

CENTRO SQUARE

PRELIMINARY PLAN NO. 120250180, SITE PLAN NO. 820250090 AND FOREST CONSERVATION PLAN NO. F20250010




The Preliminary, Site, and Forest Conservation Plans propose to construct 200 dwelling units, including 108 townhomes and 92 multi-family units, including 15% MPDUs under the Standard Method of Development in the CRNF-1.25, C-0.0, R-1.25, H-60 Zone

COMPLETED: 11/28/2025

PLANNING BOARD HEARING DATE: 12/11/2025

MCPB ITEM NO. 7

Planning Staff

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LOCATION/ADDRESS

7501 Standish Place, Derwood; approximately 500 feet north of the East Gude Drive and Crabbs Branch Way intersection

MASTER PLAN

2021 Shady Grove Sector Plan Minor Master Plan Amendment

ZONE

CRNF-1.25, C-0, R-1.25, H-60'

PROPERTY SIZE

13.86 Acres

APPLICANT

Tri Point Homes DC Metro Inc.

ACCEPTANCE DATE

June 26, 2025

REVIEW BASIS

Chapter 50 Subdivision, Chapter 59 Zoning, and Chapter 22A Forest Conservation

Summary:

- Staff recommends approval of the Preliminary Plan, Site Plan, and Final Forest Conservation Plan with conditions
- The applications will implement Local Map Amendment (LMA) No. H-156, 7501 Metro Park Standish Place by providing 200 dwelling units, including 108 townhomes and 92 multifamily units in the form of two-over-twos, including 15% MPDUs under the Standard Method of Development.
- The Central Park, located adjacent to Crabbs Branch Way, will exceed the maximum value of 65 dBA Ldn for exterior recreation areas. The Applicant is seeking relief from this requirement.

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SECTION 1: RECOMMENDATIONS AND CONDITIONS

PRELIMINARY PLAN 120250180

Staff recommends approval with conditions of Preliminary Plan No. 120250180. All site development elements shown on the latest electronic version of the Preliminary Plan No. 120250180 as of the date of this Staff Report submitted via ePlans to the M-NCPPC, are required except as modified by the following conditions.¹

GENERAL APPROVAL

1. This Preliminary Plan is limited to eight (8) lots for up to 92 two-over-twos² (multi-unit dwelling units) and one hundred and eight (108) lots for up to 108 attached (townhouse) dwelling units, including at least 15% MPDUs, and parcels for private roads, open space and stormwater management.

ADEQUATE PUBLIC FACILITIES

2. The Adequate Public Facilities (“APF”) review for the Preliminary Plan will remain valid for five (5) years from the initiation date (as defined in Montgomery County Code Section 50.4.3.J.5).

PLAN VALIDITY PERIOD

3. The Preliminary Plan will remain valid for three (3) years from its initiation date (as defined in Montgomery County Code Section 50.4.2.G), and before the expiration date of this validity period, a final record plat for all property delineated on the approved Preliminary Plan must be recorded in the Montgomery County Land Records or a request for an extension filed.

OUTSIDE AGENCIES

4. The Planning Board has reviewed and accepts the recommendations of the Montgomery County Department of Transportation (“MCDOT”) in its letter dated November 10, 2025, and incorporates them as conditions of the Preliminary Plan approval. The Applicant must comply with each of the recommendations in the letter, which may be amended by MCDOT if the amendment does not conflict with any other conditions of the Preliminary Plan approval.

¹ For the purposes of these conditions, the term “Applicant” shall also mean the developer, the owner or any successor (s) in interest to the terms of this approval.

² Two-over-twos are categorized as “apartments,” or “multi-unit living” based on Section 59.4.1.3.D. of the Montgomery County Code.

