solution. I have no opinion on the short term solution but would agree with whatever the committee ultimately thinks best.

We support the recommendation by the Coalition for the Capital Crescent Trail to build a bridge over Little Falls Parkway as a long term solution and to move the crossing of the trail to the light at the intersection of Arlington Rd. and Little Falls Parkway in the short term. This would restore the roads to 4 lanes as they were before the bicycle accident.

Thanks for another opportunity to comment on this project. I have previously sent comments directly to the MNCPPC; I never hear anything back. The loss of life is tragic and should be avoided. Cars will always prevail over bikes and pedestrians. Another fact, bicyclists on the CCT almost NEVER stop when crossing Little Falls Parkway. Bicyclists want to be treated on roads as equal to cars but essentially never follow traffic laws--don't stop at stop signs, traffic lights, don't signal, don't issue warnings when passing pedestrians on trails. Bicyclists' own behavior contributes to this problem. The current configuration was an unwise knee jerk reaction to a tragic accident where the bicyclist, I read in the news accounts, did not stop at the stop sign. The main mode of transportation in our community is cars. No matter what any governing authority does short of banning cars, that will remain the case. Public transportation is inconvenient and too expensive. The bridge proposal is too expensive and in the end people will likely not use it. People will not walk up or bike up as it will take more energy and more time. I would argue that the path needs to be rerouted to be co-located with an existing traffic signal and bicyclists and pedestrians on the CCT need to be governed by traffic signals at that intersection. Traffic flow experts can better judge whether Arlington or Hillandale roads make more sense. Traffic in our area is only getting worse, the current configuration installed by MNCPPC only

APPENDIX G CORRESPONDENCE

makes that congestion worse. Once the Westbard development takes off, traffic will become much worse; particularly on Little Falls Parkway. The county has encouraged business and residential highrise development in the area without a commensurate analysis and improvement of the road system, irrationally believing that public transportation will address the growth. When they are reconfiguring the CCT, I would also look to measures that would encourage slowing down of bicyclists when coming to the crossing-speed bumps? Of course, bicyclists are well suited and masters of avoiding obstacles, so don't know how realistic that is.

If reducing the potential for accidents with pedestrians/cyclist is the goal, I believe the only two viable options are (a) to remove the conflict (tunnel/bridge) or (b) to reduce the potential for conflict by keeping the parkway reduced to two lanes. Since there is no funding right now for option (a), I strongly suggest maintaining the reduced traffic pattern. Trying to move runners and cyclists to cross at the light is not viable. Many simply won't follow this option, which will potentially result in an increase in accidents. Even though they will be in the wrong, you will get people crossing at multiple locations in areas that would not be expected by drivers, which is very dangerous. To reduce traffic speeds on Hillandale, perhaps you could consider adding speed bumps or a speed camera. Considering the thousands of pedestrians and cyclists that cross the parkway on the CCT, increased volume on Hillandale is a much safer alternative to increased and faster traffic on the parkway crossing the CCT.

I support the CCCT recommendation.

We are current residents of Westmoreland Hills but soon to be moving to the Somerset area of Chevy Chase. We drive through Little Falls and Arlington on a near daily basis. Our family would support going back to the full 4 lanes of traffic and creating a new traffic light at the current crossing. If that cannot be accomplished then we would support building a bridge over the roadway. The current conditions puts an undue burden on drivers and actually makes for unsafe driving conditions (with everyone needed to merge at the last minute into one lane). Adding a signal would keep the speed down and make for a safe passage for all the walkers/bikers who use the trail.

With the Montgomery County planning entities for Westbard Residential expansion assuming that many hundreds and hundreds of new residents there will be walking or biking back and forth to the Metro in downtown Bethesda daily via the Capital Crescent Trail, there seems to be no other option but putting a bridge over the Little Falls Parkway where the Capital Crescent Trail crosses it.

I would like to restore Little Falls Parkway to 4 lanes, to have bikes cross the Parkway at an existing light, and longer term, to reduce the risk further by building a bridge for bicyclists to use.

The best solution, quite obviously, is to remove the issue by pedestrian bridge or tunnel; it's also the most expensive. Second best is a controlled intersection: cheaper and a minor nuisance, but at least safe. The worst is the present situation, which is both a nuisance and dangerous because it gives pedestrians and bikers a false sense of security, while motorists aren't sure what they're supposed to do; and bikers ignore the stop sign for them.

I support the idea of directing the cyclists and walkers to the light that is already near the trail and restore the Parkway to four lanes for cars. Building a bridge over Little Falls, like River Road, makes good sense in the long term.

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The “road diet” seems effective to me, both as runner/cyclist and a motorist on LFP. Keep it 2 lanes. I don’t agree with CCT Coalition’s suggestion of trying to get back to 4 lanes with a long-term solution like a bridge— seems unnecessary— nor moving the crossing to Arlington Rd. The temporary posts could be configured better and perhaps there’s a more permanent way for the implementation to be less confusing for new motorists, but those are manageable details. Thanks for soliciting opinions.

Count me among the 92% who want four lanes. But keep the new speed of 25 MPH and put more flashing lights around such as those in the UVA campus in Charlottesville.

I also agree with the Coalition’s letter.

As a driver and a biker, though not a commuter, through that intersection I'd vote for #5/#9...reduce to 1 lane and stop-sign (#5) or speed-table cross-walk (#9). It seems to be working now except for the reflective pylons ironically making seeing trail users harder to see. The bridge over River Rd is a climb that I curse when running/biking up as compared to simple stopping at the other intersection. That said, if left at 4 lanes, then #3 (signal) seems best approach to giving equal access to trail and road users. Thanks!

We vote to reopen the parkway to four lanes and redirect bikers to the light for the time being. But for the future work for a bridge over for those using the Crescent Trail.

I both drive and bike through there regularly, and have thought quite a bit about how to fix the problem (as my wife will attest). I should state up front that I am not a traffic engineer -- I am in advertising and design, which is actually more relevant to the problem that you might think. It's my business to direct people's attention to what I want them to see, and to direct attention away from distractions. The current temporary set-up is, as I'm sure you are aware, a mess. The presence of all those plastic rods distracts drivers from focusing on what is important -- stopping and looking for bikes or pedestrians. The stop signs themselves -- and here's the greatest fundamental error -- are not really stop signs at all. They make stopping optional -- only if pedestrians or bikes are present is the driver required to stop. But that, alas, leaves it to the judgement of the driver. Are those pedestrians moving slowly enough that I don't really have to stop? Are they paying attention? Are those bikes far enough away? I'm in a hurry, after all! What we need to do is take the judgment about stopping out of the driver's hands. Stopping should be mandatory, without exception. There are two ways to do this: stop signs and lights. In my opinion, stop signs, in combination with some speed bumps preceding the intersection to slow traffic down, will ultimately be more effective and less expensive, while improving traffic flow back to four lanes. The implementation, however, is absolutely key. First, the speed bumps. How far outside the intersection should they be placed? 50 feet? 100 feet? I don't know, this is where a traffic engineer's experience comes into play. Maybe there should be two bumps leading up to the intersection. But slowing people down, forcibly, is one major step closer to getting people to stop altogether. (And please, while we're on the subject, not those short, harsh, high speed bumps that make your car shudder and damage your suspension; smoother ones will be just as effective without rattling your nerves.) Next, as you approach the intersection there should be two stop signs, one to the left of the left lane and one to the right of the right lane (four total, counting both directions), so you can't possibly miss them. These should not be oversize, as you do not want to block vision of any pedestrians or bikes. They could even be a little smaller than, and placed, if anything, a little lower than the standard height so as not to obstruct view.

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Okay, why not stop lights? In short, people run stop lights. Again, it puts judgement into the drivers' hands. Hey, it's green, step on it! Can I make it thought that yellow? I'm in a hurry, after all! A stop sign eliminates the uncertainty and the judgment by conveying the message that you must stop every time, no exceptions, whether there are bikes or pedestrians present or not. A stoplight means it's okay sometimes to breeze through without stopping -- not the message that we want to convey. Finally -- and this is only real expense we're adding here -- light the intersection well so that at night, or even at dusk, there's no chance of not seeing the stop signs, or not seeing pedestrians or bikers in dark clothing with no lights or reflective tape. Option: Set up a camera in each direction. Inform the drivers they are being watched, and put up a sign "Full Stop or Get a Ticket." As a driver, I don't like traffic cameras more than anyone else, but around schools and intersection like this one, I can accept it as a necessary evil. Now, what do we do from the pedestrian and biker perspective? First, the temporary decision to put a kink in the route, forcibly slowing down the bikes, was a good one. Keep it. It works. I'd consider putting up (smaller) stop signs on each side, with the message: "Stop. Look Both Ways." There will be some bikers who blow past these, you're never going to get all of them to slow down completely, but it should help. Some reactions to the other proposals: a) I'm glad that at least one of the proposals included stop signs. They're the cheapest, cause the least disruption, open up both lanes, and reduce visual clutter. But they need to be supplemented with at least the speed bumps, and there should be a total of four so they cannot be missed. b) The bridge or tunnel options will be hugely expensive, hugely disruptive and, given the short length, will have to be absurdly steep, which the bikers will hate (believe me). The long bridge on the CCT over River Road is what you can do when you have plenty of length, but you don't have that length here. c) Closing Little Falls down to one lane each way permanently is also not a good solution. The traffic back-up at certain times of day is made unnecessarily worse; it really needs to be two lanes each way. Thank you.

I strongly support keeping Little Falls at two lanes. The current system is working very well in terms of safety and traffic and is far preferable to bikers crossing at a light. The small inconvenience to us drivers is well worth it.

APPENDIX G CORRESPONDENCE
Citizens Coordinating Committee on Friendship Heights

August 14, 2018

Mr. Andrew Tsai, PE
Project Manager
Montgomery Parks
9500 Brunett Ave.
Silver Spring, MD 20901

Re: Capital Crescent Trail Crossing at Little Falls Parkway

Dear Mr. Tsai:

Thanks again for your most informative presentation to us at the Citizens Coordinating Committee on Friendship Heights (CCCFH) regarding the Capital Crescent Trail alternatives under study for increasing safety at the Trail/Little Falls Parkway intersection. Our organization consists of 18 communities focused on planning and zoning issues affecting our neighborhoods.

While the Capital Crescent Trail (CCT) is heavily utilized by bicyclists, it is predominantly used by children and adult pedestrians and runners. A core problem is that the CCT has become a bicycle commuter route. It was never intended as such. This results in safety issues in conflicts with the pedestrians and runners on the trail and with the automobiles on the Little Falls Parkway at the Trail/Parkway intersection.

Aggravating the safety issue is the disregard that so many bicyclists have for the rules of the road.

Furthermore, Little Falls Parkway has long been a primary vehicular route for access to downtown Bethesda where automobile traffic has become extraordinarily congested. That congestion will be compounded with the near-term population and employment increase resulting from expanded development in the Westbard Sector and in the Bethesda Sector.

It is our understanding that you are currently narrowing the list of alternatives to three. Our strongly preferred alternative is discussed below.

Constricting traffic on Little Falls Parkway to accommodate bicyclists is not at all an optimal solution to the safety issue, and additionally will most certainly result in increased cut-through traffic in nearby neighborhoods – already the case as a result of the temporary Parkway constriction, “road diet,” utilizing bollards.

A good solution for the short-term is your department’s alternative for relocating the CCT to cross Little Falls Parkway at the traffic signal at Arlington Road. This would create a safe environment for both Trail users and vehicles, and it could be achieved quickly and at minimal cost. Traffic in both directions could be restored to four lanes on Little Falls Parkway, thereby reducing the cut through traffic that was increased by lane reduction which afflicts the nearby neighborhoods of Kenwood and Somerset. It would also reduce the possibility of vehicular accidents that can occur in the Parkway, especially in the evening hours when the bollard pattern is not clearly visible to turning motorists coming off Arlington Road onto Little Falls Parkway.

Representing the Communities of Brookdale, Drummond, Chevy Chase Village, Chevy Chase West,
Village of Friendship Heights, Glen Echo Heights, Kenwood, Kenwood Condominium, Kenwood House Cooperative, Kenwood
Place Condominium, Somerset, Somerset House, Springfield, Sumner Village Condominium, Westmoreland, Westbard Mews,
Westwood Mews and Wood Acres

APPENDIX G CORRESPONDENCE

Ideally, we would like to see a bridge over Little Falls Parkway to facilitate vehicular flow and provide safety to pedestrians and bicyclists. However, that is a long-term solution and will require considerable study before implementation. We need an interim solution now and we strongly prefer that which we cited in the paragraph above.

Thank you in advance for your time and attention to the collective interests and concerns of our communities.

Sincerely,



Harold Pfohl, Chair
Citizens Coordinating Committee on Friendship Heights

c. Mr. Andrew Frank

APPENDIX G CORRESPONDENCE

Mr. Andrew Tsai, PE, Project Manager
M-NCPPC Montgomery Parks
9500 Brunette Avenue
Silver Spring, MD 20901

Dear Mr. Tsai:

The Kenwood Citizens Association, which represents more than 230 households, has a keen interest in developing a SAFE solution to the intersection of the Capital Crescent Trail and Little Falls Parkway. Because of our proximity, many of our residents are often on the trail or on the parkway.

This trail, which is so widely used, is **not a commuter trail**. It is specifically a **recreational trail** not only for bikers, but also for walkers and runners. All who use this trail need to be safe at that intersection.

The current temporary approach using bollards is actually dangerous: the bollards are hard to see at night and the reduction to one lane is unexpected, forcing drivers to move too quickly to that single lane. Furthermore, cyclists rarely stop before the intersection even though signs tell them to do so.

The immediate solution we see would relocate the trail and crosswalk to the Arlington Rd. traffic signal. The traffic signal should remain red for both Arlington Rd drivers and Little Fall Parkway drivers for, say, 45 seconds to allow trail users to cross. This pattern is being used successfully at Bethesda Ave. and Arlington Rd.

Although this solution could potentially be a permanent one, it's clear to us that a more ideal answer is a bridge over the parkway at the current intersection, so motorists and trail users would not have to interact. We realize there may be a significant cost to implement this approach but it should at least be given serious analysis.

In either case, it's extremely important to have free flowing traffic on the parkway because we know that the population and business growth in Bethesda and the future Westbard complex will generate major increases in car usage. If cars can move freely, without having to suddenly stop for trail users, the chances of an accident will be diminished.

Thank you for your time and attention to this important matter.

Sincerely,



David Barron
President, Kenwood Citizens Association,
On behalf of the KCA Board



August 15, 2018

Andrew Tsai, P.E., Project Manager
9500 Brunett Ave.
Silver Spring, MD 20901
Andrew.Tsai@MontgomeryParks.org

Re: Capital Crescent Trail Crossing at Little Falls Parkway

Dear Mr. Tsai,

I am writing on behalf of the Washington Area Bicyclist Association ("WABA"), our 1,500 Montgomery County members and the thousands of other Montgomery County residents who have supported actions by WABA in the recent past. WABA wishes to comment on the plans for a revised crossing of Little Falls Parkway by the Capital Crescent Trail.

The Capital Crescent Trail is one of the most widely used trails in the DC area, and as such is of great importance to the region. The placement of a temporary narrowing of the Parkway traffic lanes in the wake of the death of 81 year old bicyclist Ned Gaylin at this crossing was an appropriate and necessary measure. We applaud Montgomery Parks for taking swift action to protect trail users as well as undertaking a thorough study of alternatives for a permanent, safe solution for this crossing.

Having reviewed the potential alternatives presented by Montgomery Parks at a recent public meeting on June 13, 2018, We feel that any alternative chosen must absolutely maintain the road diet currently in place, leaving no more than one through traffic lane on the Parkway in each direction. Restoring the Parkway to its former configuration of two lanes in each direction would also restore the dangerous nature of this crossing possibly leading to more crashes and even fatalities.

Keeping in place the temporary road diet on Little Falls is also important given the County's commitment to Vision Zero, the goal to end traffic fatalities and serious injuries. Restoring the road to four lanes of car traffic would undermine that commitment. Further, the Parks Department is currently undertaking an audit of all trail crossings in the County. The solution that Parks chooses here should be a prototype for improving similar crossings County-wide. You can set a wholly positive precedent by leaving the road diet on Little Falls in place.

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The need to maintain this road diet would exclude alternatives 1, 2, 3 and 10 and we strongly oppose all of those alternatives. It is likely that bridge and tunnel alternatives (6 and 7) would be cost prohibitive and are therefore unlikely to be chosen, though we note that if resources were not at all constrained those alternatives would provide enhanced safety for trail users.

Of the remaining alternatives (4, 5, 8, 9, 11 and 12), we feel they are all acceptable and provide a safe access for trail users through the crossing. Montgomery Parks should choose among those alternatives to provide the most cost efficient and safe solution.

Please contact Peter Gray at peter@waba.org or 202-518-0524 x231 to follow up. Thank you for considering our comments,

Best Regards,

A handwritten signature in blue ink, appearing to read "Greg Billing", with a long horizontal flourish extending to the right.

Greg Billing
Executive Director

KENWOOD

FOREST II

Management Office: 6658A Hillandale Road, Chevy Chase, Maryland 20815 (301) 657-2683 E-mail kenwoodforest2@verizon.net

October 23, 2018

Mr. Andrew Tsai, P.E.
Montgomery County Dept. of Parks
9500 Brunett Ave.
Silver Spring, Md. 20901

Dear Mr. Tsai:

I am the President of the Board of Directors of Kenwood Forest II, a community of 279 residences located along Hillandale Rd., Bradley Blvd. and Chevy Chase Dr. I am writing to present the Board's views concerning the proposals under consideration at the intersection of Little Falls Parkway (LFP) and the Capital Crescent Trail (CCT).

We understand that you are seeking to make the LFP-CCT intersection safer, and we share that goal. But in seeking to making this intersection safer, we are concerned that surrounding areas, including Kenwood Forest II, may become less safe. Specifically, we believe that the "temporary" road diet along LFP should not be made permanent, because the residents of Kenwood Forest II would become less safe. Therefore, any proposal that would make the road diet permanent should be rejected.

The road diet, which reduced lanes on LFP from 4 to 2, resulted in a diversion of traffic from LFP to Hillandale Rd., thereby increasing traffic on this street significantly. More than 130 townhomes in our community are located along Hillandale Rd. Many of the residents of these townhomes have small children who are apt to run out onto the street. The increased traffic along Hillandale Rd. also increases the likelihood of traffic accidents as cars of residents attempt to pull out of parking areas onto the road.

Accordingly, we are most supportive of a pedestrian bridge that would span all four lanes of LFP. We understand that such a proposal would be costly, but it would guarantee that traffic would pose no danger to users of the CCT, and it would enable cars to use all four lanes of LFP, thereby reducing the traffic going through our community and increasing the safety of our residents. It would fully separate pedestrian traffic from automobile traffic thereby being the safest option for users of

APPENDIX G CORRESPONDENCE

the CCT while reducing traffic and increasing safety along Hillandale Rd.

Adopting such an option would also be important in planning for future growth. Future growth in Bethesda will place an additional burden on roadways and trails to accommodate increasing numbers of automobiles, cyclists and pedestrians. Building a bridge over LFP's four lanes, would enable LFP to be restored to four lanes of traffic, thereby enabling it to transport more efficiently the growth in traffic that the future will bring while also enabling cyclists and pedestrians to use the CCT safely and without interruption.

We would also support having the CCT cross the LFP at Arlington Rd., with caveats. We do not understand why such a crossing would necessitate the permanent closure of two lanes of LFP that would be a part of that plan. After all users of the CCT would be crossing at an intersection with a traffic light.

We also oppose what is designated on your map as Trail Connector Opt. A, which would connect the Little Falls Trail with the CCT, crossing Hillandale Rd.. You have noted that there is a "strong desire" not to have such a connector, and the Planning Commission correctly rejected this proposed connector a couple of years ago. In rejecting it, it noted that such a connector, "would create a new, unsafe trail crossing not located at a traffic light." Building such a connector should also be rejected because it would result in the unnecessary destruction of trees and plant life on the border of Kenwood Forest II.

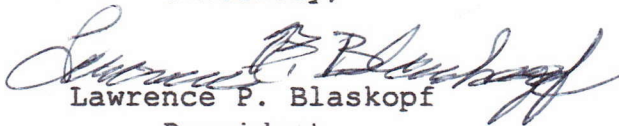
Regarding the proposal to have the CCT cross the LFP at Arlington Rd., the crossing of the Georgetown Branch Trail (the extension of the CCT) at Connecticut Ave. can serve as a template. There, the Trail was diverted slightly at Connecticut Ave. to a crossing at a traffic light. There was no reduction in the lanes along Connecticut Ave., and to our knowledge, there were no significant accidents. Similarly, here the CCT can be diverted slightly to the crossing at Arlington Rd. without any reduction of lanes along the LFP.

Ultimately, the Kenwood Forest II Board supports a decision in which LFP returns to four lanes, thereby significantly reducing the flow of traffic along Hillandale Rd. where more than 130 of our townhomes are located.

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Thank you for giving us the opportunity to present our views concerning this matter.

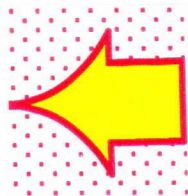
Sincerely,



Lawrence P. Blaskopf

President
Kenwood Forest II

cc: Hans Riemer
President
Montgomery County Council
100 Maryland Ave.
Rockville, Md. 20850



APPENDIX G CORRESPONDENCE

November 14, 2018

Andrew Tsai, PE, Project Manager
Park Development Division
Montgomery Parks – Maryland National Capital Park & Planning Commission
9500 Brunett Avenue
Silver Spring, MD 20901

Re: Capital Crescent Trail – Little Falls Parkway Crossing Design

Dear Mr. Tsai:

In 2017, there was a fatal accident at the mid-block crossing of the Capital Crescent Trail of the Little Falls Parkway. After the fatal accident, the Park Department acted swiftly to mitigate some of the danger by reducing Little Falls Parkway to two lanes. However, this was just a temporary solution. Trail users still cross the Parkway at mid-block, which creates a traffic bottleneck at the intersection of the Capital Crescent Trail and the Little Falls Parkway with traffic often backing up 10 or more cars deep. Furthermore, we continue to see many bikers ignoring the speed limits and stop signs on the trail and blowing right through the trail crossing with little regard to street traffic. The situation is in dire need of fixing both to ensure trail user safety and to improve traffic flow through the area. We, the undersigned residents and leaders of eight neighborhood associations in the surrounding area, believe the only safe solution to the trail crossing is to build a bridge. In addition, we strongly urge that the proposed Permanent Road Diet (reduction to two lanes) on Little Falls Parkway be eliminated altogether from any alternative so that we return to the original four lanes on Little Falls Parkway.

Recommended Solution: Alternative C with modifications to allow for safe crossing and improved traffic flow

The best and safest option for the Capital Crescent Trail Crossing at Little Falls Parkway is a modified version of Alternative C, the Pedestrian Bridge. Since the pedestrian bridge completely elevates trail traffic above the parkway, it allows trail users to avoid traffic altogether and proceed safely along the trail without having to navigate a street crossing. Furthermore, traffic on Little Falls Parkway and Arlington Road will flow under the bridge without fear of hitting a biker or pedestrian. However, we believe that Alternative C needs to be further modified to improve traffic flow and several aspects of the bridge design will need to be developed further.

1. Return Little Falls to Four Lanes and Leave Arlington Road as Is:

The key modification needed is the removal of the proposed road diets on both Little Falls Parkway and Arlington Road. Since its creation in 1962, Little Falls Parkway has had two lanes in each direction and served as a major connector between the busy shopping center at Bradley and Arlington and the residential areas off of River Road, and

use of this connector will no doubt increase as development in Bethesda and at Westbard continues. More traffic is created in the summer by the well-used Bethesda Pool. With the bridge in place, we see no reason to limit traffic through the area, and in fact conclude returning Little Falls Parkway to four lanes is safest for the greater Little Falls community. Any safe and fair solution must consider the impact on neighboring communities. The current reduction to only two lanes is harmful to the safety of neighboring communities, especially Kenwood Forest II and Kenwood, because drivers cut through local streets to avoid the backed-up traffic on Little Falls Parkway.

2. Design Bridge to Limit Impact on the Local Environment:

We also request that the bridge be designed to limit its impact on tree cover and the Willett Branch. As currently proposed, the bridge will cross a tributary of the Willett Branch. When designing the bridge, this crossing needs to be done carefully to preserve the tributary and not cause any damage during construction.

3. Clarify Impacts and Access:

As the Parks Department refines Alternative C, we feel the department must clarify several aspects before doing further design work. First, a bridge that spans four lanes will likely necessitate longer ramps. We would like more information on how a longer span impacts tree cover and trail access. Second, the diagram of the bridge shown in the October 2018 documents provides limited information about access to the trail from Little Falls Parkway going North or South. As the Bethesda Pool is a favorite starting and ending point for many users, this needs to be examined further and shared with the community. Access to the trail from the sidewalks and trails along Little Falls needs to be carefully considered.

Interim Solution: Street-level detour until bridge is complete

Since it may take time to arrange sufficient funding for the bridge, we recommend that the Parks Department implement, as an interim solution, a modified version of Alternative B, Trail Reorientation to Traffic Signal at Arlington Road. Diverting the trail traffic to the light at Arlington Road will significantly improve trail user safety, while also improving automobile traffic flow. Pedestrians and bicyclists will be forced to cross at a controlled intersection—the best way to prevent another fatal accident. However, the following modifications need to be made to Alternative B—which we also note will reduce its cost:

1. North of Little Falls Parkway - Do not divert the trail over to Arlington Road prior to the Parkway. As currently designed, the proposed diversion to Arlington Road provides Southbound bicyclists a straight shot at the intersection, which will do little to reduce bicycle speeds and will encourage bicyclists to ignore any traffic signals at the intersection. Instead, maintain the current trail pathway heading south toward Little Falls Parkway and add a turn closer to the Parkway Westward towards the signal at Arlington

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Road. By sending trail users to a controlled intersection at a slower speed, the crossing becomes much safer.

2. Remove the trail connection proposed between Little Falls Trail and the Capital Crescent Trail behind the Bethesda Pool. This connection essentially trades the unsafe road crossing at Little Falls Parkway for an unsafe crossing of Hillandale Road. It is imperative that the solution to the unsafe crossing to Little Falls does not create another dangerous crossing mid-block at Hillandale Road. Furthermore, due to concerns about safety and impact on the Willett Branch, this trail connection concept was already rejected unanimously by the Planning Board in 2016.
3. As noted above, remove the road diet and restore Little Falls Parkway to four lanes. With a safe crossing at the Arlington Road traffic light, the road diet is not needed to further improve trail safety and will continue to create a bottleneck on Little Falls Parkway that will decrease safety in nearby neighborhoods.

Thank you for your attention. Please contact any of the undersigned individuals and their communities if you would like to discuss this matter further.

Sincerely,

Harold Pfohl
Chair, Citizens Coordinating Committee on
Friendship Heights
harry.cccfh@gmail.com

David Barron
President, Kenwood Citizens Association
davidbarron13@gmail.com

Helen Davies
President, Kenwood Forest Condominium
pixleychick@gmail.com

Larry Blaskopf
President, Kenwood Forest II
lblaskopf@msn.com

Joan Barron
Co-President, Chevy Chase West
Neighborhood Association
jmbarron479@gmail.com

Lynn Balzer-Martin
Kenwood Forest II Resident
lynnb2k@aol.com

Celia Martin
President, Westmoreland Citizens Association
celiamartin@comcast.net
Damian Whitham
President, District 1 Neighbors
damian@d1n.org

Sarah Morse
Executive Director, Little Falls Watershed
Alliance
morsekathan@gmail.com

Elizabeth Hurwit
Chair, Traffic Committee, Town of Somerset
eahurwit@gmail.com

Jenny Sue Dunner
Kenwood Neighborhood Resident
jennysuedailey@aol.com

Pat Johnson
Kenwood Neighborhood Resident
pdjohnson01@yahoo.com

APPENDIX G CORRESPONDENCE

Jean Iker
Kenwood House Resident
jean.iker@comcast.net

David Kathan
Town of Somerset Resident
dkathan@gmail.com

CC: Mike Riley, Director of Parks, M-NCPPC Montgomery Parks
Casey Anderson, Chair, Montgomery County Planning Board
Ike Leggett, Montgomery County Executive
Roger Berliner, Montgomery County Councilmember
Marc Elrich, Montgomery County Councilmember
Nancy Floreen, Montgomery County Councilmember
George Leventhal, Montgomery County Councilmember
Craig Rice, Montgomery County Councilmember
Hans Riemer, Montgomery County Councilmember
Tom Hucker, Montgomery County Councilmember
Sidney Katz, Montgomery County Councilmember
Nancy Navarro, Montgomery County Councilmember



December 11, 2018

Andrew Tsai, P.E., Project Manager
Montgomery Parks
9500 Brunett Ave.
Silver Spring, MD 20901
Andrew.Tsai@MontgomeryParks.org

Re: Capital Crescent Trail Crossing at Little Falls Parkway

Mr. Tsai,

On behalf of the Washington Area Bicyclist Association ("WABA") and its 1,500 Montgomery County members, I write to offer comments on the proposed improvements to the Capital Crescent Trail Crossing at Little Falls Parkway.

Montgomery Parks' 2017 action to remove travel lanes and reduce the speed limit on Little Falls Parkway near the Capital Crescent Trail was a controversial, but undeniably successful move to improve a deadly design. Since the changes were put in place, the intersection is working well. Crashes dropped by 67%, average vehicle speeds dropped, driver delay increased only a few seconds, and Little Falls still carries 97% of the car traffic it did before. Interactions between drivers and trail users are more predictable, more visible, and less stressful. And the combination of lower speeds and better visibility ensure that if crashes do happen, severe injuries are unlikely. Fundamentally, the design works. Its greatest flaw is that it is ugly.

For a permanent solution, we urge Montgomery Parks to move ahead with Alternative A. Overall, the design and operation are very similar to the existing conditions. It maintains the road diet, the lower speed limit, and excellent visibility, but improves upon existing conditions by adding a raised crosswalk and more visual cues to remind and encourage drivers that they are expected to yield to trail users. Finally, the new trail connections to neighborhood streets, asphalt removal, and permanent slower speeds will help restore Little Falls Parkway to its original purpose as a park.

Considering the other proposed options, Alternative B is a clear step in the wrong direction. Routing the trail to the traffic light forces everyone to wait longer. It adds new kinks and sharp turns to the trail, new environmental impacts from the trail along Arlington Rd, and may results

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in worse visibility at the intersection. At about double the cost of Alternative A, it is a higher cost for a worse experience for everyone.

Alternative C is understandably tempting. A bridge eliminates the crossing entirely and allows drivers to move unimpeded below. The trail along the parkway creates the same new connections as in Alternative A and most of the extra pavement can be removed. However, at an estimated \$4 million (\$3.2 million more than A), it is hard to justify the financial cost and environmental impact of this solution. While we do not object to a bridge at Little Falls Parkway, we encourage the department to weigh the benefits of installing a single bridge here against needed safety improvements at dozens of similarly hazardous road crossings across the park trail system.

Thank you for considering our comments,

Garrett Hennigan
WABA Community Organizer

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HOUSE OF REPRESENTATIVES
WASHINGTON, D. C. 20515

DORIS O. MATSUI
6TH DISTRICT, CALIFORNIA

December 12, 2018

Andrew Tsai, PE, Project Manager
Park Development Division
Montgomery Parks- MNCPPC
9500 Brunett Avenue
Silver Spring, Maryland 20901

Re: Capital Crescent Trail- Little Falls Parkway Crossing Design

Dear Mr. Tsai:

In 2017 there was a fatal accident at the mid-block crossing of the Capital Crescent Trail and Little Falls Parkway. A temporary solution was enacted by the Park Department in the name of safety. The current configuration has to be fixed to ensure trail user safety, to improve traffic flow and reduce cut through traffic in adjacent neighborhoods.

You have received letters from many neighborhoods recommending permanent changes to this restrictive solution. The letter of November 16th is from leaders of eight neighborhood associations, the Capital Crescent Trail Board, and the Citizens Coordinating Committee of Friendship Heights, an organization representing 18 communities in the Bethesda-Chevy Chase area. This letter recommends that a bridge be built (Plan C) and that the "road diet" be lifted from Little Falls Parkway and Arlington Road.

However, we need an interim solution because funds for a bridge will take time to acquire. A realistic solution (a modified version of *Alternative Plan B*) that is strongly supported, puts the crossing of the trail at the light on Arlington Road and restores Little Falls Parkway to four lanes.

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Since its creation in 1962, Little Falls Parkway has served as a major connector between Bradley Boulevard, Arlington Road and surrounding neighborhoods, as well as the lower area of Bethesda Chevy Chase from Western Avenue to MacArthur Boulevard. The development of Little Falls Parkway has an interesting and unique history. The U.S. **Congressional Capper-Cramton Act of 1930** paved the way for the acquisition, establishment and development of the George Washington Parkway as well as providing for the acquisition of lands in the District of Columbia, Maryland and Virginia for a comprehensive “*park, parkway and playground system for the National Capital*”. It is troubling that the solutions Montgomery County Parks Department has recommended sets policy without due process. Parks has ignored the serious concerns of and impact on neighboring communities in the greater area. *The policy appears to be directed to a narrow group of users rather than the broad interpretation of the original intent of that early Congress.* It is important to remember that the Capital Crescent Trail is a *recreational* trail for many users. The greatest number being walkers.

What is of great concern is the “road diet” policy. The term and policy of a “road diet” was established as recently as 2014. The strategy was created to implement the idea of getting people out of their cars and choosing to walk or bike. This policy is not fair to neighborhoods that exist in urbanized areas and is unrealistic to implement on an established commuter road. Little Falls Parkway is a commuter connector that is heavily used. The future use of this parkway will only increase as development plans for Bethesda and the Westbard sector are realized within the next 5 years.

