

MR #2024002 – BRINK ROAD BRIDGE OVER GREAT SENECA CREEK

Description

Mandatory Referral review for the rehabilitation and widening of an existing bridge on Brink Road over Great Seneca Creek. Modified bridge will be wider to accommodate improved hydrology and improved travel lanes and shoulders on the structure.

MR2024002
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MCPB Item No 5
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LOCATION:

Brink Road at Great Seneca Creek

MASTER PLAN

Agricultural & Rural Open Space
Functional Master Plan

APPLICANT

Montgomery County Department of
Transportation

ACCEPTANCE DATE

October 12, 2023

REVIEW BASIS

Md. Land Use Article, Section 20-301, et
seq.



Summary

- Staff recommends transmittal of comments to the Montgomery County Department of Transportation.
- The Planning Board’s review of a Mandatory Referral is advisory.

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SECTION 1 – RECOMMENDATIONS

Staff recommends transmitting the following comments to the Montgomery County Department of Transportation (MCDOT):

1. Widen the proposed bridge by eight feet for a total bridge width of 45 feet four inches (42 feet bridge clearance) in order to accommodate the entire master planned vision with the following improvements:
 - a 12-foot-wide sidepath on the south side, composed of a 1.5-foot-wide buffer from the bridge parapet, an 8-foot-wide sidepath, and a 2.5-foot-wide buffer with a protective railing,
 - a two-foot-wide shoulder between the planned sidepath and the travel lanes,
 - two 11-foot-wide travel lanes,
 - a six-foot-wide shoulder on the north side, and
 - the sidepath across this bridge should connect to the Seneca Greenway Trail on the east side of the bridge.
2. MCDOT should complete a detour study as part of future design efforts to ensure that traffic impacts during construction will be minimal or mitigated by construction mitigation efforts.
3. Prior to any land disturbing activities, the Applicant must have a Forest Conservation Exemption Plan that matches the final layout and design of the project including the LOD.
4. Mitigation for impacts to park trees (with a six” DBH or greater) damaged or removed, shall either be (1) replacement planting on parkland at a rate of one inch to one inch diameter or (2) a monetary per inch caliper basis at the rate of \$100/diameter inch, to be paid to Montgomery Parks prior to the completion of construction.
5. Construction plans must be submitted to the M-NCPPC Department of Parks for review as part of the Park Construction Permit process to ensure that all work is performed in accordance with M-NCPPC standard details, specifications, and policies. No work on parkland may occur until an approved Park Construction Permit is issued for the project.
6. MCDOT must continue to coordinate with M-NCPPC on the refinement of the trail connections and proposed stormwater facilities designs and tie-ins.
7. Montgomery Parks tree mitigation will be fulfilled through the on-site planting of a diverse tree, shrub, and shrub palette approved by Montgomery Parks.
8. MCDOT must compensate the M-NCPPC at fair market value for all land that will transfer from M-NCPPC’s Great Seneca Stream Valley Park to MCDOT’s road right-of-way, estimated at approximately 14,000 square feet. Payment for the transferred land must occur before issuance of the Park Construction Permit. The new right-of-way area will be transferred to MCDOT by M-NCPPC via a Release Agreement recorded in the County Land Records.

9. MCDOT must provide an appropriate parcel of new parkland as a replacement for the State Program Open Space (POS) funded parkland this project will convert to right-of-way, approximately 9,870 square feet, prior to the issuance of a Park Construction Permit.

SECTION 2 - INTRODUCTION

MCDOT's is proposing a project to rehabilitate a structurally deteriorating roadway bridge that carries Brink Road over Great Seneca Creek, within the Great Seneca Stream Valley Park. This project will widen and rehabilitate the bridge and will increase the roadway width to accommodate two traffic lanes, one in each direction, along with shoulders next to each traffic lane. The bridge will be slightly raised to accommodate larger stormflows. A map of the project location is provided in Figure 1.

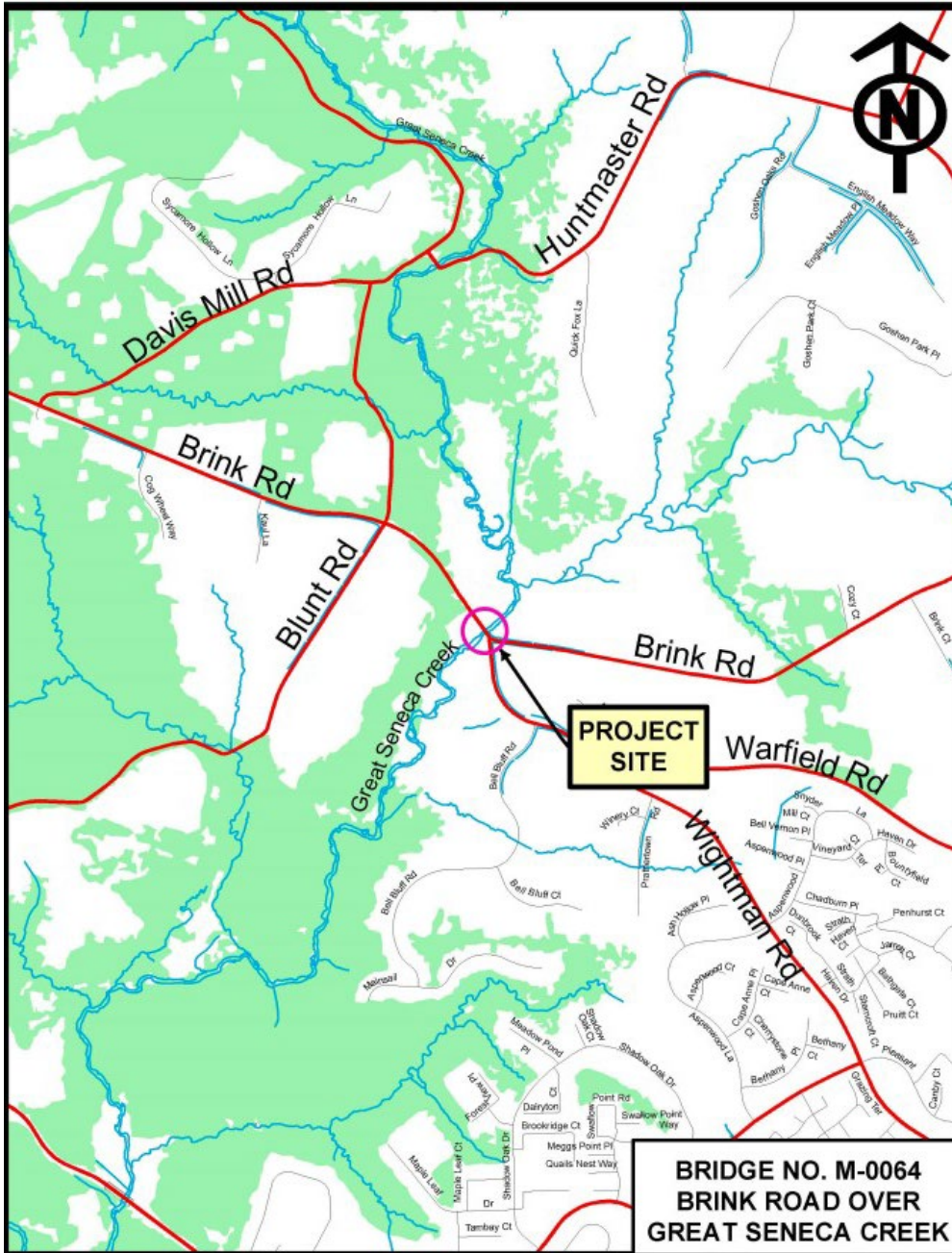


Figure 1: Brink Road Bridge Project Location Map

SECTION 3 – PROJECT DESCRIPTION

Project Description

MCDOT currently owns and maintains the existing Brink Road Bridge over Great Seneca Creek. Its responsibility regarding this bridge is to provide a safe, reliable bridge structure and road surface to adequately convey the current and future traffic volumes along the bridge and storm events below the bridge. Because the current Brink Road bridge is structurally deteriorating, MCDOT will rehabilitate and widen the bridge abutments and will replace the superstructure and deck. A current low point of the bridge with a history of overtopping by the streamflow will also be slightly raised to better accommodate storm events. The proposed work also includes improved crosswalks and a new parking lot for the nearby trailhead, as well as improvements to the intersection with Wightman Road, which is located directly adjacent to the bridge. The roadway and bridge will be closed to vehicular traffic during construction, which will improve construction safety and constructability and result in a shorter construction duration and lower construction costs. This project will not change the functional classification of Brink Road or impact traffic capacity at its completion. A copy of the Brink Road Design Plans is included with this staff report as Attachment A.

