

MANDATORY REFERRAL NO. 2024020 – NORTH HIGH STREET EXTENDED



Description

This is a Mandatory Referral review for the Montgomery County Department of Transportation proposal for the improvement and extension of North High Street in Olney, Maryland. The project will connect with Morningwood Drive at a three-way stop controlled intersection and will install other pedestrian and bicycle safety improvements.

No. MR2024020

MCPB

2425 Reddie Drive

Item 12

Floor 14

09/12/2024

Wheaton, MD 20902

SA	Sofia Aldrich, Planner IV – Transportation, Countywide Planning & Policy Division Sofia.Aldrich@montgomeryplanning.org , 301-495-4528
DA	David Anspacher, Chief, Countywide Planning & Policy Division David.Anspacher@montgomeryplanning.org , 301-495-2191

LOCATION

North High Street between Georgia Avenue (MD 97) and Morningwood Drive

MASTER PLANS

2018 Master Plan of Highways and Transitways, 2005 Olney Master Plan

APPLICANT

Montgomery County Department of Transportation

ACCEPTANCE DATE

May 20, 2024, delayed until July 27, 2024 due to FCP Exemption/LOD issue

REVIEW BASIS

Md. Land Use Article, Section 20-301, et seq.

Summary

- Montgomery County Department of Transportation project to extend and improve North High Street, connecting it to Morningwood Drive.
- The proposed project is fully funded as part of the North High Street Extended (FY 25 CIP Number P502310).
- Montgomery Planning Staff recommends approval of the Mandatory Referral with comments and transmittal of comments to Montgomery County Department of Transportation.
- The Planning Board review of a Mandatory Referral is pursuant to the Land Use Article of the Maryland Annotated Code, Section 20-301, et seq.

CONTENTS

Section 1 – Recommendations	2
Section 2 – Project Description.....	2
Project Description.....	2
Background	6
Surrounding Neighborhood.....	7
Section 3 – Mandatory Referral Authority and Process	11
Section 4 – Mandatory Referral Analysis and Findings	13
Master Plan Consistency	13
Other Mandatory Referral Uniform Standard Findings	17
Transportation Best Practices	17
Environment.....	22
Historic Preservation.....	23
Section 5 – Community Outreach.....	23
Section 6 – Conclusion	23
Section 7 – Attachments	23

Figure 1: Study Area Map.....	3
Figure 2: Proposed Roadway Typical Cross Section	3
Figure 3: North Main Street Termination.....	4
Figure 4: Proposed Project Plan View	4
Figure 5: Existing Street Rights-of-Way (Source: MCAtlas.org)	5
Figure 6: Profile of North High Street with Proposed Design.....	6
Figure 7: North High Street, Looking West toward Georgia Avenue (MD 97)	7
Figure 8: North High Street, Looking East toward Morningwood Drive	8
Figure 9: Stairway Connection to North High Street.....	9
Figure 10: Drop-Off Loop on West Side of Morningwood Drive	9
Figure 11: Morningwood Drive Looking North/West.....	10
Figure 12: Morningwood Drive Looking South/East	10
Figure 13: Alternative 3 Plan View Recommended by Planning Board	14
Figure 14: Alternative 3 Cross Section Recommended by Planning Board.....	14
Figure 15: Existing Zoning.....	15
Figure 16: Comparison of Existing, Proposed and Ultimate Cross Sections.....	16
Figure 17: Diagram from Complete Streets Design Guide showing the Active Zone and the Street Zone	18
Figure 18: Proposed North High Street/Morningwood Drive Intersection Missing Crosswalk.....	19
Figure 19: Sidewalk-Level Driveways.....	20
Figure 20: Realign Sidewalk to Cross Driveway at a Perpendicular Angle	21
Figure 21: Sidewalk Shift with Goddard School Sidewalk Retained	21

SECTION 1 – RECOMMENDATIONS

Planning Staff recommends the transmittal of the following comments to the Montgomery County Department of Transportation:

Transportation Recommendations

1. Coordinate with MDOT SHA to explore improvement needs for the intersection of Georgia Avenue and North High Street, once this project is complete.
2. Provide a crosswalk on the north leg of the North High Street and Morningwood Drive intersection with appropriate ADA-accessible curb ramps.
3. Close or relocate the existing drop-off loop located on the west side of Morningwood Drive.
4. Provide raised crossings at all driveways—also called sidewalk-level driveways—in line with the *2024 Complete Streets Design Guide*.
5. Retain the existing sidewalk alignment on the south side of the road along the Goddard School property and modify the alignment of the proposed sidewalk to connect to it.
6. Realign a short section of the proposed sidewalk to cross the alley driveway at a perpendicular angle.
7. Prioritize increasing Active Zone facility widths to meet the Town Center Street guidelines in the *2024 Complete Streets Design Guide* in the following order:
 - a. Street Buffer: widen the Street Buffer to six feet. Where on-street parking is provided, reduce this buffer width to three feet (11-foot wide total buffer width).
 - b. Sidewalk: widen the Sidewalk to the default 10 feet (8 feet minimum) width. At this point, not only will it be more consistent with the CSDG, but it will essentially become a sidepath due to the lack of off-street bicycle facilities with this design.

Environmental Recommendations

8. When replacing street trees, use native tree species specified in the [Montgomery County Tree Manual](#).

SECTION 2 – PROJECT DESCRIPTION

Project Description

The Montgomery County Department of Transportation (MCDOT) proposes to extend North High Street to the west to connect with Morningwood Drive at a three-way intersection. The site location is shown in Figure 1.