To enact a “road diet” on Little Falls Parkway is a major policy change. The Montgomery County Park Department is reducing lanes on a major road. Concern for the safety of all: neighborhoods, walkers, bikers and drivers must be considered. In the interim, the modified version of Alternative Plan B is strongly supported. It would ensure safety and would lessen the impact from growing development. It would allow bikers and walkers to cross safely, it would restore Little Falls Parkway

APPENDIX G CORRESPONDENCE

to its original four lanes, reduce cut through traffic and improve traffic flow. It is a safe and practical solution.

Sincerely,


DORIS MATSUI
Member of Congress

CC: Michael Riley, Director of Parks, M-NCPPC Montgomery Parks
Andrew Franks, M-NCPPC Montgomery Parks
Casey Anderson, Chair, Montgomery County Planning Board
Councilmember Albornoz, Montgomery County
Councilmember Friedson, Montgomery County
Councilmember Glass, Montgomery County
Councilmember Hucker, Montgomery County
Councilmember Jawando, Montgomery County
Councilmember Katz, Montgomery County
Councilmember Navarro, Montgomery County
Councilman Rice, Montgomery County
Councilmember Riemer, Montgomery County

APPENDIX G CORRESPONDENCE



DEPARTMENT OF TRANSPORTATION

Marc Elrich
County Executive

Al R. Roshdieh
Director

MEMORANDUM

April 26, 2019

TO: Andrew Tsai, P.E., Project Manager
Park Development Division – Montgomery Parks
Maryland-National Capital Parks and Planning Commission (M-NCPPC)

FROM: John Hoobler, Capital Projects Manager
Montgomery County Department of Transportation (MCDOT)

SUBJECT: Capital Crescent Trail & Little Falls Parkway Intersection Improvement Project

On behalf of the Montgomery County Department of Transportation (MCDOT), I am writing this letter regarding the Capital Crescent Trail Pedestrian Crossing Improvement Plan by Montgomery Parks. The plan calls for a permanent road diet along Little Falls Parkway, a Maryland-National Capital Park and Planning Commission-owned (M-NCPPC) and maintained roadway, from Arlington Road to Hillandale Road. MCDOT's vision is to create a seamless transportation system for people of all ages, incomes, and abilities to support a vibrant and sustainable community. Our mission is to move people and connect places with the best transportation choices and services.

Based on the study performed by Sabra & Associates for the M-NCPPC, no significant change in vehicle traffic between the pre-road diet and post-road diet conditions, except for along Hillandale Road, are planned. The portion of Hillandale Road owned and maintained by the MCDOT extends from approximately Willett Parkway to Bradley Boulevard. The increase in traffic volumes reported in the study do not appear to introduce capacity issues along Hillandale Road. In addition to the studies MCDOT has performed along Hillandale Road, safety improvements that include new pedestrian infrastructure at the intersection with Chevy Chase Drive are planned.

Please let me know if you have any questions or need additional information by contacting me at (240) 777-2192 or John.Hoobler@montgomerycountymd.gov.

JCH:jch

cc: Michael L. Paylor, PE, PTOE, Chief, Traffic Engineering and Operations, MCDOT
Mark Terry, Section Chief, Traffic Engineering Studies Section, MCDOT

Division of Traffic Engineering and Operations

100 Edison Park Drive, 4th Floor · Gaithersburg, Maryland 20878 · 240-777-2190 · FAX: 240-777-2080

Email – Trafficops@montgomerycountymd.gov
www.montgomerycountymd.gov/dot



Capital Crescent Trail Crossing at Little Falls Parkway Review

April 9, 2019, 11:18 AM

Contents

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ii.	Individual registered positions	3

Capital Crescent Trail Crossing at Little Falls Parkway Review

Montgomery Parks staff is seeking public feedback on the three alternate concept plans to improve the Capital Crescent Trail crossing at Little Falls Parkway.

Summary Of Registered Positions

As of April 9, 2019, 11:18 AM, this forum had:

Attendees:	898
Registered Positions:	319
Hours of Public Comment:	16.0

Topic Start

October 12, 2018, 12:49 PM

Capital Crescent Trail Crossing at Little Falls Parkway Review

Montgomery Parks staff is seeking public feedback on the three alternate concept plans to improve the Capital Crescent Trail crossing at Little Falls Parkway.

Individual Registered Positions

Name not shown

inside Bethesda/Chevy Chase
October 12, 2018, 4:24 PM

I prefer Concept C

Option C (the bridge) is by far the best, and would be a good use of county funds. Option A is far better than B, which is terrible. (However, if option A were chose, it would be better to keep the current traffic island between lanes. Without the safe space between opposing lanes of traffic, cyclists and pedestrians will have to wait for cars to come to a complete stop in both directions before proceeding, instead of now when a user can proceed to the middle when the first lane is stopped. As a result, both trail users and cars will wait longer at that crossing, on average.)

Either Option A or Option C are clearly preferable to Option B, which has no redeeming qualities. If Option C is chosen, the road should not be narrowed but returned to 4 lanes. Option C is clearly the best for both cyclists and motorists, but it is not clear if the cost is worth it over Option A. Does the assessment take into account likely future traffic increases? The current increased travel time for cars is not bad. But with increased traffic that wait will increase, which will additionally support Option C.

Name not shown

inside Silver Spring
October 12, 2018, 11:47 PM

I prefer Concept C

Ralph Wooden

inside Bethesda/Chevy Chase
October 12, 2018, 5:34 PM

I do not like any options listed

I agree with the other writer that Concept B is the worst: increased wait times for everyone! I WOULD prefer Concept C, except WHY do we need to decrease the number of lanes? We were doing just fine with four lanes; northbound car traffic split off at Arlington Rd., and the road narrowed; southbound traffic and left turns from Arlington Rd. had lanes to accommodate them. The only problem was Trail traffic, mostly bicycles who ignored their own stop signs. Since the meeting last summer, I have believed that a bridge was a great idea; we could put things back the way they were for cars, and trail traffic would be safe. Why on earth narrow Arlington Rd. and Little Falls Parkway?

Name not shown

inside Bethesda/Chevy Chase
October 13, 2018, 6:38 PM

I prefer Concept C

Bridge best, safest, & least wait times for everyone. Option B gives more wait time to everyone.

As a fallback, Option A is good, but leave the current island between the car lanes! It gives pedestrians & bikers a safety zone to make sure the 2nd lane of car traffic is clear before crossing it.

Name not shown

inside Bethesda/Chevy Chase
October 13, 2018, 10:14 PM

I prefer Concept B

I frequently bike and drive through the area in question. From the perspective of both a driver and a biker, Concept B looks like the most cost effective, safest, and most efficient method of solving the crossing and driving problem. Better lighting at the juncture where the trail crosses Little Falls Parkway would be make it easier for

Name not shown

inside Bethesda/Chevy Chase
October 12, 2018, 9:49 PM

I prefer Concept A

Capital Crescent Trail Crossing at Little Falls Parkway Review

Montgomery Parks staff is seeking public feedback on the three alternate concept plans to improve the Capital Crescent Trail crossing at Little Falls Parkway.

drivers to see pedestrians and bikers in the evening hours.

Ross Filice

inside Bethesda/Chevy Chase
October 14, 2018, 2:24 PM

I prefer Concept A

Preferred Alternative A has been proven safe with minimal disruption over the past two years. Crashes have been dramatically reduced and no fatalities have occurred. Disruption to vehicle traffic has been minimal with only 3% decrease/diversion - and planned Parks and MCDOT road diets and calming measures in the area will further mitigate this. Vehicle delays have only been 7 seconds on average - this is an extraordinarily small price to pay for improved safety at this location.

Alternative B would further increase the delay for both trail users and vehicles while diverting double the traffic. It costs more, has more environmental impact, and trail users and drivers are both likely to be tempted to ignore the proposed three-way signal.

Alternative C would be safest, but is far more expensive with greater environmental impact - and Alternative A has been proven to work well.

As one of the core Vision Zero principles states, human life and safety should be prioritized over mobility of the road system - and certainly when it only costs an average of 7 seconds per vehicle. Concerns about diversion of traffic can and will be mitigated with ongoing project and MCDOT plans. Concerns regarding area construction and growth should and are being addressed with the Purple Line, rapid bus transit, and making this trail safer and welcoming as a transportation corridor - increasing capacity for predominantly single-occupancy vehicles is contrary to Vision Zero and the wrong direction to take in the face of increasingly alarming environmental projections such as the recent U.N. report.

Name not shown

inside Bethesda/Chevy Chase
October 14, 2018, 4:18 PM

I prefer Concept C

Bike and pedestrian traffic will increase significantly with time at this intersection. A pedestrian bridge is the only viable long-term solution. With a non-bridge solution, vehicular traffic will eventually choke to a crawl at the intersection as pedestrian traffic increases. A pedestrian bridge is also the safest option for all.

Ryan Thomas

inside Bethesda/Chevy Chase
October 15, 2018, 8:46 AM

I prefer Concept C

Safest for pedestrians and cyclists and fastest for cars, win win.

Name not shown

inside Bethesda/Chevy Chase
October 15, 2018, 1:13 PM

I prefer Concept C

A win-win for everyone. Reduces wait time for cars, allows pedestrians and cyclists safe passage over what is currently the busiest intersection on the Capital Crescent Trail. Expense of \$4 million is higher, but well worth the benefits over the expected life of the project.

Cornelius Davies

inside Bethesda/Chevy Chase
October 15, 2018, 5:29 PM

I prefer Concept C

I WOULD prefer Concept C, except WHY do we need to decrease the number of lanes? We were doing just fine with four lanes; a bridge could span 4 lanes. Why would you reduce the car traffic lanes? It is a quality of life, quality of transportation issue.

Name not shown

APPENDIX G OPEN TOWN HALL LOG

Capital Crescent Trail Crossing at Little Falls Parkway Review

Montgomery Parks staff is seeking public feedback on the three alternate concept plans to improve the Capital Crescent Trail crossing at Little Falls Parkway.

inside Bethesda/Chevy Chase
October 15, 2018, 5:56 PM

I prefer Concept C

While the most expensive, the bridge is also the safest and most efficient for drivers, who won't have to worry about a speed bump, or waiting for crossing cyclists and pedestrians at the signal. It's also the safest and most efficient for trail users. This intersection has been a peril and a hassle for drivers and trail users for years and has required a lot of attention and work. If we're going to spend money to fix this thing and save lives, we might as well spend the money to fix it once and for all.

Meg Hobbins

inside Bethesda/Chevy Chase
October 15, 2018, 7:48 PM

I prefer Concept A

Concept A has worked well over the last two years - everyone is safer and vehicle traffic has been only minimally delayed by an average of 7 seconds. There is no need to return to the former dangerous design. Concept C would be the next best option because it would be safe though quite expensive. Concept B would require a lot of trail refactoring and delays trail users and cars more than the current design so that doesn't make much sense.

Name not shown

inside Bethesda/Chevy Chase
October 16, 2018, 8:56 AM

I prefer Concept C

The bridge option makes the most sense for both drivers and trail users. It offers the lowest impediments to traffic flow both on the road and on the trail. A bridge should have been built years ago.

Name not shown

inside Bethesda/Chevy Chase
October 16, 2018, 9:10 AM

I prefer Concept C

The bridge is the only truly safe option - every other option risks the lives of our citizens including me!

I cross this intersection 3 or 4 times a week often in the dark (with a flash light). Most drivers are alert and courteous but it's all too often that I have a close call where the driver doesn't see me and slows without stopping or starts after someone else passes without seeing me crossing at the same time.

This intersection scares me every day.

Please build a bridge! I will thank you everytime I cross it.

Name not shown

inside Bethesda/Chevy Chase
October 16, 2018, 10:26 AM

I do not like any options listed

Whatever solution you choose you should deal with an issue that I am much concerned with. This is that while there are signs on the trail saying that it is "closed" at nightfall, people use it after dark, particularly in the fall and winter months when darkness comes early. This is a nightmare for drivers passing the trail intersection, which is not lighted. In evening and night hours it is very difficult for drivers to see whether there is anyone approaching the crossing on the trail -- yet at such times when I invariably slow my car there are occasions when I can see trail users who are approaching Little Falls Parkway in the darkness. My car's headlights do not show them when they are merely approaching the roadway. This is a disaster waiting to happen. You should somehow face up to the fact that people are using the trail after dark, either by opening it at this times or by regularly arresting those who use it after dark.

Rob Danegger

inside Bethesda/Chevy Chase
October 16, 2018, 11:19 AM

I prefer Concept C

Capital Crescent Trail Crossing at Little Falls Parkway Review

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Option C is be the only option that is being presented, which increases safety while not materially disrupting traffic flow on the trail and/or roadway

Noting that the current call to action appears to be motivated by the death of cyclist. Pedestrian <-> auto traffic management is problematical, but less of a demonstrated safety issues. So, solutions must carefully bicycle traffic glow and safety. Unfortunately, options A & B both fail to address the clear problem with bicycles that do not follow posted signs, requiring them to stop before entering the intersection.

Both A and B attempt to improve the current situation, where real-world behavior is causing safety problems, with solutions that assume perfect-world behavior going forward, where cars, pedestrians and cyclists all follow laws and posted signage. This is unreasonable

Solution A states: "No change in trail user wait times." There is effectively zero wait-time for current bicycle traffic. Bicyclist do not stop before entering the crossing area. So, forcing cars to slow for a speed table may do little to impact safety.

Solution B moves the crossing to an area that is controlled with a stoplight. However, it is reasonable to assume that users' behavior will remain the same, and that most will enter the crossing area without regard for the signal. West/ Northbound auto traffic must continue to be allowed to make a right-turn on red. Changing the control at Little Falls and Arlington to "no turn on red" for West/ Northbound traffic may have an enormously detrimental impact on traffic flow. This would be unacceptable given the fact that 1) the current wait time for West/ Northbound traffic to turn onto Arlington Rd is effectively zero, 2) traffic to the roadway is heavily used during times of the day and night, an in certain weather conditions, when there is essentially no trail usage; and 3) current behavior suggests that a significant number of cyclists will proceed across the intersection even if the traffic signal is green for cars if they perceive an opening.

Name not shown

inside Bethesda/Chevy Chase
October 16, 2018, 11:51 AM

I prefer Concept A

Concept C sounds like a good idea, but it is expensive. Building the bridge costs a lot of money and requires MoCo Parks to take on a new maintenance liability.

Concept A has a proper balance between cost and safety.

Cynthia Green

inside Bethesda/Chevy Chase
October 16, 2018, 11:53 AM

I prefer Concept C

The pedestrian bridge is the only safe way to separate vehicles and walkers/bikers. It may cost a bit more than the alternatives, but it will avoid deaths and injuries. The Capital Crescent Trail is very popular and is likely to become even more crowded in future as a place to experience nature and an environmentally friendly commuter route.

Jared Irvine

inside Bethesda/Chevy Chase
October 16, 2018, 1:52 PM

I prefer Concept C

C is the only concept that solves the problem for both bikes and traffic. It is more expensive but is the only solution. Choosing the concepts will end up being temporary solutions and only add to the total cost. Jared Irvine

Phil Fellini

inside Bethesda/Chevy Chase
October 16, 2018, 4:29 PM

I prefer Concept A

Strongly oppose Concept C. It is far too expensive and will require constant funding for upkeep. Concept A is a moderate, fiscally responsible solution.

APPENDIX G OPEN TOWN HALL LOG

Capital Crescent Trail Crossing at Little Falls Parkway Review

Montgomery Parks staff is seeking public feedback on the three alternate concept plans to improve the Capital Crescent Trail crossing at Little Falls Parkway.

Name not shown

inside Bethesda/Chevy Chase
October 16, 2018, 4:32 PM

I prefer Concept C

A bridge makes sense here. Concept B is only OK in the interim. I strongly prefer a bridge so that LF PKWY is restored to a four lane road. Remove the road diet - which is causing problems with cut-through traffic and is harming traffic flow.

David Churchill

inside Bethesda/Chevy Chase
October 16, 2018, 4:58 PM

I prefer Concept C

Investing in safe infrastructure with a separate grade seems worth it for one of the most popular sections of the most popular trails.

Name not shown

inside Bethesda/Chevy Chase
October 16, 2018, 4:58 PM

I prefer Concept C

Name not shown

inside Bethesda/Chevy Chase
October 16, 2018, 6:55 PM

I prefer Concept C

Name not shown

inside Bethesda/Chevy Chase
October 16, 2018, 7:27 PM

I prefer Concept C

Name not shown

inside Bethesda/Chevy Chase
October 16, 2018, 7:33 PM

I prefer Concept A

Name not shown

outside Planning Areas
October 16, 2018, 7:35 PM

I prefer Concept C

Pedestrian overpass has been needed here for a long time. Drivers are too impatient to accommodate crossing pedestrians/bikers.

Name not shown

inside Bethesda/Chevy Chase
October 16, 2018, 8:58 PM

I prefer Concept C

Pat Garvey

inside Bethesda/Chevy Chase
October 16, 2018, 8:58 PM

I prefer Concept C

The bridge is the best long term solution, the bridge at River Road has made a huge difference at that intersection and is a delight for all users: drivers, bikers, walkers, runners and strollers.

The CCT is a long term trail for Bethesda. Budget the funds! Concept B is a lose lose for all parties. No one will wait for a light from the CCT except mothers with children. Come on, bikers will not wait a 3 cycle of lights. Let's be realistic. The bridge is expensive but the River Road bridge proves it is a successful solution. Little Falls Parkway should go back to 4 lanes.

Concept A is doable, but if it is chosen, then improve the lighting of the Parkway at the speed bump. And get rid of at least half of the road sticks. They do nothing but cause clutter and confusion, especially after the crossing in both directions. The Park Division should use as their guide, "First don't make matters worst!"

APPENDIX G OPEN TOWN HALL LOG

Capital Crescent Trail Crossing at Little Falls Parkway Review

Montgomery Parks staff is seeking public feedback on the three alternate concept plans to improve the Capital Crescent Trail crossing at Little Falls Parkway.

Name not shown

inside Bethesda/Chevy Chase
October 16, 2018, 9:19 PM

I prefer Concept C

James Stuart

inside Bethesda/Chevy Chase
October 17, 2018, 12:08 AM

I prefer Concept C

My preference is C, A then B in that order

william isola

inside Bethesda/Chevy Chase
October 17, 2018, 9:15 AM

I prefer Concept C

Name not shown

inside Bethesda/Chevy Chase
October 17, 2018, 9:34 AM

I prefer Concept C

Name not shown

inside Bethesda/Chevy Chase
October 17, 2018, 11:06 AM

I prefer Concept C

i ride the trail to work throughout the year and think the same option as works on River Road would be the safest for both cars and riders.

Name not shown

inside Bethesda/Chevy Chase
October 17, 2018, 11:11 AM

I prefer Concept C

The traffic into Bethesda is already impossible. I have stopped going because of how long it now takes at Little Falls Parkway with the new two lane pattern. The only solution that makes any sense at all is an overpass bridge - pedestrians will be safe and traffic will flow much easier - win win.

Name not shown

inside Bethesda/Chevy Chase
October 17, 2018, 11:11 AM

I prefer Concept C

Name not shown

inside Bethesda/Chevy Chase
October 17, 2018, 11:12 AM

I prefer Concept C

Name not shown

inside Bethesda/Chevy Chase
October 17, 2018, 11:14 AM

I prefer Concept A

If Concept A is working, and is \$3.2 million less than the overpass, why not? I drive on the Parkway every day, and it has not been particularly burdensome. I appreciate that the bikers really like to go FAST, but if an extra \$3.2 mil is going to be spent, I'd rather it be spent on a dedicated walking lane (as they have in Minneapolis, bikers' haven), because the trail is not safe for pedestrians, particularly with young children. Also, a bridge is more difficult for people with limited mobility.

Kevin Murphy

inside Bethesda/Chevy Chase
October 17, 2018, 12:13 PM

I prefer Concept A

APPENDIX G OPEN TOWN HALL LOG

Capital Crescent Trail Crossing at Little Falls Parkway Review

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Name not shown

inside Bethesda/Chevy Chase
October 17, 2018, 12:30 PM

I prefer Concept C

James Bergmann

inside Bethesda/Chevy Chase
October 17, 2018, 12:36 PM

I prefer Concept C

I vote for C,B A in that order. Bikers continue to blow through the STOP sign on both sides of the trail. I have NEVER seen a biker stop. Drivers unite and fight this vocal minority. We want our road back.

Name not shown

outside Planning Areas
October 17, 2018, 12:49 PM

I prefer Concept C

Name not shown

inside Bethesda/Chevy Chase
October 17, 2018, 1:14 PM

I prefer Concept C

Name not shown

inside Bethesda/Chevy Chase
October 17, 2018, 1:16 PM

I prefer Concept C

Name not shown

inside Bethesda/Chevy Chase
October 17, 2018, 1:30 PM

I prefer Concept C

Name not shown

inside Bethesda/Chevy Chase
October 17, 2018, 1:38 PM

I prefer Concept A

Name not shown

inside Bethesda/Chevy Chase
October 17, 2018, 1:43 PM

I prefer Concept C

Name not shown

inside Bethesda/Chevy Chase
October 17, 2018, 3:00 PM

I prefer Concept A

As a resident who uses the intersection to walk to the pool, commute on a bike, and/or drive through the area, I appreciate the attention this is getting. To me, Concept A is the most economical way to promote safety and increase the ease of non-motorist access through the area. I applaud the County for including a road diet in the designs. I encourage the County to better integrate this plan with the recently finished bike lanes on Glenbrook Rd (between Bradley and Fairfax).

Jessica Hirschhorn

inside Bethesda/Chevy Chase
October 17, 2018, 3:41 PM

I prefer Concept A

Name not shown

inside Bethesda/Chevy Chase
October 17, 2018, 3:59 PM

I prefer Concept C

Capital Crescent Trail Crossing at Little Falls Parkway Review

Montgomery Parks staff is seeking public feedback on the three alternate concept plans to improve the Capital Crescent Trail crossing at Little Falls Parkway.

Name not shown

inside Bethesda/Chevy Chase
October 17, 2018, 4:07 PM

I prefer Concept B

The "bandaid" approaches that have been tested at the trail crossing have not resulted in a safer crossing for trail users or for drivers. As a regular trail user (runner/walker), I have noticed that most bikers make no attempt to stop at the current stop signs. In addition, at dusk this intersection is even more dangerous with trail users assuming they can be seen! I think Concept B (middle in cost) is the best approach for safety.

Name not shown

inside Bethesda/Chevy Chase
October 17, 2018, 5:00 PM

I prefer Concept B

One of the problems with the current configuration (and Option A) is that it places no responsibility on bikers to be responsible as they cross Little Falls. Even though pedestrians have the right of way, bikers on their bikes DO NOT HAVE THE RIGHT OF WAY yet they regularly ride across Little Falls as though they did. Moving the crossing to a light where the bikers much comply will do more to reduce accidents than any option other than C. The problem with C is that it is very expensive and intrusive.

JOSEPH NASON

inside Bethesda/Chevy Chase
October 17, 2018, 5:49 PM

I prefer Concept C

Concept C is best and it seems worth the investment given the significant trail usage at that location. My second choice would be Concept A because the current crossing seems to be working fine and is relatively safe for pedestrians. I am not in favor at all of Concept B (reorienting the trail).

Name not shown

inside Bethesda/Chevy Chase
October 17, 2018, 6:40 PM

I prefer Concept C

Hi Neighbors and Staff,

Remember that this is not an average street - it's the unique National Park Service Capital Crescent Trail and viewed by many as one of the regions truly exceptional recreation resources.

Its used by 10,000 people a day*.

Does that not make improving safety worth investing more than the average street?

To put this in context, our neighborhood pool, which serves, at the peak 300, users a day, cost over \$3M to rebuild a few years ago. And its my understanding that the cost to install a stop light can often top \$500K**. Despite those costs, we still build pools and install stoplights when they are needed. When's the last time we built a pedestrian bridge?

The investment per pedestrian over 10 years is small. Start with \$3.2m over 10 years which works out to be \$320k a year (not counting maintenance) divide by 365 days a year and you get the daily cost of \$876 which, when spread across the 10K* daily trail users, works out to 9 cents per crossing.

Should we not be willing to spend 9 cents to protect our citizens and our children at a dangerous intersection where several of our neighbors have already been killed and injured?

But wait, there's more! With the bridge, vehicle traffic will not have to stop, either, so we are improving the life of the drivers for that 9 cents as well.

By this analysis, \$3.2M is a bargain. And we won't have to spend money revising this intersection again in a few years after the next tragedy.

Thanks for enduring my analysis!

*I was told a traffic study counted 10,000 pedestrians &

Capital Crescent Trail Crossing at Little Falls Parkway Review

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bikers on this section of trail.

** I Googled cost to install a traffic light.

John Crowley

inside Bethesda/Chevy Chase

October 17, 2018, 7:13 PM

I prefer Concept C

Concept C strikes me as the safest alternative, and the only one that completely separates the trail traffic from the road traffic. Despite the likely higher cost, we must prioritize safety, in view of the deaths and serious injuries that have brought this issue to the fore.

Maureen Jais-Mick

inside Bethesda/Chevy Chase

October 17, 2018, 7:40 PM

I prefer Concept A

I like the current set up, but I would make one suggestion - that at both Little Falls and a block later at Dorset, that you enforce the top sign for pedestrians and bicyclists.

M Cheng

inside Bethesda/Chevy Chase

October 17, 2018, 10:32 PM

I prefer Concept B

Frankly I think all three options are unreasonable. The injury or death of any cyclist or pedestrian is unacceptable. But, cyclists do not stop on the trail before crossing LFP. There is a stop sign on each side and they blow through it. Pedestrians are easier to adjust for. Bicyclists are NOT the primary mode of transportation in this region and until which time bicyclists obey traffic laws they are subject to, the majority should not be negatively impacted by a minority that does not follow the law. The two 'road diet' solutions (frankly a silly euphemism for reducing four lanes to two) negatively impact traffic. And, how about some speed bumps to make the bicyclists slow down if not stop. If you can find the money for the pedestrian overpass and

force the bicyclists and pedestrians to go up and over the bridge, my hat is off to you. And, meanwhile the county is increasing development both in downtown Bethesda and Westbard which will only add to traffic on Arlington and LFP. Concept B as an interim step before installing Concept C.

Charles Smith

inside Bethesda/Chevy Chase

October 17, 2018, 10:45 PM

I prefer Concept C

Thomas Holzman

inside Bethesda/Chevy Chase

October 18, 2018, 7:09 AM

I prefer Concept C

Name not shown

inside Takoma Park

October 18, 2018, 8:30 AM

I prefer Concept C

Separating pedestrians and cyclists from cars is always the preferred choice.

Robert Metzler

inside Bethesda/Chevy Chase

October 18, 2018, 9:47 AM

I prefer Concept C

Concept A has a raised bike/walk way. There will be accidents when bicycles go over the edge. Bad idea!

Name not shown

inside Bethesda/Chevy Chase

October 19, 2018, 11:51 AM

Capital Crescent Trail Crossing at Little Falls Parkway Review

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I prefer Concept C

Expensive, but has absolute benefits to both trail users and highway users.

Name not shown

inside Bethesda/Chevy Chase

October 20, 2018, 1:35 AM

I prefer Concept C

Safety for all bikers, pedestrians and drivers is the main goal for any future plan. The pedestrian bridge is the only option that guarantees that goal can be met. It also allows traffic to flow freely, reduces rush hour back ups and aggravated drivers honking, eliminates the need to cut through otherwise quiet neighborhoods, and allows pedestrians and bikers to travel safely and without crossing delays.

It will be expensive but worth it. For once I would like to get some benefit from my taxes!

If that is not possible, I vote for plan B.

Jacqueline Tront

inside Bethesda/Chevy Chase

October 22, 2018, 3:57 AM

I prefer Concept C

Karen Mitrano Snyder

inside Bethesda/Chevy Chase

October 22, 2018, 9:38 AM

I prefer Concept A

Concept A has proven to be a safe alternative via the success of the temporary arrangement. Besides being effective, it is lower in cost and has a low environmental impact. Small, flashing, yellow lights for drivers not familiar with the situation should be added if not already in the plan. Option B would be my second choice. The Bridge in Option C, besides being the most costly choice and having a higher negative environmental impact, would have steep hills on either side, especially if cycling toward downtown

Bethesda. Many would simply not be able to use it.

Name not shown

inside Bethesda/Chevy Chase

October 22, 2018, 4:27 PM

I prefer Concept C

The safest choice is to keep both the busy trail and street moving without mixing the two.

Name not shown

inside Bethesda/Chevy Chase

October 22, 2018, 7:12 PM

I prefer Concept C

I find the current set-up (which Concept A would in effect make permanent) unacceptable. It has resulted in unnecessary traffic back-ups, especially during rush hour. All users, Pedestrians, bicyclists and drivers, are too frequently not appropriately watchful. Drivers have avoided the area by going through residential neighborhoods, on streets without sidewalks that are not designed to carry the traffic, also unacceptable. Little Falls should be returned to four lanes, with either a pedestrian bridge (Concept C, my first preference) or a trail rerouting and crossing at Arlington Road (with a crossing cycle that stops all traffic and with right turns on red not permitted) (Concept B, my second choice). Making the current arrangement permanent (Concept A) would be no solution at all and creates other problems.

John Nuckols

inside North Bethesda

October 23, 2018, 11:19 AM

I prefer Concept C

The key to safe and recreationally beneficial pedestrian pathways is unfettered connectivity. Linkage of trails across high volume automobile roadways via overpasses or underpasses is really the only viable connectivity option that promotes use of trails across age and skill level,

Capital Crescent Trail Crossing at Little Falls Parkway Review

Montgomery Parks staff is seeking public feedback on the three alternate concept plans to improve the Capital Crescent Trail crossing at Little Falls Parkway.

whether cycling, running, or strolling. Residents of Montgomery County would be well-served when sound decisions are made to invest in multi-use trails with optimal connectivity. The current design of the crossing of the Capital Crescent Trail at Little Falls Parkway creates a dangerous and disruptive bottleneck for both trail users and automobile drivers. In my opinion, Options A and B would only exacerbate the problem. It's redesign as an overpass is a prime opportunity to greatly enhance the crossing experience for both groups, as well serve to promote the goal of connectivity as stated in the County's master plan for its pedestrian path network.

James Donohoe

inside Bethesda/Chevy Chase
October 24, 2018, 1:16 PM

I prefer Concept A

It is awful that it took a death to cut this road down to one lane each way, but it is much safer now. Most drivers are now courteous and aware that they are supposed to stop. The speed table will take care of the rest. Save the \$4MM for the bridge and put it toward needed safety improvements elsewhere - such as protected bike lanes in downtown Bethesda. And keep the median!

Sara Robinson

inside Bethesda/Chevy Chase
October 25, 2018, 4:02 PM

I do not like any options listed

I strongly oppose all of the 3 options as outlined. The current option A is untenable, causes too many traffic problems and is far too dangerous. Option C could be great but is way too expensive and unnecessary. Option B of crossing at the light would be fine if that was all that was involved. But the inclusion of a further bridge on Hillendale is totally unacceptable, it would severely disrupt the park and is totally unnecessary and will cause further safety issues. There is simply no reason bike riders, of which I am one, can not cross at the light, either at Hillendale or Arlington, and then return to the existing trail without the destructive construction included in Option B as outlined. The only viable option would be option B without the

additional construction. This option would provide for traffic flow, safety and reasonable cost. Thank you for considering this reasonable alternative.

Name not shown

inside Bethesda/Chevy Chase
October 29, 2018, 10:16 AM

I prefer Concept C

The bridge over Little Falls Pkwy is the ONLY safe option for all parties. Bikers and some walkers routinely violate their safety and then motorists safety by jay walking, speeding, being abusive to those citizens urging bikers to SLOW DOWN and stop at STOP signs. I urge MD Park Police to be more proactive at the Dorset / Crescent Trail intersection where many bike rider violations occur every single day. The LFs road diet also contributes to Road Rage with impatient drivers becoming aggressive. Biker and pedestrian 'education' to safety is non compliant- police enforcement is the only solution.

Mark Friedrichs

inside Bethesda/Chevy Chase
October 29, 2018, 9:59 PM

I prefer Concept C

Perhaps the only workable long term solution given the current expectation of increased trail and vehicle traffic over time.

Jimmy Mrose

inside Bethesda/Chevy Chase
October 29, 2018, 10:01 PM

I prefer Concept C

Ken Kramer

inside Bethesda/Chevy Chase
October 29, 2018, 10:13 PM

I prefer Concept A

APPENDIX G OPEN TOWN HALL LOG

Capital Crescent Trail Crossing at Little Falls Parkway Review

Montgomery Parks staff is seeking public feedback on the three alternate concept plans to improve the Capital Crescent Trail crossing at Little Falls Parkway.

Name not shown

inside Bethesda/Chevy Chase
October 29, 2018, 11:37 PM

I prefer Concept C

While unfortunately the most expensive option, option C is clearly the best. In another example of "you get what you pay for," this is the only option which actually separates drivers from trail users. As a daily commuter on the trail, I have seen atrocious behavior from both trail users and drivers more times than I can count. The two collisions I've seen close up and the innumerable close calls convince me that separation is the only option. I have been around long enough to remember when there was a level crossing at River Road. I'm sure there was some suggestion to divert trail users to Little Falls or the entrance to Kenwood to cross, but I think we can all agree that the current bridge is far and away the most safe option (sadly, it took a cyclist's death to make that happen as well).

Name not shown

inside Bethesda/Chevy Chase
October 30, 2018, 7:43 AM

I prefer Concept B

It is sad that we need to invest public funds all because the bikers and pedestrians have failed to heed the STOP sign that applies to them. It is pretty simple - when you see a STOP sign, you stop and do not proceed until there is no traffic. I am a heavy user of the CCT, and I can count on one hand when I have seen other users heed the traffic directive to STOP. Maybe if the Parks Dept. had come out to issue jay walking tickets to users and reminded people of their obligation to STOP and yield to the oncoming traffic there would have been no fatalities and no need to spend public funds on fixing this "problem."