The existing Brink Road bridge consists of an approximately 25-foot-wide steel beam, single span bridge constructed circa 1972. The bridge is 58 feet in length. The bridge carries two traffic lanes, one in each direction without any shoulders or pedestrian/bicycle accommodation. Photos of the existing bridge are shown below in Figure 2 and Figure 3.



Figure 2: Brink Road Bridge over Great Seneca Creek – side view



Figure 3: Brink Road Bridge looking west from Wightman Road intersection

BRIDGE PROPOSED CROSS SECTION

The bridge will be designed with a 37-foot four inch-wide superstructure and with a roadway clearance of 34 feet as shown below in Figure 4. As proposed, the bridge will accommodate two traffic lanes, one in each direction, along with six-foot wide shoulders.

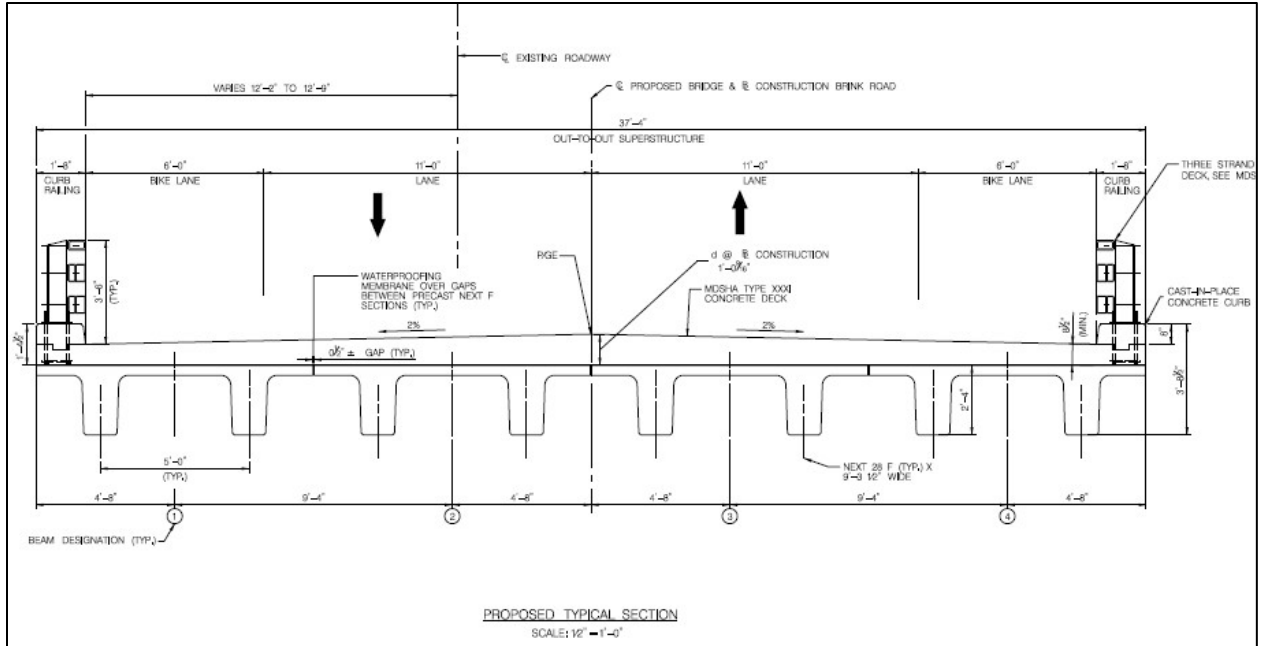


Figure 4: Proposed Bridge Cross Section

A plan view drawing was submitted as part of the mandatory referral submission and this is shown below in Figure 5. This improvement will impact a 1,000-foot-long section of Brink Road/Wightman Road. This includes the 58-foot-long proposed bridge, and modifications to the Brink Road/Wightman Road intersection extending from the east side of Great Seneca Creek for approximately 200 feet along the east leg of Brink Road and for 100 feet along the south leg of Wightman Road.

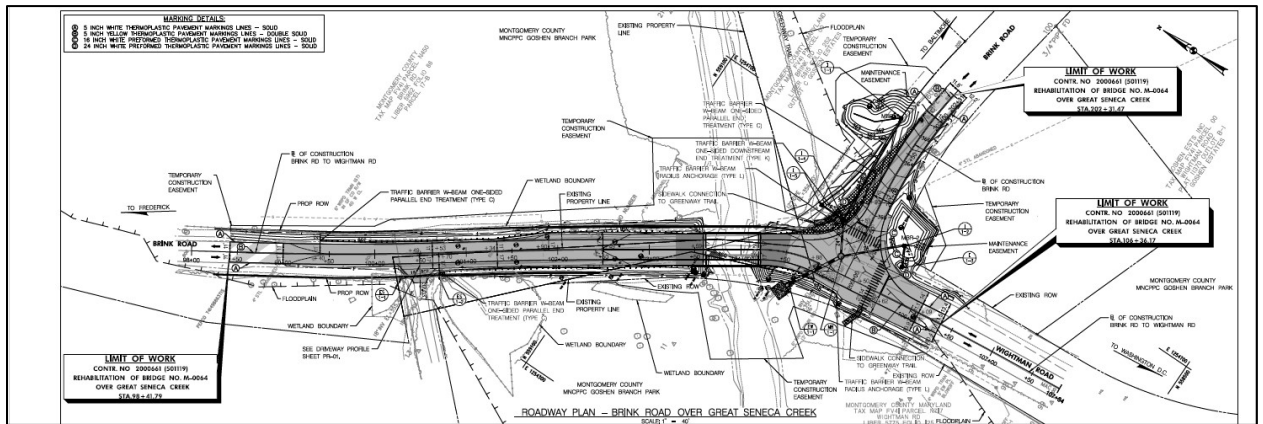


Figure 5: Plan View of Proposed Bridge Design

In order to conceptualize the proposed modification and widening of the existing bridge structure, a plan view drawing of the bridge is shown below in Figure 6. The existing bridge is highlighted with grey shading to illustrate the difference in width between the existing bridge and modified bridge. Bridge abutment extensions and row wing walls will be constructed to widen the bridge to the south by approximately 12 feet.