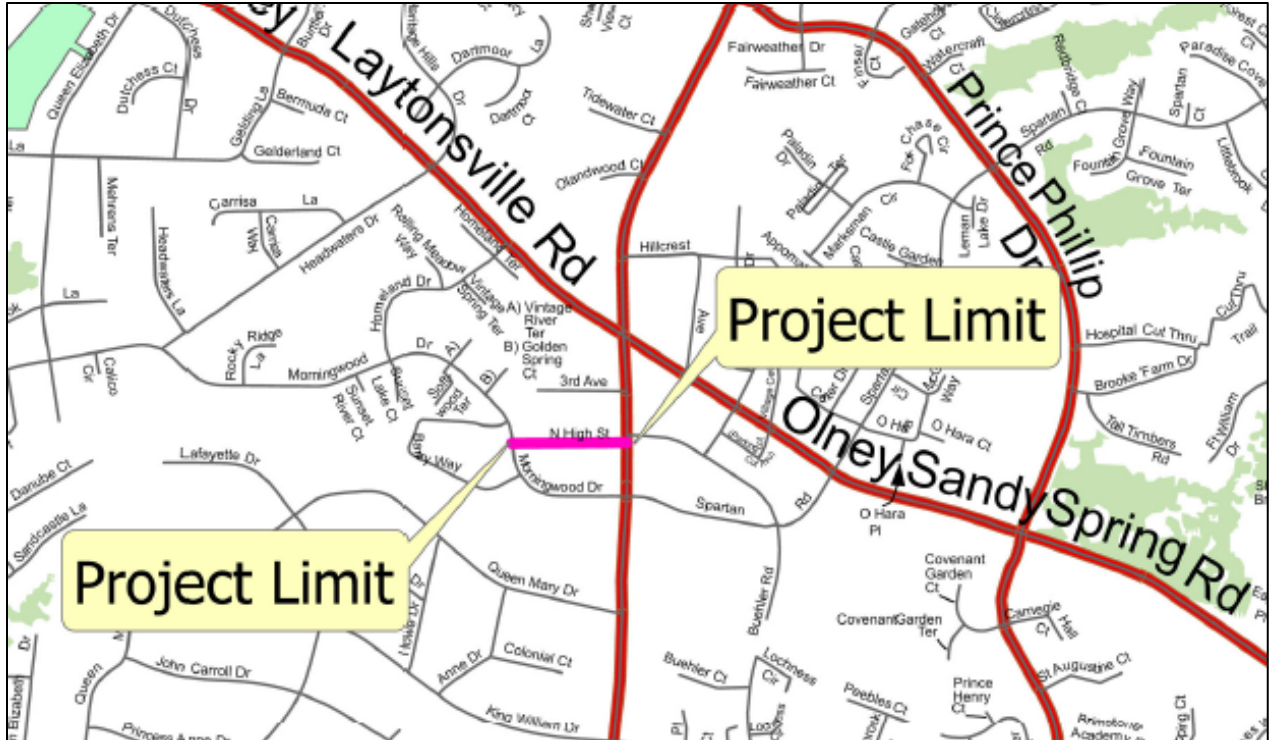


Figure 1: Study Area Map

The proposed project will extend North High Street a short distance to create a new intersection with Morningwood Drive and construct a six-foot wide sidewalk along the south side of the street. Due to the grade difference between North High Street and Morningwood Drive, regrading of North High Street is required. The typical roadway section in Figure 2 shows generally the proposed configuration of the project, although some sections of the street will be designed with on-street parking on the south side of the street. A photo of the existing street termination from the east is shown in Figure 3. The plan view of the proposed design is shown in Figure 4. Engineering drawings for the project are included in Attachment A.

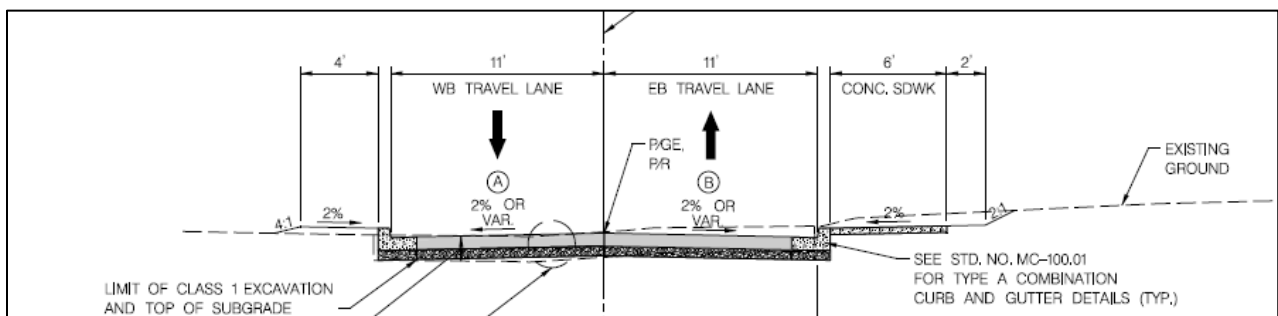


Figure 2: Proposed Roadway Typical Cross Section



Figure 3: North Main Street Termination

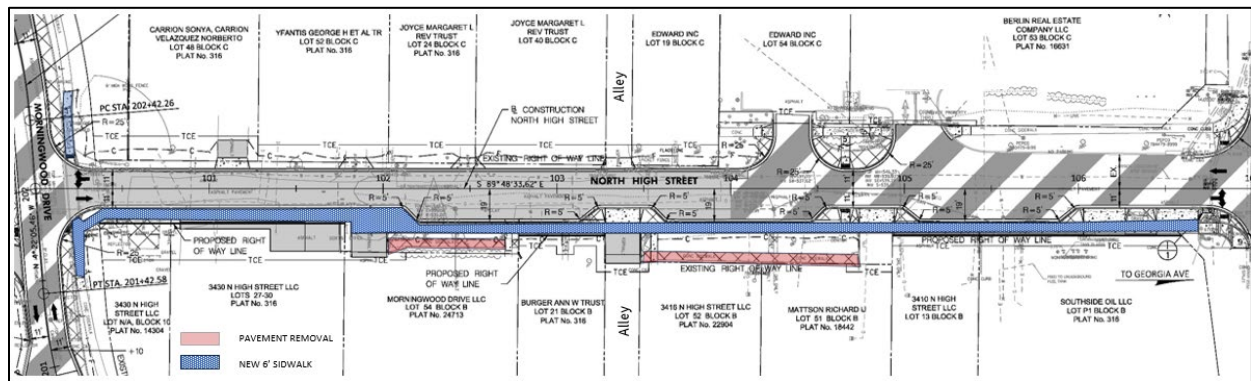


Figure 4: Proposed Project Plan View

In Figure 4 above, the proposed sidewalk is highlighted in blue dotted shading, and the existing sidewalk removal is highlighted in red shading. This project is unusual in that two sections of existing sidewalk with generous buffers (190 feet in length and approximately 32 percent of the total street distance) are recommended to be removed and replaced with a sidewalk with no buffer: in front of the Goddard School and in front of two properties at 3412 and 3416 North High Street. The rationale behind this is that providing a street buffer between the street and the sidewalk would require

purchasing additional right-of-way, which would exceed the project budget. The proposed project is not impacting right-of-way to the residential properties on the north side of the street.

The master plan right-of-way for North High Street is 70 feet, but currently the existing right-of-way widths vary along the street length as shown below in Figure 5 from 30 feet to 76 feet. At project completion, North High Street will continue to have an irregular right-of-way.