Name not shown

inside Bethesda/Chevy Chase
October 30, 2018, 8:38 AM

I prefer Concept C

Name not shown

inside Bethesda/Chevy Chase
October 30, 2018, 9:57 AM

I prefer Concept A

Bonnie Blades

inside Bethesda/Chevy Chase
October 30, 2018, 10:02 AM

I prefer Concept A

Jane Gomes

inside Bethesda/Chevy Chase
October 30, 2018, 2:13 PM

I prefer Concept C

The pedestrian bridge is the safest option for trail users and drivers. Nearby, I feel 100% more comfortable on the CCT crossing River Road on the pedestrian bridge, especially with young children, than I would utilizing a crosswalk. An underground tunnel would be another option; was that deemed too expensive?

Thank you to MC Parks for the analysis and ongoing work to make Montgomery County safer for all residents and visitors.

Name not shown

inside Bethesda/Chevy Chase
October 30, 2018, 4:28 PM

I prefer Concept A

Leslie Kefauver

inside Bethesda/Chevy Chase
October 30, 2018, 4:37 PM

I prefer Concept C

APPENDIX G OPEN TOWN HALL LOG

Capital Crescent Trail Crossing at Little Falls Parkway Review

Montgomery Parks staff is seeking public feedback on the three alternate concept plans to improve the Capital Crescent Trail crossing at Little Falls Parkway.

Douglas Tyson

inside Bethesda/Chevy Chase

October 30, 2018, 6:30 PM

I prefer Concept C

This is the safest option for bikers/walkers

Name not shown

inside Bethesda/Chevy Chase

October 30, 2018, 8:01 PM

I prefer Concept A

Name not shown

inside Bethesda/Chevy Chase

October 30, 2018, 10:35 PM

I prefer Concept C

I regularly access this intersection as both a cyclist and driver. With the interim "diet", most drivers are sensitive to pedestrian traffic, however, every once in a while one just zips through without looking. While this is much improved over the pre-diet situation, it still leaves me occasionally uncomfortable. Given the modest cost (particularly recurring costs), I could live with a permanent "diet" (Concept A). My preference, however, is Concept C as it would eliminate road/pedestrian interaction. Concept B seems like an expensive workaround that leaves everyone dissatisfied.

Name not shown

inside Bethesda/Chevy Chase

October 30, 2018, 10:35 PM

I prefer Concept C

Jack McCune

inside Bethesda/Chevy Chase

October 31, 2018, 8:11 AM

I prefer Concept C

Name not shown

inside Bethesda/Chevy Chase

October 31, 2018, 10:36 AM

I prefer Concept C

Name not shown

inside Bethesda/Chevy Chase

October 31, 2018, 10:59 AM

I prefer Concept B

I like how Concept B has a connector to the Crescent trail to the trail to Norwood Park. That would be very useful to residents in West Chevy Chase. I also like that it connects to the parking lot west of Arlington Road. The parking lot is used by a number of people who drive to the Crescent trail. I like that pedestrians and cyclists now cross Little Falls at a major intersection where cars have to stop anyway. That seems the most sensible and safe solution.

My concerns with Concept C is that some people will circumvent the bridge and cut across Little Falls (like they do at River Road), and it now has THREE crossings across Little Falls. I don't see how that will improve safety at all. It just increases the number of places where accidents can occur. Plus Concept C is very expensive. I'm not confident that the people voting for Concept C really look carefully enough to see that there are now THREE crossings at Little Falls.

Concept A also proposes THREE crossings of Little Falls. I don't think that improves safety for the reasons discussed above for Plan C.

I drive through this intersection daily and I still have to be very careful because a number of pedestrians and cyclists still do not stop at the intersection.

Name not shown

inside Bethesda/Chevy Chase

October 31, 2018, 12:12 PM

I prefer Concept C

APPENDIX G OPEN TOWN HALL LOG

Capital Crescent Trail Crossing at Little Falls Parkway Review

Montgomery Parks staff is seeking public feedback on the three alternate concept plans to improve the Capital Crescent Trail crossing at Little Falls Parkway.

Carl Becker

inside Bethesda/Chevy Chase
October 31, 2018, 1:15 PM

I prefer Concept C

A pedestrian bridge over Little falls parkway is:
1) the safest solution
2) the most automobile and traffic friendly solution
3) the most pedestrian and biker friendly solution
As downtown bethesda grows and westbard redevelops we will need more automobile lanes on little falls (than the current restricted flow) and more pedestrian handling capacity on the crescent trail. The forward thinking solution is to restore the roadway to its previous configuration while adding a pedestrian bridge.

Name not shown

inside Bethesda/Chevy Chase
October 31, 2018, 1:38 PM

I prefer Concept A

Name not shown

inside Bethesda/Chevy Chase
October 31, 2018, 1:52 PM

I prefer Concept C

Name not shown

inside Bethesda/Chevy Chase
October 31, 2018, 2:13 PM

I prefer Concept C

Road diet is a poor option that has led to dangerous near incidents at the intersection. A bridge would eliminate this. Quite unfortunate that a tragic rider error that led to a fatality has created this situation. Common sense and caution on the part of trail users (of which I am one) would prevent nearly all accidents.

Name not shown

inside Bethesda/Chevy Chase
October 31, 2018, 2:17 PM

I prefer Concept C

Make it safe and simple. Pedestrian/bike bridge works great at River. This is just as big an intersection and the current solution is horrible. Too much crap added to the roadway makes it too hard to process where and what to look for. The changes to the road have made it more difficult to navigate and process from a driving perspective. The number of people using the intersection on path/bridge will only increase going forward with completion of Purple Line.

Name not shown

inside Bethesda/Chevy Chase
October 31, 2018, 4:40 PM

I prefer Concept C

Name not shown

inside Bethesda/Chevy Chase
October 31, 2018, 5:46 PM

I prefer Concept A

Name not shown

inside Bethesda/Chevy Chase
October 31, 2018, 6:03 PM

I prefer Concept B

Jocelyn Witt

inside Bethesda/Chevy Chase
October 31, 2018, 6:18 PM

I prefer Concept C

I think a pedestrian bridge would be the safest alternative for the Capital Crescent Trail.

APPENDIX G OPEN TOWN HALL LOG

Capital Crescent Trail Crossing at Little Falls Parkway Review

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Meryl Silver

inside Bethesda/Chevy Chase
October 31, 2018, 7:01 PM

I prefer Concept A

Barry Cutler

inside Bethesda/Chevy Chase
October 31, 2018, 9:16 PM

I prefer Concept C

I was not for C at first, but Carl Becker convinced me. It is the only solution that isn't a band-aid and considers the future.

Barry Cutler (Sumner)

Name not shown

inside Bethesda/Chevy Chase
October 31, 2018, 9:56 PM

I prefer Concept A

Name not shown

inside Bethesda/Chevy Chase
November 1, 2018, 7:52 AM

I prefer Concept C

Kamel Saidi

inside Bethesda/Chevy Chase
November 1, 2018, 8:51 AM

I prefer Concept C

This is the safest and least disruptive (once it is built) to vehicle traffic and trail users.

Name not shown

inside Bethesda/Chevy Chase

November 1, 2018, 2:39 PM

I do not like any options listed

I am a frequent walker on the CCT and driver along Little Falls Parkway. The current temporary pylons are very dangerous and obstruct driver's vision. Why can't the users of the CCT utilize a pedestrian walk sign like the rest of the folks on Bethesda? The traffic light at Arlington Road and Hillandale can be set to "Red" while the walk sign is in cycle on the CCT. This system will not affect vehicular traffic and save a huge amount of money. As a walker on the trail, I can be patient and wait to cross. We don't have pedestrian bridges over every road. The County does not have unlimited resources and this would be a good place to save.

Name not shown

inside Bethesda/Chevy Chase
November 2, 2018, 10:27 AM

I prefer Concept A

Helen Davies

inside Bethesda/Chevy Chase
November 2, 2018, 10:40 AM

I prefer Concept C

The pedestrian bridge is the only way to keep cars from interacting with the growing number of pedestrians and cyclists in this area. No one should have to die just trying to cross an already marked pedestrian crossing. Currently it's the bikes and cyclists that have the stop sign, not the cars. With a double lane pedestrian crossing there is always the possibility of one car not being able to view a bike or jogger crossing in front of the other car. The pedestrian bridge (assuming it's a ramp and not steps) keeps everyone safe and allows the traffic to resume two lane travel.

Sharon Metcalf

inside Bethesda/Chevy Chase
November 2, 2018, 12:43 PM

Capital Crescent Trail Crossing at Little Falls Parkway Review

Montgomery Parks staff is seeking public feedback on the three alternate concept plans to improve the Capital Crescent Trail crossing at Little Falls Parkway.

I prefer Concept C

Rose Beale

outside Planning Areas
November 2, 2018, 4:19 PM

I prefer Concept A

Name not shown

outside Planning Areas
November 4, 2018, 6:18 AM

I prefer Concept C

I use this route all the time. So glad something will be done for safety.

Name not shown

inside Bethesda/Chevy Chase
November 4, 2018, 5:19 PM

I prefer Concept C

Kathy Daniel

inside Bethesda/Chevy Chase
November 6, 2018, 11:03 AM

I prefer Concept C

Concept A seems like an OK course of action, considering the price tag, but I chose C because traffic (vehicle, bicycle, and pedestrian) is only going to increase in that area. The existing modification is already resulting in traffic backups. For the long term, the larger investment is justified.

Chad Young

inside Bethesda/Chevy Chase
November 6, 2018, 2:35 PM

I prefer Concept A

Crossing at Arlington seems like an option that makes no sense. What's to stop anyone from just crossing as usual? Then you have people crossing at Arlington (following the new rules) and those that don't... making it a guessing game for drivers.

A pedestrian bridge seems like a large sum of money and an eye sore... plus the time it would take to install would be a headache.

Pedestrians, bikers, runners and drivers all need to take care and be respectful and patient at this intersection. The way it is works when everyone is aware, patient and respectful.

Joel Marcus-Kurn

inside Bethesda/Chevy Chase
November 7, 2018, 9:03 PM

I prefer Concept A

One of the reasons why the CCT is such a success is its ease of use for cyclists. The existing road crossing at Arlington has worked very well, and making it permanent will preserve its benefits for cyclists at a reasonable cost. Rerouting the trail would be a serious mistake because it would undercut the very ease of use that has made the trail so user friendly.

Garrett Hennigan

outside Planning Areas
November 8, 2018, 2:07 PM

I prefer Concept A

When the Parks Department installed the temporary road diet and reduced the speed limit following Mr. Gaylin's death at this intersection, they correctly prioritized the safety of trail users over the desire to move as many cars as fast as possible through the intersection. This was the right move, and it has proven to be far safer and not nearly the doomsday traffic scenario that some drivers have complained about.

For the permanent fix, Parks should stick with what works

Capital Crescent Trail Crossing at Little Falls Parkway Review

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and use the savings to improve similarly dangerous trail intersections across the County. Alternative A is the best option. Perhaps in the future, when more pressing safety issues are solved, Alternative C may make sense. Alternative B would be a step in the wrong direction for both trail users and drivers.

Name not shown

inside Bethesda/Chevy Chase
November 10, 2018, 12:06 PM

I do not like any options listed

I live on Hillandale Rd., close to the crossing. I object to each of the "concepts" because they include making the "road diet" on Little Falls Parkway permanent. This "road diet" has resulted in a lot more traffic passing on Hillandale Rd., through a densely populated residential area, and has resulted in greater hazards to my personal safety, both as a motorist and a pedestrian. Given the increasing development in the area, i.e. downtown Bethesda and Westbard, Little Falls Parkway should not be reduced to two lanes. This area is not like the area where other two lane parkways run, such as Sligo Creek Parkway and Beach Drive. There is a lot more development.

David Van Mourik

inside Bethesda/Chevy Chase
November 12, 2018, 2:04 PM

I do not like any options listed

As a resident who lives on Hillandale Road and a regular user of the Capital Crescent Trail, I propose an option that does not restrict the lanes on Little Falls Parkway as this drives traffic onto Hillandale Road. A pedestrian bridge over Little Falls, similar to what is in place over River Road is the safest option as it removes all interaction between automobiles and pedestrian/bike traffic, however this option should span all 4 lanes of Little Falls Parkway. The increased traffic on Hillandale Road from the temporary road diet has made it dangerous to cross the street, park in and pull out of our parking spots. Drivers use Hillandale as a race way going around cars waiting for traffic to pass so residents can park. This results in near collisions as drivers are racing up and down the street. The drivers then honk

their horns because the residents parking or pulling out "are in the way." Please reconsider the pedestrian bridge over all 4 lanes over Little Falls Parkway.

Name not shown

inside Bethesda/Chevy Chase
November 12, 2018, 2:11 PM

I prefer Concept C

I prefer option C as a trail user who has experienced the danger when traffic in only 1 of 2 lanes in a single direction stops for you. However, something else should be done related to Hillandale and the Hillandale/Bradley intersection. As a resident who lives effectively above the intersection, I hear a chorus of honks and skidding brakes both during rush hour and over the duration of most weekend days. The aggressive driving up and down the road is dangerous both to pedestrians crossing and traffic pulling in and out of the parking spaces.

Name not shown

inside Bethesda/Chevy Chase
November 12, 2018, 3:34 PM

I do not like any options listed

By closing both lanes, traffic is diverted through a family neighborhood on Hillandale. There are cross walks and many children. The increased traffic could result in accidents involving children and is also a nuisance to the neighborhood. Keep 2 lanes and move the cross walk to the light on Hillandale. Bikers and pedestrians can wait for a green light.

Name not shown

inside Bethesda/Chevy Chase
November 12, 2018, 3:50 PM

I prefer Concept C

We already have seen a large increase in traffic along Hillandale where so many of us have young children and pets. It's obvious the large majority of these drivers are

Capital Crescent Trail Crossing at Little Falls Parkway Review

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cutting through recklessly and it's not the right solution to leave that is. I am for a raised pedestrian walk bridge across Twin Falls, but you also need to install some sort of mechanism to slow drivers along Hillandale. We bought our homes there for its tranquility and would be devastated if it became a major thoroughfare for traffic. You need to take that into consideration and our voice into any decision.

Mary Cahill

inside Bethesda/Chevy Chase
November 12, 2018, 4:26 PM

I prefer Concept A

I am totally opposed to making the section of the Little Falls Parkway closed at Hillandale Rd. I have lived on Hillandale Rd. for over 30 years. The traffic has already increased with the partial closing of Little Falls Parkway. We have many families with babies and young children walking and playing along Hillandale Rd. School Buses and transit to the subway come through Hillandale Rd., as well as the truck required for garbage and recycling. These townhomes were here before the trail and were purchased with the idea that this would be a safe place to live and still be able to walk to downtown Bethesda, which has been totally overbuilt in the past 30 years. Streets are constantly blocked with construction vehicles and workers. Please let us hold onto the one place that still provides a haven from the high rises in Bethesda. If Little Falls Rd is closed at the intersection with Hillandale, Hillandale will become more of a throughway for impatient drivers. I am more interested in the safety of the people here than I am concerned about the delay in driving time or the wait at the intersection for bicyclists, walkers, runners, baby carriages. I enjoy using the trail for walking and it is already dangerously overrun with fast bikers. Please take into account the guidelines for civility that Bethesda used to represent. Mary Cahill, JD, 6663 Hillandale Rd.

Judith Bernstein

inside Bethesda/Chevy Chase
November 12, 2018, 5:16 PM

I do not like any options listed

I am against all three plans as they involve keeping only one lane of Little Falls Parkway open for traffic. I live on Hillandale Road and since the closure of one lane on Little Falls the volume of traffic on our street has doubled. Our residential street is now dangerous to cross, difficult to access parking spaces with speeding automobiles on our tail, noisy and deteriorating with numerous potholes. Has anyone considered two lanes of traffic on Little Falls with a traffic light installed at the trail crossing?

Marty Chase

inside Bethesda/Chevy Chase
November 12, 2018, 6:46 PM

I do not like any options listed

I am opposed to the permanent closing of lanes on Little Falls Parkway because it has increased traffic on Hillandale Road, which has made it significantly difficult and dangerous for residents to back their autos out of their residential parking places. It has also added greatly to noise and litter on Hillandale Road. Importantly, too, it has created a safety hazard for the numerous children who reside on Hillandale Road.

Name not shown

inside Bethesda/Chevy Chase
November 12, 2018, 6:59 PM

I do not like any options listed

I am opposed to permanently closing lanes on Little Falls Parkway because it will shift traffic to Hillandale Road, which will create dangerous traffic conditions for residents who must back out of their parking spaces. It will also cause significant noise as well as a dangerous street for the numerous children living on Hillandale Road.

Name not shown

inside Bethesda/Chevy Chase
November 13, 2018, 5:32 PM

I prefer Concept C

APPENDIX G OPEN TOWN HALL LOG

Capital Crescent Trail Crossing at Little Falls Parkway Review

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I dislike all of the options, but think concept c is the least problematic of them. The temporary road diet has already made the Hillandale neighborhood more dangerous for pedestrians and drivers. An increasing numbers of people who used to access Little Falls via Arlington are speeding down Hillandale to bypass the bottleneck. If the county is serious about it's 0 in 2030 initiative, it must take the safety of Hillandale residents into consideration too. Anything that slows traffic on Little Falls will encourage cars to divert onto Hillsdale. The county should reduce risk, not relocate it.

Name not shown

inside Bethesda/Chevy Chase
November 14, 2018, 10:46 AM

I prefer Concept B

the traffic for this option would be similar or same to that on Little Falls and Dorset.

michael Skinker

inside Bethesda/Chevy Chase
November 14, 2018, 10:56 AM

I prefer Concept C

The pedestrian bridge should be built in a way to retain the 4 lane traffic pattern that existed on Little Falls Parkway before the temporary "road diet" was installed. The current temporary "road diet" diverts too much traffic to Hillandale Road and the Kenwood Forest Community. Parking for the town homes on Hillandale Road is perpendicular to the road and already at times requires difficult and dangerous maneuvering to pull out into the thru traffic, especially during rush hours. There are many children that live in the development and the increase in traffic is creates a more unsafe environment. Speed bumps on the hill are not really appropriate and not the answer. Reducing traffic thru the community rather than diverting more traffic to Hillandale Road will help.

James Sheesley

inside Gaithersburg Vicinity
November 14, 2018, 4:38 PM

I prefer Concept C

Gordon Chaffin

outside Planning Areas
November 14, 2018, 6:08 PM

I prefer Concept A

I'm Gordon Chaffin. Moved to DC in 2010. Lived in Silver Spring 2011-2015 and heavy CCT user as runner/cyclist. The data show that almost no traffic is being diverted into the nearby neighborhoods. Speeding has also not increased, despite the claims of everyone voting here for the expensive, unnecessary bridge. Option A is the best way to minimize cost, environmental impact, and maximize safety where it currently is in most danger: the crossing of the trail and the main road.

Meigs Ranney

inside Bethesda/Chevy Chase
November 14, 2018, 6:49 PM

I prefer Concept C

A pedestrian bridge over Little Falls Parkway is the only proposal that makes sense, it certainly works well at Mass Avenue and Old Georgetown Road. It is important for the county to provide the greatest safety for Crescent Trail users and a bridge will do that and keep traffic moving at a more reasonable pace on Little Falls Parkway. The lane closures have increased traffic on Hillandale Road and the residents of KFII, who only have on the street parking, are finding it more and more difficult to get out of their parking spaces safely. The number of cars waiting to go through the light at Bradley and Hillandale increase daily. In what way do Concepts A and B help the current problem? None

Bryant Cabo

inside Silver Spring
November 15, 2018, 12:44 PM

I prefer Concept C

Concept C is great for hosting a running race from

Capital Crescent Trail Crossing at Little Falls Parkway Review

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Bethesda to Georgetown or training a simulated race with only one stop at Dorset Ave. My position is going to be Concept C because I believe that runners and bikers should have freeways just like cars meaning they don't have to stop at a stoplight or stop sign and this is a first step.

Elizabeth H

inside Bethesda/Chevy Chase
November 16, 2018, 7:39 PM

I prefer Concept C

Thank you for the opportunity to comment. Given the need for a safe and fair solution for all users of the trail and parkway and local roads, I support the bridge as clearly the safest option. In addition, in the face of imminent development throughout Bethesda (Purple Line extension, Marriott headquarters, Westbard redevelopment, etc.) I urge planners both to restore Little Falls Parkway to the four lanes it has always had and to follow the recommendations in the community letter of November 14, 2018, from officials and residents of the neighborhoods surrounding the CCT at Little Falls.

Name not shown

inside Bethesda/Chevy Chase
November 18, 2018, 3:59 PM

I prefer Concept C

As a frequent user of the Capital Crescent Trail for both recreation (jogging, biking) and occasional commuting (to Bethesda Metro), I strongly favor Concept C as the safest and most user-friendly solution to the issue of the Little Falls Parkway crossing. The County should do its utmost to minimize the environmental impact while ensuring safety of trail users and motorists alike. Also, to minimize traffic congestion, the County should preserve the two-lanes of traffic each way. The Parkway has been a critical alleviator of traffic out of downtown Bethesda and narrowing this road will not be a welcome development for motorists. Thanks for giving this due consideration.

Name not shown

inside Bethesda/Chevy Chase
November 18, 2018, 9:19 PM

I prefer Concept A

Minimal change to a solution that is currently working.

Name not shown

inside Bethesda/Chevy Chase
November 19, 2018, 7:46 AM

I prefer Concept C

I support Concept C provided it preserves the nearby environment as suggested by some other commentators. However there is another concern that needs to be addressed - the safety of walkers. Bikers by and large ignore speed limits and do not pass walkers giving adequate attention to their safety. For example, if there are walkers passing each other as they walk in the opposite direction many bikers do not wait until one side of the path is clear. Rather they squeeze between the walkers and many do it without reducing speed. They basically do not give way to walkers. The speed of bikers also is a threat to walkers. The trail is not meant to be an expressway for bikers as one commentator suggested. One option would be to put speed bumps along the path to make bikers slow down (but this would probably not do much to reduce the problem). Another option - require bikes to have identifiable registration tags and install speed cameras to monitor their speed and fine those who exceed speed limits. If nothing is done to deal with this issue the next casualties are going to be children and others who do not have time to get out of the way of bikers.
A Glen Echo resident and daily walker on the trail.

Name not shown

inside Bethesda/Chevy Chase
November 19, 2018, 4:11 PM

I prefer Concept C

Since the lane closures the traffic is terrible! I have to keep the storm windows closed even in the summer due to noise and pollution. Also have had to sleep in the back bedroom

APPENDIX G OPEN TOWN HALL LOG

Capital Crescent Trail Crossing at Little Falls Parkway Review

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due to morning traffic.

Patricia Johnson

inside Bethesda/Chevy Chase
November 20, 2018, 7:39 AM

I prefer Concept B

I think an "alternative Plan B" is the practical solution and will work ONLY if Little Falls Parkway is restored to all four lanes open in BOTH directions. The current "road diet" is unsafe for all. The bridge is a nice concept but we all know that it is the most expensive solution and there is no money available. The environmental problems that building a bridge will cause will not be solved easily or in a timely manner. So the best interim solution is to move the trail to the light at Arlington Road and open Little Falls Parkway (and Arlington Road) to its original traffic plan (plan B with all lanes opened). That solution is safe and protects bikers, walkers and the surrounding neighborhoods from cut through traffic which is happening now. Please see our letter (sent to Mr. Tsai) dated November 14th which requests this plan with all lanes restored. The letter is from eight neighboring civic associations and the Chevy Chase Coalition of Friendship Heights (representing 18 neighborhoods). Restore all lanes on Little Falls Parkway. A 'road diet' is not a good solution which will be even more unacceptable when planned density is realized in Bethesda and the Westbard Sector within the next five years. Patricia Johnson, Kenwood Citizens Association/CCCFH

Mikel Frazee

inside Bethesda/Chevy Chase
November 20, 2018, 2:41 PM

I prefer Concept C

This seems to be the best solution for all; residents, drivers, runners, walkers, bicyclist, rollerblade folks, skateboarders, baby strollers.... Did I forget anyone?

Kristie Mcgehee

inside Bethesda/Chevy Chase
November 20, 2018, 7:17 PM

I prefer Concept C

I would like to see Little Falls Parkway restored to the four lanes it has always had. I encourage planners to follow the recommendations in the letter of November 14, 2018, from all the surrounding neighborhoods

Name not shown

inside Bethesda/Chevy Chase
November 23, 2018, 3:28 PM

I prefer Concept C

Name not shown

inside Bethesda/Chevy Chase
November 23, 2018, 3:29 PM

I prefer Concept C

Colin Warren

inside Bethesda/Chevy Chase
November 23, 2018, 8:21 PM

I prefer Concept A

The road diet has worked well, and drivers are now cautious and courteous. A provides the most benefit per dollar spent. While C may look attractive, it is too expensive. That money would be better spent making other Bethesda roads safer for bikes (I'm a biker and a driver).

Adele O'Dowd

inside Bethesda/Chevy Chase
November 24, 2018, 8:20 AM

I prefer Concept C

Name not shown

inside Bethesda/Chevy Chase
November 24, 2018, 12:57 PM

APPENDIX G OPEN TOWN HALL LOG

Capital Crescent Trail Crossing at Little Falls Parkway Review

Montgomery Parks staff is seeking public feedback on the three alternate concept plans to improve the Capital Crescent Trail crossing at Little Falls Parkway.

I do not like any options listed

November 26, 2018, 9:49 AM

By indicating a position of "none," I do not mean I have no view. I mean I intensely dislike the 3 options that are given. All 3 options described involve a road diet in which Little Falls Parkway narrows from 2 to 1 lane at Hillandale Road (where I live) which is HIGH DENSITY RESIDENTIAL. So where will backed-up traffic go? Where it's been going: Up Hillandale. There has already been an alarming increase in traffic, and the traffic has been moving too fast. All three options will perpetuate this arrangement.

I prefer an option with no road diet. However, if a road diet is genuinely necessary, then it should begin at River Road. That way, all of Little Falls Parkway will be 2 lanes (one in each direction), and there will be no bottleneck causing a spillover onto a high-density residential street such as Hillandale. By persisting with the current three options, Montgomery Parks is doing its best to ensure that the next injuries or fatalities occur on Hillandale instead of Little Falls. Thanks, Montgomery Parks. It's nice to know how little you care about safety outside of your jurisdiction.

Jeffrey Neale

inside Bethesda/Chevy Chase
November 24, 2018, 3:03 PM

I prefer Concept C

Mary Barbery

inside Bethesda/Chevy Chase
November 25, 2018, 10:09 AM

I prefer Concept C

Name not shown

inside Bethesda/Chevy Chase
November 25, 2018, 8:55 PM

I prefer Concept C

Name not shown

inside Bethesda/Chevy Chase

I prefer Concept C

Whatever decision is made, I very much hope that Little Falls will become a 4-lane road again. Safety for ALL is paramount but we must pay attention to traffic clogs and those most directly impacted by those neighbors. The feedback shared here with regards to how awful Little Falls as a two-lane road for neighbors is compelling.

David Barron

inside Bethesda/Chevy Chase
November 26, 2018, 11:34 AM

I prefer Concept B

MUST INCLUDE RESTORING LITTLE FALLS PARKWAY AT CAPITAL CRESCENT TRAIL TO 4 LANES

Name not shown

outside Planning Areas
November 26, 2018, 10:39 PM

I prefer Concept C

Why does it look like all options include the lane closures described for the absurd "Road Bulimia" option? Option C is clearly the best. If a bridge is built, LFP and Arlington Road should remain two lanes in each direction? Wouldn't that be the entire point of the bridge? If not, I don't think you have accurately described the options.

Ann Dougherty

inside Bethesda/Chevy Chase
November 27, 2018, 8:21 AM

I prefer Concept A

Ann Bolten

inside Bethesda/Chevy Chase

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Capital Crescent Trail Crossing at Little Falls Parkway Review

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November 28, 2018, 4:56 PM

I prefer Concept C

Concept C is the only truly safe option for both pedestrians and drivers. The current road diet is both inconvenient and not particularly safe for either pedestrians or drivers. I fully agree with the recommendations in the community letter of November 14, 2018.

John Stewart

inside Bethesda/Chevy Chase
November 28, 2018, 6:44 PM

I prefer Concept C

I strongly oppose continuing the so-called "road diet." There is simply no justification for it. The tragic death of a cyclist, who I understand was on a recumbent bike in pre-daylight hours and who failed to stop at the stop sign on the path, would not have been prevented by the road diet. The reality is that many cyclists ignore stop signs and safety measures, which is why I favor Option C (with the restoration of traffic lanes). Option B would not work because cyclists would ignore the rerouting and the light. The only effective solution to our endemic problems with bike and car traffic is to separate them. Cyclists would be able to ride through without risk under Option C, and cars would too.

Name not shown

inside Bethesda/Chevy Chase
November 28, 2018, 10:15 PM

I prefer Concept C

Please eliminate the road diet, which has already increased cut-through traffic in surrounding neighborhoods. Thank you.

Name not shown

inside Bethesda/Chevy Chase
November 28, 2018, 10:47 PM

I prefer Concept A

Dan Mendelson

inside Bethesda/Chevy Chase
November 28, 2018, 10:48 PM

I prefer Concept C

As a frequent user of the trail and little falls parkway, this seems like the only safe solution. This is truly an issue of life and limb for our community. thank you for taking it seriously.

Name not shown

inside Bethesda/Chevy Chase
November 28, 2018, 11:12 PM

I prefer Concept C

I strongly prefer option C, with ALL 4 lanes of traffic being restored! The current arrangement is unsightly, for an area that is supposed to be a parkway of greenery! It feels more like the Los Vegas strip, with all of the ballard's, roaring, signs, and reflective tape. When the weather is nice, having only one lane for cars causes traffic to pile-up, with cars having long waits for the steady stream of pedestrian & bike traffic to have a break from people crossing Little Falls Parkway. This causes drivers to lose patience and... not slow for the cross walk, drive down the restricted area, or to take short cuts & speed through the nearby residential areas. Even with stop signs, bikers & pedestrians still do not stop before crossing the road. So all other options, other than option C's bridge, would still put users of the trail at risk from cars.

Name not shown

inside Bethesda/Chevy Chase
November 29, 2018, 9:25 AM

I prefer Concept C

restore Little Falls to 4 lanes pls

APPENDIX G OPEN TOWN HALL LOG

Capital Crescent Trail Crossing at Little Falls Parkway Review

Montgomery Parks staff is seeking public feedback on the three alternate concept plans to improve the Capital Crescent Trail crossing at Little Falls Parkway.

Name not shown

inside Bethesda/Chevy Chase
November 29, 2018, 10:01 AM

I prefer Concept C

Guillermo Israilevich

inside Bethesda/Chevy Chase
November 29, 2018, 10:08 AM

I prefer Concept C

Please also eliminate the road diet

Name not shown

inside Bethesda/Chevy Chase
November 29, 2018, 10:44 AM

I prefer Concept C

The lighting is always a problem at night and visibility is poor. I like the idea of the bridge over the Parkway. Little Falls Parkway should go back to 4 lanes also.

Lucretia Marmon

inside Bethesda/Chevy Chase
November 29, 2018, 10:53 AM

I prefer Concept C

I very much favor option C. We live in the last house on Cumberland, just off Little Falls. It is obvious that the two lane option now available on Little Falls causes a long back-up line during rush hours...morning and night. A pedestrian and bicycle bridge over Little Falls is the perfect answer.

Pamela Kenny

inside Bethesda/Chevy Chase
November 29, 2018, 12:01 PM

I prefer Concept C

We are Somerset residents and worry about the increase in traffic through Greysone/Surrey and Dorset where our elementary school children are crossing the road to get to school. We have already seen an increase in traffic and angry drivers with the Little Falls diet. Please make our roads safe and eliminate the diet and add a bridge at Little Falls near Arlington so we have no more deaths there.

Steven Heydemann

inside Bethesda/Chevy Chase
November 29, 2018, 12:38 PM

I prefer Concept C

The pedestrian overpass seems the most definitive way to keep people, bikes, and pets away from cars and trucks. If that is the goal, and it's one I support, I would prefer option C to the others. My family uses the trail and crosses that intersection often. The bridge would be a big improvement over current conditions.

Stephen Surko

inside Bethesda/Chevy Chase
November 29, 2018, 12:47 PM

I prefer Concept A

There should be NO mid-block trail crossing on Hillandale no matter what option they end up choosing. Option B has a new trail crossing on Hillandale. This mid-block crossing was unanimously turned down by the planning board in 2016 as unsafe.. I don't know why it's on the table again, but to propose it, is just to substitute one deadly mid-block trail crossing with another one. We don't need the Little Falls parkway problem solved by creating another dangerous mid-block crossing. Hillandale is a busy street and the crossing they propose is on the curve - another potential for a fatal accident.

Name not shown

inside Bethesda/Chevy Chase
November 29, 2018, 3:53 PM

I prefer Concept C

APPENDIX G OPEN TOWN HALL LOG

Capital Crescent Trail Crossing at Little Falls Parkway Review

Montgomery Parks staff is seeking public feedback on the three alternate concept plans to improve the Capital Crescent Trail crossing at Little Falls Parkway.

I urge planners to restore Little Falls Parkway to the four lanes it has always had.

thanks,

-Scott

Patricia Friedman

inside Bethesda/Chevy Chase
November 29, 2018, 4:50 PM

I prefer Concept C

Name not shown

inside Bethesda/Chevy Chase
November 29, 2018, 8:19 PM

I prefer Concept C

I think a bridge is preferable from the safety perspective. At the same time, there should be speed limits for road bikers and for e-bikers!

Name not shown

inside Bethesda/Chevy Chase
November 29, 2018, 8:24 PM

I prefer Concept C

Concept C is safe and good for everyone, users of the trail as well as users of Little Falls Parkway.

Name not shown

inside Bethesda/Chevy Chase
November 29, 2018, 8:57 PM

I prefer Concept C

Please restore Little Falls Parkway to four lanes rather than the current "diet" of only two lanes.