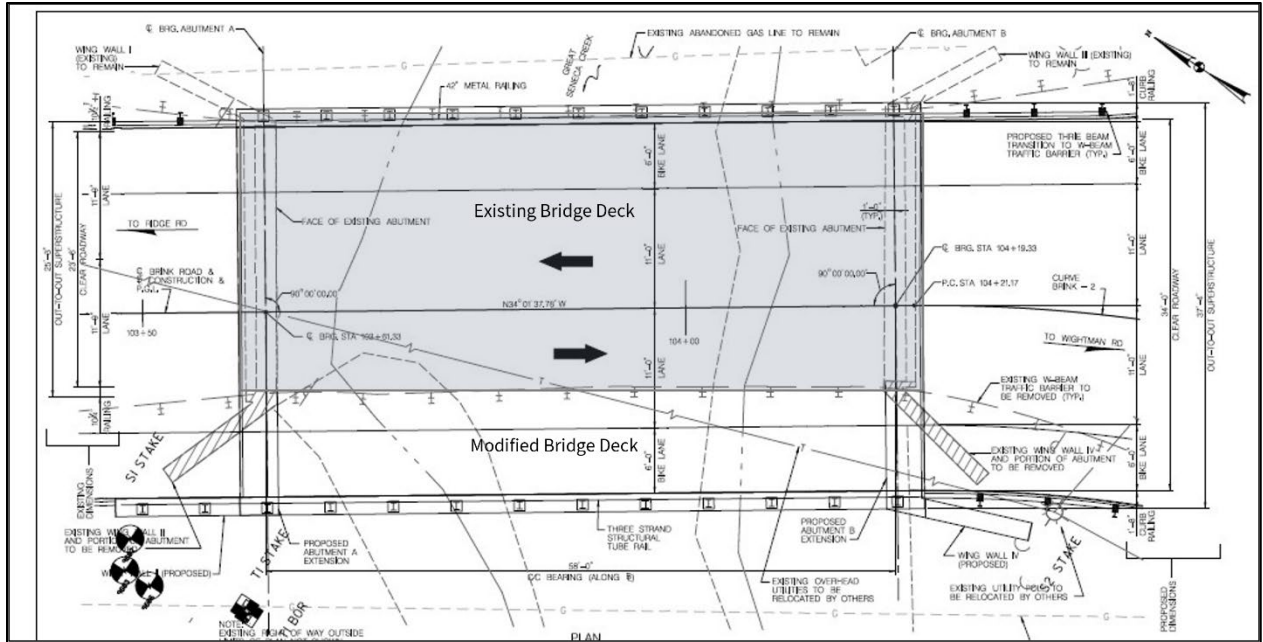


Figure 6: Brink Road Plan View Existing versus Modified Bridge Deck

Existing Roadway

Brink Road within the project area is primarily a two-way, two-lane, open section arterial roadway that runs in an east-west direction. The posted speed limit is 35 mph. The average annual daily traffic (AADT) on Brink Road in 2022 was approximately 13,200 vehicles per day west of Great Seneca Creek (between Wildcat Road and MD 27 / Ridge Road). The right-of-way along Brink Road varies considerably, but it is approximately 45-feet-wide at Great Seneca Creek and widens out to 60 feet to the east of the creek. Travel lane widths on both Brink Road and Wightman Road are typically 11-feet-wide, with minimal existing shoulders.

Surrounding Land Use

Within the site vicinity, the surrounding land use, as shown below in Figure 7, is parkland immediately adjacent to the proposed bridge structure, and residential neighborhoods to the east and west.

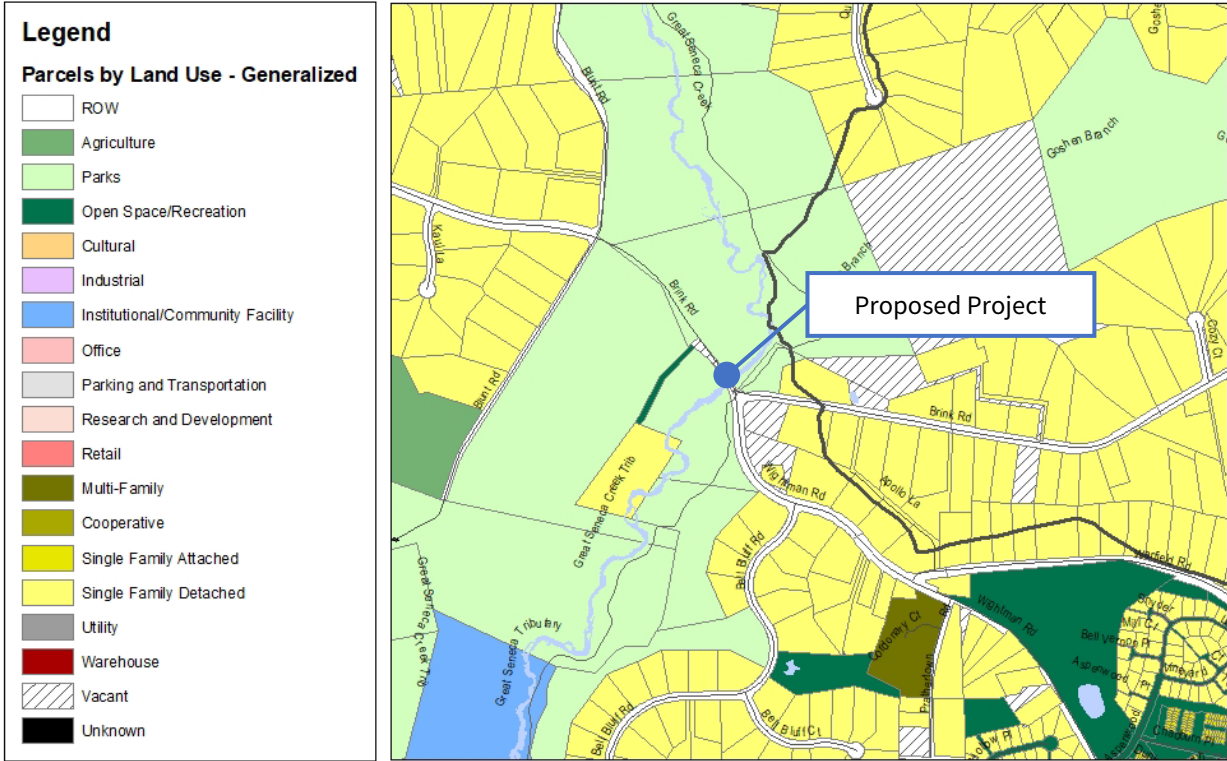


Figure 7: Existing Land Use

SECTION 4 – MANDATORY REFERRAL AUTHORITY AND PROCESS

Mandatory Referral review is guided by the Montgomery Planning Mandatory Referral Review Uniform Standards (December 2022), and the authority granted through the Maryland Land Use Article, Section 20-301, et.seq. As set forth in Sections 20-301 and 20-302, the Montgomery County Planning Board has jurisdiction over mandatory referral projects presented by the federal government, State of Maryland, Montgomery County government, Montgomery County Board of Education, and public utilities, among others, for:

- (1) acquiring or selling land;
- (2) locating, constructing or authorizing a road, park, public way or ground, public building or structure, or public utility; or
- (3) changing the use of or widening, narrowing, extending, relocating, vacating or abandoning any of the previously mentioned facilities.

The Planning Board must review such projects and transmit comments on the proposed location, character, grade and extent of the activity to the project applicant.

As described in the Uniform Standards, the Planning Board considers all relevant land use and planning aspects of the proposal including, but not limited to, the following:

- (1) whether the proposal is consistent with the County's General Plan, functional plans, the approved and adopted area master plan or sector plan and any associated design guidelines, and any other public plans, guidance documents, or programs for the area;
- (2) whether the proposal is consistent with the intent and the requirements of the zone in which it is located;
- (3) whether the nature of the proposed site and development, including, but not limited to, its size, shape, scale, height, arrangement, design of structure(s), massing, setback(s), site layout, and location(s) of parking is compatible with the surrounding neighborhood and properties;
- (4) whether the locations of buildings and structures, open spaces, landscaping, recreation facilities, and pedestrian and vehicular circulation systems are adequate, safe, and efficient;
- (5) whether the proposal has an approved NRI/FSD and a preliminary SWM Concept Plan, and meets the requirements of the Forest Conservation law (Chapters 19 and 22A of the Montgomery County Code);
- (6) whether a Preliminary or a Final Water Quality Plan has been reviewed by the Planning Board if the project is located in a Special Protection Area. In addition, for a Water Quality Plan on public property, the Board must determine if the plan meets any additional applicable standards for Special Protection Areas;
- (7) whether or not the site would be needed for park use if the proposal is for disposition of a surplus public school or other publicly owned property; and
- (8) whether alternatives or mitigation measures have been considered for the project if the proposal is inconsistent with the General Plan or other plans and policies for the area, or has

discernible negative impacts on the surrounding neighborhood, the transportation network, the environment, historic resources (including burial sites), or other resources.