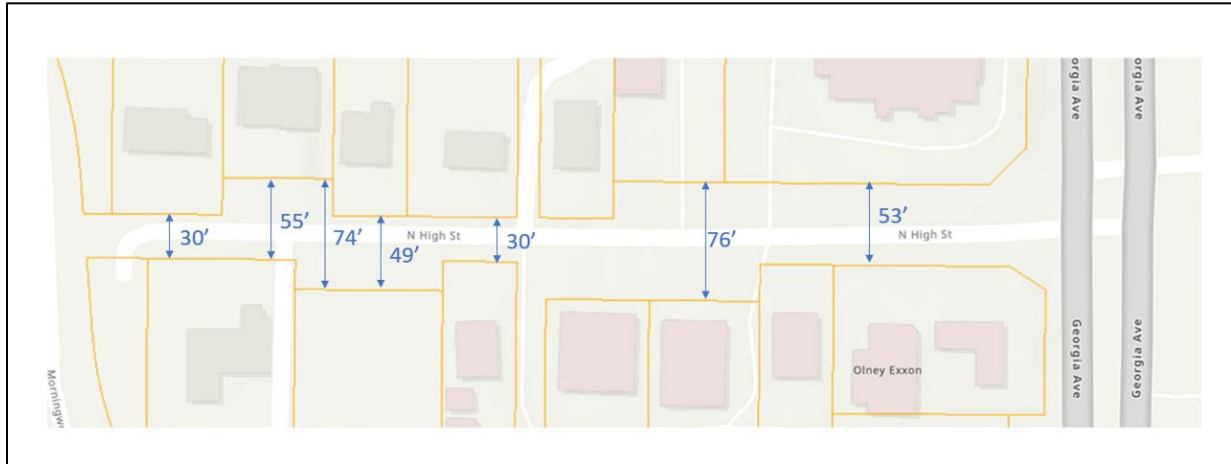


Figure 5: Existing Street Rights-of-Way (Source: MCAAtlas.org)

The proposed project will require the regrading of the western section of North High Street in order to connect smoothly to Morningwood Drive. Figure 6 displays the street profile showing existing elevations and proposed grade of the project.

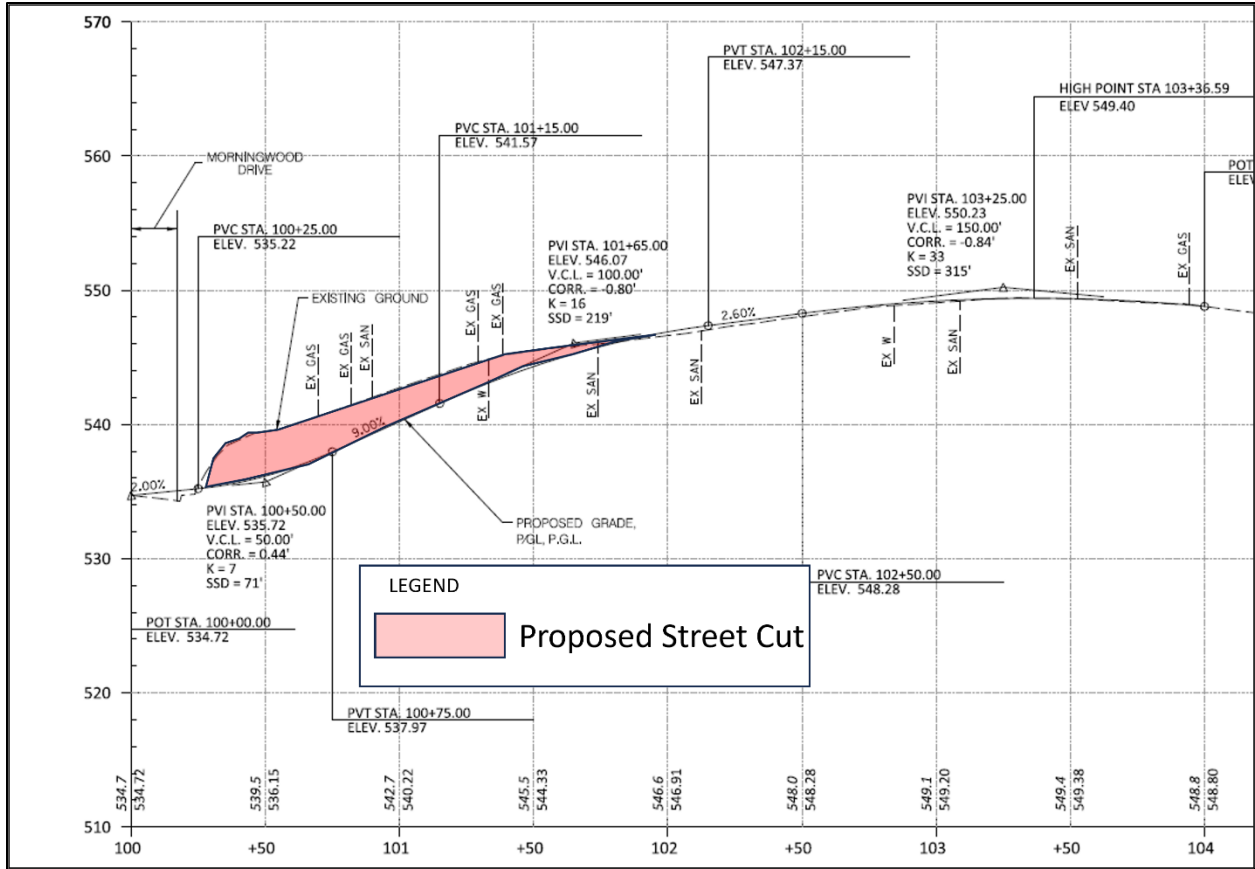


Figure 6: Profile of North High Street with Proposed Design

Background

The proposed project was presented to the Planning Board in 2021 (see Attachment B for Planning Board transmittal letter and staff report). During that review, the Planning Board considered two alternatives put forth by MCDOT:

- Alternative #1: Sidewalk on South Side
- Alternative #2: Sidewalk on South Side, Sidepath on North Side

Ultimately, the Planning Board recommended a third option that would be similar to Alternative #2, except a sidepath would be on the south side of the road and a sidewalk (to be added as part of redevelopment) would be on the north side of the road. The Board also recommended that MCDOT should continue to explore the best intersection configuration and operations for the intersection of Morningwood Drive with North High Street.

This project was recommended in the 2005 *Olney Master Plan* and developing street grids is supported in *Thrive Montgomery 2050*. The project (CIP number P502310) is fully funded in the capital budget with expected construction in FY26.

Surrounding Neighborhood

The surrounding neighborhood is mainly small, local businesses on the south side of the road and a mix of residential and commercial along the north side. The area north of North High Street along Georgia Avenue (MD 97) is part of the Olney Village commercial area. North High Street operates primarily as an access road for these mixed uses and has high on-street parking use from the adjacent commercial businesses. North High Street currently has a pedestrian connection on its western end via a stairway to Morningwood Drive. There are discontinuous sidewalks along both sides of the road.

NORTH HIGH STREET

North High Street is located on the west side of Georgia Avenue approximately 320 feet north of the Morningwood Drive signalized intersection. The intersection of Georgia Avenue (MD 97) with North High Street is unsignalized. The intersection is subject to southbound backups stretching back North High Street making egress from North High Street particularly challenging.

North High Street is a two-lane, two-way Town Center Street with a 25-mph target speed and the Working Draft of the 2024 *Master Plan of Highways and Transitways Technical Update* proposes to retain this designation. It has two distinct characteristics: on the eastern end, the roadway is wider with adequate room for on-street parking (see Figure 7), while on the western end, parking is only provided infrequently (see Figure 8). The total length of North High Street is approximately 600 feet.

The posted speed limit is 25 mph on North High Street and 30 mph on Georgia Avenue (MD 97). In 2023, the calculated MDOT SHA Annual Average Daily Traffic (AADT) volumes along Georgia Avenue to the south of Morningwood Drive was approximately 30,000 vehicles per day. In addition, MCDOT conducted peak hour counts and traffic analysis in 2020 (see Attachment C).