5. Before recording a plat for the Subject Property, the Applicant must satisfy MCDOT's requirements for access and improvements.
6. The Planning Board has reviewed and accepts the recommendations of the Montgomery County Department of Permitting Services ("MCDPS"), Water Resources Section in its stormwater management concept letter dated October 30, 2025, and incorporates them as conditions of the Preliminary Plan approval. The Applicant must comply with each of the recommendations in the letter, which may be amended by MCDPS – Water Resources Section if the amendment does not conflict with any other conditions of the Preliminary Plan approval.
7. The Planning Board has reviewed and accepts the recommendations of the Montgomery County Department of Permitting Services ("MCDPS"), Fire Department Access and Water Supply Section in its letter dated October 20, 2025, and incorporates them as conditions of approval. The Applicant must comply with each of the recommendations in the letter, which MCDPS may amend if the amendment does not conflict with other conditions of Preliminary Plan approval.
8. The Planning Board has reviewed and accepts the recommendations of the Montgomery County Department of Housing and Community Affairs ("DHCA"), in its letter dated November 18, 2025, and incorporates them as conditions of approval. The Applicant must comply with each of the recommendations in the letter, which DHCA may amend if the amendment does not conflict with other conditions of Preliminary Plan approval.

OTHER APPROVALS

9. The Applicant must comply with binding elements of County Council Resolution No. 20-850 approving Local Map Amendment No. H-156.
10. Before approval of a record plat or any demolition, clearing or grading for the Subject Property, the Applicant must receive Staff certification of this Preliminary Plan.
11. Except clearing and grading associated with the demolition of building and paving, there shall be no clearing or grading of the site before recordation of plat(s).
12. If an approved site plan amendment for the Subject Property substantially modifies the lot or right-of-way configuration or quantities shown on this Preliminary Plan, the Applicant must obtain approval of a Preliminary Plan amendment before certification of the site plan amendment.

FUTURE SITE PLAN APPROVAL REQUIRED

13. Before clearing or grading demolition or recording a plat for the Subject Property, the Applicant must receive Staff certification of Site Plan No. 820250090. The number and location of site elements, including but not limited to buildings, dwelling units, on-site parking, site circulation, sidewalks, and bikeways will be determined through site plan review and approval.

TRANSPORTATION

Frontage Improvements on Existing Roads

14. The Applicant must provide the following dedications and show them on the record plat(s) for the following existing roads:
 - a) All land necessary to accommodate 40 feet from the existing pavement centerline along the Subject Property frontage for Crabbs Branch Way.
15. Before the recordation of plat, the Applicant must satisfy all necessary requirements of MCDOT to ensure construction of an asphalt sidepath separated from traffic by a street buffer with trees along the Property frontage on Crabbs Branch Way, varying in width, following the specifications outlined in the approved cross-section on Sheet 07-SITE-820250090-SP-6 of the Site Plan.
 - a) The minimum dimensions for the asphalt sidepath must be eight (8) feet, and six (6) feet for the street buffer with trees.

Private Roads

16. The Applicant must provide Private Streets A, B, and C, and Private Alleys A, B, C, D, E, F, G, H, I, J, K, L, and M (collectively the Streets and Alleys are referred to as the “Private Roads”) as shown on the certified plan set on Sheet 07-PREL-120250180-PP7 of the Preliminary Plan, including any sidewalks, parking spaces, storm drainage, facilities, street trees, street lights, private utility systems and other necessary improvements as required by the Preliminary and Site Plans, within the delineated private road areas, subject to the following requirements:
 - a) The record plat must show the Private Roads in separate parcels.
 - b) The Private Roads must be subject by reference on the plat to the Declaration of Restrictive Covenant for Private Roads recorded among the Land Records of Montgomery County, Maryland, in Book 54062 at Page 338, and the terms and conditions in the Montgomery County Code § 50-4.3.E et seq regarding private roads. The Covenant includes, but is not limited to, the following requirements/conditions:
 - i) The Applicant, at its expense, shall design, construct, and maintain the Private Roads.
 - ii) The Applicant, at its sole cost and expense, shall properly and continually maintain (including ordinary and capital maintenance and removal of snow, ice, litter, and other obstructions and hazards as soon as conditions reasonably allow), repair, and replace any portion of the Private Road and all improvements located within the Private Road, in good condition and repair for safe use and operation of the Private Road. The Applicant must maintain a commercially reasonable budget (operating and capital, as applicable) to address both short-term and long-term maintenance, and reserves for capital repairs. The Applicant must provide certification of the reserves to the Planning Board or its Staff every two (2) years (or every five (5) years in the event there are no below-ground