SECTION 5 – MANDATORY REFERRAL ANALYSIS AND FINDINGS

Master Plan Consistency

The Brink Road bridge over Great Seneca Creek is located within the boundaries of the Agricultural & Rural Open Space Functional Master Plan.

Master Plan of Highways and Transitways: Brink Road between MD 27 / Ridge Road and Wightman Road is classified in the Master Plan of Highways and Transitways as a Country Connector with two lanes and an 80-foot-wide master planned right-of-way and no current master planned target speed; however, the road currently has a posted speed limit of 35 mph.

Bicycle Master Plan: Brink Road between MD 27 / Ridge Road and Wightman Road has a proposed sidepath on the south side of Brink Road as shown below in Figure 8. This sidepath is not identified as a prioritized bikeway in the Bicycle Master Plan.

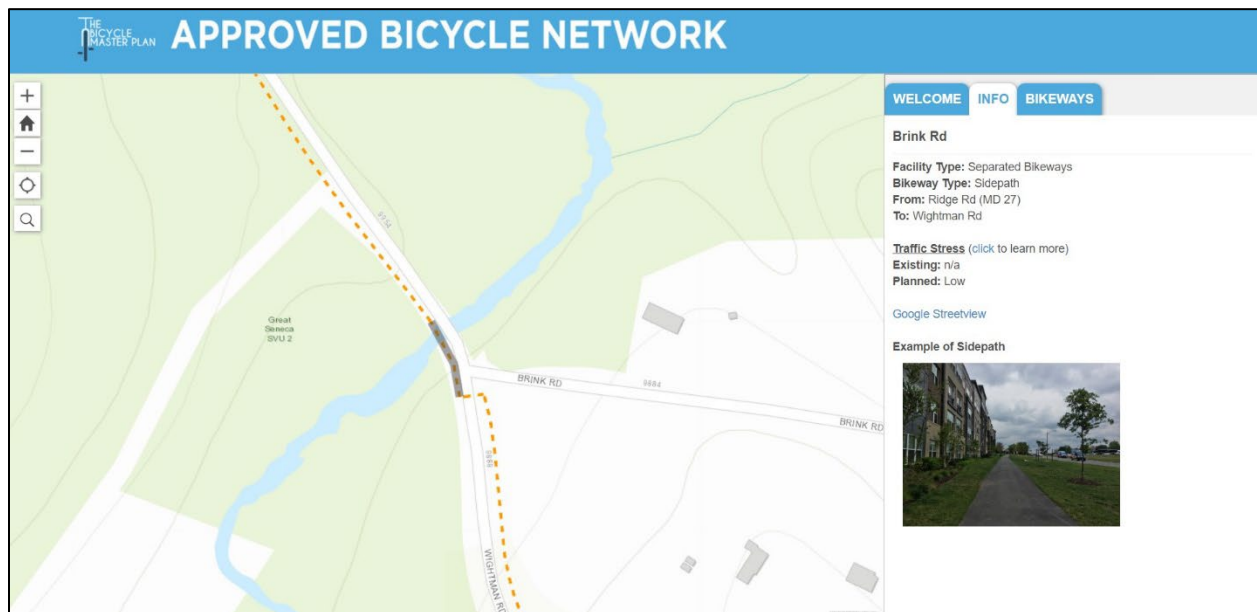


Figure 8: Bicycle Master Plan Bikeway Recommendations for Brink Road Area

The proposed cross section is not fully consistent with the *Bicycle Master Plan*, as it does not include a sidepath on the south side of the bridge.

Recommendation: Widen the proposed bridge by eight feet for a total bridge width of 45 feet four inches (42 feet bridge clearance) in order to accommodate the master-planned sidepath:

- a 12-foot-wide sidepath on the south side, composed of a 1.5-foot-wide buffer from the bridge parapet, an 8-foot-wide sidepath, and a 2.5-foot-wide buffer with a protective railing,
- a two-foot-wide shoulder between the planned sidepath and the travel lanes,
- two 11-foot-wide travel lanes, and
- a six-foot-wide shoulder on the north side.
- The sidepath across this bridge should connect to the Seneca Greenway Trail on the east side of the bridge.

This is illustrated in Figure 9.

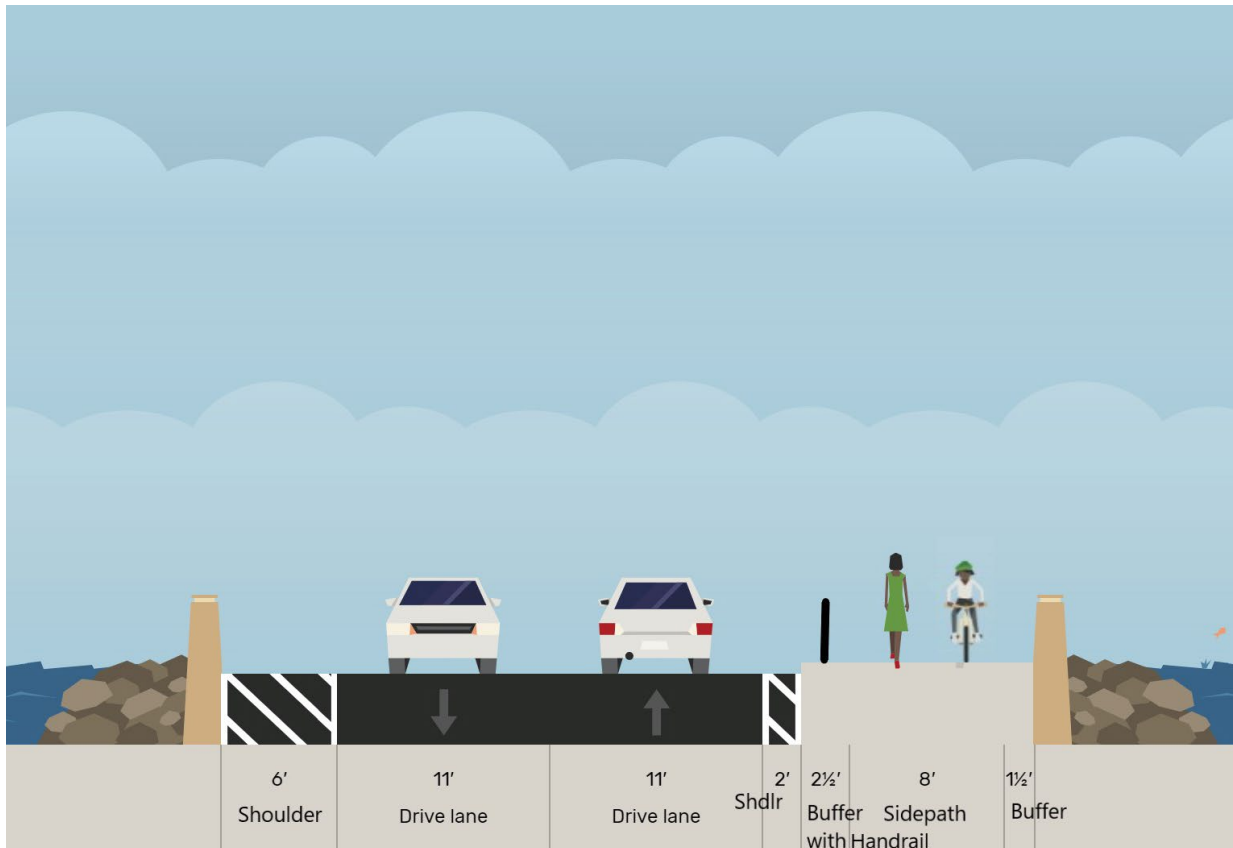


Figure 9: Recommended Bridge Cross Section with Master Plan Vision

Transportation Best Practices

COMPLETE STREETS DESIGN GUIDE

The typical cross section for a Country Connector in the Complete Streets Design Guide is shown below in Figure 10.