Figure 7: North High Street, Looking West toward Georgia Avenue (MD 97)



Figure 8: North High Street, Looking East toward Morningwood Drive

MORNINGWOOD DRIVE VICINITY

Morningwood Drive is a two-lane, two-way east-west Neighborhood Connector and experiences high traffic use throughout the day. Morningwood Drive, according to MDOT SHA, carries approximately 4,855 vehicles per day. Along Morningwood Drive, a stairway is provided to connect pedestrians from Morningwood Drive up to North High Street (which is at a higher elevation) as shown in Figure 9. There is private drop-off loop on the west side of the street in what will become part of the intersection with North High Street (see Figure 10) and is located on a curved section of Morningwood Drive, as demonstrated in Figure 11 and Figure 12, which create operational challenges. Sidewalks exist along both sides of Morningwood Drive with adequate buffers.

MCDOT conducted a safety analysis in 2021 (Attachment D) to address safety concerns along Morningwood Drive and to assess potential improvements/measures to support the proposed street extension. A major recommendation of this study was the lowering of the posted speed limit on Morningwood drive from 30 mph to 25 mph. Consistent with Vision Zero practices, information from this safety assessment has been used to inform the project design.



Figure 9: Stairway Connection to North High Street



Figure 10: Drop-Off Loop on West Side of Morningwood Drive



Figure 11: Morningwood Drive Looking North/West



Figure 12: Morningwood Drive Looking South/East

SECTION 3 – MANDATORY REFERRAL AUTHORITY AND PROCESS

Mandatory Referral review is guided by the Montgomery Planning Mandatory Referral Review Uniform Standards (December 2022), and the authority granted through the Maryland Land Use Article, Section 20-301, et.seq. As set forth in Sections 20-301 and 20-302, the Montgomery County Planning Board has jurisdiction over mandatory referral projects presented by the federal government, State of Maryland, Montgomery County government, Montgomery County Board of Education, and public utilities, among others, for:

- (1) acquiring or selling land;
- (2) locating, constructing or authorizing a road, park, public way or ground, public building or structure, or public utility; or
- (3) changing the use of or widening, narrowing, extending, relocating, vacating or abandoning any of the previously mentioned facilities.

The Planning Board must review such projects and transmit comments on the proposed location, character, grade and extent of the activity to the project applicant.

As described in the Uniform Standards, the Planning Board considers all relevant land use and planning aspects of the proposal including, but not limited to, the following:

- (1) whether the proposal is consistent with the County's General Plan, functional plans, the approved and adopted area master plan or sector plan and any associated design guidelines, and any other public plans, guidance documents, or programs for the area;
- (2) whether the proposal is consistent with the intent and the requirements of the zone in which it is located;
- (3) whether the nature of the proposed site and development, including but not limited to its size, shape, scale, height, arrangement, design of structure(s), massing, setback(s), site layout, and location(s) of parking is compatible with the surrounding neighborhood and properties;
- (4) whether the locations of buildings and structures, open spaces, landscaping, recreation facilities, and pedestrian and vehicular circulation systems are adequate, safe, and efficient;
- (5) whether the proposal has an approved NRI/FSD and a preliminary SWM Concept Plan, and meets the requirements of the Forest Conservation law (Chapters 19 and 22A of the Montgomery County Code);
- (6) whether a Preliminary or a Final Water Quality Plan has been reviewed by the Planning Board if the project is located in a Special Protection Area. In addition, for a Water Quality Plan on public property, the Board must determine if the plan meets any additional applicable standards for Special Protection Areas;
- (7) whether or not the site would be needed for park use if the proposal is for disposition of a surplus public school or other publicly-owned property; and

- (8) whether alternatives or mitigation measures have been considered for the project if the proposal is inconsistent with the General Plan or other plans and policies for the area, or has discernible negative impacts on the surrounding neighborhood, the transportation network, the environment, historic resources (including burial sites), or other resources.

SECTION 4 – MANDATORY REFERRAL ANALYSIS AND FINDINGS

Master Plan Consistency

As described in the Uniform Standards outlined in Section 3 of this staff report, the Planning Board considers whether the proposal is consistent with the County's General Plan, *Thrive Montgomery 2050*, functional plans, area master plans, and any associated design guidelines.

As stated previously, North High Street is an existing two-lane Town Center Street stub street that has been planned to be extended to Morningwood Drive since the approval of the 2005 *Olney Master Plan*. Town Center Streets are recommended to have wide sidewalks on either side of the road, a street buffer separating the walkway from the roadway and may have on-street parking, where space is available. While Planning Staff's preference is usually to implement the full master plan vision, in some instances this is not feasible initially and interim steps are reasonable. The proposed design represents a substantial step in achieving the master plan vision, though it does not fully achieve that vision.

The plan achieves the following components of the master plan vision:

- Extends North High Street to Morningwood Drive
- Sets the posted speed limit at 25 mph
- Improves walking conditions by providing a continuous sidewalk on the south side of the road

The 2018 *Bicycle Master Plan* does not recommend a bicycle facility, however, the default typical bicycle facility for a Town Center Street in the Complete Streets Design Guide is one-way separated bike lanes. Given the transitional location of this street and the proposed connection to Morningwood Drive, the default bicycle facility for a neighborhood connector (sidepath) seems more appropriate in this case. During the previous Planning Board briefing, the Planning Board selected a street cross section (Alternative 3) that provided a sidepath on the south side of North High Street (with future design projects to add a sidewalk on the north side when redevelopment opportunities occur), but MCDOT instead proposes to construct a sidewalk on the south side. This alternative is shown in plan view in Figure 13, and the approximate cross section is illustrated below in Figure 14.