- parking structures in the Private Road). The reserves must be adequate to cover the costs of needed repairs.
- iii) The Applicant must post and retain signage to notify the public that the Private Road is not publicly maintained and to provide contact information to handle complaints, concerns, or questions regarding the Private Road.
 - iv) Approved sub-grade for private roads to be six-inch Graded Aggregate Base (GAB).
- c) Before issuance of the first building permit, the Applicant must deliver to the Planning Department, with a copy to MCDPS, certification by a professional engineer licensed in the State of Maryland that the Private Road has been designed and the applicable building permits will provide for construction in accordance with the paving detail and cross-section specifications required by the Montgomery County Road Code, as may be modified on this Preliminary Plan or a subsequent Site Plan, and that the road has been designed for safe use including horizontal and vertical alignments for the intended target speed, adequate typical section(s) for vehicles/pedestrians/bicyclists, ADA compliance, drainage facilities, sight distances, points of access and parking, and all necessary requirements for emergency access, egress, and apparatus as required by the Montgomery County Fire Marshal.
 - d) Before approval to release any portion of the site plan performance bond, the Applicant must deliver to the Planning Department and MCDPS inspector certification by a professional engineer licensed in the State of Maryland that the Private Roads and Alleys have been constructed in accordance with the paving detail and cross-section specifications required by the Montgomery County Road Code, as may be modified on the Preliminary Plan or Site Plan, including in-place density testing of the roadway soil sub-grade, aggregate base, and asphalt, and that the road has been constructed for safe use including horizontal and vertical alignments for the intended target speed, adequate typical section(s) for vehicles/pedestrians/bicyclists, ADA compliance, drainage facilities, sight distances, points of access and parking, and all necessary requirements for emergency access, egress, and apparatus as required by the Montgomery County Fire Marshall.

RECORD PLATS

- 17. There shall be no clearing or grading of the site before recordation of plat(s).
- 18. The record plat must show necessary easements.

Notes and Labels

- 19. The record plat must reflect all areas under common ownership.
- 20. The record plat must reference the Common Open Space Covenant recorded among the Montgomery County Land Records at Book 28045 Page 578 ("Covenant").

DEVELOPMENTS WITH MPDUS

Centro Square

PRELIMINARY PLAN NO. 120250180, SITE PLAN NO. 820250090 AND FOREST CONSERVATION
PLAN NO. F20250010

21. The final number of MPDUs as required by condition 1 above will be determined at the time of site plan approval.

CERTIFIED PRELIMINARY PLAN

22. The certified Preliminary Plan must contain the following notes:

a) *Unless specifically noted on this plan drawing or in the Planning Board conditions of approval, the building footprints, building heights, on-site parking, site circulation, and sidewalks shown on the Preliminary Plan are illustrative. The final locations of buildings, structures and hardscape will be determined at the time of site plan approval. Please refer to the zoning data table for development standards such as setbacks, building restriction lines, building height, and lot coverage for each lot.*

23. Before submittal of the Certified Preliminary Plan, the Applicant must make the following changes:

- a) Show resolutions and approval letters on the certified set
- b) Show private roadway details and cross sections on the certified set.