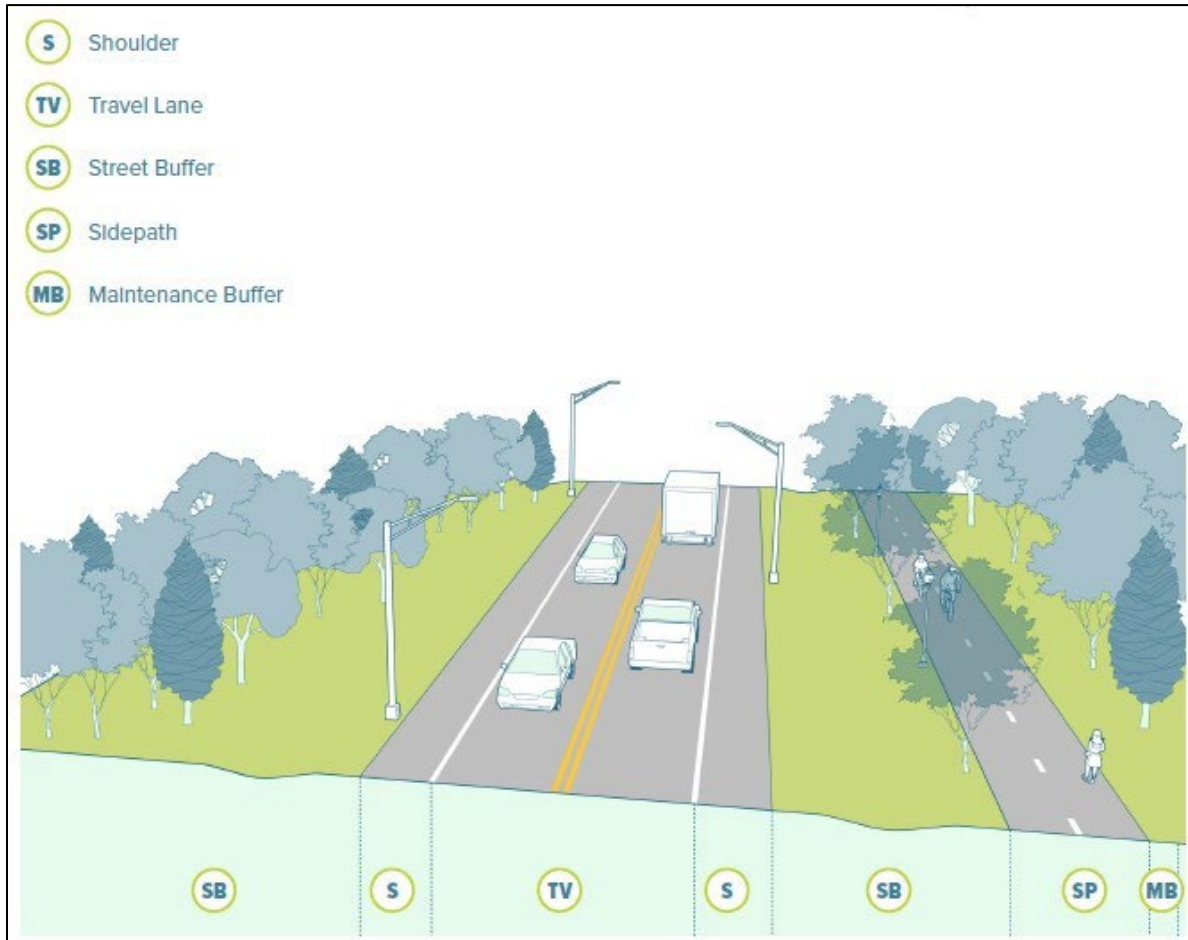


Figure 10: Country Connector Typical Cross Section

Per the Complete Streets Design Guide, a Country Connector street should have the following default design standards:

- Target Speed: 40 mph
- Travel lanes: 11 feet default
- Shoulders: six feet default (when no bicycle facilities are explicitly provided)
- Bicycle Facilities: Sidepath on one side of the street (10 feet default, eight feet min) or bikeable shoulders (10 feet default, five feet minimum)
- Sidewalk: None
- Sidepath: 10 feet default, eight feet minimum

The proposed design is consistent with the CSDG street type, except that it lacks pedestrian and bicycle accommodations.

DETOUR STUDY

A traffic detour study was submitted by MCDOT evaluating existing peak hour traffic conditions. This study, however, appears to be incomplete in that it did not forecast likely traffic diversions, evaluate detour traffic operations on those roads, or propose detour mitigation. The traffic detour route and recommendations, shown below in Figure 11, appear sound and logical and should minimize the potential for any undue neighborhood traffic impacts.

Recommendation: MCDOT should complete a detour study as part of future design efforts to ensure that traffic impacts during construction will be minimal or mitigated by construction mitigation efforts.

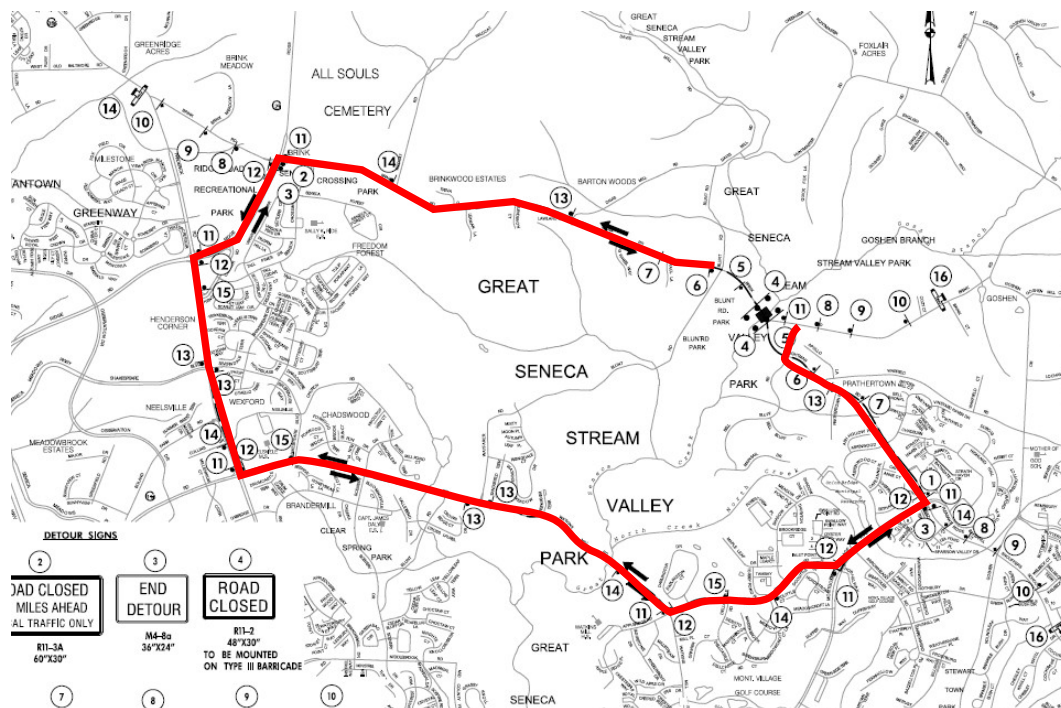


Figure 11: Proposed Detour Route During Construction

Environment

ENVIRONMENTAL GUIDELINES

The Brink Road Bridge crosses Great Seneca Creek in the Great Seneca Creek watershed within the Great Seneca Stream Valley Park. This project proposes the replacement of the existing bridge, approximately 1,000 linear feet of approach roadway work, improvements to the intersection with Wightman Road and improved crosswalks and parking for park users. Most of the project is located in

the floodplain and stream valley buffer associated with Great Seneca Creek. The banks of the Great Seneca Creek are forested, with approximately 8,841 square feet of forest within the limits of disturbance (LOD).