Figure 13: Alternative 3 Plan View Recommended by Planning Board

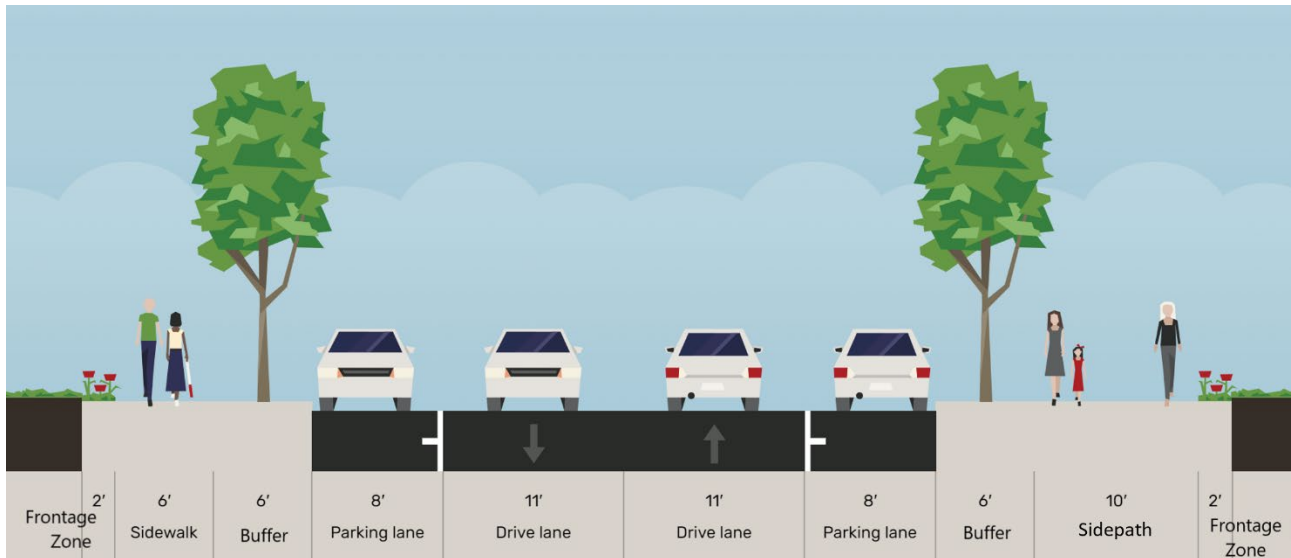


Figure 14: Alternative 3 Cross Section Recommended by Planning Board

While this project represents a substantial step in the right direction, it has a few deficiencies. The proposed street design only provides a six-foot wide sidewalk on the south side of North High Street, often with no street buffers between the street and the sidewalk. There is an existing five-foot wide sidewalk on the north side of North High Street between Georgia Avenue (MD 97) for a distance of 330 feet (fronting existing commercial parcels). The remainder of North High Street on the north side has single family homes with no existing sidewalks likely due to the proximity of these existing homes to the required sidewalk. That said, these properties are zoned CRT mixed use, as shown below in Figure 15, so there is some incentive for redevelopment, at which time the ultimate sidewalk on the north side of North High Street can be achieved.



Figure 15: Existing Zoning

Constructing interim road improvements in the correct ultimate location is critical to ensuring that future street enhancements develop efficiently and cost-effectively. Figure 16 shows how the existing cross section, the proposed cross section and the ultimate typical cross section envisioned by the county’s master plans align. The location of the street and on-street parking along the south or eastbound curb are in a location that aligns with the ultimate cross section. Establishing curb lines is important, as relocating them is costly. In the proposed section, the sidewalk on the south side of the road, however, is not aligned with the ultimate section (six feet) and is not wide enough to accommodate bikes, as recommended by a previous Planning Board. This sidewalk can therefore be considered an interim improvement, and when full right-of-way acquisition is achieved, it will need to be removed in order to construct the ultimate cross section. The Existing Cross Section shown is for the section in front of the Goddard School. The Ultimate Cross Section sidepath (north side) is aligned with the Goddard School sidewalk (it would need to be widened in the future if that sidewalk segment was retained and incorporated into the applicant’s design).

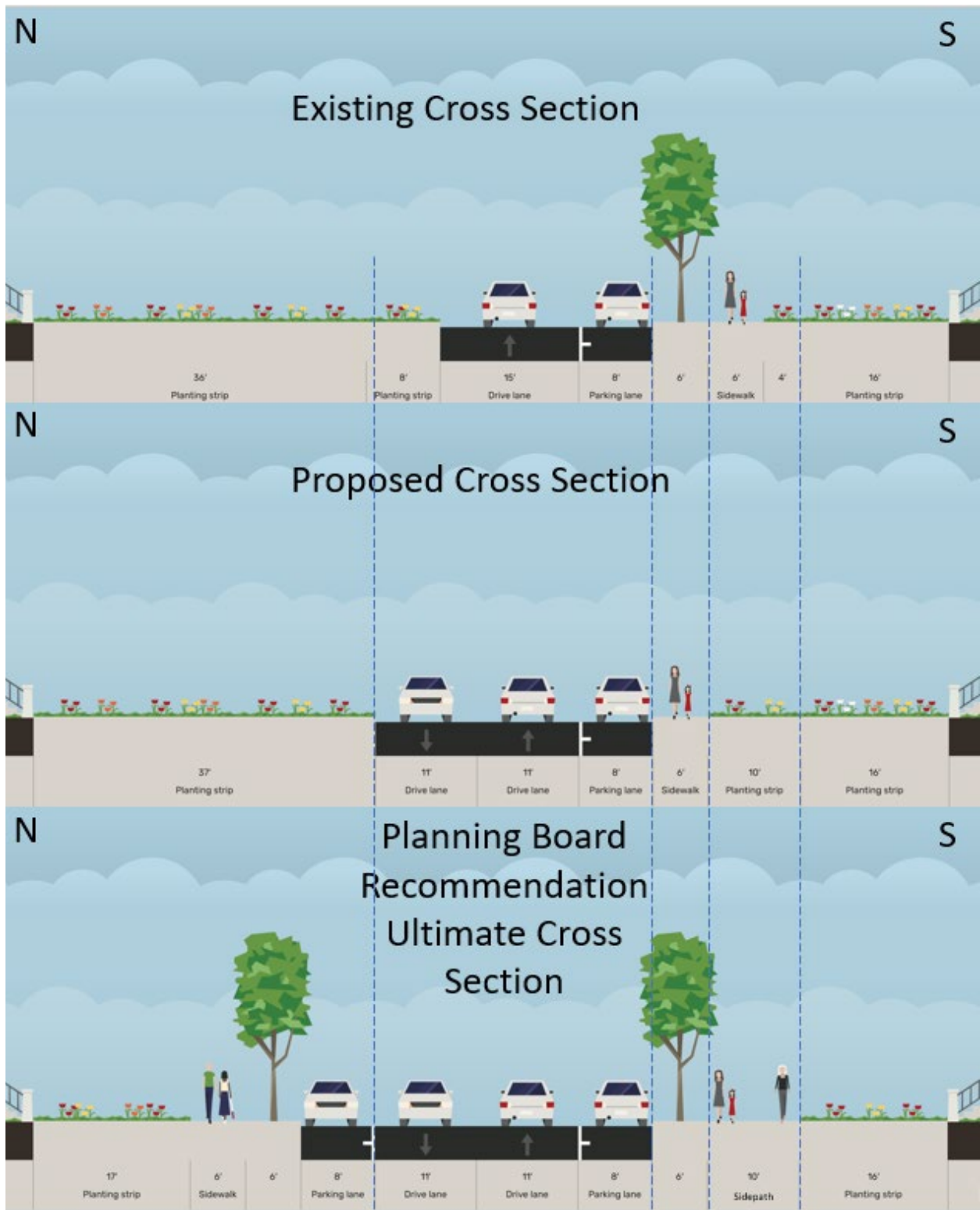


Figure 16: Comparison of Existing, Proposed and Ultimate Cross Sections

Other Mandatory Referral Uniform Standard Findings

Regarding the other aspects outlined in the Uniform Standards in this staff report, not all of them apply to Mandatory Referrals for public transportation projects. The project is consistent with the aspects of the Uniform Standards listed below, and reasoning is provided. All other aspects are not applicable to this project.