Maura Vanderzon

inside Bethesda/Chevy Chase
November 29, 2018, 9:47 PM

I prefer Concept C

There are just too many people (including many school-aged children) on the CCT on a daily basis to take a chance on the road crossing here. Many drivers just don't "get it" and don't stop appropriately for peds and cyclists. Though more expensive, I've always believed that a bridge overpass is the logical solution here. This crossing is only going to become busier, so just do it now. Reopen Little Falls in two lanes to prevent the gridlock there and because no "road diet" will be necessary once there's an overpass. Thanks for being transparent about the process and for requesting feedback from locals!

marc Geffroy

inside Bethesda/Chevy Chase
November 29, 2018, 10:44 PM

I prefer Concept C

concept a & b are impractical long term but the only tenable option, C, is expensive. is there a way perhaps to offset the cost a bit w user fees (eg, bike license fees, charge for parking at the trail parking lot on little falls) and or state/ federal grants or even private grants? could the county proffer the C plan cost from Regency for the Westbard redevelopment site plan approval? Arguably, westbard benefits from the trail as an amenity.

Name not shown

inside Bethesda/Chevy Chase
November 30, 2018, 6:58 AM

I prefer Concept C

The road diet should be eliminated as it causes cut through traffic through the quiet, adjacent neighborhoods that didn't used to exist. A raised bridge would solve danger to pedestrians and cyclists. I also believe that speed cameras should be placed on little falls parkway between River Rd and Dorset to slow down traffic and stop "drag racers".

APPENDIX G OPEN TOWN HALL LOG

Capital Crescent Trail Crossing at Little Falls Parkway Review

Montgomery Parks staff is seeking public feedback on the three alternate concept plans to improve the Capital Crescent Trail crossing at Little Falls Parkway.

Name not shown

inside Bethesda/Chevy Chase
November 30, 2018, 10:04 AM

I prefer Concept C

Name not shown

inside Bethesda/Chevy Chase
November 30, 2018, 10:47 AM

I prefer Concept C

Zola Dincin Schneider

inside Bethesda/Chevy Chase
November 30, 2018, 12:18 PM

I prefer Concept C

For the safety of all who are using Little Falls Pkwy, I strongly urge adopting Concept C, with LFP going back to a 4-lane roadway.

Zola Dincin Schneider
Warwick Place, Town of Somerset

Lucile Freeman

inside Bethesda/Chevy Chase
November 30, 2018, 10:38 PM

I prefer Concept C

Aviva Rosenthal

inside Bethesda/Chevy Chase
December 2, 2018, 11:23 AM

I prefer Concept C

I believe strongly that we need to go back to proper 4 lanes for cars. I very much want safety for all (walkers, bikers) but also for drivers. The current two lane diet is quite dangerous for drivers and unnecessary to solving the problem at hand.

Name not shown

inside Bethesda/Chevy Chase
December 2, 2018, 11:45 AM

I prefer Concept C

harold pfohl

inside Bethesda/Chevy Chase
December 2, 2018, 8:34 PM

I prefer Concept B

Prefer concept C as soon as it is affordable. B for the interim. Remove road diet and restore Little Falls Parkway at Capitol Crescent Trail to 4 lanes.

Name not shown

inside Bethesda/Chevy Chase
December 2, 2018, 10:28 PM

I prefer Concept B

I actually prefer Concept C, but being a realist, until money is available for C, I can live with B. But, even with my preference for B for now, I would like to see the road diet removed and have Little Falls Parkway at the CCT restored to four lanes.

Heather Gerth

inside Bethesda/Chevy Chase
December 3, 2018, 7:30 AM

I prefer Concept C

A permanent long term solution is best. While a pedestrian bridge is more expensive, it is by far the safest option. The trail is used by all ages and a pedestrian bridge will help to ensure that everyone is safe.

Name not shown

inside Bethesda/Chevy Chase
December 3, 2018, 8:21 AM

APPENDIX G OPEN TOWN HALL LOG

Capital Crescent Trail Crossing at Little Falls Parkway Review

Montgomery Parks staff is seeking public feedback on the three alternate concept plans to improve the Capital Crescent Trail crossing at Little Falls Parkway.

I prefer Concept B

I prefer concept C as soon as it is affordable. I prefer B for the interim. Remove road diet and restore Little Falls Parkway at Capitol Crescent Trail to 4 lanes."/

Barry Miller

inside Bethesda/Chevy Chase
December 3, 2018, 9:52 AM

I prefer Concept B

As a frequent user of both the Capital Crescent Trail and Little Falls Parkway, I prefer concept C as soon as it is affordable. I prefer B for the interim. Remove road diet and restore Little Falls Parkway at Capitol Crescent Trail to 4 lanes.

Name not shown

inside Bethesda/Chevy Chase
December 3, 2018, 10:43 AM

I prefer Concept B

"I prefer concept C as soon as it is affordable. I prefer B for the interim. Remove road diet and restore Little Falls Parkway at Capitol Crescent Trail to 4 lanes."

Bill McCloskey

inside Bethesda/Chevy Chase
December 3, 2018, 3:40 PM

I prefer Concept C

Joan Barron

inside Bethesda/Chevy Chase
December 3, 2018, 7:36 PM

I prefer Concept C

I am going with C but am under no illusion that the County will do this in the foreseeable future. I believe the road should go back to 4 lanes and a form of concept B should

be in place. The "road diet" just makes cars look for alternative routes and my neighbors on Hillandale will suffer the most in terms of traffic and safety.

Celia Martin

inside Bethesda/Chevy Chase
December 3, 2018, 8:14 PM

I prefer Concept B

I prefer concept C as soon as it is affordable. I prefer B for the interim. Remove the road diet and restore Little Falls Parkway at Capitol Crescent Trail to 4 lanes

Name not shown

inside Bethesda/Chevy Chase
December 3, 2018, 8:38 PM

I prefer Concept C

It provides the safest and fastest route for trail users and road users alike. It is easily the most dangerous intersection on the CCT between Georgetown and Bethesda, and it therefore makes the trail as a whole a more consistently safe route.

R Porter

inside Bethesda/Chevy Chase
December 3, 2018, 8:54 PM

I do not like any options listed

I believe the simplest solution is probably the best solution. First, no matter what, we should install bright LED, or similar, lighting such as those at the intersection of Little Falls and Dorsett. Second, install rumble strips before the trail crossing along with a raised speed hump at the trail crossing that would force slowing traffic. Finally, there should be some mechanism to force/encourage those on the trail to stop before crossing.

Another simple option would be to install a speed camera and set the speed at 15 MPH. That would actually generate revenue. This could be done in addition to the above.

Capital Crescent Trail Crossing at Little Falls Parkway Review

Montgomery Parks staff is seeking public feedback on the three alternate concept plans to improve the Capital Crescent Trail crossing at Little Falls Parkway.

At a minimum, we should install lights at the crossing, and it's a bit surprising that with all the fuss about this, we haven't even done that.

All of the options presented come at really significant cost and will take a lot of time. These above are cheaper and could be done much quicker to solve the problem and still keep the traffic moving at four lanes.

If these simple solutions are not really considered then I would vote option C since that is what will actually solve the problem for the long term and be the safest, despite the cost and time to make it work.

Laura kolton

inside Bethesda/Chevy Chase
December 3, 2018, 9:05 PM

I prefer Concept B

I prefer concept C as soon as it is affordable. I prefer B for the interim. Remove the road diet and restore Little Falls Parkway at Capitol Crescent Trail to 4 lanes.

Brian Israel

inside Bethesda/Chevy Chase
December 3, 2018, 9:23 PM

I prefer Concept C

I regularly use the Crescent Trail to access downtown Bethesda by bike and believe the convenience and safety of a bridge, relative to the status quo or diverting trail traffic to a traffic light, would be well worth the cost in view of the volume of trail users. Diversion to a traffic light would make me less likely to prefer biking to driving downtown.

Lesley Ann Sand

inside Bethesda/Chevy Chase
December 3, 2018, 10:45 PM

I prefer Concept B

"I prefer concept C as soon as it is affordable. I prefer B for the interim. Remove the road diet and restore Little Falls Parkway at Capitol Crescent Trail to 4 lanes."

Joseph J. Geraci

inside Bethesda/Chevy Chase
December 3, 2018, 11:28 PM

I prefer Concept B

Trail should transition to the direction of the light. Plant trees and mountain laurel to steer trail to the light. Have at it. Thanks.

Name not shown

inside Bethesda/Chevy Chase
December 3, 2018, 11:28 PM

I prefer Concept B

We would prefer the pedestrian bridge as soon as it is affordable as it is the only really safe alternative for bikers and joggers to cross the road. It works extremely well on River Road. In the meantime, we prefer Concept B, remove the road diet and restore Little Falls Parkway at Capital Crescent Trail to 4 lanes. Currently it is a dangerous traffic situation. Bikers and joggers NEVER stop and most never even look before they cross.

Name not shown

inside Bethesda/Chevy Chase
December 4, 2018, 6:06 AM

I prefer Concept B

I prefer concept C as soon as it is affordable. I prefer B for the interim. Remove the road diet and restore Little Falls Parkway at Capitol Crescent Trail to 4 lanes.

David Stern

inside Bethesda/Chevy Chase
December 4, 2018, 7:43 AM

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Capital Crescent Trail Crossing at Little Falls Parkway Review

Montgomery Parks staff is seeking public feedback on the three alternate concept plans to improve the Capital Crescent Trail crossing at Little Falls Parkway.

I prefer Concept C

I also favor re-opening the second lane on Little Falls if the overpass is built.

Edward Brownfield

inside Bethesda/Chevy Chase

December 4, 2018, 9:02 AM

I prefer Concept C

Concept C is the best option. Priority needs to be given to bike and pedestrian traffic.

The stance of my neighborhood association is that we should respond with Option B while we wait for Option C to be affordable. I do not think this is a good solution.

Postponing Option C until it is "affordable" is no resolution. "Affordable" is a subjective word and the bridge will never be built if that is considered the criteria for when it is done. Meanwhile, proceeding with Option B in the interim is a waste of 1.5 million dollars if the ultimate goal is a bridge. Why not use that 1.5 million towards building the bridge now?

Name not shown

inside Bethesda/Chevy Chase

December 4, 2018, 9:40 AM

I prefer Concept B

I prefer concept C as soon as it is affordable. I prefer B for the interim. Remove the road diet and restore Little Falls Parkway at Capitol Crescent Trail to 4 lanes.

Name not shown

inside Bethesda/Chevy Chase

December 4, 2018, 10:34 AM

I prefer Concept B

Immediately remove the road diet and restore Little Falls Parkway at Capitol Crescent Trail to 4 lanes. I prefer

concept C as soon as it is affordable. I prefer B for the interim.

Name not shown

inside Bethesda/Chevy Chase

December 4, 2018, 10:37 AM

I prefer Concept B

I prefer concept C as soon as it is affordable. I prefer B for the interim. Remove the road diet and restore Little Falls Parkway at Capitol Crescent Trail to 4 lanes.

Brian Burns

inside Bethesda/Chevy Chase

December 4, 2018, 11:09 AM

I prefer Concept B

I prefer concept C as soon as it is affordable. I prefer B for the interim. Remove the road diet and restore Little Falls Parkway at Capitol Crescent Trail to 4 lanes.

Name not shown

inside Bethesda/Chevy Chase

December 4, 2018, 11:14 AM

I prefer Concept B

I prefer concept C as soon as it is affordable. I prefer B for the interim. Remove the road diet and restore Little Falls Parkway at Capitol Crescent Trail to 4 lanes.

William Howe

inside Bethesda/Chevy Chase

December 4, 2018, 11:39 AM

I prefer Concept C

I prefer Concept C (find the funding) and reopening all lanes on Little Falls Parkway.

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Capital Crescent Trail Crossing at Little Falls Parkway Review

Montgomery Parks staff is seeking public feedback on the three alternate concept plans to improve the Capital Crescent Trail crossing at Little Falls Parkway.

Francis McCormick

inside Bethesda/Chevy Chase
December 4, 2018, 12:52 PM

I prefer Concept C

Name not shown

inside Bethesda/Chevy Chase
December 4, 2018, 2:45 PM

I prefer Concept B

I would like Concept B as interim solution with Concept C (overpass) as long term solution. Please restore Little Falls Parkway to 4 lanes where CC Trail crosses. Make funds available for overpass!

Name not shown

inside Bethesda/Chevy Chase
December 4, 2018, 3:07 PM

I prefer Concept B

"I prefer concept C as soon as it is affordable. I prefer B for the interim. Remove the road diet and restore Little Falls Parkway at Capitol Crescent Trail to 4 lanes."

Name not shown

inside Bethesda/Chevy Chase
December 4, 2018, 3:10 PM

I prefer Concept B

I strongly urge that Little Falls Parkway at the Capitol Crescent be restored to four car-driving lanes.

David Forman

inside Bethesda/Chevy Chase
December 4, 2018, 3:41 PM

I prefer Concept B

I prefer concept C as soon as it is affordable. I prefer B for the interim. Remove road diet and restore Little Falls Parkway at Capitol Crescent Trail to 4 lanes.

Annette Bowen

inside Bethesda/Chevy Chase
December 4, 2018, 5:21 PM

I prefer Concept B

-- I actually don't like any of these plans. If I must choose between them, then I would prefer to go with Concept C once the funding is available. In the meantime, B would be the best option.

- I strongly believe that the "road diet" be removed and that 4 lanes be restored. The narrowing of traffic has created unnecessary backups during heavier traffic time. The growth and development of Bethesda will only lead to more traffic and more backups.

-There are a number of options that can alert drivers to the need to yield to bikers & walkers. One alternative is the light that AU has recently installed on Nebraska.

Name not shown

inside Bethesda/Chevy Chase
December 5, 2018, 9:01 AM

I prefer Concept C

During the time it takes to plan, fund and construct a pedestrian overpass, implement Concept B now so that it is in place prior to and during construction of the overpass.

robert bein

inside Bethesda/Chevy Chase
December 5, 2018, 10:00 AM

I prefer Concept B

Remove the road diet and restore 4 lanes to Little Falls Parkway. This is necessary because all the new construction in Bethesda and the future Westbard development will significantly increase--and slow down--

APPENDIX G OPEN TOWN HALL LOG

Capital Crescent Trail Crossing at Little Falls Parkway Review

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traffic flow on Little Falls Parkway, causing more cut through traffic in nearby neighborhoods. While Concept C is the better long term solution, it's expensive and difficult to implement so Concept B should be put in place now.

M Dagenais

inside Bethesda/Chevy Chase
December 5, 2018, 10:32 AM

I prefer Concept B

Restore the full four lanes on Little Falls Parkway. This is safest and most efficient for both commuters and residents

Name not shown

inside Bethesda/Chevy Chase
December 5, 2018, 10:36 AM

I prefer Concept B

Michael Shuler

inside Bethesda/Chevy Chase
December 5, 2018, 10:39 AM

I prefer Concept B

Mike Shuler 20815
Remove the road diet and restore 4 lanes to Little Falls Parkway. This will make the roads and trails safer for pedestrians, motorists, and cyclists.

Kathryn Rizik

inside Bethesda/Chevy Chase
December 5, 2018, 10:39 AM

I prefer Concept B

Remove the road diet. Little Falls Parkway must be restored to 4 lanes.

Michael Hotchkiss

inside Bethesda/Chevy Chase
December 5, 2018, 10:42 AM

I prefer Concept B

Remove the road diet and restore 4 lanes to Little Falls Parkway

Significant change is occurring in this part of Bethesda/Chevy Chase. We must look forward for a solution that will work for the future. Restricting Little Falls Parkway to a single lane each direction will back up traffic for many blocks through many intersections. All four lanes must be part of any solution along with safety measures to manage the Capital Crescent Trail pedestrian and cycle traffic.

Jacques Smith

inside Bethesda/Chevy Chase
December 5, 2018, 10:44 AM

I prefer Concept B

You must immediately remove the road diet and restore 4 lanes to Little Falls Parkway. This is both necessary and justified because all the new construction in Bethesda and the future Westbard development will significantly increase--and slow down--traffic flow on Little Falls Parkway, causing more cut through traffic in nearby neighborhoods.

Vickie Allin

inside Bethesda/Chevy Chase
December 5, 2018, 10:44 AM

I prefer Concept B

I prefer concept C as soon as it is affordable. I prefer concept B for the interim. Remove the road diet and restore Little Falls Parkway at Capitol Crescent Trail to 4 lanes.

Name not shown

inside Bethesda/Chevy Chase

APPENDIX G OPEN TOWN HALL LOG

Capital Crescent Trail Crossing at Little Falls Parkway Review

Montgomery Parks staff is seeking public feedback on the three alternate concept plans to improve the Capital Crescent Trail crossing at Little Falls Parkway.

December 5, 2018, 11:01 AM

I prefer Concept B

Road needs to be made 4 lanes again. It's congested and there's a lot more traffic. Moving trail users to the crosswalk will make it safe for more cars.

Name not shown

inside Bethesda/Chevy Chase

December 5, 2018, 11:03 AM

I prefer Concept B

Mike McNamara

inside Bethesda/Chevy Chase

December 5, 2018, 11:43 AM

I prefer Concept B

Remove the road diet and restore 4 lanes to Little Falls Parkway. I am a 20815 Resident.

Virginia Voorhees

inside Bethesda/Chevy Chase

December 5, 2018, 11:48 AM

I prefer Concept B

Please remove the "road diet" and restore the 4 lanes on Little Falls. It is very unsafe to have so much cut through in our neighborhood because of the increased development--no sidewalks so scary for us and our children.

Debby Demaree

inside Bethesda/Chevy Chase

December 5, 2018, 12:02 PM

I prefer Concept B

Roads are for cars! Reopen LF parkway, put in a safe signal.

Brenda Murray

inside Bethesda/Chevy Chase

December 5, 2018, 12:21 PM

I prefer Concept B

Brenda P. Murray 20815

Please remove the road "diet" and restore 4 lanes to Little Falls Parkway. Thank you.

Mathews Pierson

inside Bethesda/Chevy Chase

December 5, 2018, 12:47 PM

I prefer Concept B

Remove the road diet and restore 4 lanes to Little Falls Parkway

Gino Picasso

inside Bethesda/Chevy Chase

December 5, 2018, 12:59 PM

I prefer Concept B

My name is Gino Picasso, and reside at 5204 Oakland Road. I am an avid cyclist and use the trail frequently so I strongly support the safety measures being taken. As a resident in the area, I also find myself on the Little Falls Parkway very often, and have to contend with the inconvenience of the blocked off lanes. I would strongly request that you remove the road diet and restore 4 lanes to Little Falls Parkway. This is absolutely necessary because all the new construction in Bethesda and the future Westbard development will significantly increase--and slow down--traffic flow on the Parkway, causing even more cut through traffic in nearby neighborhoods.

John Oliver

inside Bethesda/Chevy Chase

December 5, 2018, 1:10 PM

I prefer Concept B

Capital Crescent Trail Crossing at Little Falls Parkway Review

Montgomery Parks staff is seeking public feedback on the three alternate concept plans to improve the Capital Crescent Trail crossing at Little Falls Parkway.

Remove the road diet and restore 4 lanes to Little Falls Parkway

Name not shown

inside Bethesda/Chevy Chase
December 5, 2018, 1:57 PM

I prefer Concept B

I support concept B, if the Little Falls Parkway road diet is removed and all 4 lanes are restored to Little Falls Parkway. This is necessary because all the new construction in Bethesda and the future Westbard development will significantly increase--and slow down--traffic flow on Little Falls Parkway, causing more cut through traffic in nearby neighborhoods.

In addition, the significant problem in this recreational trail are those bikers who do not obey traffic laws, exceed the speed limits on the trail, and cause a threat to walkers and runners on the recreational trail. While that is not all the bikers, a substantial number of them cause the problems. Having 4 lanes on Little Falls Parkway and the traffic light will allow traffic to flow and ensure that those bikers that do not obey the laws slow down and reduce the risk of injury to walkers and joggers on the trail.

Kay Stevens

inside Bethesda/Chevy Chase
December 5, 2018, 2:23 PM

I prefer Concept B

I live in the Kenwood neighborhood and am a retired Planner from the Montgomery County Department of Transportation. I prefer option B (moving the CCT crossing to the intersection of Arlington Rd and Little Falls Parkway) but with removal of the "road diet" which will no longer be necessary when CCT users are crossing at a traffic signal. In fact, I find the "road diet" a hazard to drivers at night because it comes upon you so unexpected. I do not see that maintaining the "road diet" gains anything in terms of safety once the trail is relocated, and the increase in development in Bethesda will surely put more cars on Little Falls Parkway, making the road narrowing

even more of a driving hazard.

Name not shown

inside Bethesda/Chevy Chase
December 5, 2018, 2:50 PM

I prefer Concept B

It would be much safer to cross at the Arlington Road light. Also, 2 lanes will be insufficient with all the traffic that is going to increase.

Thomas Woodward

inside Bethesda/Chevy Chase
December 5, 2018, 6:44 PM

I prefer Concept B

This is necessary because all the new construction in Bethesda and the future Westbard development will significantly increase--and slow down--traffic flow on Little Falls Parkway, causing more cut through traffic in nearby neighborhoods.

Harriet Shugerman

inside Bethesda/Chevy Chase
December 5, 2018, 8:24 PM

I prefer Concept B

Prefer Concept C as soon as money is budgeted. Strongly prefer restoration of Little Falls Pkwy to 4 lanes

arlene bein

inside Bethesda/Chevy Chase
December 5, 2018, 9:14 PM

I prefer Concept B

While Concept C sounds like the ideal solution, it will take time and a lot of money to implement, and there may be environmental issues to resolve as well because the bridge will probably cover as much as 100 feet from end to end.

Capital Crescent Trail Crossing at Little Falls Parkway Review

Montgomery Parks staff is seeking public feedback on the three alternate concept plans to improve the Capital Crescent Trail crossing at Little Falls Parkway.

So in the meantime, I believe Concept B is the way to go. The road diet should also be removed and 4 lanes restored to Little Falls Parkway. This is necessary because all the new construction in Bethesda and the future Westbard development will significantly increase--and slow down--traffic flow on Little Falls Parkway, causing more cut through traffic in nearby neighborhoods and creating safety issues for the people, especially children, walking and playing in those neighborhoods.

Name not shown

inside Bethesda/Chevy Chase
December 6, 2018, 8:11 AM

I prefer Concept B

Greetings to the Montgomery County Park Staff. Thank you for maintaining such beautiful spaces for us to enjoy. Regarding the options for the Capital Crescent Trail at Little Falls Pkwy I favor Option "B" with additional comments. Please remove the road diet and restore 4 lanes to Little Falls Parkway. The current situation is cumbersome. This is necessary because all the new construction in Bethesda and the future Westbard development will significantly increase--and slow down--traffic flow on Little Falls Parkway, causing more cut through traffic in nearby neighborhoods.

keith lindgren

inside Bethesda/Chevy Chase
December 6, 2018, 10:01 AM

I prefer Concept B

Donna Eacho

inside Bethesda/Chevy Chase
December 6, 2018, 2:50 PM

I prefer Concept B

Remove the road diet and restore 4 lanes to Little Falls Parkway!

Name not shown

inside Bethesda/Chevy Chase
December 7, 2018, 2:28 PM

I prefer Concept B

I am a resident of Sumner in Bethesda, and am greatly affected by the current road diet. It is MOST unfortunate that a recumbent biker was killed at this intersection, but the current situation is most dangerous to many more. (Cars are confused and I have seen many near collision misses.) I request that all four lanes of Little Falls be restored and the road diet be removed.

Malcolm O'Hagan

inside Bethesda/Chevy Chase
December 7, 2018, 3:12 PM

I do not like any options listed

Restore to original with control bumps on the trail and full stop on Little Falls Parkway. This is the simplest and most cost effective solution which should be tried before any other option.

John Gill

inside Bethesda/Chevy Chase
December 7, 2018, 5:17 PM

I do not like any options listed

None of the proposed options is a good one. I propose a button be placed for the pedestrians or cyclists to push when they are at the current intersection, that, when pressed, would activate a newly installed red light facing motorists, who must then stop. The pedestrians or bicyclists would have 20 seconds to cross the street. The red light could not be reactivated until one full minute had elapsed since the end of the last 20 second pedestrian/bicycle crossing time period. If a pedestrian or cyclist pushed the button before the one minute period had elapsed, he or she would not be able to cross until the full minute had elapsed since the end of the last 20 second pedestrian/cyclist crossing period. The road would become a four lane road again. There would be orderly

APPENDIX G OPEN TOWN HALL LOG

Capital Crescent Trail Crossing at Little Falls Parkway Review

Montgomery Parks staff is seeking public feedback on the three alternate concept plans to improve the Capital Crescent Trail crossing at Little Falls Parkway.

traffic passing through the intersection. And pedestrians and cyclists would not have to wait longer than one minute to cross once they reached the intersection and pushed the button. In almost all scenarios, it would be less than one minute. Drivers would have to stop at a red light to allow pedestrians and cyclists to cross for 20 seconds.

Name not shown

inside Bethesda/Chevy Chase
December 7, 2018, 9:53 PM

I do not like any options listed

I would prefer the following: Restore Little Falls to 4 Lanes. It is dangerous and slow at rush hour and conditions will only deteriorate with the new development. Install a pedestrian/cyclist button to activate a red light to stop traffic for 20 seconds and then with a minute wait before subsequent light activation.

Name not shown

inside Bethesda/Chevy Chase
December 9, 2018, 10:02 AM

I prefer Concept A

Melissa Glover

inside Bethesda/Chevy Chase
December 9, 2018, 5:44 PM

I do not like any options listed

what a difficult place. those white guard things are a distraction more than a safety feature. They hinder views of pedestrians or bikes. I think a flashing red light with NO visual distractions except a huge sign say stop ahead for pedestrians!!

Philip Wilcox

inside Bethesda/Chevy Chase
December 10, 2018, 1:41 PM

I prefer Concept B

The "road diet" will no longer be needed for safety if, as Option B is adopted, proposes, both cars and bikers will be required to stop at red lights. One lane, as current experience shows, creates a bottleneck slowing traffic on this thoroughfare. Also, when Trail users reach the Parkway, the modified paths should have sharp - almost right angle - turns so the routes parallel the Parkway to the crossing at Arlington Blvd., to slow traffic and avoid much tree cutting. PWilcox, Bethesda/Sumner

Name not shown

inside Bethesda/Chevy Chase
December 10, 2018, 5:03 PM

I prefer Concept B

MB FitzGerald 20815 REMOVE the Road Diet and restore 4 lanes to Little Falls Parkway. This is necessary because of all the new construction in Bethesda and the future Westbard development

Name not shown

inside Bethesda/Chevy Chase
December 10, 2018, 8:39 PM

I prefer Concept A

It is working and achieves the right balance. I am a local resident who commutes on the CCT.

Gabrielle Sabharwal

inside Bethesda/Chevy Chase
December 11, 2018, 12:20 PM

I prefer Concept B

Gabrielle Sabharwal, 20815. "Please remove the road diet and restore 4 lanes to Little Falls Parkway. This is necessary because all the new construction in Bethesda

Capital Crescent Trail Crossing at Little Falls Parkway Review

Montgomery Parks staff is seeking public feedback on the three alternate concept plans to improve the Capital Crescent Trail crossing at Little Falls Parkway.

and the future Westbard development will significantly increase, and slow down, traffic flow on Little Fall Parkway, causing more cut through traffic in nearby neighborhoods."

David Johnson

inside Bethesda/Chevy Chase
December 12, 2018, 6:50 PM

I prefer Concept B

This is an alternative Plan B. The only way to equitably serve all constituents is to eliminate the road diet and open all four lanes on Arlington Blvd and Little Falls Parkway. This is a safe solution for walkers, bikers and the surrounding neighborhoods. This is necessary because the narrowing of the roads cause unsafe driving conditions especially at night. Little Falls Parkway is an established commuter road and in light of the oncoming considerable development in Bethesda and the Westbard Sector, this road will be impacted even more greatly than it already is. Open the Parkway and Arlington Road, cross everyone safely at the Arlington Light at Little Falls. Thank you, David Johnson, Kenwood, 20815

T. Maryann Hekimian

inside Bethesda/Chevy Chase
December 15, 2018, 9:08 AM

I prefer Concept C

I prefer concept C as soon as it is affordable. I prefer B for the interim. Remove road diet and restore Little Falls Parkway at Capitol Crescent Trail to 4 lanes.

bruce levin

inside Bethesda/Chevy Chase
December 15, 2018, 1:22 PM

I prefer Concept A

I live in Sumner and work in Bethesda. When I drive this is my commuting route. I have found virtually no material delays due to the current arrangement which is a good

traffic calming measure for what was a bit of a speedway. I also use this segment to walk to downtown and as a cyclist for commuting and recreation and appreciate the improved safety. The modest cost of Concept A along with increased green space are pluses.

Lindy Hart

inside Bethesda/Chevy Chase
December 17, 2018, 12:36 AM

I prefer Concept B

Lindy Hart 20815
Remove the road diet and restore the needed four lanes to Little Falls Parkway.

Name not shown

inside Bethesda/Chevy Chase
December 17, 2018, 2:57 PM

I prefer Concept C

Name not shown

inside Bethesda/Chevy Chase
December 17, 2018, 3:06 PM

I prefer Concept C

Amanda Hewitt

inside Bethesda/Chevy Chase
December 17, 2018, 3:15 PM

I prefer Concept C

Name not shown

inside Bethesda/Chevy Chase
December 17, 2018, 5:05 PM

I prefer Concept C

Any ground-level crossing is unacceptable in my view,

APPENDIX G OPEN TOWN HALL LOG

Capital Crescent Trail Crossing at Little Falls Parkway Review

Montgomery Parks staff is seeking public feedback on the three alternate concept plans to improve the Capital Crescent Trail crossing at Little Falls Parkway.

given the increasing traffic load on Arlington, Hillandale, Fairfax Roads and Little Falls Parkway. A crossing at Arlington WITH A TRAFFIC LIGHT to permanently halt traffic while pedestrians cross might be a less expensive alternative but still presents some risks, given that bikes don't always wait for green lights.

Name not shown

inside Bethesda/Chevy Chase
December 17, 2018, 9:03 PM

I prefer Concept B

Good Evening,
The trail should be diverted to the Arlington Road traffic signal. This would decrease travel times. We would still have to wait at the signal just like it is now. However, traffic would no longer be burden with the dangers of the present crossing which now slows traffic. Without this danger, motorists would travel without the impediment of the current crossing location. Therefore, the Concept B would not only increase safety (by requiring crossing via traffic signal), it would decrease travel time. Thank you.

Rodney Scott

inside Bethesda/Chevy Chase
December 17, 2018, 11:24 PM

I prefer Concept C

Name not shown

inside Bethesda/Chevy Chase
December 18, 2018, 12:23 PM

I prefer Concept C

Little Falls Parkway provides an easy way for traffic to by pass much of Bethesda by taking cars off of Bradley Blvd. As Bethesda continues to grow, keeping the traffic running smoothly should be a goal. A bridge will keep cyclists safe and traffic flowing.

Dennis DuFour

inside Bethesda/Chevy Chase
December 19, 2018, 11:30 AM

I prefer Concept B

Dear Sir,

We need to restore Little Falls Parkway to 4 lanes. I appreciate your consideration in advance.

Dennis DuFour
Chevy Chase, MD 20815
Kenwood Subsection

Kristin Roeser

inside Bethesda/Chevy Chase
December 19, 2018, 12:21 PM

I do not like any options listed

Option B could work, if modified as follows:

Restore Little Falls to 4 Lanes. The "Road Diet" is ineffective and dangerous -- conditions will worsen with the new development. Install a pedestrian/cyclist button to activate a red light to stop traffic for 20 seconds and then with a minute wait before subsequent light activation.

Name not shown

inside Bethesda/Chevy Chase
December 19, 2018, 12:54 PM

I prefer Concept C

A trail bridge, as in the case of other CCT crossings, is the only safe option.

Amy Egan

inside Bethesda/Chevy Chase
December 19, 2018, 1:05 PM

I prefer Concept B

APPENDIX G OPEN TOWN HALL LOG

Capital Crescent Trail Crossing at Little Falls Parkway Review

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William Becker

inside Bethesda/Chevy Chase
December 19, 2018, 3:54 PM

I prefer Concept B

Remove the road diet and restore 4 lanes to Little Falls Parkway

Carroll Dunn

inside Bethesda/Chevy Chase
December 20, 2018, 7:22 AM

I prefer Concept B

I agree with Concept B but strongly urge the staff to restore Little Falls Pkwy to 4 lanes. The development in the area and proposed development at Westbard will increase traffic on the Pkwy. If left only to 2 lanes, it will result in commuters cutting thru adjoining neighborhoods.

Lisa Hotchkiss

inside Bethesda/Chevy Chase
December 20, 2018, 7:51 AM

I prefer Concept B

Remove the road diet and restore 4 lanes to Little Falls Parkway.

Name not shown

inside Bethesda/Chevy Chase
December 20, 2018, 8:14 AM

I prefer Concept B

Plan B is fair and safe. Please restore all lanes. This is necessary because new construction in Bethesda and future Westbard development will significantly increase--and slow down--traffic flow on Little Falls Parkway, causing more cut through traffic in nearby neighborhoods

Name not shown

inside Bethesda/Chevy Chase
December 20, 2018, 9:11 AM

I prefer Concept A

County needs to enforce rules on CCT: keep to right, give warning when passing, pass only on left, don't walk/run northbound in the southbound shoulder, don't stop on trail, follow speed limit of 15 mph, don't wear headphones or earbuds, no smoking or vaping. Also, ticket drivers who don't stop for trail users.

David Kathan

inside Bethesda/Chevy Chase
December 21, 2018, 2:55 PM

I prefer Concept C

I prefer the bridge option -- Concept C, but only with modifications (no road diet on Little Falls Parkway and Arlington Rd, and protection of surrounding streams). During the interim while the bridge is funded and built, I recommend a modified version of Concept B (no road diet, no trail connector behind the Bethesda Pool, and starting the trail diversion to the Arlington Rd. intersection closer to Little Falls Parkway).