While the Environmental Guidelines are designed to protect environmental features by restricting development in stream valley buffers, disturbance is allowed for unavoidable road and utility crossings on a case-by-case. In this case, the bridge is replacing an existing bridge and disturbance has been minimized in the addition of the shoulder directly adjacent to the traffic lanes. This allows the proposed project to meet the Environmental Guidelines section IV.A.1.f even though additional disturbance is required for the increased bridge width due to the shoulder and sidepath as proposed in staff recommendations.

FOREST CONSERVATION

A Forest Conservation Exemption request was confirmed on November 21, 2022, Plan No. 42023004E, for the proposed project. The proposed project is exempt from Article II of the Montgomery County Code, Chapter 22A (Forest Conservation Law), Section 22A-5(e) because the site is a county highway construction activity that is subject to Section 22A-9 of the Forest Conservation Law which states:

a. General

1. This section applies to construction of a highway by the County as part of an approved Capital Improvements Program project.
 2. The construction should minimize forest cutting or clearing and loss of specimen or champion trees to the extent possible while balancing other design, construction, and environmental standards. The constructing agency must make a reasonable effort to minimize the cutting or clearing of trees and other woody plants.
- b. If the forest to be cut or cleared for a county highway project equals or exceeds 20,000 square feet, the constructing agency must reforest a suitable area at the rate of one acre of reforestation for each acre of forest cleared.
- c. Reforestation for County highway projects must meet the standards in subsections 22A-12(e), (g) and (h).
- d. Any mitigation requirement for loss of specimen or champion trees must be based on the size and character of the tree.

The confirmed Forest Conservation Exemption plan shows the removal of 8,841 square feet of forest. If the final construction plans show forest removal over 20,000 square feet of forest, this project will be subject to reforestation requirements under 22A-9 and will have to replant an equivalent amount of forest. The plan also proposes the removal of numerous trees ranging in size from eight inch caliper up to approximately 26 inch diameter breast height (DBH). The confirmed exemption includes a tree save plan that requires 11,326 square feet (0.26 acres) of mitigation for the project consisting of stream bank plantings of live stakes, wrapped soil lifts, tree plantings, shrub plantings and floodplain seed mix plantings.

Recommendation: Prior to any land disturbing activities the Applicant must have a Forest Conservation Exemption Plan that matches the final layout and design of the project including the LOD.

Historic Preservation

The Brink Road Bridge over Mill Creek is not listed on either the Montgomery County Master Plan for Historic Preservation or the Locational Atlas and Index of Historic Sites.

There are no Master Plan historic sites or districts within the area of the Brink Road Bridge Mandatory referral. Historic preservation staff understand that Montgomery Parks Cultural Resources Stewardship staff have previously coordinated with MCDOT to ensure that a nearby archaeological site on parkland remains outside the LOD. MCDOT should continue to work with Montgomery Parks Cultural Resources Stewardship staff as needed regarding any changes to the LOD on parkland, and to resolve any other concerns raised by Montgomery Parks.

Parks Department Review

PARKLAND AND RESOURCE DESCRIPTION

The proposed bridge construction occurs in MCDOT right-of-way and on parkland. The project will result in temporary and permanent impacts within the project area. Great Seneca Stream Valley Park has nine units and consists of over 1,500 acres, and protects the Great Seneca stream valley with undeveloped open space consisting of riparian and upland forest and forested wetlands. This project falls within Stream Valley Units 2 and 3. The only active recreation amenity located in the vicinity of the project is the natural surface Seneca Greenway Trail, which crosses both Brink Road and Wightman Road.

The majority of the project extent falls within the 100-year FEMA floodplain for Great Seneca Creek. The existing bridge spans Great Seneca Creek, which is a tributary to the Potomac River. Great Seneca Creek is designated as a Use Class I-P stream which means the stream is classified as a water contact recreation, protection of aquatic life, and public water supply stream. The Maryland Department of the Environment (MDE) enforces time of Year Restrictions for Use I streams that begin March 1 and extend through June 15; this means no instream work can occur during this time. Photos of the Brink Road Bridge and Great Seneca Creek are shown in Figure 12.

There are no official State or Federal records for listed plant or animal species within the delineated project area. There is Forest Interior Dwelling Bird habitat adjacent to the project site, and as such disturbance to the riparian corridor should be minimized.



Figure 12: Great Seneca Creek

PARKLAND IMPACTS

The MCDOT proposed bridge will be approximately twelve feet wider than the existing bridge to accommodate the wider abutments and provide for six-foot-wide shoulders in either direction. Any additional widening of the bridge to accommodate a sidepath may add parkland impacts on the southern side of the bridge and those impacts will have to be reviewed and approved during the Park Construction Permit review. The increased bridge width will require permanent impact on parkland for the bridge structure and shoulders, and temporary impacts on parkland for the construction of the bridge and roadway. The project extents are shown in Figure 13.

Approximately 14,000 square feet (0.32 acre) of Great Seneca Stream Valley Park will transfer from M-NCPPC to MCDOT control to provide the necessary MCDOT right-of-way to accommodate the new bridge, a permanent impact to the park. In addition, approximately 37,652 square feet (0.86 acre) of parkland are expected to have temporary construction impacts from this project.

Tree impacts will consist of the removal of 17 live trees, consisting of 254-inch DBH. Mitigation for impacts to park trees (with a 6-inch DBH or greater) damaged or removed, shall either be (1) replacement planting on parkland at a rate of one inch to one inch diameter or (2) a monetary per inch caliper basis at the rate of \$100/diameter inch, to be paid to Montgomery Parks prior to completion of construction. The tree plantings described in this section are intended to fulfill Montgomery Parks tree mitigation requirements and any other regulatory tree requirements are separate from these requirements.

Recommendation: Mitigation for impacts to park trees (with a 6-inch DBH or greater) damaged or removed, shall either be (1) replacement planting on parkland at a rate of one inch to one inch diameter or (2) a monetary per inch caliper basis at the rate of \$100/diameter inch, to be paid to Montgomery Parks prior to the completion of construction.

Use of the adjacent Seneca Greenway Trail will be maintained throughout project construction. Improvements to the crosswalks at Brink Road and Wightman Road will be made. To mitigate for projected impacts, a parking lot will be developed adjacent to the intersection of Brink Road and Wightman Road. There is a great need for the created parking lot, which will be ADA-compliant and will increase accessibility and safe access to the Seneca Greenway Trail for park users.

Great Seneca Creek flows underneath the Brink Road Bridge with a natural channel and no impairments or blockages to aquatic organism passage. No stream work is proposed for this project and the cross-section under the bridge will remain the same length over the creek. However, the bridge deck will be slightly higher, allowing more capacity for stream flows under the bridge. Montgomery Parks appreciates MCDOT's commitment to improving the stream conditions at Great Seneca Creek by building a new bridge with a slightly raised low point to allow for more stream capacity underneath.