SITE PLAN 820250090

Staff recommends approval of Site Plan No. 820250090, for the construction of 200 dwelling units, including 15% Moderately Priced Dwelling Units. The development must comply with the binding elements for Floating Zone Plan No. H-156 as listed in the County Council Resolution 20-850 dated June 17, 2025. All site development elements shown on the latest electronic version of the Site Plan, as of the date of this Staff Report submitted via ePlans to the M-NCPPC, are required except as modified by the following conditions.³

DENSITY, HEIGHT & HOUSING

1. Density

The Site Plan is limited to a maximum of 200 dwelling units (100 Townhomes and 70 two-over-two -units) including 15% or 30 MPDUs.

2. Height

The development is limited to a maximum height of 60 feet, as measured from the average grade along the building facing the applicable abutting and confronting property from the, as illustrated on the Certified Site Plan.

3. Moderately Priced Dwelling Units (MPDUs)

- c) The Planning Board has reviewed and accepts the recommendations of Montgomery County Department of Housing and Community Affairs (DHCA) in its letter dated November 18, 2025 and incorporates them as conditions of the Site Plan approval. The Applicant must comply with each of the recommendations in the letter, which DHCA may amend provided that the amendments do not conflict with other conditions of the Site Plan approval.
- d) The development must provide 15% MPDUs or MCDHCA-approved equivalent consistent with the requirements of Chapter 25A and the applicable Master Plan.
- e) Before issuance of any building permit for any residential unit(s), the MPDU agreement to build between the Applicant and the MCDHCA must be executed.

NOISE

- 4. Before the issuance of the first building permit, the Applicant/developer/builder must provide certification to M-NCPPC Staff from an engineer who specializes in acoustical treatments that:

³ For the purposes of these conditions, the term "Applicant" shall also mean the developer, the owner or any successor (s) in interest to the terms of this approval.

- a) The building shell for residential dwelling units affected by exterior noise levels projected at or above 65 dBA Ldn will attenuate the projected exterior noise levels to an interior level not to exceed 45 dBA Ldn.
5. If any changes occur to the Site or Preliminary plan which affect the validity of the noise analysis dated April 6, 2025, acoustical certifications, and/or noise attenuation features, a new noise analysis will be required to reflect the changes and new noise attenuation features may be required.
6. Before issuance of any Use and Occupancy Certificate or Final Inspection, whichever is relevant, for any of the noise impacted units, a Professional Engineer must certify to the Planning Department and Department of Permitting Services that the noise impacted units have been constructed in accordance with the certification of an engineer that specializes in acoustical treatments.
7. For all noise impacted residential dwelling units, the Applicant/developer/builder must disclose in writing to all prospective purchasers that those homes are impacted by transportation noise. Such notification may be accomplished by inclusion of this information and any measures to reduce the impacts in brochures and promotional documents and must be included in any noise impacted sales contracts, any illustrative site plan(s) on display within any sales related offices(s); in Homeowner Association documents; with all Deeds of Conveyance of noise impacted units; and by inclusion on all signature subdivision and site plans. A copy of this notification must be provided to the Planning Department and Montgomery County Department of Permitting Services, Site Plan Enforcement Section prior to the issuance of a Use and Occupancy Certificate or final inspection, whichever is relevant, for any noise impacted residential unit.

OPEN SPACE, FACILITIES AND AMENITIES

8. Public and Common Open Space, Facilities, and Amenities
 - a) The Applicant must provide a minimum of 25,504 square feet of public open space on-site.
 - b) The Applicant must provide a minimum of 29,940 square feet of common open space on-site.
 - c) Before release of the surety bond, the Applicant must construct the streetscape improvements along the property's frontage on Crabbs Branch Way.
 - d) Before the issuance of use and occupancy certificates (excluding core and shell) and release of the surety bond for the residential development, all public open and common space areas on the Subject Property must be completed.
9. Common Open Space Covenant

The record plat must reference the Common Open Space Covenant recorded at Book 28045 Page 578 (Covenant).
10. Recreation Facilities

The Applicant must provide the required recreation facilities as shown on the Certified Site Plan (CSP) prior to the final Use an Occupancy permit.