For more information, see the letter that I helped draft and I co-signed from the communities surrounding the CCT crossing dated November 14. In this letter we urge planners to restore Little Falls Parkway to the four lanes it has always had.

Barbara Thomason

inside Bethesda/Chevy Chase
December 22, 2018, 6:23 PM

I prefer Concept B

Remove the road diet and restore 4 lanes to Little Falls Parkway. This will ensure a better flow and stop cars and other traffic using neighborhood streets to avoid this intersection.

APPENDIX G OPEN TOWN HALL LOG

Capital Crescent Trail Crossing at Little Falls Parkway Review

Montgomery Parks staff is seeking public feedback on the three alternate concept plans to improve the Capital Crescent Trail crossing at Little Falls Parkway.

thomas thomason

inside Bethesda/Chevy Chase

December 22, 2018, 7:19 PM

I prefer Concept B

Thomas E. Thomason at 6008 Kennedy Drive 20815 - Kenwood: Please remove the road diet and restore 4 lanes to Little Falls Parkway. The diet is very very dangerous for traffic, particularly for those who are not already familiar with the arrangement. With Concept B selected, there is no reason for the diet - it does not minimize the danger for pedestrians or bicyclists but dramatically increases the likelihood of automobile accidents. The diet as currently configured is a serious traffic hazard which needs to be removed to go back to 4 free flowing lanes. Thank you.

Jeff Kirkham

inside Bethesda/Chevy Chase

December 24, 2018, 6:39 PM

I prefer Concept A

Permanent road diet is the optimal solution. Traffic calming is appropriate in this location.

Bob Ferguson

inside Kemp Mill/4 Corners

December 25, 2018, 10:35 AM

I prefer Concept C

The pedestrian bridge is clearly the safest and most convenient for both autos and trail users. Alternate B is less expensive, but with the heavy usage of the trail, mixing bikes, strollers, roller bladers, walkers, etc at a traffic light, then turning them all loose at the same time when the light goes green, has the potential to produce messy, dangerous situations. In the long run, the difference in cost of the two options will be forgotten, but the increased safety will live on.

Donald Dunner

inside Bethesda/Chevy Chase

December 26, 2018, 12:18 PM

I prefer Concept B

Remove the road diet and restore the 4 lanes to Little Falls Parkway.

caroline cooper

inside Bethesda/Chevy Chase

December 27, 2018, 9:54 AM

I prefer Concept B

For the reasons many others have already stated, Concept B appears to be the safest option and obviates the dangerous potential traffic problems that the "road diet" has created.

Alexandra Acosta

inside Bethesda/Chevy Chase

December 27, 2018, 10:00 AM

I prefer Concept B

I support Concept B as the safest for pedestrians and bicyclists. I also support making the road diet permanent and eliminating visual clutter along LFP between Arlington and Hillandale. This will help drivers see more clearly and feel assured that they will proceed safely in this high-use area. As a Somerset resident, I bike and walk on the CCT and drive along LFP frequently.

Nessa Spitzer

inside Bethesda/Chevy Chase

December 27, 2018, 10:21 AM

I prefer Concept B

Remove road diet and restore 4 lanes to Little Falls Pkwy to avoid traffic building up and slowing down significantly, especially after the serious development in this area is realized.

Capital Crescent Trail Crossing at Little Falls Parkway Review

Montgomery Parks staff is seeking public feedback on the three alternate concept plans to improve the Capital Crescent Trail crossing at Little Falls Parkway.

Name not shown

inside Bethesda/Chevy Chase
December 27, 2018, 10:46 AM

I prefer Concept B

Remove the road diet and restore 4 lanes to Little Falls Parkway.

DOUGLAS DOLAN

inside Bethesda/Chevy Chase
December 27, 2018, 2:12 PM

I prefer Concept B

Remove the road diet and restore 4 lanes to Little Falls Parkway. This is necessary because all the new construction in Bethesda and the future Westbard development will significantly increase--and slow down--traffic flow on Little Falls Parkway, causing more cut through traffic in nearby neighborhoods.

Howard Marlowe

inside Bethesda/Chevy Chase
December 27, 2018, 6:21 PM

I prefer Concept C

Name not shown

inside Bethesda/Chevy Chase
December 27, 2018, 7:30 PM

I prefer Concept C

Option C is my choice for the long-term solution provided the ecological impact can be minimized. Option A should be selected for the short-term while the bridge is planned and constructed. I would be okay with Option A as the long-term solution but for safety and good traffic flow (both motorized and non-motorized), the bridge is a more viable long-term option (also considering cost). Option B is not a viable option in any scenario and should not be utilized. Option B will likely result in creating a higher risk situation over time.

Name not shown

inside Bethesda/Chevy Chase
December 28, 2018, 4:28 AM

I prefer Concept B

Name not shown

inside Bethesda/Chevy Chase
December 28, 2018, 7:20 AM

I prefer Concept B

I prefer Concept B, with all lanes restored on Little Falls Parkway. While not a perfect solution, this seems to strike a reasonable balance between the interests of various users of LFP and the CCT, and also keeps in mind the likely consequences of the planned development along River Road.

Name not shown

inside Bethesda/Chevy Chase
December 28, 2018, 11:34 AM

I prefer Concept B

Concept B makes the most sense in terms of balancing safety and fiscal concerns. But it must be paired with a restoration of four lanes of traffic on Little Falls - 1) construction in Bethesda is diverting traffic from Wisconsin Avenue, with curbside lanes closed to accommodate construction, primarily on the southbound side but also about to start on the northbound side; 2) increased density in Bethesda, with 4 million square feet in the pipeline in the last year and another 3-4 million still available, will continue the pressure on the local road system; 3) traffic under the 2-lane scenario is diverting to narrow neighborhood streets, often without sidewalks and not designed for commuter traffic, creating problems there that are exacerbated by apps like WAZE; and 4) Option B clearly presents safety improvements for all users - cyclists, pedestrians, and drivers.

Name not shown

inside Bethesda/Chevy Chase

APPENDIX G OPEN TOWN HALL LOG

Capital Crescent Trail Crossing at Little Falls Parkway Review

Montgomery Parks staff is seeking public feedback on the three alternate concept plans to improve the Capital Crescent Trail crossing at Little Falls Parkway.

December 30, 2018, 12:17 PM

I prefer Concept C

As a frequent trail user and driver on this section of roadway, the only truly safe proposal is C. No one should face death for choosing to ride a bicycle. One loss of life is too much. This intersection continues to be dangerous. Our first concern must be safety.

Blair Levin

inside Bethesda/Chevy Chase
December 30, 2018, 12:45 PM

I prefer Concept B

Many thanks, particularly to the staff, for the work in creating these options and for asking our opinions.

Name not shown

inside Bethesda/Chevy Chase
December 30, 2018, 1:21 PM

I prefer Concept C

Name not shown

inside Bethesda/Chevy Chase
December 30, 2018, 1:22 PM

I prefer Concept C

Name not shown

inside Bethesda/Chevy Chase
December 30, 2018, 2:52 PM

I prefer Concept B

Kambiz Fotoohi

inside Bethesda/Chevy Chase
December 31, 2018, 1:16 PM

I prefer Concept B

Please restore the 4 lanes on Little Falls Parkway by removing the so called Road Diet. The presented options were limited as displayed ,however, as resident of Chevy Chase for over 25 years and hopefully longer, the traffic flow in/out of that section of Bethesda has become restrictive any many ways and with future developments as planned, I hope you give it more consideration. Thank you

Name not shown

inside Bethesda/Chevy Chase
January 1, 2019, 7:07 PM

I prefer Concept C

I live in KFII community-right across from the Bethesda Pool. Traffic on Hillandale has become exceptionally heavy since Little Falls Pkwy has been reduced to one lane each way. (Therefore, please do not consider Concept A.) Concept C seems like the safest alternative with the least disruption to the surrounding community. Neither of the trail connection options for Concept B makes sense in that they both create a new problem--a new entrance onto the trail where bike/pedestrian traffic is already moving--which is not safe. Also, within Concept B, Trail Connection Option A --which involves an elevated boardwalk around the pool--was soundly rejected by the community and by the MoCo Planning Board. Please do not revive this defeated option. Again, Concept C makes the most sense to bikers and walkers in the adjacent neighborhood. Thank you.

Name not shown

inside Bethesda/Chevy Chase
January 5, 2019, 3:43 PM

I prefer Concept C

Michael Sheehan

inside Bethesda/Chevy Chase
January 7, 2019, 10:49 AM

APPENDIX G OPEN TOWN HALL LOG

Capital Crescent Trail Crossing at Little Falls Parkway Review

Montgomery Parks staff is seeking public feedback on the three alternate concept plans to improve the Capital Crescent Trail crossing at Little Falls Parkway.

I prefer Concept A

Kenneth Swab

inside North Bethesda
January 8, 2019, 4:31 PM

I prefer Concept A

Add rumble strips or speed bumps to the trail at the intersection with Little Falls to slow down the bicyclists (not all of them do so, but there is a significant percentage) who ignore the pedestrians, stop signs and speed limits on that stretch the CCT.

Name not shown

inside Takoma Park
January 10, 2019, 5:30 PM

I prefer Concept A

This road is dangerous, so I support a safety improvement. But MoCo is broke and now is not the time to spending \$4M on a bridge when Concept A will cost 1/4 of the cost and only add 7 seconds to driver times.

Officials need to balance safety with fiscal prudence. Clearly Concept A meets these goals best.

Lawrence Walders

inside Bethesda/Chevy Chase
January 18, 2019, 8:38 PM

I prefer Concept A

The biggest danger is at night when walkers and bicyclists are hard to see, particularly when they are wearing dark clothing. If there is no bridge, the County should install a streetlight at the intersection of the Trail and Little Falls.

Name not shown

inside Bethesda/Chevy Chase
January 18, 2019, 9:36 PM

I prefer Concept A

Functionally the current arrangement works well. There are only two issues. First, it is very dark at night with no streetlight. CCT traffic can easily surprise cars by crossing suddenly. Some lighting at the crossing is the only functional improvement needed. Second, it looks shabby. Alternative A seems to improve this aspect quite well.

Thanks for your attention to this intersection.

Name not shown

inside Bethesda/Chevy Chase
January 18, 2019, 10:55 PM

I prefer Concept A

As a cyclist and a car commuter, I think the current road diet has been a good solution and would like to see it made permanent.

L.A. Woolley

inside Bethesda/Chevy Chase
January 20, 2019, 4:22 PM

I prefer Concept B

Please move the intersection and restore Little Falls Parkway to four lanes.

Renee Stewart

inside Bethesda/Chevy Chase
January 20, 2019, 4:40 PM

I prefer Concept B

Cathy Ensslen

inside Bethesda/Chevy Chase
January 21, 2019, 7:26 AM

I prefer Concept B

Please restore Little Falls Parkway to 4 lanes.

Capital Crescent Trail Crossing at Little Falls Parkway Review

Montgomery Parks staff is seeking public feedback on the three alternate concept plans to improve the Capital Crescent Trail crossing at Little Falls Parkway.

This "road diet" is scary. Many times I have encountered motorists either forgetting or not knowing about the sudden reduction to 2 lanes and quickly move over with, or without a turn signal, into my lane and causing me to slam on my brakes.

Then there is the situation where I stop for a walker, runner, or cyclist, and the impatient driver behind me wants me to keep driving through the crosswalk and not stop. How do I know this? He's practically in my "back seat". This causes me stress worrying if he's gonna crash into the back of me.

Please move the crosswalk to Arlington Rd. I believe this is a safer solution for all.

Name not shown

inside Bethesda/Chevy Chase
January 21, 2019, 8:34 AM

I prefer Concept C

Susan Harding

inside Bethesda/Chevy Chase
January 21, 2019, 9:17 AM

I prefer Concept B

Concept B improves safety and road/path conditions for cars and trail users at a reasonable cost. Option A maintains many of the unsafe features as before and option C is both expensive and unfriendly to challenged trail users.

Name not shown

inside Bethesda/Chevy Chase
January 21, 2019, 10:12 AM

I prefer Concept B

I prefer Option B, but we must also remove the "road diet" on Little Falls Parkway and restore it to four lanes.

Name not shown

inside Bethesda/Chevy Chase

January 21, 2019, 11:15 AM

I prefer Concept A

While the ballards and signage are unsightly, Concept A seems to be currently working as a compromise for both motorists and trail users. Making it permanent by replacing the ballards with plantings and aesthetic lighting is my preference.

Name not shown

inside Bethesda/Chevy Chase
January 21, 2019, 11:49 AM

I prefer Concept B

what is concept C? There definitely needs to be a traffic light change at the Crescent Park Little Falls Pkwy. intersection. We need a left turn arrow at Mass. Ave. and Little Falls Pkwy, so that more cars turning left can get through.

Anne Fishman

inside Bethesda/Chevy Chase
January 21, 2019, 12:38 PM

I prefer Concept B

Concept C would be good if it were not probably expensive and take too long to put into place. Concept A -- the current state-- works very poorly for drivers. So that leaves me to prefer concept B, however it would be preferable to remove the diet and restore 4 lanes to the Parkway.

Name not shown

inside Bethesda/Chevy Chase
January 21, 2019, 12:57 PM

I prefer Concept C

Safest alternative. We won't have to revisit and tweak like the others which have the potential for something not to work well - safety, timing, new traffic patterns, congestion, etc.

Capital Crescent Trail Crossing at Little Falls Parkway Review

Montgomery Parks staff is seeking public feedback on the three alternate concept plans to improve the Capital Crescent Trail crossing at Little Falls Parkway.

Bien Gooi

inside Bethesda/Chevy Chase
January 21, 2019, 1:06 PM

I prefer Concept A

Remove Road Diet and restore Little Falls Pkwy to 4 lanes.

Name not shown

inside Bethesda/Chevy Chase
January 21, 2019, 5:12 PM

I prefer Concept B

Name not shown

inside Bethesda/Chevy Chase
January 21, 2019, 6:00 PM

I prefer Concept C

The pedestrian/biker overpass provides the most safety, no delay of traffic, no diversion of traffic into the neighborhood, and allows the reopening of Little Falls Parkway to four lanes. When the Purple Line is finished and the hiker/biker trail from Bethesda to Silver Spring, commuter traffic on the Capital Crescent Trail can be expected to increase. Instead of going with an unsatisfactory partial measure now, we should go ahead with the pedestrian biker overpass project now.

Name not shown

inside Bethesda/Chevy Chase
January 21, 2019, 11:29 PM

I prefer Concept C

Ropbert Mertz

inside Bethesda/Chevy Chase
January 22, 2019, 9:46 AM

I prefer Concept C

but would be happy with Concept B as an interim while C is being designed and implemented. Continuation of the current restricted traffic pattern on Little Falls Parkway is a dreadful solution and must be terminated by the implementation of B as an interim measure to an eventual bridge or tunnel - Concept C - at the current alignment of the CCT ASAP.

Name not shown

inside Bethesda/Chevy Chase
January 22, 2019, 2:10 PM

I prefer Concept B

I think Little Falls must be widened to 4 lanes at this point. It is a real bottleneck.

Michael Simpson

inside Kensington/Wheaton
January 27, 2019, 4:22 PM

I prefer Concept A

Name not shown

inside North Bethesda
January 27, 2019, 9:51 PM

I prefer Concept B

Aligning the path with an established intersection and traffic light is the most straightforward and understandable concept. I am concerned that a pedestrian overpass will be avoided by pedestrians and bikers alike (I used to do that when I was in high school). This would likely lead to unregulated crossing of Little Falls Parkway, a more dangerous situation. Moving to the intersection and re-aligning the trail will also avoid the additional congestion/confusion at the pool exit.

David Cloud

inside Bethesda/Chevy Chase
January 31, 2019, 10:52 AM

Capital Crescent Trail Crossing at Little Falls Parkway Review

Montgomery Parks staff is seeking public feedback on the three alternate concept plans to improve the Capital Crescent Trail crossing at Little Falls Parkway.

I prefer Concept C

There needs to be a fundamental redesign of this intersection. The current ad hoc arrangement is a mess, with neither drivers nor trail users clear on who has right of way. Plus the poor lighting makes it even more dangerous. Trail users are required to stop before crossing and drivers are required to yield. Who has right of way. Please fix this with an overpass. Everyone will be better served and safer. Drivers coming from Mass Ave already face difficulty getting to Bethesda. Don't make it harder.

Jonathan Bernstein

inside Silver Spring

January 31, 2019, 6:06 PM

I prefer Concept A

In a Vision Zero time, where we want to encourage bike commuting, especially along the Capital Crescent Trail, let's not divert the trail for the convenience of drivers. So for me Concept A is the least costly and best permanent solution.

Name not shown

inside Silver Spring

February 2, 2019, 11:55 PM

I prefer Concept A

APPENDIX G EMAILS

From: [Jamie Heller](#)
To: [Riley, Mike](#); [Tsai, Andrew](#)
Cc: [Jamie Heller](#); [Sarah Morse \(morsekathan@gmail.com\)](mailto:morsekathan@gmail.com)
Subject: Capital Crescent Crossing at Little Falls Parkway
Date: Wednesday, May 23, 2018 3:04:25 PM

Dear Sirs:

Thank you for your work on the trail crossing. I use that crossing frequently as both a biker and a driver. In reviewing your options, the one that seems most attractive and likely low cost would be re-routing the trail to the light signal. My concern is that if the light is timed like most lights then traffic will be delayed when no bikes are present. Since the signal now has a right turn arrow off Little Falls that is mostly permissive, this would delay traffic. Also my experience is that bikers violate signals (I am guilty) when no traffic is present. This could create a new risk.

I don't know what the right decision is, but I would hope that the data presented on the options would show:

- Cost of implementation
- Modelled delay impact relative of new option relative to status quo at peak and non-peak time periods for bikers and traffic in each direction.

With that information you might get more intelligent citizen input.

Thanks for your consideration

Jamie Heller

Jamie Heller
Hellerworx, Inc.
4803 Falstone Avenue
Chevy Chase, MD 20815
ph (301) 654-1980
cell (202) 425-3524
fax (866) 908-7901
jamie@hellerworx.com
www.hellerworx.com

APPENDIX G EMAILS

From: [robert l bein](#)
To: [Tsai, Andrew](#); [Malcolm O'Hagan](#); [Barron, David](#)
Subject: Re: Crescent Trail
Date: Thursday, May 24, 2018 9:23:47 AM

Mr. Tsai,

As Malcolm O'Hagan has said, the problem is much more the fault of the cyclists than the car drivers. I'd like to point out that the increased use of stop signs is not effective, witness the intersection of the trail and Dorset Avenue. The cars stop but the cyclists don't! They charge through the intersection as if there were no stop signs. And they curse you if you remind them they should stop at that intersection.

You've probably already thought of this, but if the *trail crossing is moved* to the traffic light at either Hillandale or Arlington Rd, and a 30 second delay before changing the light to green is instituted to give walkers/runners/cyclists a chance to cross, it would dramatically increase safety without interfering with traffic flow.

I may not be able to make it to the meeting but I did want to pass on my thoughts.

Robert Bein

ps. fyi, David Barron who is copied in, is president of the Kenwood Citizens Association which represents more than 230 homes in Kenwood.

On 5/24/2018 8:35 AM, Tsai, Andrew wrote:

Good morning Mr. O'Hagan,
Thanks for your comments. We are considering many potential solutions, including increased usage of Stop signs at this crossing. A speed bump on the trail itself is difficult due to accessibility, safety, and maintenance requirements. At the June 13 meeting we'll present some potential solutions and we will set aside as much time as possible for public discussion and input where you will have the opportunity to speak.

Andrew Tsai, P.E.
Project Manager
Park Development Division - Montgomery Parks
Maryland-National Capital Parks and Planning Commission (M-NCPPC)
9500 Brunett Avenue
Silver Spring, MD 20901
Andrew.Tsai@montgomeryparks.org
Office: (301) 495-2508

APPENDIX G EMAILS

From: Malcolm O'Hagan <ohagans@mac.com>
Sent: Wednesday, May 23, 2018 6:35 PM
To: Tsai, Andrew <Andrew.Tsai@montgomeryparks.org>
Cc: Bein Robert <rlbein@verizon.net>
Subject: Crescent Trail

I love the Crescent Trail and as a jogger I have enjoyed it for years. I would like to have an opportunity to speak at the hearing at June 13 on the Little Falls crossing. In the meantime I offer the following comment.

The cyclists, and not the motorists, are the problem. On countless occasions I have had to scream at cyclists racing to work in the morning to slow down. They are reckless, with no regard for the safety of others on the trail. They think it is their private speedway.

The solutions being considered are draconian and not warranted. There is a very easy and very inexpensive solution:

1. Post STOP signs on Little Falls Parkway at the crossing **AND STOP signs on the trail**
2. Put a **Speed Bump** on the trail before the crossing. This will slow the cyclists, believe me.

I live in Kenwood and I hate our speed bumps, but they do the job.

Respectfully

Malcolm O'Hagan
301 656 5771

APPENDIX G EMAILS

From: [Malcolm O'Hagan](#)
To: [Tsai, Andrew](#)
Cc: [Bein Robert](#)
Subject: Re: Crescent Trail
Date: Friday, May 25, 2018 11:12:30 AM

Dear Mr Tsai,

Please allow me to impose on your time once more to refute the arguments against speed bumps based on personal experience.

The only person who could be injured in any way by a speed bump is a cyclist **who is going too fast**. When I wheel my handicapped grandson around the neighborhood in his wheelchair there is never the slightest problem in crossing speed bumps. When my four year old granddaughter cycles to the park not only does she not have any problem with the speed bumps, she likes them. When my eighty year old neighbor perambulates around Kenwood speed bumps are never a hazard. So the "accessibility" argument against speed bumps just does not hold up. If there are statistics to the contrary I would like to see them.

I'm not sure what leaf removal maintenance equipment you are referring to is, and what the issue is. Leaves certainly do not accumulate around speed bumps, and I have never seen "leaf removal" equipment on the Crescent Trail. The maintenance equipment I have seen on the trail would have no issue with speed bumps.

Why would people walk around speed bumps? These are bumps, not mounds. If cyclists try to do it, they will at least have to slow down.

In addition to jogging I also enjoy cycling on the trail. As a cyclist I have an innate inclination to go faster than I should. I respect warning signs such as STOP signs and will exercise caution but not necessarily slow down unless I see a car about to cross the trail. The only thing guaranteed to get cyclists to SLOW DOWN is a speed bump. Stop signs and traffic lights will not necessarily do so. The bend in the trail at Little Falls Parkway has been a definite help in slowing cyclists.

I am opposed to wasting my money and the money of other tax payers on expensive solutions that are not warranted by the facts. I look forward to having the opportunity to express my concerns at the hearing.

Respectfully

Malcolm O'Hagan

On May 24, 2018, at 2:35 PM, Tsai, Andrew
<andrew.tsai@montgomeryparks.org> wrote:

Mr. O'Hagan,

I do agree that there are some cyclists who ride unsafely on the trail and on public

APPENDIX G EMAILS

roads. I also agree with you that speed bumps can be a useful traffic calming measure on public roadways, but on a shared use trail they present several issues. From an accessibility standpoint, less than able bodied trail users, smaller children, and people in wheelchairs will have difficulties. As you mentioned below, they could potentially injure trail users (not just bicyclists, but are a tripping hazard for joggers and pedestrians). From a maintenance perspective, they present an issue with leaf removal maintenance equipment. Plus, people will just walk or ride around the speed bump given the opportunity. However at this conceptual design stage nothing is off the table and the main purpose of the June meeting is to get an idea of what regular trail users such as yourself would like to see to improve the safety of the crossing.

Thanks,
Andrew

From: Malcolm O'Hagan <ohagans@mac.com>
Sent: Thursday, May 24, 2018 10:30 AM
To: Tsai, Andrew <Andrew.Tsai@montgomeryparks.org>
Cc: Bein Robert <rlbein@verizon.net>
Subject: Re: Crescent Trail

Dear Mr Tsai,

I thank you for your response.

As an engineer (mechanical and not civil I should add!) I find it hard to understand the difficulties you refer in regard to speed bumps. What is the problem of accessibility? I am not aware of any maintenance issues relating to the speed bumps in Kenwood. The speed bumps in Kenwood were reinstalled in a day after the roads were resurfaced last year. What is the safety concern - that a speeding cyclist will be bumped? If that were to happen the cyclist would be to blame, and the cyclist would be careful to not let it happen again. A warning can be painted on the trail SPEED BUMP AHEAD.

The safest solution of all, which I would like but am not advocating, is to ban cyclists from the trail and limit use to pedestrians. Speeding cyclists will continue to be a menace and they will be the cause of more injuries not just at crossings.

Respectfully

Malcolm O'Hagan

On May 24, 2018, at 8:35 AM, Tsai, Andrew

APPENDIX G EMAILS

<andrew.tsai@montgomeryparks.org> wrote:

Good morning Mr. O'Hagan,
Thanks for your comments. We are considering many potential solutions, including increased usage of Stop signs at this crossing. A speed bump on the trail itself is difficult due to accessibility, safety, and maintenance requirements. At the June 13 meeting we'll present some potential solutions and we will set aside as much time as possible for public discussion and input where you will have the opportunity to speak.

Andrew Tsai, P.E.
Project Manager
Park Development Division - Montgomery Parks
Maryland-National Capital Parks and Planning Commission (M-NCPPC)
9500 Brunett Avenue
Silver Spring, MD 20901
Andrew.Tsai@montgomeryparks.org
Office: (301) 495-2508

From: Malcolm O'Hagan <ohagans@mac.com>
Sent: Wednesday, May 23, 2018 6:35 PM
To: Tsai, Andrew <Andrew.Tsai@montgomeryparks.org>
Cc: Bein Robert <rlbein@verizon.net>
Subject: Crescent Trail

I love the Crescent Trail and as a jogger I have enjoyed it for years. I would like to have an opportunity to speak at the hearing at June 13 on the Little Falls crossing. In the meantime I offer the following comment.

The cyclists, and not the motorists, are the problem. On countless occasions I have had to scream at cyclists racing to work in the morning to slow down. They are reckless, with no regard for the safety of others on the trail. They think it is their private speedway.

The solutions being considered are draconian and not warranted. There is a very easy and very inexpensive solution:

1. Post STOP signs on Little Falls Parkway at the crossing **AND STOP signs on the trail**
2. Put a **Speed Bump** on the trail before the crossing. This will slow the

APPENDIX G EMAILS

cyclists, believe me.

I live in Kenwood and I hate our speed bumps, but they do the job.

Respectfully

Malcolm O'Hagan
301 656 5771

APPENDIX G EMAILS

From: [Maurizio Guadagni](#)
To: [Tsai, Andrew](#)
Subject: Fwd: New Announcement - REMINDER: Meeting re: trail crossing at Little Falls Pkwy
Date: Tuesday, June 12, 2018 10:39:03 PM

Dear Andrew,

I am a frequent user of the Capital Crescent Trail.

I would opt for a Pedestrian Bridge, similar to the one over River Road.

Thanks

--

Maurizio Guadagni
Em: maurizio.guadagni@gmail.com
Skype Name maurizioguadagni

Begin forwarded message:

From: No-Reply <no-reply@kcacherrytrees.org>
Date: Jun 12, 2018 at 7:18 PM
To: Maurizio Guadagni <maurizio.guadagni@gmail.com>
Subject: New Announcement - REMINDER: Meeting re: trail crossing at Little Falls Pkwy



kcacherrytrees.org - New Announcement

Announcement Name	REMINDER: Meeting re: trail crossing at Little Falls Pkwy
Date & Time	06/12/2018 7:17 PM
Group	All Neighbors
Importance	Normal
Web Link	https://www.montgomeryparks.org/projects/directory/capital-crescent-trail-crossing-at-little-falls-parkway/
Description	Montgomery County Parks is inviting the public to weigh in from 7 to 9 p.m., June 13, at Somerset Elementary School on a proposal to change the Capital Crescent Trail crossing at Little Falls Parkway. The road was reduced to one lane in each direction by the addition of bollards after a man was struck by a vehicle and killed at the crossing while riding a recumbent bicycle. The parks department is considering making the "road diet" permanent, among other options. This is an opportunity for you to offer your own suggestions on how to make the crossing safer, whether it's the "road diet" or moving the trail so it crosses Little Falls at the signal light at Hillandale or Arlington Road, or some other idea.

Click on the link for details about the meeting.

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[Click here](#) to no longer receive these email notifications.

APPENDIX G EMAILS

From: [Mark Cheng](#)
To: [Tsai, Andrew](#)
Subject: Capital Crescent Trail Crossing at Little Falls Parkway Input
Date: Saturday, June 16, 2018 8:53:50 AM

Dear Mr Tsai:

I appreciate the opportunity to provide my opinion on proposed solutions to the Capital Crescent Trail Crossing at Little Falls Parkway.

First, the loss of life is tragic and must be avoided regardless of 'fault.' In collisions between cars and bicycles or pedestrians, the bicyclists and pedestrians will always lose. When the current "road diet" was implemented, I forwarded my concerns to M-NCPPC Montgomery Parks. Those concerns have been borne out.

I copied the proposals on the table from your website and respectfully submit comments on each.

- Permanent Road Diet: Making the interim two-lane Little Falls Parkway at the CCT crossing permanent;
- Permanent Road Diet with Roundabout: Making the interim two-lane Little Falls Parkway at the CCT crossing permanently with a roundabout replacing the Arlington Road at Little Falls Parkway signalized intersection.

The sheer growing volume of traffic entering and exiting Bethesda via Little Falls Parkway and Arlington Road renders the road diet unreasonable. The backups on Little Falls Parkway and Arlington Road during rush hour are unacceptable. The solution to this situation cannot be decided in isolation from the County's plan to expand development in downtown Bethesda. We cannot reduce surface road access while the County is clearly simultaneously growing destinations for surface traffic.

- Trail Reorientation: Relocating the CCT to cross at one of the traffic signals at Arlington Road or Hillendale Road;

I suggested trail redirection when the road diet was first implemented. This seems to me to be the least intrusive, most effective, and most economical. I would reorient the trail to cross Little Falls Parkway at Hillendale (originally I suggested north of Arlington but I see that would then require crossings of both Little Falls and Arlington). The crossing would occur with the light at Hillendale.

- Midblock Traffic Signal: Installing a traffic signal or HAWK signal (a pedestrian activated signal) at the CCT crossing;

APPENDIX G EMAILS

A third stopping point (in addition to Arlington and Hillendale will really negatively impact traffic on Little Falls Parkway).

- Pedestrian Tunnel or Bridge;

Common elsewhere in the world but I presume economically unfeasible. People will be too lazy to climb a bridge. I look at people trying to cross River Road at the CCT crossing, dashing across River Road rather than go up to the overpass.

- Dynamic Lane Use: Using signalization to control lane usage (two lanes to one and/or northbound versus southbound) along Little Falls Parkway depending on the time of day and day of the week;

Not sure what this really accomplishes and I see people often ignore 'signalization.'

- Little Falls Parkway Closure: Entirely close Little Falls Parkway to vehicles between Arlington Road and Hillendale Road.

Completely unreasonable given the critical role played by the Little Falls and Arlington artery into Bethesda.

Bottom line, however, and it may not be PC, but the consistent refusal of bicyclists on the Capital Crescent Trail to stop at Little Falls Parkway before crossing is a critical factor in this situation. It is my personal observation at this location and pretty much anywhere else in the DC area, bicyclists want to be treated a vehicles but obey no vehicular rules, regulations, or laws. They do not observe traffic lights, stop signs, do not signal, do not use tax payer provided bike lanes-MacArthur Blvd case in point. They want to ride whenever, wherever, and however they wish. When called on it, they are belligerent. I walk my dog on the Little Falls Trail and bicyclists assume they have the right of way, and at high speed.

I maintain that while we need to bring cars to the speed limit on Little Falls Parkway and have drivers more alert to foot traffic, the deciding factor in preventing further tragedy at this crossing without seriously disrupting what is in fact the primary mode of transportation in this region (unquestionably, and growing even more due to county development decisions), is regulation of bicyclist behavior. If they stop and observe the stop sign, like most pedestrians, they will be safer. But self-righteously zipping across the intersection at 15-25 miles an hour clearly is a recipe for disaster. I read that the gentleman who was riding his low profile recumbent bike when he was struck and killed, did NOT stop as required at the crossing. Again, no one should die or be injured, but clearly that is a critical factor in making this intersection safer for everyone.

APPENDIX G EMAILS

I don't know whether fencing, speed bumps, something to channel and slow the bicyclists would work better, they seem to skirt all other rules, but that could be something to consider. I should think if one monitored pedestrian, bicyclist, and vehicular use of this intersection, vehicles would come out ahead by far.

Thanks for your kind consideration.

Mark Cheng
Carvel Rd
Bethesda

APPENDIX G EMAILS

From: [Helen Davies](#)
To: [Tsai, Andrew](#)
Subject: Input on Capital Crescent Trail proposals
Date: Wednesday, June 20, 2018 2:43:02 PM

Hi Andrew,

Sorry I missed the meeting on 6/13 but I wanted to give input on the proposals. The only proposal that is a win win for both the bikers, pedestrians and the car drivers is to build a bridge over Little Falls Parkway. This completely eliminates any possible interaction with bikes, peds and cars and also allows traffic to resume two lanes in both directions.

All the other proposals seem to have weaknesses, although they may be less expensive alternatives.

The proposal to completely close the section of the parkway between Arlington and Hillandale is a non starter given the amount of DC bound traffic in the morning.

The proposal to build a roundabout doesn't seem to solve the safety issue with the bikes and pedestrians.

The proposal rerouting folks on the trail down to Hillandale might work but if there's any possibility they can go straight folks might cheat.

Narrowing the lanes permanently to one lane does not solve the safety problems of all the bikes and people crossing an active roadway.

Putting in a HAWK will likely not be used compliantly by the bikers.

Thanks for the opportunity to comment.