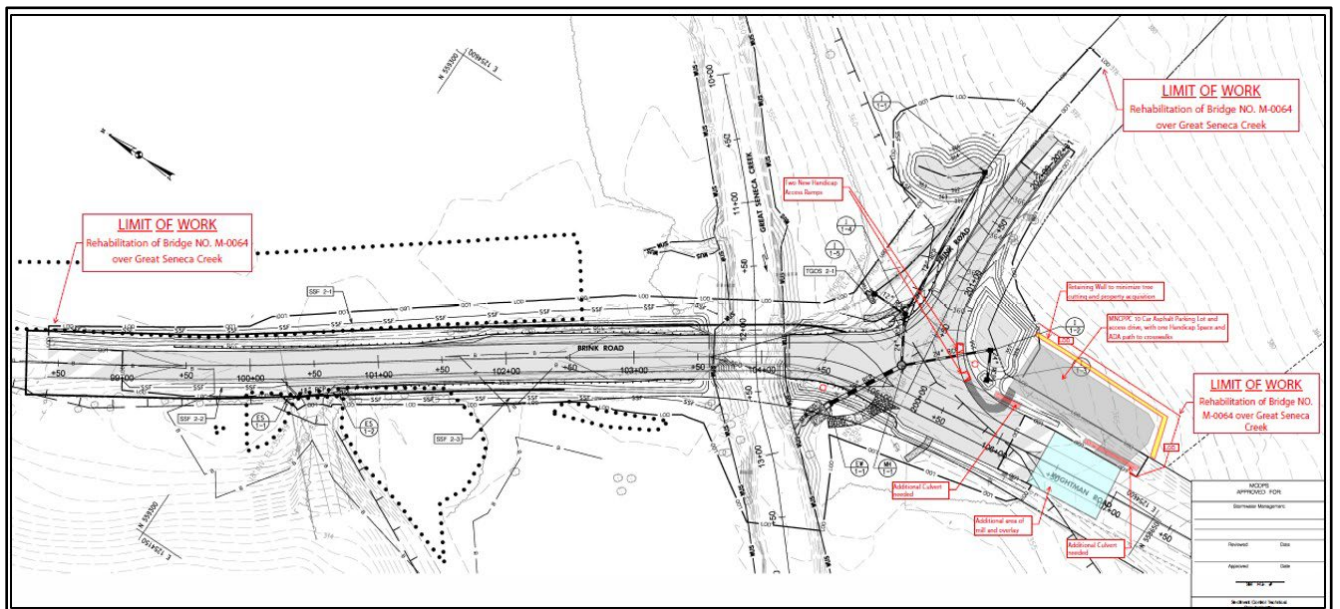


Figure 13: Project Extents

PARK CONSTRUCTION PERMIT

MCDOT will be required to obtain a Park Construction Permit from Montgomery Parks prior to commencement of any construction activities on parkland. Plans submitted for Park Construction Permit review must include existing topography, utilities, and identify and locate all trees (with size and species) larger than 6-inch DBH and greater within 100 feet of the proposed LOD on park property. During Park Construction Permit Review, park staff will work with MCDOT to minimize impacts to parkland to the greatest extent possible and avoid all critical resources identified. MCDOT will continue to coordinate with M-NCPPC on the refinement of the trail connection and proposed stormwater facilities designs and tie-ins. Montgomery Parks tree mitigation will be fulfilled through the on-site planting of a diverse tree and shrub palette approved by Montgomery Parks.

Recommendation: Construction plans must be submitted to the M-NCPPC Department of Parks for review as part of the Park Construction Permit process to ensure that all work is performed in accordance with M-NCPPC standard details, specifications, and policies. No work on parkland may occur until an approved Park Construction Permit is issued for the project.

Recommendation: MCDOT must continue to coordinate with M-NCPPC on the refinement of the trail connections and proposed stormwater facilities designs and tie-ins.

Recommendation: Montgomery Parks tree mitigation will be fulfilled through the on-site planting of a diverse tree, shrub, and shrub palette approved by Montgomery Parks.

RIGHT-OF-WAY EXPANSION

The current project plan indicates approximately 14,000 square feet (0.32 acres) of Great Seneca Stream Valley Park needs to be added to the MCDOT right-of-way to accommodate the new bridge structure and roadway improvements, creating a permanent impact to the park. Since this land is County-titled parkland under control and management of the Commission via the 1972 County-Commission Agreement, property rights will be transferred to right-of-way via a Release Agreement that the Commission will execute and place in the county land records. Since the funds used to acquire this land was appropriated to the Commission for parks and open space purposes, M-NCPPC must be compensated at fair market value for the land area that will no longer be serving park purposes. Payment for the new right-of-way area must be made to Montgomery Parks prior to issuance of the Park Construction Permit. Commission land that is proposed to be transferred to MCDOT is shown in purple in Figure and Figure .

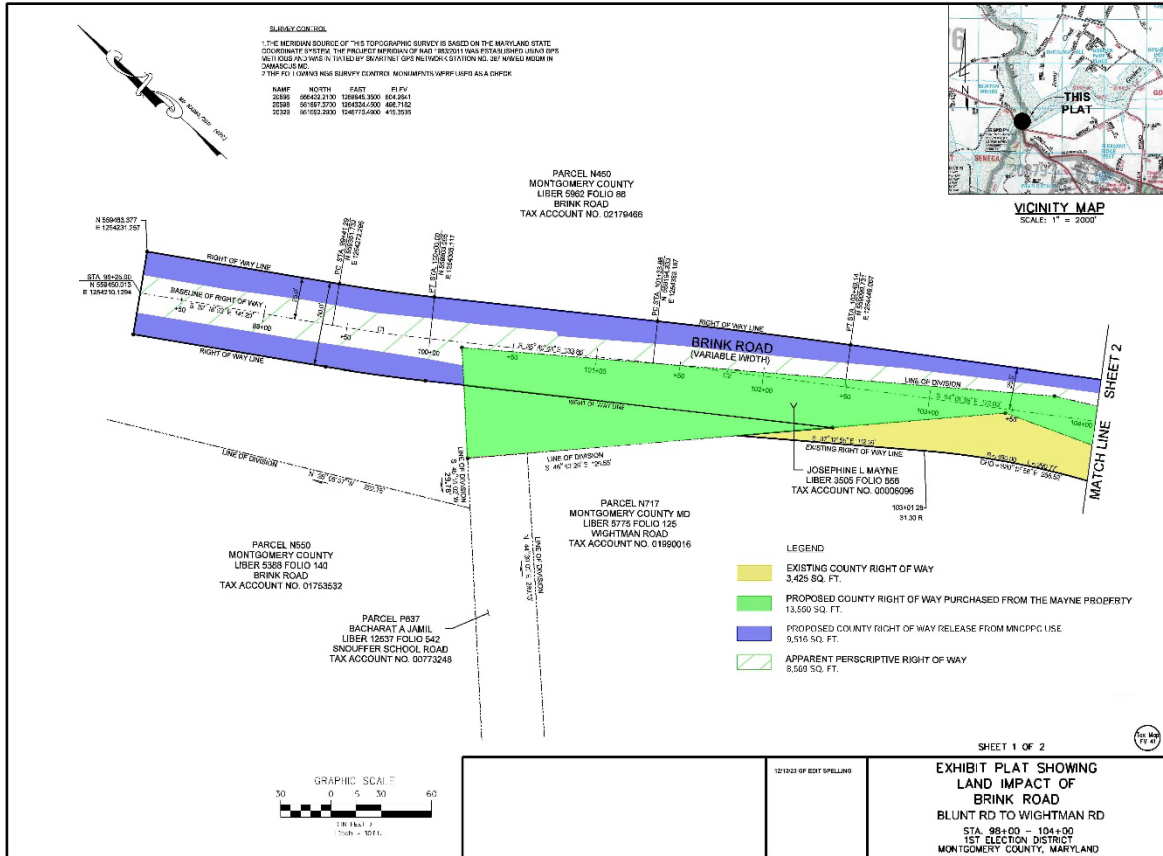


Figure 14: Parkland to Be Converted to MCDOT Right-of-Way (Sheet 1 of 2)