11. Maintenance of Public Amenities

The Applicant is responsible for maintaining all publicly accessible amenities including, but not limited to the pedestrian connections, open grass areas, multiage playground, urban plaza, picnic and seating areas, outdoor game stations, and outdoor fitness stations.

TRANSPORTATION & CIRCULATION

12. Transportation

The Planning Board has reviewed and accepts the recommendations of the Montgomery County Department of Permitting Services Right-of-Way Section (DPS-ROW) in its memo dated October 9, 2025, and incorporates them as conditions of approval. The Applicant must comply with each of the recommendations in the memo, which DPS-ROW may amend if the amendments do not conflict with other conditions of Site Plan approval.

13. Private Roads

The Applicant must provide Private Streets A, B, and C, and Private Alleys A, B, C, D, E, F, G, H, I, J, K, L, and M as required by the accompanying Preliminary Plan No. 120250180, as may be amended.

- a) Before issuance of the first use and occupancy permit, the Applicant must install a high-visibility continental crosswalk with a receiving ADA-compliant ramp to the east side of Standish Place at the exact location where a receiving ramp and crosswalk are currently located, adjacent to the west leg of the intersection of Private Street C and Standish Place.
 - a. The exact location of the high-visibility continental crosswalk with a receiving ADA-compliant curb ramp must be shown and illustrated at the Certified Site Plan, and receive approval from Planning Staff.
- b) Before issuance of the first use and occupancy permit, the Applicant must install a high-visibility continental crosswalk across the west leg of the southern intersection of Standish Place and Crabbs Branch Way, aligned with the existing curb ramps.
- c) Before issuance of the first use and occupancy permit, the Applicant must install mountable truck aprons at the following locations, to achieve a curb radius of 15 feet, as required under Section 49.32.f of the Montgomery County Code:
 - i. At the west and east legs of the intersection of Public Street A with Public Street B.
 - ii. At the west and east legs of the intersection of Public Street A with Public Street C.
 - iii. The exact location of the mountable truck aprons must be shown, illustrated, and dimensioned at the Certified Site Plan, and receive approval from Planning Staff.
 - iv. The Applicant must construct all curb radii of Private Alleys A, B, C, D, E, F, G, I, J, K, L, and M to a maximum of 15 feet.

14. Pedestrian & Bicycle Circulation

- a) The Applicant must provide the following master planned pedestrian and bicycle facilities, the exact location, design, and construction of which must comply with requirements of the Montgomery County Department of Transportation, Division of Traffic Engineering and Operations, before the right-of-way permit:
 - i. Crabbs Branch Way: an asphalt sidepath separated from traffic by a street buffer with trees along the property frontage on Crabbs Branch Way varying in width, following the specifications outlined in the approved cross-section on Sheet 07-SITE-820250090-SP-6 of the Site Plan.
 - ii. The minimum dimensions for the asphalt sidepath must be eight (8) feet, and six (6) feet for the street buffer with trees.

AGENCY COORDINATION

15. Fire and Rescue

- a) The Planning Board has reviewed and accepts the recommendations of the Montgomery County Department of Permitting Services (MCDPS), Fire Department Access and Water Supply Section in its letter dated October 20, 2025, and hereby incorporates them as conditions of approval. The Applicant must comply with each of the recommendations in the letter, which MCDPS may amend if the amendments do not conflict with other conditions of Site Plan approval.
- b) Before approval to release any portion of the site plan performance bond, the Applicant must deliver to the Planning Department, with a copy to MCDPS, certification by a professional engineer licensed in the State of Maryland that the fire access improvements have been constructed and installed per the recommendations from the Fire Department Access and Water Supply Section, and as shown in the approved MCDPS fire access plan, or as amended.
- c) Before approval to release any portion of the site plan performance bond, the Applicant must deliver to the Planning Department and MCDPS inspector certification by a professional engineer licensed in the State of Maryland that the Private Road has been constructed in accordance with the paving detail and cross-section specifications required by the Montgomery County Road Code, as may be modified on the Preliminary Plan or Site Plan, including in-place density testing of the roadway soil sub-grade, aggregate base, and asphalt, and that the road has been constructed for safe use including horizontal and vertical alignments for the intended target speed, adequate typical section(s) for vehicles/pedestrians/bicyclists, ADA compliance, drainage facilities, sight distances, points of access and parking, and all necessary requirements for emergency access, egress, and apparatus as required by the Montgomery County Fire Marshall.