Helen Davies
President
Kenwood Forest Condo Association

APPENDIX G EMAILS

From: [Phil Stewart](#)
To: [Tsai, Andrew](#)
Subject: Capital Crescent Trail and Little Falls Parkway crossing
Date: Tuesday, June 26, 2018 12:32:29 PM

Andrew Tsai --

I am responding to your call for comments regarding the intersection of Little Falls Parkway and the Capital Crescent Trail. I am a Bethesda resident and pass through that intersection multiple times each day as a runner, a cyclist and a commuter by car between my home and office. Since the reformatting of the intersection after the death of the 82-year-old cyclist, I would generously describe it as a "war zone" between cars, runners, walkers and cyclists. The narrowing of the lanes produces lengthy vehicle backups during rush hours (which are practically all day due to Bethesda's over development which shows no sign of abatement) as vehicles come to a standstill when there are trail users crossing and a near stop when they are checking for trail users approaching the intersection. At peak times, the vehicles making a left turn off of Arlington Rd. on to Little Falls Pkwy. are held for several cycles of the traffic light waiting for the lined up vehicles heading west on Little Falls Pkwy. to clear the trail intersection. As a runner and cyclist crossing the trail, I am cautious because I am not certain the drivers will actually stop. As a driver, I am already frustrated due to the delay approaching the crossing and am not feeling charitable about further delays as cyclists and runners cross.

The solution which will satisfy both the drivers and the trail users (call it a "win-win") is to build a trail overpass at the intersection. With the eastern part of the trail already lost to users due to the Purple Line construction, it seems like a gesture on the part of the county to make the remaining part of the trail more user friendly would be in order. I heard about some discussion at one point about using the money from the Ourisman settlement for this purpose which I feel was an opportunity lost.

In the meantime as funding for this project works its way through the county budgetary process, installing some high intensity street lights like to those at Little Falls Pkwy. and Dorset Ave. would be a valuable remedy for night time and early morning crossings especially during the winter months.

Sincerely,
Phil Stewart
4904 Glen Cove Pkwy.
Bethesda, MD 20816

Phil Stewart
Event Director, Credit Union Cherry Blossom
4963 Elm St., Suite 106, Bethesda, MD 20814
Phone: 301-320-6865 Fax: 301-320-9164
E-mail: pstewart@cherryblossom.org

APPENDIX G EMAILS

From: [Naomi Spinrad](#)
To: [Tsai, Andrew](#)
Cc: [Frank, Andrew](#); [MCP-Chair](#)
Subject: Little Falls/Capital Crescent Trail crossing
Date: Wednesday, July 18, 2018 12:25:20 PM

Dear Mr. Tsai:

I write as an individual, although for information purposes I'm a member of the board of the Coalition of Bethesda Area Residents and I'm a residential member of the Bethesda Downtown Plan Implementation Advisory Committee.

I'm also a regular pedestrian user of the Capital Crescent Trail between Dorset Avenue and Bethesda Avenue, and a less frequent user between Bethesda and points further south, occasionally beyond Chain Bridge. The crossing at Little Falls Parkway has been a constant part of my route since the trail was opened.

Although I believe that a bridge over Little Falls is the best, safest option, the cost for a bridge probably makes it a longer term solution. For now and for the foreseeable future, my preference would be to reroute the trail to a fully signalized crossing at Arlington Road. This would require full stops from everyone - pedestrians, cyclists, and cars - the single most important thing you say you want to do.

A protective barricade along the rerouted trail would help ensure that users do not continue to cross at a dangerous point and would provide some additional protection from vehicular traffic.

This would also reduce the potential for conflict with cars coming out of the swimming pool lot.

I've seen far too many cyclists simply blow through the stop signs on the trail, in both directions, without regard to oncoming traffic, to be confident that measures relying on stop signs at the existing crossing will be effective.

Narrowing Little Falls is a not viable option for the long term. As an active member of the Bethesda-Chevy Chase area, I strongly believe that permanently making Little Falls a two-lane road is a really bad idea. Little Falls and the streets it connects to are vital to traffic between downtown Bethesda and its neighboring communities and Westbard. Little Falls is a key connector to River Road and Massachusetts Avenue for many commuters and local shoppers. With 8 million more square feet of development in the pipeline or available on the ground in Bethesda, plus whatever is developed in Westbard, the congestion in the area will only grow. We are already seeing the traffic effects on Wisconsin Avenue, where three buildings are currently under construction in downtown Bethesda and more are expected, and three southbound lanes have been reduced to two.

So I would strongly urge you to reject any change that reduces traffic flow on Little Falls, and instead to concentrate on a realistic solution that increases safety. Moving the trail to a full signal at Arlington Road is the way to go.

Thank you for your consideration.

APPENDIX G EMAILS

Sincerely,
Naomi Spinrad
Chevy Chase West

APPENDIX G EMAILS

From: [Henry Lebard](#)
To: [Tsai, Andrew](#); [Frank, Andrew](#)
Subject: Little Falls crossing project
Date: Tuesday, July 24, 2018 4:12:08 PM

Hi both of you (Mr. Frank and Mr. Tsai),

I was just presented with a document presenting potential ideas for developing or "improving" the Little Falls Parkway crossing of the Capital Crescent Trail, including the trail and sidewalks on the East side of Bethesda Pool.

I believe that the development done to mitigate and calm traffic has been mostly sufficient, but could use a couple of small changes. As a long-time user (20+ years) of the trails in the area, and having grown up less than a 10-minute walk from this crossing, I have noticed a serious feeling of safety when approaching this intersection.

As such, I feel it a waste of funds to further develop the above ground crossing. To elaborate: I do not support a bridge or a tunnel. I also do not support a ramp or trail from the CCT to Hillandale nor do I support the creation of a new sidewalk on the east side of Hillandale.

Instead, I support the possibility of 'tables' or 'traffic quieting' methods to reduce traffic speed and improve pedestrian and cyclist safety.

Moreover, the crossing at Hillandale Rd to the Bethesda pool has become safer, but still requires some traffic calming methods (such as tables, better signage, thinner through-ways, and the like). Considering this, these posts and signage need to be monitored and replaced since motorists often drive through or into these cones or posts rendering them useless. Similarly, it may be in the county's interest in implement a safety camera (stop-sign) at the Hillandale pedestrian crossing.

Regarding Arlington Rd, I believe this road has become quite busy as cars drive quite fast down it. I support the idea of any traffic slowing methods (although not the diversion of traffic to other routes), such as medians, cameras and lower speed limits. It is important to take into account the Arlington & Bradley Blvd intersection when implementing development. This is already a very busy intersection that cannot take a long period of route diversion and construction on the Southwest portion of Arlington Rd.

I thank you for taking the time to consider my views on this project.

I would be happy to discuss or present my ideas further.

all the best,
Henry Lebard
4620 Langdrum Lane, Chevy Chase, MD
hlebard@gmail.com

APPENDIX G EMAILS

From: [Charlie](#)
To: [Tsai, Andrew](#)
Subject: Re: Capital Crescent Trail Crossing at Little Falls Parkway
Date: Monday, July 30, 2018 12:24:09 PM

Andrew, thank you for that information. What follows are the ideas that I wanted to share as an alternative Option 1 for the Crescent Trail crossing at Little Falls Parkway. I do so as a neighborhood walker, driver, and cyclist.

I predicate these thoughts on two considerations which lead me to prefer a modified version of Option 1. First of all, drivers are annoyed and traffic slowed by the two stops (or if lucky one slow down and a stop) on Little Falls Pkwy. Secondly, cyclists are even more annoyed and hindered by losing momentum that a sharp curve and stop necessitate in the present crossing and in Option 1.

Therefore, as a compromise I would have cyclists cross Little Falls at the traffic light at the corner of Little Falls Parkway and Arlington Rd. Nevertheless, I would do this by building a gentle curve in the trail on both sides of Little Falls Parkway. This would necessitate cutting into the last section of the berm along the Arlington Rd. side of the trail. As part of the compromise, it would also mean some tree cutting and replanting on the original parts of the trail on both sides of Little Falls.

I have attached a crude diagram of this modified version of Option 1.

Sincerely, Charles Whitehead

-----Original Message-----

From: Tsai, Andrew <andrew.tsai@montgomeryparks.org>
To: Charlie <cew789@aol.com>
Sent: Mon, Jul 30, 2018 9:20 am
Subject: RE: Capital Crescent Trail Crossing at Little Falls Parkway

Mr. Whitehead,

The Open Town Hall has not been set up yet. We are planning to open it this Fall when we narrow the concepts down to 3 alternatives. In the meantime, please feel free to send any comments you have to me.

Thanks
Andrew

Andrew Tsai, P.E.
Project Manager
Park Development Division - Montgomery Parks
Maryland-National Capital Parks and Planning Commission (M-NCPPC)
9500 Brunett Avenue
Silver Spring, MD 20901
Andrew.Tsai@montgomeryparks.org
Office: (301) 495-2508

From: Charlie <cew789@aol.com>

APPENDIX G EMAILS

Sent: Saturday, July 28, 2018 3:10 PM

To: Tsai, Andrew <andrew.tsai@montgomeryparks.org>

Subject: Capital Crescent Trail Crossing at Little Falls Parkway

Mr. Tsai,

I have looked for this topic in the "Open Town Hall" section of MontgomeryParks.org. Has this topic not been set up yet?

If it is in open, please direct me to where to find it.

Thank you, Charles Whitehead

APPENDIX G EMAILS

From: [John Z. Wetmore](#)
To: [Tsai, Andrew](#)
Cc: [County_Council@MontgomeryCountyMD.gov](#)
Subject: Comments on OCT - Little Falls Parkway crossing
Date: Sunday, August 5, 2018 11:17:38 PM

Regarding the Capital Crescent Trail Crossing at Little Falls Parkway:

The solution selected should follow the following guiding principles:

- 1) Trail users should only have to cross one lane of traffic in each direction. (This eliminates the Multiple Threat situation that kills many pedestrians.)
- 2) A median island should separate the two directions of traffic. (Pedestrians then need to look for gaps in traffic for one direction at a time, or wait for traffic to stop from one direction at a time.)
- 3) Deviations from the natural desire lines of trail users should be avoided. (Anything that creates a detour that increases the distance traveled by pedestrians is to be avoided.)

Having a raised crosswalk might help improve yielding behavior by drivers.

It should be possible to improve a crossing at grade level so that a bridge is not necessary. A bridge would be problematic for people entering the trail where it would already be elevated for the bridge, such as people coming from the pool, coming up from Hillandale, or coming down from Glenbrook. (The bridge makes sense at River Road, where traffic volumes are much higher, but even there a surface crossing complements the bridge.) (It would make more sense to spend money on a bridge at more problematic crossings on higher volume roads, such as Viers Mill.)

Thank you for your attention.

John Z Wetmore
john@pedestrians.org
Producer of "Peris for Pedestrians" Television

Subscribe to my YouTube channel and never miss an episode.
https://www.youtube.com/channel/UCv35o3m171H9h48m002d85f6h2696v/Cw90ic42c4c4e?list=PLd0h98b6855167c9P0C9vC9vC9v366912226713782&utm_source=YT&utm_medium=YT&utm_campaign=YT
 I'll find out more.
https://www.youtube.com/channel/UCv35o3m171H9h48m002d85f6h2696v/Cw90ic42c4c4e?list=PLd0h98b6855167c9P0C9vC9vC9v366912226713782&utm_source=YT&utm_medium=YT&utm_campaign=YT

APPENDIX G EMAILS

From: [David Forman](#)
To: [Tsai, Andrew](#)
Cc: [Martin Jim and Celia](#); [Jenny Sue Dailey](#); harry.cccfh@gmail.com
Subject: Westmoreland Citizen's Association comments on Little Falls Parkway/Capital Crescent Trail
Date: Tuesday, August 7, 2018 12:58:57 PM
Attachments: [CCT and LF Pkwy WCA FeedbackQuotes revised.docx](#)

Dear Mr. Tsai,

The Westmoreland Citizens Association (WCA) sent out an email to members, soliciting input regarding the Capital Crescent Trail crossing at Little Falls Parkway, and included the Coalition for the Capital Crescent Trail recommendation letter. Twenty-five members of the WCA responded. 84% endorsed, in whole or in part, the recommendation of the Coalition for the Capital Crescent Trail. That is, to return car traffic to 4 lanes and the prior speed limits, move the trail to an existing light at the Arlington Road crossing, and longer term, build a bridge. The complete comments of WCA members are in the attached report.

Please confirm receipt of this submission.

David S. Forman , for th e WCA
344 Falmouth Road
Bethesda, MD 20816
(301) 229-6869

APPENDIX G EMAILS

From: [Jason Amirhadji](#)
To: [Tsai, Andrew](#)
Subject: We must not return to a dangerous four-lane crossing at the Little Falls Parkway intersection
Date: Friday, August 10, 2018 11:41:11 AM

Dear Andrew Tsai,

After a cyclist was struck and killed by a driver at the intersection of the Capital Crescent Trail and Little Falls Parkway in late 2016, the local parks service put in temporary measures to slow traffic in the area. So far this "road diet" has been successful, and the Maryland National Capital Park and Planning Commission (M-NCPPC) Parks Service is considering what elements to make permanent.

Unfortunately, some residents have expressed a desire to return to the former dangerous four-lane design, even though the county has called four-lane trail crossings like this one "high risk." Please do not allow this to happen. The county has endorsed Vision Zero, an initiative to eliminate all area traffic deaths by 2030. Reworking the road to prioritize cars and speed would violate one of its principles: "Human life takes priority over mobility and other objectives of the road system." Let's get our priorities straight.

Sincerely,
Jason Amirhadji

,

APPENDIX G EMAILS

From: [Robb Dooling](#)
To: [Tsai, Andrew](#)
Subject: We must not return to a dangerous four-lane crossing at the Little Falls Parkway intersection
Date: Friday, August 10, 2018 10:49:08 AM

Dear Andrew Tsai,

After a cyclist was struck and killed by a driver at the intersection of the Capital Crescent Trail and Little Falls Parkway in late 2016, the local parks service put in temporary measures to slow traffic in the area. So far this "road diet" has been successful, and the Maryland National Capital Park and Planning Commission (M-NCPPC) Parks Service is considering what elements to make permanent.

Unfortunately, some residents have expressed a desire to return to the former dangerous four-lane design, even though the county has called four-lane trail crossings like this one "high risk." Please do not allow this to happen. The county has endorsed Vision Zero, an initiative to eliminate all area traffic deaths by 2030. Reworking the road to prioritize cars and speed would violate one of its principles: "Human life takes priority over mobility and other objectives of the road system." Let's get our priorities straight.

Sincerely,
Robb Dooling

,

APPENDIX G EMAILS

From: [Meg Hobbins](#)
To: [Tsai, Andrew](#)
Subject: We must not return to a dangerous four-lane crossing at the Little Falls Parkway intersection
Date: Friday, August 10, 2018 10:47:10 PM

Dear Andrew Tsai,

After a cyclist was struck and killed by a driver at the intersection of the Capital Crescent Trail and Little Falls Parkway in late 2016, the local parks service put in temporary measures to slow traffic in the area. So far this "road diet" has been successful, and the Maryland National Capital Park and Planning Commission (M-NCPPC) Parks Service is considering what elements to make permanent.

Unfortunately, some residents have expressed a desire to return to the former dangerous four-lane design, even though the county has called four-lane trail crossings like this one "high risk." Please do not allow this to happen. The county has endorsed Vision Zero, an initiative to eliminate all area traffic deaths by 2030. Reworking the road to prioritize cars and speed would violate one of its principles: "Human life takes priority over mobility and other objectives of the road system."

Please support a safe two-lane (or completely closed to vehicle traffic) intersection so we can keep people safe. Vehicle mobility and other concerns can be enhanced through other measures - and should not come at the expense of vulnerable trail user safety.

thanks much,
Meg

Sincerely,
Meg Hobbins

,

APPENDIX G EMAILS

From: [Michael Lasky](#)
To: [Tsai, Andrew](#)
Subject: We must not return to a dangerous four-lane crossing at the Little Falls Parkway intersection
Date: Friday, August 10, 2018 6:15:24 PM

Dear Andrew Tsai,

After a cyclist was struck and killed by a driver at the intersection of the Capital Crescent Trail and Little Falls Parkway in late 2016, the local parks service put in temporary measures to slow traffic in the area. So far this "road diet" has been successful, and the Maryland National Capital Park and Planning Commission (M-NCPPC) Parks Service is considering what elements to make permanent.

Unfortunately, some residents have expressed a desire to return to the former dangerous four-lane design, even though the county has called four-lane trail crossings like this one "high risk." Please do not allow this to happen. The county has endorsed Vision Zero, an initiative to eliminate all area traffic deaths by 2030. Reworking the road to prioritize cars and speed would violate one of its principles: "Human life takes priority over mobility and other objectives of the road system." Let's get our priorities straight.

Sincerely,
Michael Lasky

,

APPENDIX G EMAILS

From: [Damon Luciano](#)
To: [Tsai, Andrew](#)
Subject: We must not return to a dangerous four-lane crossing at the Little Falls Parkway intersection
Date: Friday, August 10, 2018 1:25:40 PM

Dear Andrew Tsai,

I am a daily bike commuter and I ride my bicycle from home in Bethesda into downtown DC via the Capital Crescent Trail. I am writing to express concerns about some alternatives studied for the intersection of the trail and Little Falls Parkway.

At a minimum, please reject any four-lane alternative in this location. As a daily bicycle commuter using this facility twice each day, I am extremely grateful for the road diet. Having multiple lanes of vehicular traffic cross a crosswalk from each direction will always be a high risk design, even with a HAWK warning system. It is also unlikely that two-lanes in each direction could be justified by the traffic volumes. Some vehicles will stop at this crossing and the stopped vehicles will obstruct other drivers' view of the crossing, making pedestrians and others harder to see and extremely vulnerable.

I also want to repeat three basic principles. (1) Two wrongs don't make a right. [Many people accuse cyclists of bad behavior. Well, there is plenty of bad behavior. A few pedestrians on the trail have been seen carrying sticks (apparently threatening to shove them into cyclists' wheel spokes if cyclists get too close). Drivers have threatened me and one driver deliberately hit me at slow speed. Denying cyclists' access to safe infrastructure that suits their needs will only worsen this problem.]

(2) We can lament declining civility in our society, but we should never let it be an excuse to forsake safety.

(3) Above and beyond this, drivers who endanger others are protected by a steel cage but the people (s)he endangers are vulnerable.

It is also commendable that so many drivers show courtesy at this intersection (although a minority do not). Many outspoken individuals in the community claim the intersection should prioritize car movements and demand all pedestrian and bicycle traffic stop and proceed on a 'rotation' basis. These demands are apparently grounded in an interpretation of the law. They are also 100% contrary to how most drivers behave at this location. Cars usually stop and wait for all trail users, regardless of who has the right of way. Given this overwhelming norm is well-established, many trail users anticipate it, and drivers do not seem to know how else to deal with the intersection. There is a simple reason it is this way: people try to do what feels natural and right to them. For most drivers, what feels right at this junction is stopping and letting vulnerable road users pass, then proceeding only after others have cleared the right of way. Some other traffic pattern (stops signs, HAWK, etc.) creates confusion and adds to the risks because it forces drivers to something less natural than yielding to vulnerable road users. I think cars should just have to stop and wait for trail users.

The goal is to reduce fatalities and disabling injuries, which exact the highest costs on society (not to implement an abstract argument).

APPENDIX G EMAILS

Sincerely,
Damon C. Luciano

Sincerely,
Damon Luciano

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APPENDIX G EMAILS

From: [Gregory Oshel](#)
To: [Tsai, Andrew](#)
Subject: We must not return to a dangerous four-lane crossing at the Little Falls Parkway intersection
Date: Friday, August 10, 2018 10:20:57 AM

Dear Andrew Tsai,

After a cyclist was struck and killed by a driver at the intersection of the Capital Crescent Trail and Little Falls Parkway in late 2016, the local parks service put in temporary measures to slow traffic in the area. So far this "road diet" has been successful, and the Maryland National Capital Park and Planning Commission (M-NCPPC) Parks Service is considering what elements to make permanent.

Unfortunately, some residents have expressed a desire to return to the former dangerous four-lane design, even though the county has called four-lane trail crossings like this one "high risk." Please do not allow this to happen. The county has endorsed Vision Zero, an initiative to eliminate all area traffic deaths by 2030. Reworking the road to prioritize cars and speed would violate one of its principles: "Human life takes priority over mobility and other objectives of the road system." Let's get our priorities straight.

Sincerely,
Gregory Oshel

,

APPENDIX G EMAILS

From: [Eric Shepard](#)
To: [Tsai, Andrew](#)
Subject: We must not return to a dangerous four-lane crossing at the Little Falls Parkway intersection
Date: Friday, August 10, 2018 1:47:36 PM

Dear Andrew Tsai,

After a cyclist was struck and killed by a driver at the intersection of the Capital Crescent Trail and Little Falls Parkway in late 2016, the local parks service put in temporary measures to slow traffic in the area. So far this "road diet" has been successful, and the Maryland National Capital Park and Planning Commission (M-NCPPC) Parks Service is considering what elements to make permanent.

Unfortunately, some residents have expressed a desire to return to the former dangerous four-lane design, even though the county has called four-lane trail crossings like this one "high risk." Please do not allow this to happen. The county has endorsed Vision Zero, an initiative to eliminate all area traffic deaths by 2030. Reworking the road to prioritize cars and speed would violate one of its principles: "Human life takes priority over mobility and other objectives of the road system." Let's get our priorities straight.

Sincerely,
Eric Shepard

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APPENDIX G EMAILS

From: [Christopher Testa](#)
To: [Tsai, Andrew](#)
Subject: Capital Crescent Trail at Little Falls Parkway
Date: Friday, August 10, 2018 10:38:38 AM

Dear Mr. Tsai,

How are you? My name is Chris Testa. I live in Bethesda Row and frequently use the Capital Crescent Trail to visit Georgetown. I would like to see either a bridge or tunnel solution at this intersection. I think it would be best for both trail users and drivers. Thank you. Good luck with the project.

Sincerely,
Chris Testa

APPENDIX G EMAILS

From: [Thornton, Joseph](#)
To: [Tsai, Andrew](#)
Subject: Capital Crescent Trail Crossing at Little Falls Parkway
Date: Friday, August 10, 2018 11:14:22 AM

I wanted to express my strong support for the current road diet at this crossing. I cross on my bike twice a day on my way to work and back, and what was an extremely dangerous crossing has now been made much safer. Please don't give in to the vociferous drivers who only care about their own convenience, without regard for the safety of others.

Thanks,

Joe Thornton
4104 East West Hwy
Chevy Chase

APPENDIX G EMAILS

From: [Zachary Weinstein](#)
To: [Tsai, Andrew](#)
Subject: We must not return to a dangerous four-lane crossing at the Little Falls Parkway intersection
Date: Friday, August 10, 2018 12:53:34 PM

Dear Andrew Tsai,

After a cyclist was struck and killed by a driver at the intersection of the Capital Crescent Trail and Little Falls Parkway in late 2016, the local parks service put in temporary measures to slow traffic in the area. So far this "road diet" has been successful, and the Maryland National Capital Park and Planning Commission (M-NCPPC) Parks Service is considering what elements to make permanent.

Unfortunately, some residents have expressed a desire to return to the former dangerous four-lane design, even though the county has called four-lane trail crossings like this one "high risk." Please do not allow this to happen. The county has endorsed Vision Zero, an initiative to eliminate all area traffic deaths by 2030. Reworking the road to prioritize cars and speed would violate one of its principles: "Human life takes priority over mobility and other objectives of the road system." Let's get our priorities straight.

Sincerely,
Zachary Weinstein

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APPENDIX G EMAILS

From: [Alex Holt](#)
To: [Tsai, Andrew](#)
Subject: We must not return to a dangerous four-lane crossing at the Little Falls Parkway intersection
Date: Saturday, August 11, 2018 8:53:22 AM

Dear Andrew Tsai,

After a cyclist was struck and killed by a driver at the intersection of the Capital Crescent Trail and Little Falls Parkway in late 2016, the local parks service put in temporary measures to slow traffic in the area. So far this "road diet" has been successful, and the Maryland National Capital Park and Planning Commission (M-NCPPC) Parks Service is considering what elements to make permanent.

Unfortunately, some residents have expressed a desire to return to the former dangerous four-lane design, even though the county has called four-lane trail crossings like this one "high risk." Please do not allow this to happen. The county has endorsed Vision Zero, an initiative to eliminate all area traffic deaths by 2030. Reworking the road to prioritize cars and speed would violate one of its principles: "Human life takes priority over mobility and other objectives of the road system." Let's get our priorities straight.

Sincerely,
Alex Holt

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APPENDIX G EMAILS

From: [Peter Epley](#)
To: [Tsai, Andrew](#)
Subject: We must not return to a dangerous four-lane crossing at the Little Falls Parkway intersection
Date: Monday, August 13, 2018 11:55:28 AM

Dear Andrew Tsai,

After a cyclist was struck and killed by a driver at the intersection of the Capital Crescent Trail and Little Falls Parkway in late 2016, the local parks service put in temporary measures to slow traffic in the area. So far this "road diet" has been successful, and the Maryland National Capital Park and Planning Commission (M-NCPPC) Parks Service is considering what elements to make permanent.

Please consider rerouting the trail to use one of the nearby intersections to create a more safer crossing. While I believe that reverting the road to its original configuration will not help, I do not think leaving the current crossing in the current location makes sense. There is too much road and trail traffic to allow both to continue unchanged.

Sincerely,
Peter Epley

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APPENDIX G EMAILS

From: [Matt Vanderwerff](#)
To: [Tsai, Andrew](#)
Subject: We must not return to a dangerous four-lane crossing at the Little Falls Parkway intersection
Date: Monday, August 13, 2018 10:20:48 AM

Dear Andrew Tsai,

After a cyclist was struck and killed by a driver at the intersection of the Capital Crescent Trail and Little Falls Parkway in late 2016, the local parks service put in temporary measures to slow traffic in the area. So far this "road diet" has been successful, and the Maryland National Capital Park and Planning Commission (M-NCPPC) Parks Service is considering what elements to make permanent.

Unfortunately, some residents have expressed a desire to return to the former dangerous four-lane design, even though the county has called four-lane trail crossings like this one "high risk." Please do not allow this to happen. The county has endorsed Vision Zero, an initiative to eliminate all area traffic deaths by 2030. Reworking the road to prioritize cars and speed would violate one of its principles: "Human life takes priority over mobility and other objectives of the road system." Let's get our priorities straight.

Sincerely,
Matt Vanderwerff

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APPENDIX G EMAILS

From: [Janet Adrian](#)
To: [Tsai, Andrew](#)
Cc: [Anderson, Casey](#)
Date: Wednesday, August 15, 2018 4:47:12 PM

I live on Highland Drive close to the intersection of the Capital Crescent Trail and Little Falls Parkway, so I am concerned about having a SAFE solution for motorists and all trail users at that point.

An immediate solution would relocate the trail and crosswalk to the Arlington Rd. traffic signal. The traffic signal should remain red for both Arlington Rd drivers and Little Fall Parkway drivers for, say, 45 seconds to allow trail users to cross. This pattern is being used successfully at Bethesda Ave. and Arlington Rd.

Although this solution could potentially be a permanent one, a more ideal answer is a bridge over the parkway at the current intersection, so motorists and trail users would not have to interact. There may well be a significant cost to implement this approach but it should at least be given serious analysis.

In either case, it's extremely important to have free flowing traffic on the parkway because population and business growth in Bethesda and the future Westbard complex will generate major increases in car usage. If cars can move freely, without having to suddenly stop for trail users, the chances of an accident will be diminished.

Who ever came up with the current crazy system will be responsible for the next Accident at this now dangerous intersection. I realize there was a fatal biking accident and the current situation is in response to that. The biker as i understand it, was on a recumbent bike which is a hazard all by itself as it is not readily seen by motorists. The solution you have adopted is overkill at best and a Definite safety hazard to pedestrians and motorists alike.

Please go back to the original 2 lane road until you can come up with a better plan.
Janet Adrian

Sent from my iPhone

APPENDIX G EMAILS

APPENDIX G EMAILS

From: [David Barron](#)
To: [Tsai, Andrew](#)
Cc: [Anderson, Casey](#); Councilmember.Riemer@montgomerycountymd.gov; [Patricia Johnson](#); [Jenny Sue](#)
Subject: Kenwood Citizens Association's Position Re: Little Falls Parkway @ Little Falls Pkwy
Date: Wednesday, August 15, 2018 4:32:22 PM
Attachments: [KCA Board Letter To Mr. Tsai.pdf](#)

Mr. Tsai-

Good afternoon. Attached, please find a letter on behalf of The Kenwood Citizens Association's Board regarding our position concerning Little Falls Parkway and The Crescent Trail. Members of our community use Little Falls Parkway (LFP) and The Capital Crescent Trail every day. Per our letter - we have offered up an immediate solution that addresses the need to open up LFP back to two lanes each direction while servicing the trail users. Again we believe the best short term solution is to move the trail crossing down to Arlington Road. This will allow equal time between motorists and trail users to cross Little Falls Parkway in a safe manner. If our collective goal is "Vision Zero" then we believe this short term solution The Kenwood Citizens Association is recommending for Little Falls Parkway at the Capital Crescent Trail is the best.

Please let me know if you have any questions. We are always happy to give feedback.

Sincerely,

David Barron
President, Kenwood Citizens Association

APPENDIX G EMAILS

From: [robert l bein](#)
To: [Tsai, Andrew](#)
Cc: [Anderson, Casey](#); Councilmember.Riemer@montgomerycountymd.gov
Subject: the dangerous intersection of Capital Crescent Trail and Little Falls Parkway
Date: Wednesday, August 15, 2018 2:36:43 PM

Dear Mr. Tsai,

I live on Brookside Drive, close to the intersection of the Capital Crescent Trail and Little Falls Parkway, so I am concerned about having a SAFE solution for motorists and all trail users at that point.

What can be done?

An immediate solution would relocate the trail and crosswalk to the Arlington Rd. traffic signal. The traffic signal should remain red for both Arlington Rd drivers and Little Fall Parkway drivers for, say, 45 seconds to allow trail users to cross. This pattern is being used successfully at Bethesda Ave. and Arlington Rd.

Although this solution could potentially be a permanent one, a more ideal answer is a bridge over the parkway at the current intersection, so motorists and trail users would not have to interact. There may well be a significant cost to implement this approach but it should at least be given serious analysis.

In either case, it's extremely important to have free flowing traffic on the parkway because population and business growth in Bethesda and the future Westbard complex will generate major increases in car usage. If cars can move freely, without having to suddenly stop for trail users, the chances of an accident will be diminished.

This is a very serious problem and I hope the Parks Dept can begin to implement a solution soon.

Thank you.

Robert Bein

APPENDIX G EMAILS

From: [harold.pfohl](#)
To: [Tsai, Andrew](#)
Cc: [Frank, Andrew](#)
Subject: Capital Crescent Trail - Little Falls Parkway Intersection
Date: Wednesday, August 15, 2018 8:14:22 AM
Attachments: [CAPITAL CRESCENT TRAIL CROSSING - LITTLE FALLS PARKWAY.pdf](#)

Mr. Tsai,

In reviewing the alternatives that you presented to us during our July meeting, we've arrived at a strong preference as outlined in the attached letter to you. Please let us know if you have any questions.

Thanks again, and we look forward to working with you as this proceeds.

Regards,

Harold Pfohl, Chair
Citizens Coordinating Committee
on Friendship Heights

APPENDIX G EMAILS

From: [Jgoodwinb](#)
To: [Tsai, Andrew](#); [Anderson, Casey](#); Councilmember.Riemer@montgomerycountymd.gov
Subject: Little Falls Crossing
Date: Thursday, August 16, 2018 9:48:15 AM

Dear Sirs,

I live on Kennedy Drive close to the intersection of the Capital Crescent Trail and Little Falls Parkway; therefore, I am very concerned about having a safe solution for motorists and all trail users at that point that preserves the current use of Little Falls Parkway.

An immediate solution would be to relocate the trail and crosswalk to the Arlington Rd. traffic signal (or alternatively the Hillandale signal). The traffic signal should remain red for both Arlington Rd drivers and Little Falls Parkway drivers for, say, 45 seconds to allow trail users to cross. This pattern is being used successfully at Bethesda Ave. and Arlington Rd.

Although this solution could potentially be a permanent one, another answer would a bridge over the parkway at the current intersection so that motorists and trail users would not have to interact. There may well be a significant cost to implement this approach, but it should at least be given serious analysis if bicycle and pedestrian traffic keeps increasing and police continue to fail to enforce the stop signs applicable to all users.

Any other solution would not be acceptable because it would divert traffic into neighborhoods and increase the risk of auto, bicycle and pedestrian injuries in the neighborhoods.

In addition, it is extremely important to have free flowing traffic on Little Falls Parkway because population and business growth in Bethesda and the future Westbard complex will generate major increases in automobile and other traffic. If cars can move freely, without having to suddenly stop for trail users, the chances of an accident will be diminished.

Sincerely,

Jackson Bennett

APPENDIX G EMAILS

From: [Garrett Hennigan](#)
To: [Tsai, Andrew](#)
Cc: [WABA Advocacy](#); [Peter Gray](#)
Subject: Comments on Capital Crescent Trail Crossing at Little Falls Parkway
Date: Thursday, August 16, 2018 2:56:36 PM
Attachments: [WABA Letter re Capital Crescent Trail Little Falls crossing.pdf](#)

Mr. Tsai,

Please see the attached comments from the Washington Area Bicyclists Association on the proposed concepts for the Capital Crescent Trail Crossing at Little Falls Parkway. We look forward to the next steps in the planning process and hope you will keep us in the loop on future decisions and meetings.