- *Consistency with the intent and the requirements of the existing zoning*
 - The proposed project is consistent with the existing commercial and residential zones in this small commercial center. It will improve safety and access to the existing land uses and support future development consistent with the zoning.
- *Compatibility with the surrounding neighborhood and properties*
 - The design, layout, and parking of the proposed project is compatible with the surrounding neighborhood and will improve travel conditions to the standard outlined in master plans for the area.
- *Adequacy, safety, and efficiency of landscaping and pedestrian and vehicular circulation*
 - The entire project aims to improve safety and efficiency of the transportation network in the project area by expanding the street grid and adding a continuous sidewalk.
- *Approval of NRI/FSD, preliminary SWM Concept Plan, and Forest Conservation law compliance*
 - The project meets these requirements. A forest conservation exemption request was granted under Section 22A-5(e) and a plan showing compliance with Section 22A-9 as a “county and municipal highway project.” The stormwater management concept for the project was approved by the Department of Permitting Services (DPS).

Transportation Best Practices

The County Council designated North High Street as a Town Center Street as part of the county’s transition to a Complete Streets roadway classification system. The Town Center Street classification corresponds with design guidance from the 2024 *Complete Streets Design Guide (CSDG)*. The following section outlines how the proposed design achieves stated design goals and best practices.

The 2024 *Complete Streets Design Guide* introduces two distinct zones of a roadway: the Active Zone and the Street Zone. The Active Zone includes the sidewalk and bike lanes, as well as a pedestrian-bike buffer between these two facilities, and a street buffer that separates these active modes of transportation from motor vehicles. The Street Zone includes the parking lane, any travel lanes for motor vehicles, turn lanes, and medians (See Figure 17).

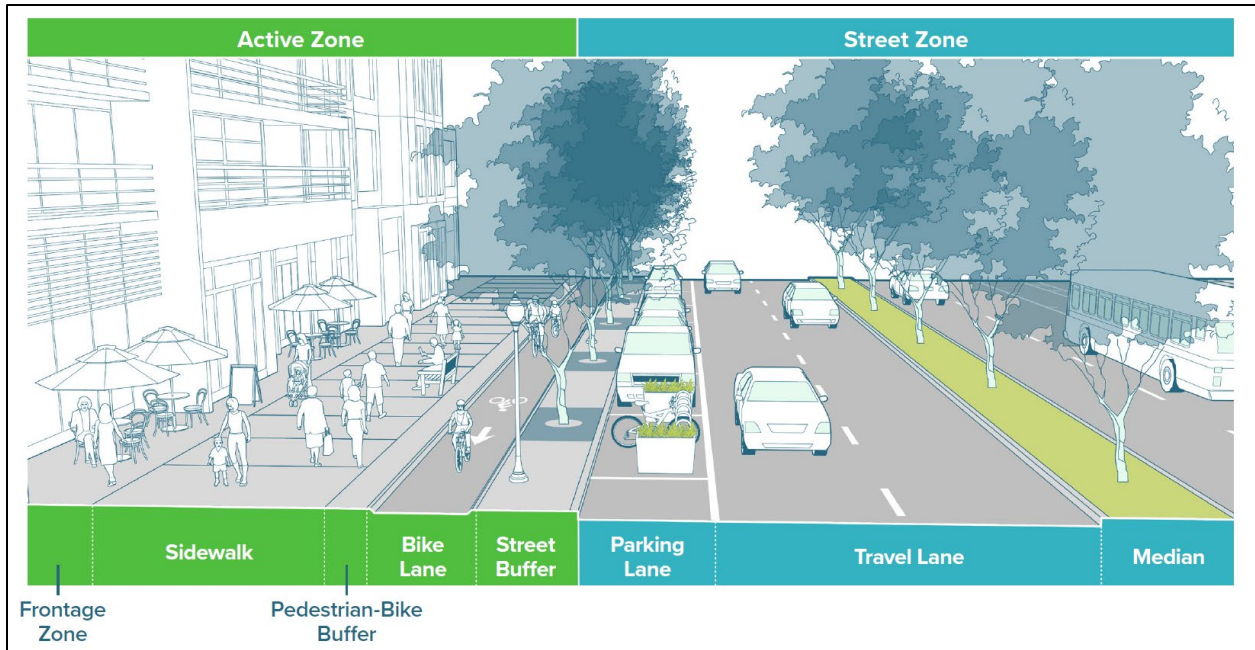


Figure 17: Diagram from Complete Streets Design Guide showing the Active Zone and the Street Zone

While the proposed Active Zone facilities either meet or fall below the minimum width standards in the 2024 *Complete Streets Design Guide*, they do represent an improvement from existing conditions.

The 2024 *Complete Streets Design Guide* also provides guidance on how frequently protected pedestrian crossings should be provided. For Town Center Streets, this recommended spacing is 400 feet. For Georgia Avenue (MD 97), the current spacing between Morningwood Drive and North High Street is only 320 feet. Future signalization at this location is therefore unlikely.

Georgia Ave (MD 97) /North High Street Intersection

1. **Coordinate with MDOT SHA to explore improvement needs for the intersection of Georgia Avenue (MD 97) and North High Street, once this project is complete.** The proposed project will have many effects on traffic circulation in this part of Olney, and this might be an opportunity to address existing access and safety issues on Georgia Avenue (MD 97).

Morningwood Drive/North High Street Intersection

2. **Provide a crosswalk on the north leg of the North High Street and Morningwood Drive intersection with appropriate ADA-accessible curb ramps** (see Figure 18Figure 19). Current practice in Montgomery County is to provide crosswalks on all intersection approaches, whenever possible. This is especially logical, as this intersection will have stop sign control on all three approaches. While the proposed plan has crosswalks on the east and south legs, the crosswalk on the north leg (shown in green shading on the graphic) is missing in the proposed design.