SITE PLAN

16. Site Design

- d) The exterior architectural character, proportion, materials, and articulation must be substantially similar to the schematic elevations shown on Sheets Arch-01 through Arch-18 of the submitted architectural drawings, as determined by M-NCPPC Staff.
- e) The exterior architectural character, proportion, materials, and articulation of the MPDUs must be substantially similar to the exterior architectural character, proportion, materials, and articulation of the market-rate units.

17. Lighting

- a) Before certified Site Plan, the Applicant must provide certification to Staff from a qualified professional that the exterior lighting in this Site Plan conforms to the latest Illuminating Engineering Society of North America (IESNA) recommendations (Model Lighting Ordinance-MLO: June 15, 2011, or as superseded) for a development of this type. All onsite exterior area lighting must be in accordance with the latest IESNA outdoor lighting recommendations (Model Lighting Ordinance-MLO: June 15, 2011, or as superseded).
- b) All onsite down-lights must have full cut-off or BUG-equivalent fixtures.
- c) Deflectors will be installed on proposed fixtures to prevent excess illumination and glare.
- d) Illumination levels generated from on-site lighting must not exceed 0.5 footcandles (fc) at the lot line, excluding areas impacted by streetlights within the right-of-way.
- e) Streetlights and other pole-mounted lights must not exceed the height illustrated on the Certified Site Plan.

18. Site Plan Surety and Maintenance Agreement

Before issuance of any building permit, sediment control permit, or Use and Occupancy Certificate (excluding core and shell), whichever comes first, the Applicant must enter into a Site Plan Surety and Maintenance Agreement with the Planning Board in a form approved by M-NCPPC Office of General Counsel that outlines the responsibilities of the Applicant. The Agreement must include a performance bond(s) or other form of surety in accordance with Section 59.7.3.4.K.4 of the Montgomery County Zoning Ordinance, with the following provisions:

- a) A cost estimate of the materials and facilities, which, upon Staff approval, will establish the surety amount.
- b) The cost estimate must include applicable Site Plan elements only for facilities located on the Property (not in the public R.O.W.), including, but not limited to plant material, on-site lighting, outdoor recreational facilities, site furniture, mailbox pad sites, trash enclosures, retaining walls, fences, railings, private roads and sidewalks, private utilities, paths and associated improvements of development, including sidewalks, bikeways, storm drainage facilities, street trees and street lights. The surety must be posted before issuance of any building permit of development and will be tied to the development program.
- c) The bond or surety must be tied to the development program, and completion of all improvements covered by the surety for each phase of development will be followed by a

site plan completion inspection. The surety may be reduced based upon inspector recommendation and provided that the remaining surety is sufficient to cover completion of the remaining work.

19. Development Program

The Applicant must construct the development in accordance with a development program table that will be reviewed and approved before the approval of the Certified Site Plan.

20. Certified Site Plan

Before approval of the Certified Site Plan the following revisions must be made and/or information provided subject to Staff review and approval:

- a) Include the stormwater management concept approval letter, development program, and Site Plan resolution on the approval or cover sheet(s).
- b) Add the following notes:
 - i. “M-NCPPC Staff must inspect all tree-save areas and protection devices before clearing and grading.”
 - ii. “Minor modifications to the limits of disturbance shown on the site plan within the public right-of-way for utility connections may be done during the review of the right-of-way permit drawings by the Department of Permitting Services.”
 - iii. “The Applicant must schedule a preconstruction meeting (pre-con), preferably on-site, with staff from the Department of Permitting Services (DPS) responsible for Certified Site Plan conformance and compliance, upon approval of the Certified Site Plan (CSP). The pre-con must occur before any site development work commencement and before any work that is covered by the site plan surety and maintenance agreement. The Applicant, along with their representatives, must attend the pre-con with DPS CSP Staff. A copy of the approved Certified Site Plan along with any subsequent amendments is required to be on-site at all times during development.”
- c) Include approved Fire Department Access Plan.
- d) Modify data table to reflect development standards approved by the Planning Board.
- e) Ensure consistency of all details and layout between Site and Landscape plans.
- f) Provide an exhibit showing recreation facilities and requirements.
- g) Show all private road sections/profiles including curb & gutter, apron, and handicap ramp details identified in Condition 13.

FOREST CONSERVATION PLAN F20250010

Staff recommends approval of Final Forest Conservation Plan No. F20250010 with the following conditions⁴:

1. Before the start of any demolition, clearing, grading, or construction associated with the accompanying plans, whichever comes first, for this Site, the Applicant must:
 - a) Record an M-NCPPC approved Certificate of Compliance in an M-NCPPC approved off-site forest bank within the Middle Rock Creek watershed or Priority Area to satisfy the afforestation requirement of 1.98 acres of mitigation credit. If no off-site forest banks exist within the Middle Rock Creek watershed or Priority Area, then the off-site requirement may be met by purchasing 1.98 acres of mitigation credits from a mitigation bank within Montgomery County outside of the Middle Rock Creek watershed or Priority Area, subject to Staff approval. If forest mitigation bank credits are not available for purchase, a fee-in-lieu payment must be made to M-NCPPC for the appropriate mitigation credits outside of the same watershed or Priority Area.
 - b) Execute a five-year Maintenance and Management Agreement (“MMA”) in a form approved by the M-NCPPC Office of the General Counsel. The MMA is required for all mitigation tree plantings, including variance tree mitigation plantings. The MMA includes invasive species management control measures as deemed necessary by the M-NCPPC Forest Conservation Inspection Staff.
 - c) Submit a cost estimate for the variance mitigation trees, five years of maintenance including invasive species management controls mulching, staking, tree protection, and tree protection removal credited toward meeting the requirements as shown on the FCP. This cost estimate must be reviewed and approved by the M-NCPPC Planning Department Inspection Staff prior to the submission of financial surety to determine the amount of the financial surety.
 - d) Submit financial surety, in a form approved by the M-NCPPC Office of the General Counsel, to the M-NCPPC Planning Department Inspection Supervisor for mitigation trees, and maintenance, including invasive species management controls, credited toward meeting the requirements of the FCP
2. Within the first planting season following the release of the first Sediment and Erosion Control Permit from the Montgomery County Department of Permitting Services for the Subject Property, or as directed by the M-NCPPC Forest Conservation Inspection Staff, the Applicant must install the variance tree mitigation plantings on the Subject Property as shown on the approved FCP. The variance tree mitigation plantings must be a minimum size of 3 caliper inches totaling 48 caliper inches, as shown on the approved FCP. Adjustments to the planting

⁴ For the purpose of these conditions, the term “Applicant” shall also mean the developer, the owner, or any successor in interest to the terms of this approval.

locations of these trees is permitted with the approval of the M-NCPPC Forest Conservation Inspection Staff.

3. The Applicant must schedule the required site inspections by M-NCPPC Forest Conservation Inspection Staff per Section 22A.00.01.10 of the Code of Montgomery County Regulations (“COMCOR”), Forest Conservation Regulations.
4. The Applicant must comply with all tree protection and tree save measures shown on the approved Final Forest Conservation Plan. Tree save measures not specified on the Final Forest Conservation Plan may be required by the M-NCPPC Forest Conservation Inspection Staff.
5. The Limits of Disturbance (“LOD”) shown on the Final Sediment and Erosion Control Plan must be consistent with the LOD shown on the approved Final Forest Conservation Plan.