Please contact Peter Gray at peter@waba.org with questions and follow-up. Thank you for the opportunity to comment on this important project,

Garrett Hennigan | Community Organizer
Washington Area Bicyclist Association

Phone: 202-518-0524 x210

Mobile: 202-656-3078

Email: garrett.hennigan@waba.org

Like us: [Facebook](#) | Follow us: [Twitter](#)

Help make your Washington Area more bikeable. [Get started here.](#)

APPENDIX G EMAILS

From: [Zachary Weinstein](#)
To: [Tsai, Andrew](#)
Subject: Capital Crescent Trail at Little Falls Parkway
Date: Thursday, August 16, 2018 3:20:15 PM
Attachments: [WABA Letter re Capital Crescent Trail Little Falls crossing.pdf](#)

Good afternoon Mr. Tsai,

I fully support making the current road diet at the crossing of the Capital Crescent Trail and Little Falls Parkway permanent. Restricting Little Falls Parkway to 1 lane in each direction has improved the safety of the crossing and the county must not knowingly make an intersection more dangerous. Montgomery County has promised to make biking a safe and viable transportation mode in the Bike Master Plan and Vision Zero plan. Please do not let this easily implementable solution go to waste.

I support the attached WABA flyer, which provides a more comprehensive statement on the issue. Thank you for your hard work improving bicycle safety in Montgomery County.

Sincerely,
Zach Weinstein

APPENDIX G EMAILS

From: [Kay Stevens](#)
To: [Tsai, Andrew](#); [Anderson, Casey](#); Councilmember.Riemer@montgomerycountymd.gov
Subject: Crossing of Capital Crescent Trail and Little Falls Parkway
Date: Friday, August 17, 2018 10:50:46 PM

I am writing to express my opinion on the intersection of the Capital Crescent Trail and Little Falls Parkway. I have lived on Chamberlin Avenue in the Kenwood neighborhood for over 20 yrs and I grew up nearby on Goldsboro Rd/Bradley Blvd. I am a retired Montgomery County employee and spent the largest part of my career as a planner in Montgomery County's Department of Transportation.

I think everyone recognizes that the current solution of narrowing the lanes on Little Falls Parkway from 4 to 2 upon the approach to the Trail is unexpected by motorists and hazardous, especially at night. It also slows traffic to a crawl, which isn't necessary when there is a better solution.

I am in favor of moving the Capital Crescent Trail to cross at the intersection of Little Falls Parkway and Arlington Rd. Appropriate traffic signals with adequate "Walk" time for both hikers and bikers would be important, as well as street lighting for visibility. The suggestion made of requiring all vehicles in all directions to stop for a short period of time to allow trail users to cross without car-turning conflicts is a good idea that warrants consideration. I do remember that at the public meeting the planners showed an option of a pedestrian/biker bridge over Little Falls Parkway as one alternative to resolve the safety problem, but I would certainly recommend first trying the less expensive at-grade hiker/biker crossing at Arlington Rd.

Thank you for the opportunity to provide my comments.

Kay B. Stevens
5331 Chamberlin Ave.
Chevy Chase, MD 20815
kaystevens@aol.com

APPENDIX G EMAILS

From: [Anthony Camilli](#)
To: [Tsai, Andrew](#)
Subject: We must not return to a dangerous four-lane crossing at the Little Falls Parkway intersection
Date: Sunday, August 19, 2018 11:21:00 PM

Dear Andrew Tsai,

After a cyclist was struck and killed by a driver at the intersection of the Capital Crescent Trail and Little Falls Parkway in late 2016, the local parks service put in temporary measures to slow traffic in the area. So far this "road diet" has been successful, and the Maryland National Capital Park and Planning Commission (M-NCPPC) Parks Service is considering what elements to make permanent.

Unfortunately, some residents have expressed a desire to return to the former dangerous four-lane design, even though the county has called four-lane trail crossings like this one "high risk." Please do not allow this to happen. The county has endorsed Vision Zero, an initiative to eliminate all area traffic deaths by 2030. Reworking the road to prioritize cars and speed would violate one of its principles: "Human life takes priority over mobility and other objectives of the road system." Let's get our priorities straight.

Sincerely,
Anthony Camilli

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APPENDIX G EMAILS

From: [John Oliver \(US - ASR\)](#)
To: [Tsai, Andrew](#); [Anderson, Casey](#); Councilmember.Riemer@montgomerycountymd.gov
Subject: Capital Crescent Trail Crossing
Date: Thursday, August 30, 2018 5:11:22 PM

Hello,

I live on Kennedy Drive, close to the intersection of the Capital Crescent Trail and Little Falls Parkway. My wife and I are frequent users of the Trail and my wife was recently nearly hit by a passing motorist. I will briefly relay the story to help bring this issue to life.

My wife was walking on the trail when she came to the Little Falls Parkway crossway. She stopped and an oncoming motorist also stopped. At the same time as my wife stopped, a bicyclist was approaching the intersection on the trail from the same direction as my wife. Since the car stopped, the bicyclist proceeded through the intersection and my wife began to cross the intersection. Once the bicyclist cleared the intersection, the motorist (presumably with their eyes on the bicyclist) began to move forward in an accelerated matter (presumably to clear the intersection swiftly). It was not until my wife screamed that the motorist slammed on their brakes and realized that she was there too. My response to this was people were generally doing what they were supposed to but a simple mistake caused by all of the activity around the intersection could have taken a very bad turn. Therefore, I am concerned about having a SAFE solution for motorists and all trail users at that point.

I would advocate a permanent such as a bridge over the parkway or the road at the current intersection, so motorists and trail users would not have to interact. There may well be a significant cost to implement this approach but it should at least be given serious analysis.

In either case, it's extremely important to have free flowing traffic on the parkway because population and business growth in Bethesda and the future Westbard complex will generate major increases in car usage. If cars can move freely, without having to suddenly stop for trail users, the chances of an accident will be diminished.

Sincerely, John Oliver, 6609 Kennedy Drive, Chevy Chase, MD 20815

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John M. Oliver

PwC | Financial Services Partner Office: [703-918-3646](tel:703-918-3646) | Mobile: [202-262-1132](tel:202-262-1132) | Email: john.m.oliver@pwc.com
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APPENDIX G EMAILS

From: [kristin.roesser](mailto:kristin.roesser@montgomerycountymd.gov)
To: Tsai, Andrew; Anderson, Casey; councilmember.reimer@montgomerycountymd.gov
Subject: Capital Crescent Trail and Little Falls Parkway
Date: Monday, September 10, 2018 11:18:20 AM

Dear Mr. Tsai:

I am a resident of Kenwood and have just learned the Kenwood Citizens Association recently sent a letter (pasted below) outlining some potential solutions designed to improve safety around the intersection of the Capital Crescent Trail and Little Falls Parkway. While I share their goal, I would urge you to consider other less costly and less intrusive means for achieving it. In particular, I believe a bridge over the parkway is excessive and does not merit serious consideration.

I use the trail on a regular basis and have observed that cyclists rarely stop before the intersection even though signs tell them to do so. If bikers and pedestrians obeyed the signs, the risk of an accident would diminish considerably.

Before exploring expensive and disruptive plans to relocate the trail and crosswalk or build a bridge (which would be unsightly and take years to complete), I would urge you to consider responses that are less draconian and draw on common sense. For example, issuing tickets to bikers that run the signs, or drivers that ignore the crosswalk, could make a major impact.

Thank you for your attention to this important matter.

Sincerely,

Kristin Roesser

6404 Kennedy DR

Chevy Chase, MD 20815

Dear Mr. Tsai:

The Kenwood Citizens Association, which represents more than 230 households, has a keen interest in developing a SAFE solution to the intersection of the Capital Crescent Trail and Little Falls Parkway. Because of our proximity, many of our residents are often on the trail or on the parkway.

This trail, which is so widely used, is not a commuter trail. It is specifically a recreational trail not only for bikers, but also for walkers and runners. All who use this trail need to be safe at that

APPENDIX G EMAILS

intersection.

The current temporary approach using bollards is actually dangerous: the bollards are hard to see at night and the reduction to one lane is unexpected, forcing drivers to move too quickly to that single lane. Furthermore, cyclists rarely stop before the intersection even though signs tell them to do so.

The immediate solution we see would relocate the trail and crosswalk to the Arlington Rd. traffic signal. The traffic signal should remain red for both Arlington Rd drivers and Little Fall Parkway drivers for, say, 45 seconds to allow trail users to cross. This pattern is being used successfully at Bethesda Ave. and Arlington Rd.

Although this solution could potentially be a permanent one, it's clear to us that a more ideal answer is a bridge over the parkway at the current intersection, so motorists and trail users would not have to interact. We realize there may be a significant cost to implement this approach but it should at least be given serious analysis.

In either case, it's extremely important to have free flowing traffic on the parkway because the population and business growth in Bethesda and the future Westbard complex will generate major increases in car usage. If cars can move freely, without having to suddenly stop for trail users, the chances of an accident will be diminished.

Thank you for your time and attention to this important matter.

Sincerely,

David Barron, President
On behalf of the KCA Board

APPENDIX G EMAILS

From: [Warren Chan](#)
To: [Tsai, Andrew](#)
Subject: Re: Little Falls and CCT Crossing
Date: Thursday, September 20, 2018 10:47:38 PM

Re: Little Falls and CCT Crossing (the “Crossing”)

To Andrew ,

Thank you for organizing the June 13, 2018 community meeting (the “Meeting”) to discuss options for the Little Falls Parkway and Capital Crescent Trail Crossing which I attended.

I am writing as a Bethesda resident and daily user of the CCT to urge MC Parks and MC DOT to keep the road diet currently in place at the Crossing and ideally improved it. I set out my reasoning for this advocacy below.

1. The Crossing is unique in Montgomery County (MC) as the number of people on the trail (on foot or bicycles, etc.) out number the people using the motorway in cars. As a consequence, more priority should be given to keeping users of the CCT safe.
2. One death is too many and restoring the traffic lanes at the Crossing will certainly lead to another fatality as a result of poor sightlines due to multiple traffic lanes. MC DOT have recognized that the same problem exists at the southbound junction of Woodmont Avenue and Wisconsin Avenue near NIH and are implementing a road diet at that junction.
3. MC Parks statistics presented at the Meeting show the dramatic reduction of incidents as a result of the road diet (300% reduction in driver crashes and 600% reduction in bike crashes). To reverse the diet would be morally wrong and negligent in light of these known facts.
4. If the road diet was in place, Mr. Gaylin would have been seen and would be alive. Let’s not risk another death. No one should die just trying to get from point A to point B on foot or on a bicycle.

I thought the 12 alternatives proposed at the meeting were a very comprehensive review of recommendations for possible improvements at the Crossing. I would highly support Alternative #9 to #12 with my preferences in reverse order (i.e. #12 is preferred, etc.).

If allowed to dream, (1). Could a protected bike lane with a pedestrian trail be installed for the entire length of the LFP making access to the CCT as well as better bicycle and pedestrian access for the neighborhood to the pool and playground?; (2). Could all or part of the LFP be closed on summer weekends until dusk to vehicles to allow recreational use? Areas for young kids to learn how to bicycle or older kids to skateboard could be set up during these closures.

Finally, I attach a photo taken at the Crossing on September 8, 2018. We should improve the safety of the Crossing for all CCT users at this important and unique intersection.

As the Washington Post stressed in a recent article, [pedestrian deaths are soaring nationwide](#) “largely because of the nation’s appetite for fast arterial roads in urban-suburban areas.”

MC has had 11 pedestrian deaths so far in 2018. Let’s not further add to this statistic due to an another accident at the Crossing.

Thanks for the chance to comment.

Best regards, Warren Chan

APPENDIX G EMAILS



On Sep 10, 2018, at 05:17, Tsai, Andrew <andrew.tsai@montgomeryparks.org> wrote:

Good morning Mr. Chan,
You can send any comments directly to me.

Thanks
Andrew

Andrew Tsai, P.E.
Project Manager
Park Development Division - Montgomery Parks
Maryland-National Capital Parks and Planning Commission (M-NCPPC)
9500 Brunett Avenue
Silver Spring, MD 20901
Andrew.Tsai@montgomeryparks.org
Office: (301) 495-2508

-----Original Message-----
From: Warren Chan <warrenchan@yahoo.com>

APPENDIX G EMAILS

Sent: Sunday, September 9, 2018 3:45 PM
To: Tsai, Andrew <andrew.tsai@montgomeryparks.org>
Subject: Little Falls and CCT Crossing - Open Town Hall Forum

Andrew,

Thanks for organizing the June 13, 2018 community meeting on this topic.

I was hoping to leave comments on this topic in the Open Town Hall forums as suggested in your presentation but I couldn't find this topic.

Do you still plan to set up a forum topic on this matter? If not, how should the community share comments? Should we send them to you?

Thanks...Warren Chan

APPENDIX G EMAILS

From: [David Barron](#)
To: [Tsai, Andrew](#); [Anderson, Casey](#); Councilmember.Riemer@montgomerycountymd.gov
Cc: [Jenny Sue Dunner](#); [Pat Johnson](#); [robert I bein](#)
Subject: WE VOTED! Kenwood Citizens Association's Vote Re: Little Falls Parkway @ Little Falls Pkwy
Date: Tuesday, October 9, 2018 2:52:58 PM
Attachments: [KCA Members Vote Yes to Moving CCT to Arlington Rd.docx](#)

Mr. Tsai, Mr. Anderson & Mr. Riemer-

Good afternoon. On August 15th, 2018, I submitted a letter on behalf of The Kenwood Citizens Association's (KCA) Board with our recommendation for that easiest, most cost effective and safest solution for the Little Falls Parkway (LFP) @ The Capital Crescent Trail (CCT). Our recommendation is to move the CCT down to the intersection at Arlington Road and restoring all lanes on the parkway for motorized vehicles. This will allow equal time for CCT users and motor vehicles on LFP. Because our collective goal is "Vision Zero" we believe this recommendation to be the best compromise. Let's be honest - it worked at Connecticut Avenue & CCT!

As we head into tonight's meeting at B-CC High School to determine the future of the CCT @ LFP, The Kenwood Citizens Association wanted to let you all know that we asked our members just a few days ago to vote on whether we move the CCT to Arlington Road or leave the intersection in its current state. The results were overwhelmingly in favor of KCA's recommendation to move the CCT to Arlington Rd. Attached, please find a list containing almost 180 KCA members who voted in favor of moving the CCT to Arlington Rd. and thus opening back up all traffic lanes - including allowing both southbound Arlington Road lanes the ability to turn left on LFP. Only 10 members of our association voted to leave the intersection in its current state.

I have been a resident of Chevy Chase almost my entire life. Anyone who lives here knows that the Little Falls Parkway is the main artery that moves traffic from Bethesda to River Road, Mass Ave, The Beltway, Westbard and beyond. The temporary traffic diet has clogged this integral parkway and thus pushed traffic through Kenwood thanks to apps like Waze. This increase in cut-through traffic continues to cause major problems because drivers are blowing through STOP signs as well as blowing through Do Not Enter signs. By allowing equal time at the intersection of Arlington Road and CCT, we will be able to achieve the balance we are all looking for while keeping our residents safe!

If you have any questions regarding KCA's recommendation and our overwhelming support for moving the CCT to Arlington Road, please feel free to call me 917-439-4576 or email me davidbarron13@gmail.com.

Thank you for your consideration and we look forward to seeing you tonight!
Sincerely,

David Barron
President, Kenwood Citizens Association

On Aug 15, 2018, at 4:31 PM, David Barron <davidbarron13@gmail.com>

APPENDIX G EMAILS

wrote:

Mr. Tsai-

Good afternoon. Attached, please find a letter on behalf of The Kenwood Citizens Association's Board regarding our position concerning Little Falls Parkway and The Crescent Trail. Members of our community use Little Falls Parkway (LFP) and The Capital Crescent Trail every day. Per our letter - we have offered up an immediate solution that addresses the need to open up LFP back to two lanes each direction while servicing the trail users. Again we believe the best short term solution is to move the trail crossing down to Arlington Road. This will allow equal time between motorists and trail users to cross Little Falls Parkway in a safe manner. If our collective goal is "Vision Zero" then we believe this short term solution The Kenwood Citizens Association is recommending for Little Falls Parkway at the Capital Crescent Trail is the best.

Please let me know if you have any questions. We are always happy to give feedback.

Sincerely,

David Barron
President, Kenwood Citizens Association

<KCA Board Letter To Mr. Tsai.pdf>

APPENDIX G EMAILS

From: [robert l bein](#)
To: [Riemer's Office, Councilmember](#); [David Barron](#)
Cc: [Jenny Sue Dunner](#); [Pat Johnson](#); [Tsai, Andrew](#); [Anderson, Casey](#)
Subject: Re: WE VOTED! Kenwood Citizens Association's Vote Re: Little Falls Parkway @ Little Falls Pkwy
Date: Wednesday, October 10, 2018 10:28:25 PM

Several members of the Kenwood Citizens Association (KCA), including me and David Barron (KCA President), attended the county sponsored meeting Tuesday night to hear the alternatives being considered for resolving the dangerous intersection of the Capital Crescent Trail and Little Falls Parkway.

We were shocked and dismayed as we spoke to Andrew Tsai of the Parks Dept and a representative of the DOT because we drew the strong feeling they had already decided on a solution, one that did not consider the impact of the enormous growth in traffic that will occur in the next few years resulting from the Westbard development and new construction in Bethesda.

We got the impression that the Parks Dept felt compelled to emphasize the “park” part of Little Falls Parkway as it was originally conceived. Nice idea, but it doesn’t recognize how times have changed and how the Parkway is really a highway now, just like Rock Creek Parkway. It’s a way for people to get into and out of Bethesda.

The proposal they are focusing on continues the reduction to one lane in each direction on Little Falls Parkway, and goes so far as to reduce the number of lanes on Arlington Rd where it crosses Little Falls Parkway! This is a recipe for gridlock as the traffic increases, and it will increase. Yes, by all means encourage people to ride bikes to work but how effective will that be on the snowy/icy days, the rainy days, and days when the temperature exceeds 90 degrees? And the vast majority of the aging population in our area will be driving cars, not riding bikes.

The letter sent to you, Mr. Tsai and Mr. Anderson outlines a better solution that provides safety for all and keeps the traffic moving, taking into account future growth.

One other inexpensive possibility we pointed out Tuesday night is to simply put a traffic light at the current crosswalk, with lag time for pedestrians and cyclists to cross the parkway. That new traffic light just has to be sequenced properly with the lights at Arlington Rd and Hillandale Rd to avoid backups.

The way in which this problem is solved is going to affect the surrounding neighborhoods—including downtown Bethesda—seriously so a realistic and practical approach recognizing future growth is essential.

Can you help us achieve this goal?

Thank you,

Robert Bein

APPENDIX G EMAILS

On 10/10/2018 9:12 PM, Riemer's Office, Councilmember wrote:

Thank you very much for sharing the results of your survey in the Community. Your participation is essential. Sincerely, Hans Riemer

—

Hans Riemer

Tue, Oct 9 at 2:53 PM, <davidbarron13@gmail.com> wrote:

Mr. Tsai, Mr. Anderson & Mr. Riemer-

Good afternoon. On August 15th, 2018, I submitted a letter on behalf of The Kenwood Citizens Association's (KCA) Board with our recommendation for that easiest, most cost effective and safest solution for the Little Falls Parkway (LFP) @ The Capital Crescent Trail (CCT). Our recommendation is to move the CCT down to the intersection at Arlington Road and restoring all lanes on the parkway for motorized vehicles. This will allow equal time for CCT users and motor vehicles on LFP. Because our collective goal is "Vision Zero" we believe this recommendation to be the best compromise. Let's be honest - it worked at Connecticut Avenue & CCT!

As we head into tonight's meeting at B-CC High School to determine the future of the CCT @ LFP, The Kenwood Citizens Association wanted to let you all know that we asked our members just a few days ago to vote on whether we move the CCT to Arlington Road or leave the intersection in its current state. The results were overwhelmingly in favor of KCA's recommendation to move the CCT to Arlington Rd. Attached, please find a list containing almost 180 KCA members who voted in favor of moving the CCT to Arlington Rd. and thus opening back up all traffic lanes - including allowing both southbound Arlington Road lanes the ability to turn left on LFP. Only 10 members of our association voted to leave the intersection in its current state.

I have been a resident of Chevy Chase almost my entire life. Anyone who lives here knows that the Little Falls Parkway is the main artery that moves traffic from Bethesda to River Road, Mass Ave, The Beltway, Westbard and beyond. The temporary traffic diet has clogged this integral parkway and thus pushed traffic through Kenwood thanks to apps like Waze. This increase in cut-through traffic continues to cause major problems because drivers are blowing through STOP signs as well as blowing through Do Not Enter signs. By

APPENDIX G EMAILS

allowing equal time at the intersection of Arlington Road and CCT, we will be able to achieve the balance we are all looking for while keeping our residents safe!

If you have any questions regarding KCA's recommendation and our overwhelming support for moving the CCT to Arlington Road, please feel free to call me 917-439-4576 or email me davidbarron13@gmail.com.

Thank you for your consideration and we look forward to seeing you tonight!

Sincerely,

David Barron
President, Kenwood Citizens Association

On Aug 15, 2018, at 4:31 PM, David Barron
<davidbarron13@gmail.com> wrote:

Mr. Tsai-

Good afternoon. Attached, please find a letter on behalf of The Kenwood Citizens Association's Board regarding our position concerning Little Falls Parkway and The Crescent Trail. Members of our community use Little Falls Parkway (LFP) and The Capital Crescent Trail every day. Per our letter - we have offered up an immediate solution that addresses the need to open up LFP back to two lanes each direction while servicing the trail users. Again we believe the best short term solution is to move the trail crossing down to Arlington Road. This will allow equal time between motorists and trail users to cross Little Falls Parkway in a safe manner. If our collective goal is "Vision Zero" then we believe this short term solution The Kenwood Citizens Association is recommending for Little Falls Parkway at the Capital Crescent Trail is the best.

Please let me know if you have any questions. We are always happy to give feedback.

APPENDIX G EMAILS

Sincerely,

David Barron
President, Kenwood Citizens Association

<KCA Board Letter To Mr. Tsai.pdf>

APPENDIX G EMAILS

From: [C.Nguyen](#)
To: [Kines, Charles](#); kblackmon@bethesda.org; [Frank, Andrew](#); [Tsai, Andrew](#); [Devlin, Jeff](#); adriane.clutter@montgomerycountymd.gov; mmaffeo@flourateeter.com; [Williams, Derrick](#); dana.stroman@hallmarkjb@gmail.com; jenniferparks@comcast.net; qiangwan@live.com; jerryparks@comcast.net; keithlawson1@yahoo.com; [Kelli Lawson](#); bobminai@aol.com; [Vu Bui](#); [Malka Ostchega](#); [Karla Lopez](#)
Cc: hallmarkjb@gmail.com; jenniferparks@comcast.net; qiangwan@live.com; jerryparks@comcast.net; keithlawson1@yahoo.com; [Kelli Lawson](#); bobminai@aol.com; [Vu Bui](#); [Malka Ostchega](#); [Karla Lopez](#)
Subject: Bethesda Barracuda's Swim Team Concerns regarding changes to Little Falls Parkway
Date: Friday, October 19, 2018 6:02:38 PM

Good afternoon Montgomery County Staff,

It was a pleasure to meet you during the October 9th meeting to discuss options for the Capital Crescent Trail crossing at Little Falls Parkway. I am part of a group of families responsible for organizing summer swim meets for the Bethesda Barracudas Swim Team at the Bethesda Outdoor County Pool located at Little Falls Parkway and Hillandale Road.

Our team is one of the largest teams in the Montgomery County Swim League (MCSL). Earlier this year, between May and July, our team of over 200 swimmers, ranging from age 5 to age 18, practiced and raced at various times in the morning and the evening at the Bethesda Outdoor County Pool.

Following up on our discussion, I would like to highlight certain issues raised repeatedly by families who participate in the summer swim meets held at the Bethesda Outdoor County Pool. These meets are held regularly on Wednesday evenings ([5pm-9pm](#)) and Saturday mornings ([8am-11am](#)) over five weeks, beginning around mid-June and ending around mid-July. Each week, our team swims against another team in Division A of MCSL.

One issue concerns the challenge of finding parking whenever there is a swim meet at the Bethesda Outdoor County Pool. Although there is a second parking lot located at Little Falls Parkway and Arlington Road (in addition to the first parking lot located at Little Falls Parkway and Hillandale Road), families still complain about the lack of available parking spaces. If changes to the Capital Crescent Trail crossing result in reducing the already insufficient number of parking spaces, that would present an even greater challenge to families.

A second issue concerns the safety of young swimmers and parents walking from/to the first parking lot located at Little Falls Parkway and Hillandale Road. If the entrance/exit on Little Falls Parkway to this parking lot were to be removed as part of changes to the Capital Crescent Trail crossing, that would drastically affect the flow of traffic and, as a result, negatively impact the safety of pedestrians.

A third issue concerns the safety of pedestrians walking from/to the second parking lot located at Little Falls Parkway and Arlington Road. Additional lighting and traffic calming measures would be helpful to alert drivers to the presence of pedestrians.

I imagine that your office is very busy, so your consideration of these issues is greatly appreciated.

I welcome any advice or assistance that can be provided.

Thank you,
Chi Nguyen

APPENDIX G EMAILS

Bethesda Barracudas Parent Organizer

APPENDIX G EMAILS

From: [Oriel](#)
To: [Tsai, Andrew](#)
Cc: Councilmember.Rierner@montgomerycountymd.gov
Subject: Letter regarding the intersection of Little Falls parkway (LFP) and the Capital Crescent Trail (CCT)
Date: Tuesday, October 23, 2018 11:41:38 AM
Attachments: [Letter from Kenwood Forest II Condominiums to MC Dept. of Parks.pdf](#)

Dear Mr. Andrew Tsai:

Please find attached a letter from Kenwood Forest II Condominiums related to the intersection of Little Falls Parkway (LLP) and the Capital Crescent Trail (CCT). Also, I sent you a hard copy of this letter via first class mail and cc to Mr. Hans Rierner, Montgomery County Council President.

Thank you for your consideration.

Oriel Jimenez: General Manager
Kenwood Forest II Condominiums
6658A Hillandale Road
Chevy Chase, MD 20815
(301) 657-2683
Kenwoodforst2@verizon.net

APPENDIX G EMAILS

From: [Bob Yetvin](#)
To: [Tsai, Andrew](#)
Subject: CCT crossing at Little Falls Parkway
Date: Friday, November 2, 2018 12:42:22 PM

Hi Andrew,

I attended the Oct 9th meeting at B-CC High School and have been both riding my bike and driving through the Capital Crescent Trail crossing at Little Falls Parkway for almost 20 years.

There is **no doubt in my mind that the current road diet of reducing Little Falls Parkway to one lane has increased biker and driver safety. The road diet should be formalized and continue.** Building a pedestrian bridge or tunnel would certainly increase safety but doesn't seem like a cost effective solution. Also, it makes no sense to move the bike crossing to traffic lights at Arlington or Hillandale Roads. The bike trail is too busy to try to divert pedestrians and bike riders to other crossings.

Thank you for all your work on this effort.

Sincerely,

Bob Yetvin
4911 Chevy Chase Blvd.
Chevy Chase, Md

APPENDIX G EMAILS

From: [Yohannes Bennehoff](#)
To: [Tsai, Andrew](#)
Subject: I support Alternative A at the Little Falls Parkway intersection
Date: Wednesday, November 7, 2018 12:16:06 PM

Dear Andrew Tsai,

Two years ago, a cyclist was tragically struck and killed by a driver at the intersection of the Capital Crescent Trail (CCT) and Little Falls Parkway. Reducing the car lanes to one each way and lowering the speed limit has worked incredibly well, and Montgomery County should make the changes permanent.

Since these changes were introduced, there has been a 67% reduction in crashes without any fatalities. Traffic has only decreased here by 3%, and drivers have only had to wait for an additional seven seconds on average. The response is well-aligned with the county's Vision Zero commitment and its Two-Year Action Plan to have zero road deaths and serious injuries by 2030.

This is an excellent opportunity to solidify a positive step towards embracing Vision Zero and improving safety and environmental impact for this area and the county. I urge you to implement and support Alternative A; it is a safe, cost-effective, and minimally disruptive solution that has been proven to work well over the last two years.

Sincerely,
Yohannes Bennehoff
5847 33rd Pl
Hyattsville, MD 20782

APPENDIX G EMAILS

From: [Benjamin Englert](#)
To: [Tsai, Andrew](#)
Subject: I support Alternative A at the Little Falls Parkway intersection
Date: Wednesday, November 7, 2018 1:37:03 PM

Dear Andrew Tsai,

See below for the GGW form letter, which I endorse but to which I would add the following comments. With many others, I commute by bike on the Capital Crescent Trail (CCT) several times per week. Virtually all Little Falls Parkway drivers and CCT cyclists are considerate, alert, and safe; however, the infrastructure change has been a tremendous benefit to help prevent unnecessary injuries, deaths, and just simple road rage. Please make the change permanent.

Two years ago, a cyclist was tragically struck and killed by a driver at the intersection of the CCT and Little Falls Parkway. Reducing the car lanes to one each way and lowering the speed limit has worked incredibly well, and Montgomery County should make the changes permanent.

Since these changes were introduced, there has been a 67% reduction in crashes without any fatalities. Traffic has only decreased here by 3%, and drivers have only had to wait for an additional seven seconds on average. The response is well-aligned with the county's Vision Zero commitment and its Two-Year Action Plan to have zero road deaths and serious injuries by 2030.

This is an excellent opportunity to solidify a positive step towards embracing Vision Zero and improving safety and environmental impact for this area and the county. I urge you to implement and support Alternative A; it is a safe, cost-effective, and minimally disruptive solution that has been proven to work well over the last two years.

Sincerely,
Benjamin Englert
2603 Spencer Rd
Chevy Chase, MD 20815

APPENDIX G EMAILS

From: [Ross Filice](#)
To: [Tsai, Andrew](#)
Subject: I support Alternative A at the Little Falls Parkway intersection
Date: Wednesday, November 7, 2018 8:24:10 PM

Dear Andrew Tsai,

Two years ago, a cyclist was tragically struck and killed by a driver at the intersection of the Capital Crescent Trail (CCT) and Little Falls Parkway. Reducing the car lanes to one each way and lowering the speed limit has worked incredibly well, and Montgomery County should make the changes permanent.

Since these changes were introduced, there has been a 67% reduction in crashes without any fatalities. Traffic has only decreased here by 3%, and drivers have only had to wait for an additional seven seconds on average. The response is well-aligned with the county's Vision Zero commitment and its Two-Year Action Plan to have zero road deaths and serious injuries by 2030.

This is an excellent opportunity to solidify a positive step towards embracing Vision Zero and improving safety and environmental impact for this area and the county. I urge you to implement and support Alternative A; it is a safe, cost-effective, and minimally disruptive solution that has been proven to work well over the last two years.

Sincerely,
Ross Filice
4620 Norwood Drive
Chevy Chase, MD 20815

APPENDIX G EMAILS

From: [Margaret Hobbins](#)
To: [Tsai, Andrew](#)
Subject: I support Alternative A at the Little Falls Parkway intersection
Date: Wednesday, November 7, 2018 8:24:52 PM

Dear Andrew Tsai,

Two years ago, a cyclist was tragically struck and killed by a driver at the intersection of the Capital Crescent Trail (CCT) and Little Falls Parkway. Reducing the car lanes to one each way and lowering the speed limit has worked incredibly well, and Montgomery County should make the changes permanent.

Since these changes were introduced, there has been a 67% reduction in crashes without any fatalities. Traffic has only decreased here by 3%, and drivers have only had to wait for an additional seven seconds on average. The response is well-aligned with the county's Vision Zero commitment and its Two-Year Action Plan to have zero road deaths and serious injuries by 2030.

This is an excellent opportunity to solidify a positive step towards embracing Vision Zero and improving safety and environmental impact for this area and the county. I urge you to implement and support Alternative A; it is a safe, cost-effective, and minimally disruptive solution that has been proven to work well over the last two years.

Sincerely,
Margaret Hobbins
4620 Norwood Drive
Chevy Chase, MD 20815

APPENDIX G EMAILS

From: [Madeline Koewler](#)
To: [Tsai, Andrew](#)
Subject: I support Alternative A at the Little Falls Parkway intersection
Date: Wednesday, November 7, 2018 7:15:21 PM

Dear Andrew Tsai,

Two years ago, a cyclist was tragically struck and killed by a driver at the intersection of the Capital Crescent Trail (CCT) and Little Falls Parkway. Reducing the car lanes to one each way and lowering the speed limit has worked incredibly well, and Montgomery County should make the changes permanent.

Since these changes were introduced, there has been a 67% reduction in crashes without any fatalities. Traffic has only decreased here by 3%, and drivers have only had to wait for an additional seven seconds on average. The response is well-aligned with the county's Vision Zero commitment and its Two-Year Action Plan to have zero road deaths and serious injuries by 2030.

This is an excellent opportunity to solidify a positive step towards embracing Vision Zero and improving safety and environmental impact for this area and the county. I urge you to implement and support Alternative A; it is a safe, cost-effective, and minimally disruptive solution that has been proven to work well over the last two years.

Sincerely,
Madeline Koewler
Bethesda, MD 20816

APPENDIX G EMAILS

From: [Clark Larson](#)
To: [Tsai, Andrew](#)
Subject: I support Alternative A at the Little Falls Parkway intersection
Date: Wednesday, November 7, 2018 1:14:42 PM

Dear Andrew Tsai,

Two years ago, a cyclist was tragically struck and killed by a driver at the intersection of the Capital Crescent Trail (CCT) and Little Falls Parkway. Reducing the car lanes to one each way and lowering the speed limit has worked incredibly well, and Montgomery County should make the changes permanent.

Since these changes were introduced, there has been a 67% reduction in crashes without any fatalities. Traffic has only decreased here by 3%, and drivers have only had to wait for an additional seven seconds on average. The response is well-aligned with the county's Vision Zero commitment and its Two-Year Action Plan to have zero road deaths and serious injuries by 2030.

This is an excellent opportunity to solidify a positive step towards embracing Vision Zero and improving safety and environmental impact for this area and the county. I urge you to implement and support Alternative A; it is a safe, cost-effective, and minimally disruptive solution that has been proven to work well over the last two years.