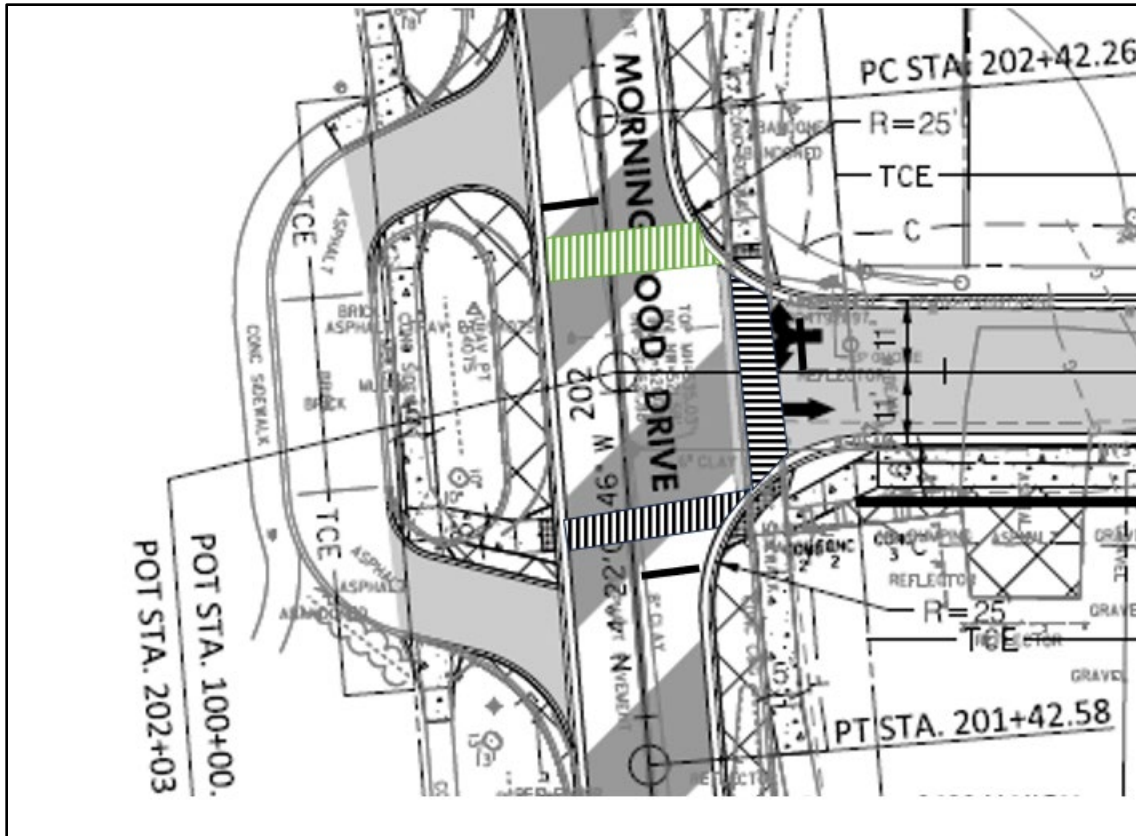


Figure 18: Proposed North High Street/Morningwood Drive Intersection Missing Crosswalk

3. **Close or relocate the existing drop-off loop located on the west side of Morningwood Drive.**
 The existing drop-off loop on the HOC property at 3500 Morningwood Drive will be located within the functional intersection area of the Morningwood Drive/North High Street intersection and is unsafe as proposed. As shown in Figure 18 above, movements into and out of the drop-off-loop will occur extremely close to the proposed intersection.

Traffic Calming and Visibility

4. **Provide raised crossings at all driveways—also called sidewalk-level driveways – in line with the 2024 Complete Streets Design Guide.** Sidewalk-level driveways (see Figure 19) are a recommended design element for Town Center Streets in the 2024 Complete Streets Design Guide.



Figure 19: Sidewalk-Level Driveways

5. **Retain the existing sidewalk alignment on the south side of the road along the Goddard School property and modify the alignment of the proposed sidewalk to connect to it.** The Goddard School sidewalk segment is in the correct ultimate location. Rather than remove this segment as proposed by MCDOT, it would be better to retain the Goddard School sidewalk in its current location.
6. **Realign a short section of the proposed sidewalk to cross the alley driveway at a perpendicular angle.** As designed, this sidewalk design does not accommodate pedestrians with vision impairments, as a pedestrian walking westbound wishing to remain on the sidewalk past the driveway must maneuver at an angle within the driveway apron. With a revised design as shown in Figure 20 using the proposed sidewalk design, this sidewalk connection will be significantly enhanced. In this graphic, the dashed red line displays the travel path of a pedestrian which requires a sharp lateral move that coincides with the alley, whereas the dashed blue line displays a shifting of the sidewalk so that the travel path of the pedestrian crossing the alley is perpendicular, simplifying this movement. If the sidewalk design is modified to match the location of the Goddard School sidewalk (consistent with the previous recommendation), a similar sidewalk shift (see Figure 21) will be needed as this issue will likely still remain.