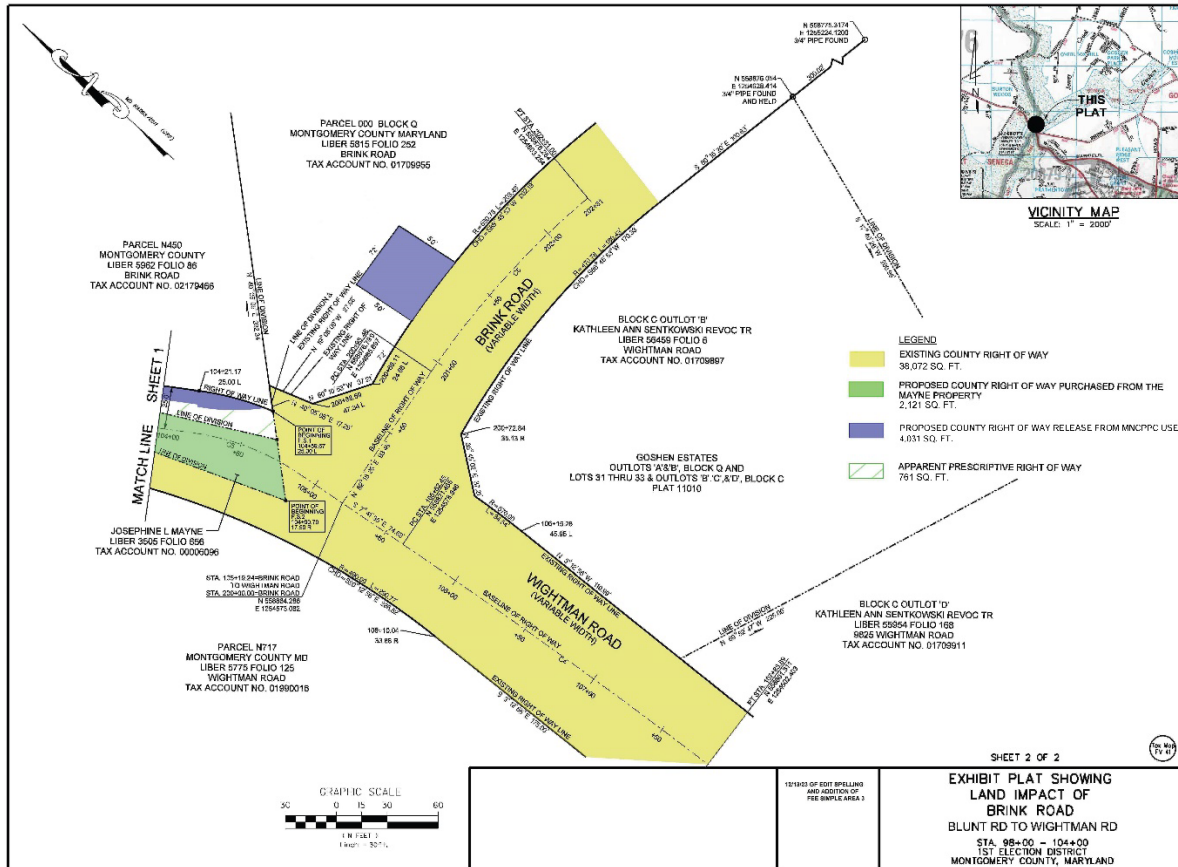


Figure 15: Parkland to Be Converted to MCDOT Right-of-Way (Sheet 2 of 2)

Recommendation: MCDOT must compensate the M-NCPPC at fair market value for all land that will transfer from M-NCPPC’s Great Seneca Stream Valley Park to MCDOT’s road right-of-way, estimated at approximately 14,000 square feet. Payment for the transferred land must occur before issuance of the Park Construction Permit. The new right-of-way area will be transferred to MCDOT by M-NCPPC via a Release Agreement recorded in the County Land Records.

PROGRAM OPEN SPACE CONVERSION REQUIREMENTS

Approximately 9,870 square feet of the total parkland being transferred into MCDOT right-of-way was originally acquired with State Program Open Space (POS) funds that will result in a “conversion” of POS parkland that must be addressed. Maryland Department of Natural Resources (MD DNR, the agency that implements the POS program) requires that any POS purchased land converted away from public recreation and open space be replaced with new parkland that is of equal or greater land area, appraised land value, and recreational value.

To satisfy these POS conversion requirements, MCDOT proposes to purchase part of the Sentowski property at the corner of Brink Road and Wightman Road and transfer ownership to M-NCPPC (see Figure). This replacement land will be approximately 10,000 square feet to meet the land area

standard and will provide a new parking lot for park patrons that allows for safe, ADA-compliant access to the Seneca Greenway trail, an important recreational benefit. Appraisals will be done to confirm that the final replacement land meets the value portion of the requirements.

MD DNR has conducted a preliminary review of this parcel for the proposed conversion and supports M-NCPPC and MCDOT moving forward with this approach. After project plans, land negotiations, and appraisals are further along in the process, M-NCPPC will submit to MD DNR the formal application for conversion of POS parkland. After MD DNR has processed the conversion proposal and receives approval from the Maryland Board of Public Works, M-NCPPC will work with MCDOT to complete the process, including amending existing POS covenants in the Montgomery County land records and creating a new POS covenant over the replacement POS parkland.

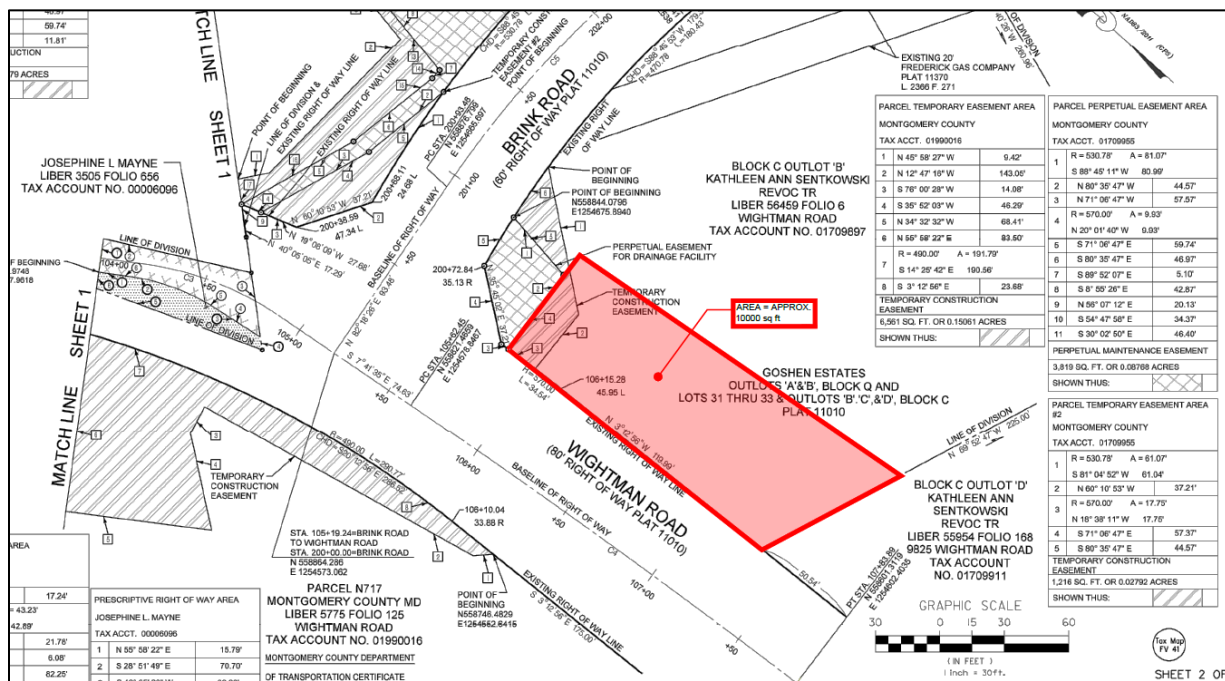


Figure 16: Proposed Replacement Parkland (Approximate) for POS Conversion Application to MD DNR

Recommendation: MCDOT must provide an appropriate parcel of new parkland as a replacement for the State Program Open Space (POS) funded parkland this project will convert to right-of-way, approximately 9,870 square feet, prior to the issuance of a Park Construction Permit.

SECTION 7 – COMMUNITY OUTREACH

No public meetings have been held to-date by MCDOT for this project. A public meeting is anticipated in early 2024.

After staff accepted the Mandatory Referral for review, Montgomery Planning notified local civic and homeowners' associations and other interested parties of this proposal. As of the date of this report, Planning staff have received one comment on this project from the public. This comment is included with this report as Attachment B. All public comments on this project will be discussed during the staff presentation.

SECTION 8 - CONCLUSION

Planning staff recommends transmittal of the recommendations noted above as comments to MCDOT.

SECTION 9 - ATTACHMENTS

Attachment A: Brink Road Design Plans

Attachment B: Public Comments received by Montgomery Planning