Sincerely,
Clark Larson
2307 Michigan Avenue
Silver Spring, MD 20910

APPENDIX G EMAILS

From: [Alan Mairson](#)
To: [Tsai, Andrew](#)
Subject: I support Alternative A at the Little Falls Parkway intersection
Date: Wednesday, November 7, 2018 2:16:22 PM

Dear Andrew Tsai,

Two years ago, a cyclist was tragically struck and killed by a driver at the intersection of the Capital Crescent Trail (CCT) and Little Falls Parkway. Reducing the car lanes to one each way and lowering the speed limit has worked incredibly well, and Montgomery County should make the changes permanent.

Since these changes were introduced, there has been a 67% reduction in crashes without any fatalities. Traffic has only decreased here by 3%, and drivers have only had to wait for an additional seven seconds on average. The response is well-aligned with the county's Vision Zero commitment and its Two-Year Action Plan to have zero road deaths and serious injuries by 2030.

This is an excellent opportunity to solidify a positive step towards embracing Vision Zero and improving safety and environmental impact for this area and the county. I urge you to implement and support Alternative A; it is a safe, cost-effective, and minimally disruptive solution that has been proven to work well over the last two years.

Sincerely,
Alan Mairson
5624 Lamar Road
Bethesda, MD 20816

APPENDIX G EMAILS

From: [Ramtin Rahmani](#)
To: [Tsai, Andrew](#)
Subject: I support Alternative A at the Little Falls Parkway intersection
Date: Wednesday, November 7, 2018 5:46:12 PM

Dear Andrew Tsai,

I have biked here many times and seen drivers who stop in one lane have the driver behind them switch lanes to avoid being impeded. It creates a dangerous situation. Please support and continue with plan A for the Little Falls Parkway intersection. Thank you for your time.

Two years ago, a cyclist was tragically struck and killed by a driver at the intersection of the Capital Crescent Trail (CCT) and Little Falls Parkway. Reducing the car lanes to one each way and lowering the speed limit has worked incredibly well, and Montgomery County should make the changes permanent.

Since these changes were introduced, there has been a 67% reduction in crashes without any fatalities. Traffic has only decreased here by 3%, and drivers have only had to wait for an additional seven seconds on average. The response is well-aligned with the county's Vision Zero commitment and its Two-Year Action Plan to have zero road deaths and serious injuries by 2030.

This is an excellent opportunity to solidify a positive step towards embracing Vision Zero and improving safety and environmental impact for this area and the county. I urge you to implement and support Alternative A; it is a safe, cost-effective, and minimally disruptive solution that has been proven to work well over the last two years.

Sincerely,
Ramtin Rahmani
Arlington, VA 22201

APPENDIX G EMAILS

From: [Zachary Weinstein](#)
To: [Tsai, Andrew](#)
Subject: I support Alternative A at the Little Falls Parkway intersection
Date: Wednesday, November 7, 2018 3:47:24 PM

Dear Andrew Tsai,

Two years ago, a cyclist was tragically struck and killed by a driver at the intersection of the Capital Crescent Trail (CCT) and Little Falls Parkway. Reducing the car lanes to one each way and lowering the speed limit has worked incredibly well, and Montgomery County should make the changes permanent.

Since these changes were introduced, there has been a 67% reduction in crashes without any fatalities. Traffic has only decreased here by 3%, and drivers have only had to wait for an additional seven seconds on average. The response is well-aligned with the county's Vision Zero commitment and its Two-Year Action Plan to have zero road deaths and serious injuries by 2030.

This is an excellent opportunity to solidify a positive step towards embracing Vision Zero and improving safety and environmental impact for this area and the county. I urge you to implement and support Alternative A; it is a safe, cost-effective, and minimally disruptive solution that has been proven to work well over the last two years.

Sincerely,
Zachary Weinstein
8560 2nd Ave
Silver Spring, MD 20910

APPENDIX G EMAILS

From: [Gordon Chaffin](#)
To: [Tsai, Andrew](#)
Subject: I support Alternative A at the Little Falls Parkway intersection
Date: Thursday, November 8, 2018 1:55:47 PM

Dear Andrew Tsai,

Adding to their form letter here: I lived in Silver Spring for 4 years and love this intersection *specifically* because it had traffic calming features. Please don't make it like Connecticut Avenue: car-dependent and dangerous to cross.

Two years ago, a cyclist was tragically struck and killed by a driver at the intersection of the Capital Crescent Trail (CCT) and Little Falls Parkway. Reducing the car lanes to one each way and lowering the speed limit has worked incredibly well, and Montgomery County should make the changes permanent.

Since these changes were introduced, there has been a 67% reduction in crashes without any fatalities. Traffic has only decreased here by 3%, and drivers have only had to wait for an additional seven seconds on average. The response is well-aligned with the county's Vision Zero commitment and its Two-Year Action Plan to have zero road deaths and serious injuries by 2030.

This is an excellent opportunity to solidify a positive step towards embracing Vision Zero and improving safety and environmental impact for this area and the county. I urge you to implement and support Alternative A; it is a safe, cost-effective, and minimally disruptive solution that has been proven to work well over the last two years.

Sincerely,
Gordon Chaffin
3305 7th St NE Apt 4
Washington, DC 20017

APPENDIX G EMAILS

From: [Brian Chamowitz](#)
To: [Tsai, Andrew](#)
Subject: I support Alternative A at the Little Falls Parkway intersection
Date: Thursday, November 8, 2018 8:19:36 AM

Dear Andrew Tsai,

Two years ago, a cyclist was tragically struck and killed by a driver at the intersection of the Capital Crescent Trail (CCT) and Little Falls Parkway. Reducing the car lanes to one each way and lowering the speed limit has worked incredibly well, and Montgomery County should make the changes permanent.

Since these changes were introduced, there has been a 67% reduction in crashes without any fatalities. Traffic has only decreased here by 3%, and drivers have only had to wait for an additional seven seconds on average. The response is well-aligned with the county's Vision Zero commitment and its Two-Year Action Plan to have zero road deaths and serious injuries by 2030.

This is an excellent opportunity to solidify a positive step towards embracing Vision Zero and improving safety and environmental impact for this area and the county. I urge you to implement and support Alternative A; it is a safe, cost-effective, and minimally disruptive solution that has been proven to work well over the last two years.

Sincerely,
Brian Chamowitz
1328 Newton St NE
Washington, DC 20017

APPENDIX G EMAILS

From: [Rachel Clark](#)
To: [Tsai, Andrew](#)
Subject: I support Alternative A at the Little Falls Parkway intersection
Date: Thursday, November 8, 2018 10:44:03 AM

Dear Andrew Tsai,

Good morning,

Two years ago, a cyclist was tragically struck and killed by a driver at the intersection of the Capital Crescent Trail (CCT) and Little Falls Parkway. Reducing the car lanes to one each way and lowering the speed limit has worked incredibly well, and Montgomery County should make the changes permanent.

Since these changes were introduced, there has been a 67% reduction in crashes without any fatalities. Traffic has only decreased here by 3%, and drivers have only had to wait for an additional seven seconds on average. The response is well-aligned with the county's Vision Zero commitment and its Two-Year Action Plan to have zero road deaths and serious injuries by 2030.

This is an excellent opportunity to solidify a positive step towards embracing Vision Zero and improving safety and environmental impact for this area and the county. I urge you to implement and support Alternative A; it is a safe, cost-effective, and minimally disruptive solution that has been proven to work well over the last two years.

As a former resident of Bradley Blvd near the CCT and a frequent user of the CCT today, I'm appreciative of the recent improvements to the trail and encourage the county to continue its work to improve the safety of this community jewel.

Sincerely,
Rachel Clark
Washington, DC 20005

APPENDIX G EMAILS

From: [Nicholas Enz](#)
To: [Tsai, Andrew](#)
Subject: I support Alternative A at the Little Falls Parkway intersection
Date: Thursday, November 8, 2018 8:30:41 AM

Dear Andrew Tsai,

Two years ago, a cyclist was tragically struck and killed by a driver at the intersection of the Capital Crescent Trail (CCT) and Little Falls Parkway. Reducing the car lanes to one each way and lowering the speed limit has worked incredibly well, and Montgomery County should make the changes permanent.

Since these changes were introduced, there has been a 67% reduction in crashes without any fatalities. Traffic has only decreased here by 3%, and drivers have only had to wait for an additional seven seconds on average. The response is well-aligned with the county's Vision Zero commitment and its Two-Year Action Plan to have zero road deaths and serious injuries by 2030.

This is an excellent opportunity to solidify a positive step towards embracing Vision Zero and improving safety and environmental impact for this area and the county. I urge you to implement and support Alternative A; it is a safe, cost-effective, and minimally disruptive solution that has been proven to work well over the last two years.

Sincerely,
Nicholas Enz
2204 Luzerne avenue
Silver Spring, MD 20910

APPENDIX G EMAILS

From: [Robin Graziano](#)
To: [Tsai, Andrew](#)
Subject: I support Alternative A at the Little Falls Parkway intersection
Date: Thursday, November 8, 2018 12:05:08 PM

Dear Andrew Tsai,

Two years ago, a cyclist was tragically struck and killed by a driver at the intersection of the Capital Crescent Trail (CCT) and Little Falls Parkway. Reducing the car lanes to one each way and lowering the speed limit has worked incredibly well, and Montgomery County should make the changes permanent.

Since these changes were introduced, there has been a 67% reduction in crashes without any fatalities. Traffic has only decreased here by 3%, and drivers have only had to wait for an additional seven seconds on average. The response is well-aligned with the county's Vision Zero commitment and its Two-Year Action Plan to have zero road deaths and serious injuries by 2030.

This is an excellent opportunity to solidify a positive step towards embracing Vision Zero and improving safety and environmental impact for this area and the county. I urge you to implement and support Alternative A; it is a safe, cost-effective, and minimally disruptive solution that has been proven to work well over the last two years.

Sincerely,
Robin Graziano
Washington, DC 20005

APPENDIX G EMAILS

From: [Hannah Hunt](#)
To: [Tsai, Andrew](#)
Subject: I support Alternative A at the Little Falls Parkway intersection
Date: Thursday, November 8, 2018 7:50:24 AM

Dear Andrew Tsai,

Two years ago, a cyclist was tragically struck and killed by a driver at the intersection of the Capital Crescent Trail (CCT) and Little Falls Parkway. Reducing the car lanes to one each way and lowering the speed limit has worked incredibly well, and Montgomery County should make the changes permanent.

Since these changes were introduced, there has been a 67% reduction in crashes without any fatalities. Traffic has only decreased here by 3%, and drivers have only had to wait for an additional seven seconds on average. The response is well-aligned with the county's Vision Zero commitment and its Two-Year Action Plan to have zero road deaths and serious injuries by 2030.

This is an excellent opportunity to solidify a positive step towards embracing Vision Zero and improving safety and environmental impact for this area and the county. I urge you to implement and support Alternative A; it is a safe, cost-effective, and minimally disruptive solution that has been proven to work well over the last two years.

Sincerely,
Hannah Hunt
Washington, DC 20005

APPENDIX G EMAILS

From: [Sam Keller](#)
To: [Tsai, Andrew](#)
Subject: I support Alternative A at the Little Falls Parkway intersection
Date: Thursday, November 8, 2018 9:25:28 AM

Dear Andrew Tsai,

Please implement Alternative A! I bike on the Capital Crescent trail all the time and would feel so much safer with Alternative A being permanent.

Two years ago, a cyclist was tragically struck and killed by a driver at the intersection of the Capital Crescent Trail (CCT) and Little Falls Parkway. Reducing the car lanes to one each way and lowering the speed limit has worked incredibly well, and Montgomery County should make the changes permanent.

Since these changes were introduced, there has been a 67% reduction in crashes without any fatalities. Traffic has only decreased here by 3%, and drivers have only had to wait for an additional seven seconds on average. The response is well-aligned with the county's Vision Zero commitment and its Two-Year Action Plan to have zero road deaths and serious injuries by 2030.

This is an excellent opportunity to solidify a positive step towards embracing Vision Zero and improving safety and environmental impact for this area and the county. I urge you to implement and support Alternative A; it is a safe, cost-effective, and minimally disruptive solution that has been proven to work well over the last two years.

Sincerely,
Sam Keller
1801 Clydesdale Place NW
Washington, DC 20009

APPENDIX G EMAILS

From: [Wendy Leibowitz](#)
To: [Tsai, Andrew](#)
Subject: I support Alternative A at the Little Falls Parkway intersection
Date: Thursday, November 8, 2018 11:34:31 AM

Dear Andrew Tsai,

To the County Council:

The best way to remember Ned Gaylin is to make the changes instituted after his death permanent. The situation is working well--I support a raised crossing and some beautification, but keep it as it is NOW, please. It's the cheapest solution; it is tested and it works. Thank you, Wendy Leibowitz, a pedestrian, cyclist and driver

Two years ago, a cyclist was tragically struck and killed by a driver at the intersection of the Capital Crescent Trail (CCT) and Little Falls Parkway. Reducing the car lanes to one each way and lowering the speed limit has worked incredibly well, and Montgomery County should make the changes permanent.

Since these changes were introduced, there has been a 67% reduction in crashes without any fatalities. Traffic has only decreased here by 3%, and drivers have only had to wait for an additional seven seconds on average. The response is well-aligned with the county's Vision Zero commitment and its Two-Year Action Plan to have zero road deaths and serious injuries by 2030.

This is an excellent opportunity to solidify a positive step towards embracing Vision Zero and improving safety and environmental impact for this area and the county. I urge you to implement and support Alternative A; it is a safe, cost-effective, and minimally disruptive solution that has been proven to work well over the last two years.

Sincerely,
Wendy Leibowitz
4604 Highland Avenue
Bethesda, MD 20814

APPENDIX G EMAILS

From: [Howard Marks](#)
To: [Tsai, Andrew](#)
Subject: I support Alternative A at the Little Falls Parkway intersection
Date: Thursday, November 8, 2018 12:18:42 PM

Dear Andrew Tsai,

Two years ago, a cyclist was tragically struck and killed by a driver at the intersection of the Capital Crescent Trail (CCT) and Little Falls Parkway. Reducing the car lanes to one each way and lowering the speed limit has worked incredibly well, and Montgomery County should make the changes permanent.

Since these changes were introduced, there has been a 67% reduction in crashes without any fatalities. Traffic has only decreased here by 3%, and drivers have only had to wait for an additional seven seconds on average. The response is well-aligned with the county's Vision Zero commitment and its Two-Year Action Plan to have zero road deaths and serious injuries by 2030.

This is an excellent opportunity to solidify a positive step towards embracing Vision Zero and improving safety and environmental impact for this area and the county. I urge you to implement and support Alternative A; it is a safe, cost-effective, and minimally disruptive solution that has been proven to work well over the last two years.

Sincerely,
Howard Marks
777 7th St NW
Washington, DC 20001

APPENDIX G EMAILS

From: [Jennifer Pietropaoli](#)
To: [Tsai, Andrew](#)
Subject: I support Alternative A at the Little Falls Parkway intersection
Date: Thursday, November 8, 2018 12:00:02 PM

Dear Andrew Tsai,

Two years ago, a cyclist was tragically struck and killed by a driver at the intersection of the Capital Crescent Trail (CCT) and Little Falls Parkway. Reducing the car lanes to one each way and lowering the speed limit has worked incredibly well, and Montgomery County should make the changes permanent.

Since these changes were introduced, there has been a 67% reduction in crashes without any fatalities. Traffic has only decreased here by 3%, and drivers have only had to wait for an additional seven seconds on average. The response is well-aligned with the county's Vision Zero commitment and its Two-Year Action Plan to have zero road deaths and serious injuries by 2030.

This is an excellent opportunity to solidify a positive step towards embracing Vision Zero and improving safety and environmental impact for this area and the county. I urge you to implement and support Alternative A, a raised sidewalk and road "diet"; it is a safe, cost-effective, and minimally disruptive solution that has been proven to work well over the last two years.

Sincerely,
Jennifer Pietropaoli
Washington, DC 20001

APPENDIX G EMAILS

From: [Kristin Richards](#)
To: [Tsai, Andrew](#)
Subject: I support Alternative A at the Little Falls Parkway intersection
Date: Thursday, November 8, 2018 10:04:43 AM

Dear Andrew Tsai,

Two years ago, a cyclist was tragically struck and killed by a driver at the intersection of the Capital Crescent Trail (CCT) and Little Falls Parkway. Reducing the car lanes to one each way and lowering the speed limit has worked incredibly well, and Montgomery County should make the changes permanent.

Since these changes were introduced, there has been a 67% reduction in crashes without any fatalities. Traffic has only decreased here by 3%, and drivers have only had to wait for an additional seven seconds on average. The response is well-aligned with the county's Vision Zero commitment and its Two-Year Action Plan to have zero road deaths and serious injuries by 2030.

This is an excellent opportunity to solidify a positive step towards embracing Vision Zero and improving safety and environmental impact for this area and the county. I urge you to implement and support Alternative A; it is a safe, cost-effective, and minimally disruptive solution that has been proven to work well over the last two years.

Sincerely,
Kristin Richards
6203 Wagner Ln
Bethesda, MD 20816

APPENDIX G EMAILS

From: [Khaled Shami](#)
To: [Tsai, Andrew](#)
Subject: I support Alternative A at the Little Falls Parkway intersection
Date: Thursday, November 8, 2018 7:27:34 AM

Dear Andrew Tsai,

Two years ago, a cyclist was tragically struck and killed by a driver at the intersection of the Capital Crescent Trail (CCT) and Little Falls Parkway. Reducing the car lanes to one each way and lowering the speed limit has worked incredibly well, and Montgomery County should make the changes permanent.

Since these changes were introduced, there has been a 67% reduction in crashes without any fatalities. Traffic has only decreased here by 3%, and drivers have only had to wait for an additional seven seconds on average. The response is well-aligned with the county's Vision Zero commitment and its Two-Year Action Plan to have zero road deaths and serious injuries by 2030.

This is an excellent opportunity to solidify a positive step towards embracing Vision Zero and improving safety and environmental impact for this area and the county. I urge you to implement and support Alternative A; it is a safe, cost-effective, and minimally disruptive solution that has been proven to work well over the last two years.

Sincerely,
Khaled Shami
8204 Caraway Street
Cabin John, MD 20818

APPENDIX G EMAILS

From: [Seth Amgott](#)
To: [Tsai, Andrew](#)
Subject: I support Alternative A at the Little Falls Parkway intersection
Date: Friday, November 9, 2018 1:17:51 PM

Dear Andrew Tsai,

I drive to work in Bethesda on Little Falls and bicycle with my 8-year-old on the Capital Crescent Trail. Reducing the car lanes to one each way and lowering the speed limit has worked incredibly well, and Montgomery County should make the changes permanent.

Since these changes were introduced, there has been a 67% reduction in crashes without any fatalities. Traffic has only decreased here by 3%, and drivers have only had to wait for an additional seven seconds on average. The response is well-aligned with the county's Vision Zero commitment and its Two-Year Action Plan to have zero road deaths and serious injuries by 2030.

This is an excellent opportunity to solidify a positive step towards embracing Vision Zero and improving safety and environmental impact for this area and the county. I urge you to implement and support Alternative A; it is a safe, cost-effective, and minimally disruptive solution that has been proven to work well over the last two years.

Sincerely,
Seth Amgott
4008 47th St. N.W.
Washington, DC 20016

APPENDIX G EMAILS

From: [Anthony Camilli](#)
To: [Tsai, Andrew](#)
Subject: I support Alternative A at the Little Falls Parkway intersection
Date: Friday, November 9, 2018 1:02:54 PM

Dear Andrew Tsai,

For the record, I live in Takoma Park but I ride this stretch of trail often.

Two years ago, a cyclist was tragically struck and killed by a driver at the intersection of the Capital Crescent Trail (CCT) and Little Falls Parkway. Reducing the car lanes to one each way and lowering the speed limit has worked incredibly well, and Montgomery County should make the changes permanent.

Since these changes were introduced, there has been a 67% reduction in crashes without any fatalities. Traffic has only decreased here by 3%, and drivers have only had to wait for an additional seven seconds on average. The response is well-aligned with the county's Vision Zero commitment and its Two-Year Action Plan to have zero road deaths and serious injuries by 2030.

This is an excellent opportunity to solidify a positive step towards embracing Vision Zero and improving safety and environmental impact for this area and the county. I urge you to implement and support Alternative A; it is a safe, cost-effective, and minimally disruptive solution that has been proven to work well over the last two years.

Sincerely,
Anthony Camilli
Takoma Park, MD 20912

APPENDIX G EMAILS

From: [Rich Josephson](#)
To: [Tsai, Andrew](#)
Subject: I support Alternative A at the Little Falls Parkway intersection
Date: Friday, November 9, 2018 4:12:20 PM

Dear Andrew Tsai,

I regularly ride my bike on the Capital Crescent Trail and have crossed Little Falls Parkway hundreds of time. I have fallen at that intersection because even when I yield to a car, another car behind or beside that car may proceed without stopping for me. It is and has been a highly dangerous intersection.

I understand that two years ago a cyclist was struck and killed by a driver at the intersection. Reducing the car lanes to one each way and lowering the speed limit has worked well and Montgomery County should make the changes permanent.

I believe that Montgomery County should take this positive step towards embracing Vision Zero and improving safety and environmental impact for this area and the county.

I urge you to implement and support Alternative A; it is a safe, cost-effective, and minimally disruptive solution that has been proven to work well over the last two years.

Sincerely,
Rich Josephson
809 Hyde Road
Silver Spring, MD 20902

APPENDIX G EMAILS

From: [BARRY GOLDFARB](#)
To: [Tsai, Andrew](#)
Subject: I support Alternative A at the Little Falls Parkway intersection
Date: Monday, November 12, 2018 11:55:17 PM

Dear Andrew Tsai,

Two years ago, a cyclist was tragically struck and killed by a driver at the intersection of the Capital Crescent Trail (CCT) and Little Falls Parkway. Reducing the car lanes to one each way and lowering the speed limit has worked incredibly well, and Montgomery County should make the changes permanent.

Since these changes were introduced, there has been a 67% reduction in crashes without any fatalities. Traffic has only decreased here by 3%, and drivers have only had to wait for an additional seven seconds on average. The response is well-aligned with the county's Vision Zero commitment and its Two-Year Action Plan to have zero road deaths and serious injuries by 2030.

This is an excellent opportunity to solidify a positive step towards embracing Vision Zero and improving safety and environmental impact for this area and the county. I urge you to implement and support Alternative A; it is a safe, cost-effective, and minimally disruptive solution that has been proven to work well over the last two years. I have ridden through this intersection numerous times since the road diet change was implemented and can personally attest to how much it has improved the safety of all trail users.

Sincerely,
BARRY GOLDFARB
11201 Rose lane
silver spring, MD 20902

APPENDIX G EMAILS

From: [Gordon Chaffin](#)
To: [Tsai, Andrew](#)
Subject: I support Alternative A at the Little Falls Parkway intersection
Date: Wednesday, November 14, 2018 6:13:19 PM

Dear Andrew Tsai,

The form letter is below, but I just want to say as a runner, cyclist, Silver Spring resident 2011-2015, and reporter on transportation and development issues: please don't follow the NIMBYs who insist, despite all the data you've collected, that the road diet and current crossing pushed traffic to neighborhoods there. It hasn't. We know this. Speeding hasn't gone up, either. Please don't build the expensive, unnecessary bridge. Option A, please. Now, here's the form letter:

Two years ago, a cyclist was tragically struck and killed by a driver at the intersection of the Capital Crescent Trail (CCT) and Little Falls Parkway. Reducing the car lanes to one each way and lowering the speed limit has worked incredibly well, and Montgomery County should make the changes permanent.

Since these changes were introduced, there has been a 67% reduction in crashes without any fatalities. Traffic has only decreased here by 3%, and drivers have only had to wait for an additional seven seconds on average. The response is well-aligned with the county's Vision Zero commitment and its Two-Year Action Plan to have zero road deaths and serious injuries by 2030.

This is an excellent opportunity to solidify a positive step towards embracing Vision Zero and improving safety and environmental impact for this area and the county. I urge you to implement and support Alternative A; it is a safe, cost-effective, and minimally disruptive solution that has been proven to work well over the last two years.

Sincerely,
Gordon Chaffin
3305 7th St NE Apt 4
Washington, DC 20017

APPENDIX G EMAILS

From: [Grant Klein](#)
To: [Tsai, Andrew](#)
Subject: I support Alternative A at the Little Falls Parkway intersection
Date: Wednesday, November 14, 2018 10:22:41 AM

Dear Andrew Tsai,

Two years ago, a cyclist was tragically struck and killed by a driver at the intersection of the Capital Crescent Trail (CCT) and Little Falls Parkway. Reducing the car lanes to one each way and lowering the speed limit has worked incredibly well, and Montgomery County should make the changes permanent.

Since these changes were introduced, there has been a 67% reduction in crashes without any fatalities. Traffic has only decreased here by 3%, and drivers have only had to wait for an additional seven seconds on average. The response is well-aligned with the county's Vision Zero commitment and its Two-Year Action Plan to have zero road deaths and serious injuries by 2030.

This is an excellent opportunity to solidify a positive step towards embracing Vision Zero and improving safety and environmental impact for this area and the county. I urge you to implement and support Alternative A; it is a safe, cost-effective, and minimally disruptive solution that has been proven to work well over the last two years.

Though I don't live in Maryland, I often ride on the trail and end up there. I've experienced dangerous conditions for pedestrians and cyclists at this intersection and hope that you'll consider this reasonable request. Thank you!

Sincerely,
Grant Klein
1417 N St NW Apt 500
Washington, DC 20005

APPENDIX G EMAILS

From: [David Kathan](#)
To: [Tsai, Andrew](#)
Subject: Capital Crescent Trail – Little Falls Parkway Crossing -- Letter from Impacted Neighboring Communities
Date: Friday, November 16, 2018 1:38:28 PM
Attachments: [CCT Crossing LFP Community Letter.docx](#)

Mr. Tsai,

Below please find a letter to you regarding the proposals for improving safety at the crossing of the Capital Crescent Trail and Little Falls Parkway. The letter was drafted by officials and residents of the communities in the neighborhoods surrounding the crossing and from a large area of the B-CC area that use Little Falls Parkway to reach Bethesda. We believe that any safe and fair solution must consider the impact on neighboring communities and we are speaking with a unified voice.

If you would like to discuss this further, please feel free to contact me or any of the other officials and residents who signed the letter.

Regards,

David Kathan
dkathan@gmail.com

APPENDIX G EMAILS

From: [andrew hyman](#)
To: [Tsai, Andrew](#)
Subject: I support Alternative A at the Little Falls Parkway intersection
Date: Sunday, November 18, 2018 5:44:42 PM

Dear Andrew Tsai,

Two years ago, a cyclist was tragically struck and killed by a driver at the intersection of the Capital Crescent Trail (CCT) and Little Falls Parkway. Reducing the car lanes to one each way and lowering the speed limit has worked incredibly well, and Montgomery County should make the changes permanent.

Since these changes were introduced, there has been a 67% reduction in crashes without any fatalities. Traffic has only decreased here by 3%, and drivers have only had to wait for an additional seven seconds on average. The response is well-aligned with the county's Vision Zero commitment and its Two-Year Action Plan to have zero road deaths and serious injuries by 2030.

This is an excellent opportunity to solidify a positive step towards embracing Vision Zero and improving safety and environmental impact for this area and the county. I urge you to implement and support Alternative A; it is a safe, cost-effective, and minimally disruptive solution that has been proven to work well over the last two years.

Sincerely,
andrew hyman
2301 east-west highway
Silver Spring, MD 20910

APPENDIX G EMAILS

From: [Josh Boxerman](#)
To: [Tsai, Andrew](#)
Subject: I support Alternative A at the Little Falls Parkway intersection
Date: Tuesday, November 20, 2018 9:59:34 AM

Dear Andrew Tsai,

Two years ago, a cyclist was tragically struck and killed by a driver at the intersection of the Capital Crescent Trail (CCT) and Little Falls Parkway. Reducing the car lanes to one each way and lowering the speed limit has worked incredibly well, and Montgomery County should make the changes permanent.

Since these changes were introduced, there has been a 67% reduction in crashes without any fatalities. Traffic has only decreased here by 3%, and drivers have only had to wait for an additional seven seconds on average. The response is well-aligned with the county's Vision Zero commitment and its Two-Year Action Plan to have zero road deaths and serious injuries by 2030.

This is an excellent opportunity to solidify a positive step towards embracing Vision Zero and improving safety and environmental impact for this area and the county. I urge you to implement and support Alternative A; it is a safe, cost-effective, and minimally disruptive solution that has been proven to work well over the last two years.

I am a frequent cyclist who lives near the trail and uses it often. It is an asset to our community and we need to protect its users, especially children and families. Let's prioritize their safety and everyone's ability to get around and enjoy the area without a car.

Sincerely,
Josh Boxerman
5506 Burling Court
Bethesda, MD 20817

APPENDIX G EMAILS

From: cyrus817@aol.com
To: [Tsai, Andrew](#)
Subject: Trail Crossing on Little Falls Parkway
Date: Wednesday, November 28, 2018 9:53:23 AM

Mr. Tsai,

As a frequent user of the Capital Crescent Trail and a commuter on Little Falls Parkway as well as a long time resident of the area, I venture to suggest that the traffic lanes need to be open to handle the ever-increasing number of cars on the road. I sincerely hope you can construct an overpass for trail users and leave the two lanes each way open on Little Falls Parkway. It would be similar to what was planned at River Road. People are out for exercise so extra walking or riding of a bicycle should not matter.

Thank you for your consideration.

Janet H. Ansary
5425 Falmouth Road
Bethesda, MD 20816

APPENDIX G EMAILS

From: [Jenny Krieg](#)
To: [Tsai, Andrew](#)
Subject: Proposed Capital Crescent Trail bridge
Date: Thursday, November 29, 2018 6:15:13 PM

Mr. Tsai:

For the safety of pedestrians and bicyclists on Capital Crescent Trail, I am in favor of building a bridge over Little Falls Parkway.

Please re-open all car lanes on the Parkway as soon as you can. Traffic is becoming a big problem there. For now, people using the Trail can cross the Parkway at the Arlington Road stoplight.

Thank you,
Jenny Krieg

7465 Arlington Road
Bethesda, MD 20814
&
17429 Hoskinson Road
Poolesville, MD 20837

APPENDIX G EMAILS

From: [Garrett Hennigan](#)
To: [Tsai, Andrew](#)
Cc: [WABA Advocacy](#)
Subject: WABA comments on Capital Crescent Trail Crossing
Date: Tuesday, December 11, 2018 12:41:03 PM
Attachments: [WABA Comments Capital Crescent Trail Crossing Dec 2018.pdf](#)

Andrew,

Please see the attached comments from the Washington Area Bicyclist Association regarding the proposed alternatives for the Little Falls Parkway trail crossing. Do not hesitate to reach out to me if you have any questions.

Thanks for your work on this project,

Garrett

Garrett Hennigan | Community Organizer

Washington Area Bicyclist Association

Office: 202-518-0524 x210

Mobile: 202-656-3078

Email: garrett.hennigan@waba.org

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APPENDIX G EMAILS

From: [Patricia Johnson](#)
To: [Riley, Mike](#)
Cc: [Tsai, Andrew](#); [Frank, Andrew](#)
Subject: Re. Little Falls Parkway "Road Diet" and the Montgomery Parks Department 4.4 million dollar budget cuts.
Date: Friday, April 12, 2019 10:33:58 AM

Dear Mr. Riley: I saw your letter of March 26th with your impassioned plea for 1.7 million dollars in funding to be restored to the Parks Department in light of County Executive Elrich's plan to cut budgets. I understand your concerns. Our parks are so important to the county. Green space is imperative for the general health and well being of the residents. You called for your constituents to speak up on behalf of parks this week. A 4.4 million dollar budget cut is serious and a necessary result created by the extreme overall county deficit.

I do understand your consternation at the amount of reduction in services that these 4.4 million dollar cuts will effect. That is why I am questioning the wisdom of your department experts in their insistence of continuing the "road diet" on Little Falls Parkway between River Road and Arlington Road. Members of the communities that surround the area affected by the "road diet" have met with you on numerous occasions and asked that the Parkway be opened again. We asked that all constituents that use the Capital Crescent Trail and the Parkway, stop at the light at Arlington Road and cross at that intersection safely. I was at a meeting at the Parks Department in February with members of the Citizens Coordination Committee on Friendship Heights and we asked that the crossing be moved to the Arlington Light. That solution is cost effective and safe. I think it should be seriously considered in light of your now severe budget restraints. Why insist on spending money on this small block of parkway and creating more "parks" when this area is not a destination and the surrounding neighborhoods have respectfully asked that the original road be restored? The Capital Crescent Trail Board has also met with you to echo the same request.

A safe crossing can easily be handled at the Arlington Road light. The narrowing of the parkway as it exists now is dangerous and confusing to both drivers, bikers and walkers. The Capital Crescent Trail is a recreational trail. It is used by 4 walkers to every biker. The narrowing of this parkway and building out of unnecessary parkland when your department needs "1.7 million in funding restored in order to maintain the park systems and improve existing operations and programs" just doesn't make sense. To reiterate, the surrounding neighborhoods want the Parkway restored which will be the least costly solution to this unsightly and trafficked dilemma. The community on Hillandale Road (Kenwood Station) has taken a terrific traffic cut-through hit because of the "road diet". Also, in anticipation of the development that is coming to downtown Bethesda and the Westwood Shopping Center, opening the parkway will help with essential traffic flow. Montgomery County Parks solution to this problem is not fair and fiscally is not prudent when so many other park areas are in need of restoration and improvement. You list in your letter the "backlog of failing plumbing, electrical and other essential systems in aging facilities". It seems that these problems are far more important than building a new "park" that goes nowhere, can't be used by anyone and is not wanted by so many. I have also attached a photo of the traffic backup at 5pm on Little Falls Parkway from River Road to Massachusetts Avenue at rush hour. That stretch of road is narrowed to one lane.

Sincerely,

Patricia Johnson
5301 Oakland Road

APPENDIX G EMAILS

Chevy Chase, Md. 20815