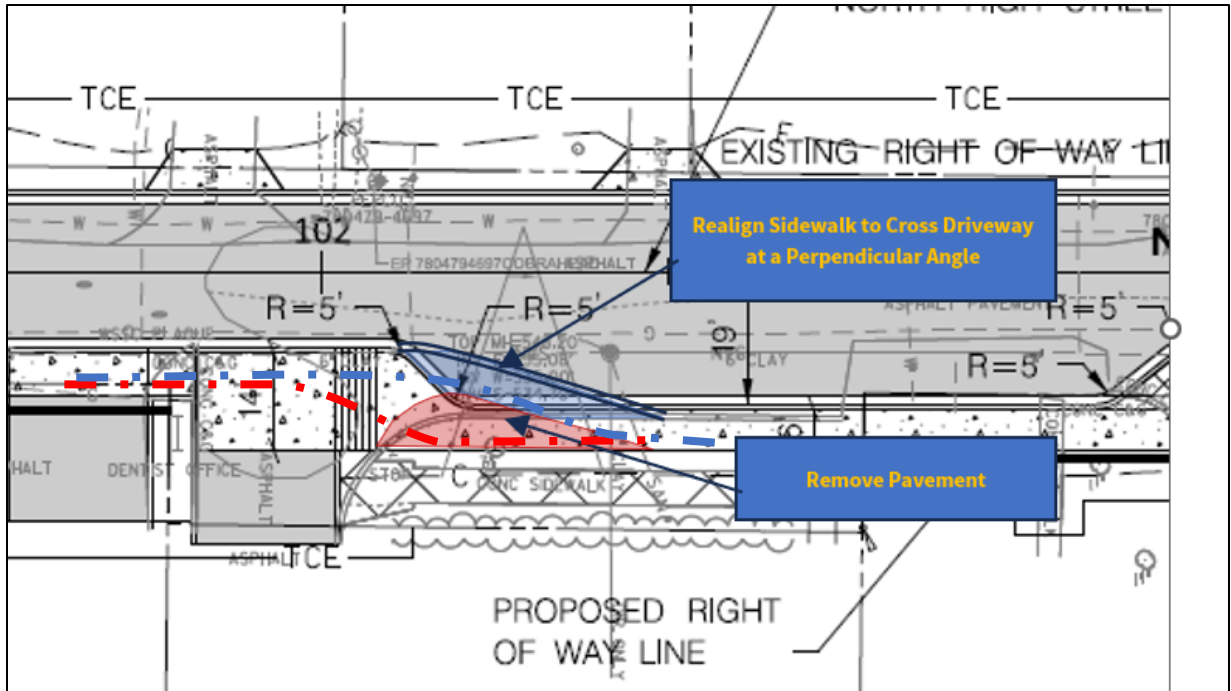


Figure 20: Realign Sidewalk to Cross Driveway at a Perpendicular Angle

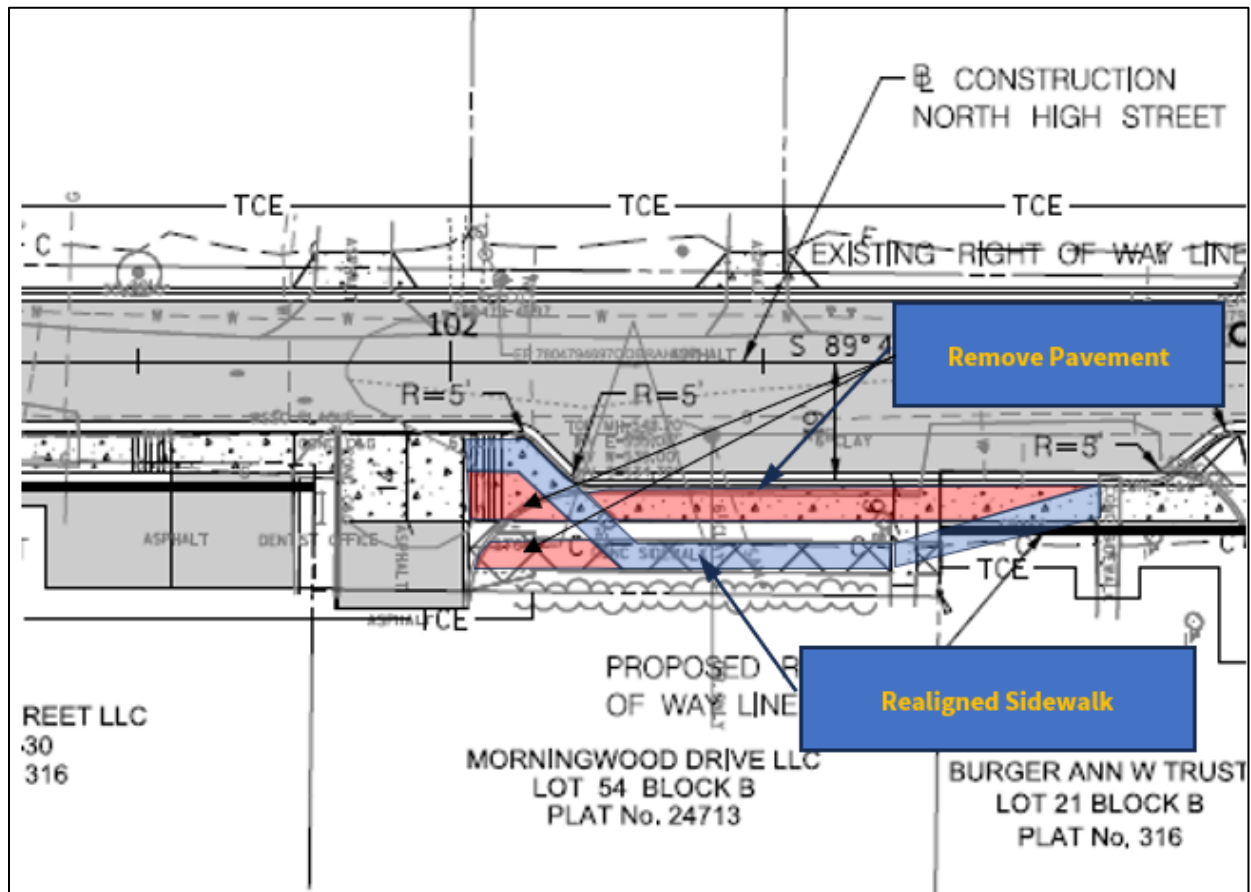


Figure 21: Sidewalk Shift with Goddard School Sidewalk Retained

Facility Widths

7. Prioritize increasing Active Zone facility widths to meet the Town Center Street guidelines in the 2024 Complete Streets Design Guide in the following order:

- a. **Street Buffer:** widen the Street Buffer to six feet. Where on-street parking is provided, reduce this buffer width to three feet (11-foot wide total buffer width).
- b. **Sidewalk:** widen the Sidewalk to the default 10 feet (8 feet minimum) width. At this point, not only will it be more consistent with the CSDG, but it will essentially become a sidepath due to the lack of off-street bicycle facilities with this design.

Environment

The project conforms to the Planning Board-approved Environmental Guidelines for Environmental Management of Development in Montgomery County. The project is within the Upper Rock Creek watershed, a Use I¹ designation.

Any environmental impacts have been minimized to the greatest extent possible but are necessary and unavoidable to achieve the design standards of the proposed bikeway. The following sections evaluate project compliance with forest conservation and stormwater management regulations.

FOREST CONSERVATION

The Project is subject to Chapter 22A, Forest Conservation Law but exempt from Article II under Section 22A-5€ as a “county and municipal highway project.” Therefore, a Forest Conservation Exemption request (Plan No. 42024268E) was granted under Section 22A-5(e) on July 17, 2024. All requirements of an exemption for a County or municipal highway construction activity are detailed in Section 22A-6(d) and Section 22A-9.

The confirmed Forest Conservation Exemption plan shows the removal of 7,840 square feet of forest. If the final construction plans show forest removal over 20,000 square feet of forest, this project will be subject to reforestation requirements under 22A-9 and will have to replant an equivalent amount of forest. The plan also proposes the removal of numerous trees ranging in size from 4” caliper up to approximately 29” diameter breast height (DBH). The confirmed exemption includes a tree save plan, which includes mitigation for the trees proposed for removal.

¹ Use I: WATER CONTACT RECREATION, AND PROTECTION OF NONTIDAL WARMWATER AQUATIC LIFE

Waters that are suitable for: water contact sports; play and leisure time activities where the human body may come in direct contact with the surface water; fishing; the growth and propagation of fish (other than trout); other aquatic life, and wildlife; agricultural water supply, and industrial water supply.

8. When replacing street trees, use native tree species specified in the [Montgomery County Tree Manual](#).

STORMWATER MANAGEMENT

The stormwater management concept for the project was approved by the Department of Permitting Services (DPS) on February 22, 2023. The Applicant requested a waiver in-lieu of meeting required stormwater management goals. The following items will need to be addressed during the detailed sediment control/stormwater management plan stage:

- a) A detailed review of the stormwater management computations will occur at the time of detailed plan review.
- b) An engineered sediment control plan must be submitted for this project.

Historic Preservation

Historic Preservation staff reviewed this project and have no historic preservation concerns.

SECTION 5 – COMMUNITY OUTREACH

MCDOT held a public community meeting for this project on April 10, 2024. The project team plans to address or incorporate the following general comments into the project design:

- Including additional traffic calming measures on Morningwood Drive, such as extending the curb extension (i.e. narrowing down the travel lanes) to the Morningwood Drive entrance of the Goddard School, and
- Adding a pedestrian refuge island near the existing mid-block crossing on Morningwood Drive near the Goddard School entrance.

SECTION 6 – CONCLUSION

Planning Staff recommends transmittal of comments to the Montgomery County Department of Transportation.

SECTION 7 – ATTACHMENTS

Attachment A: Corridor Engineering Drawings

Attachment B: Planning Board letter to MCDOT, November 17, 2021

Attachment C: North High Street Planning Analysis, October 2020, MCDOT

Attachment D: Morningwood Drive Safety Analysis, July 2021, MCDOT