SECTION 2: SITE DESCRIPTION

VICINITY

The Subject Property is located at 7501-7515, 7519, 7529, 7609-7623 Standish Place in Derwood, Maryland within the Metro Park – Standish Place office complex. The Property is bounded by Standish Place, a private road, along the northern, southern, and western site frontages, and by Crabbs Branch Way, a public road, to the east. The Property is approximately 500 feet north of the intersection of Crabbs Branch Way and East Gude Drive and is surrounded by a diversity of uses and building types, which are generally characterized as moderate-density industrial and residential uses, described in more detail below.

Directly north of the Site, across Crabbs Branch Way, is the American National Red Cross building (zoned IM-2.5, H-50’). To the north, across Standish Place, are additional moderate density industrial office parks. Further north along Crabbs Branch Way are industrial office parks up to Indianola Drive and part of the Derwood Station single-family residential neighborhood (zoned R-90). The Shady Grove Metro Station and associated mixed-use development in the CR zone are approximately one mile northwest of the Subject Property.

To the east of the Property, along Crabbs Branch Way, is the American Society of Plant Physiology building (zoned IM-2.5, H-50') and an office building (zoned CRT-2.5, C-2.0, R-0.5, H-80'). Further east is a continuation of the Derwood Station single-family neighborhood (zoned R-90 and R-200).

Immediately south of the Property, along Standish Place, are industrial uses (zoned IM-2.5, H-50' and IM-2.5, H-70'), such as an FDA office, a publisher, and a media office.

Directly west of the Property across Standish Place are additional industrial uses (zoned IM-2.5, H-50') with tenants such as a media company, telehealth services, and an engineering consultant. One block west of the Property is the WMATA Metrorail line.



Figure 1: Vicinity



Figure 2: Zoning Vicinity Map

PROPERTY DESCRIPTION

The Property is comprised of approximately 12.73 acres (554,435 square feet) and is recorded as Parcel D in the “Gude North” Subdivision among the Land Records of Montgomery County, Maryland (the “Land Records”) at Plat No. 13905 in Plat Book 118. The Property is zoned CRNF-1.25, C-0.0, R-1.25, H-60’.

The Property is on the south side of Crabbs Branch Way where the road curves to the west, approximately 500 feet north of its intersection with East Gude Drive. The Property is bounded by private road Standish Place to the north, west, and south. The site is irregularly shaped – rounded along the three sides that border Standish Place, with right-angle intersections where Standish Place meets Crabbs Branch Way on the eastern side of the Property.



Figure 3: Subject Property

The Property is currently developed with four office buildings ranging from one to three stories in height and an associated surface parking lot with 649 spaces. The existing offices are occupied by tenants such as FedEx, an electrical equipment supplier, a data center, an engineering firm, and an insurance agency. According to the Maryland State Department of Assessments and Taxation (“SDAT”) records, the existing office complex was built in 1984 and has approximately 177,126 square feet of above-grade floor area. The existing buildings are angled away and setback from the roadways (Crabbs Branch Way and Standish Place), with the surface parking located between the buildings and the street on all sides. The Property’s current vehicular access is provided via four curb cuts off Standish Place.

The topography of the site rises from approximately 442 feet above sea level at its lowest point at the southwest corner of Standish Place to 462 feet above sea level at the highest point along the Crabbs Branch Way frontage. The land is generally flat around the center of the site where much of the existing and proposed development is located, with topography ranging from 446 to 448 feet above sea level. However, there are substantial grade differentials located along the Crabbs Branch Way frontage with elevations ranging from 10 feet in height at the mid-block, 3-foot elevations towards the northern end, and 5-foot elevations towards the southern end of Crabbs Branch Way. Figures 4 and 5 demonstrate the significant grade off Crabbs Branch Way. Pedestrian access with stairs and ramps will

be provided for improved connectivity with the surrounding neighborhood and established pedestrian crosswalk.

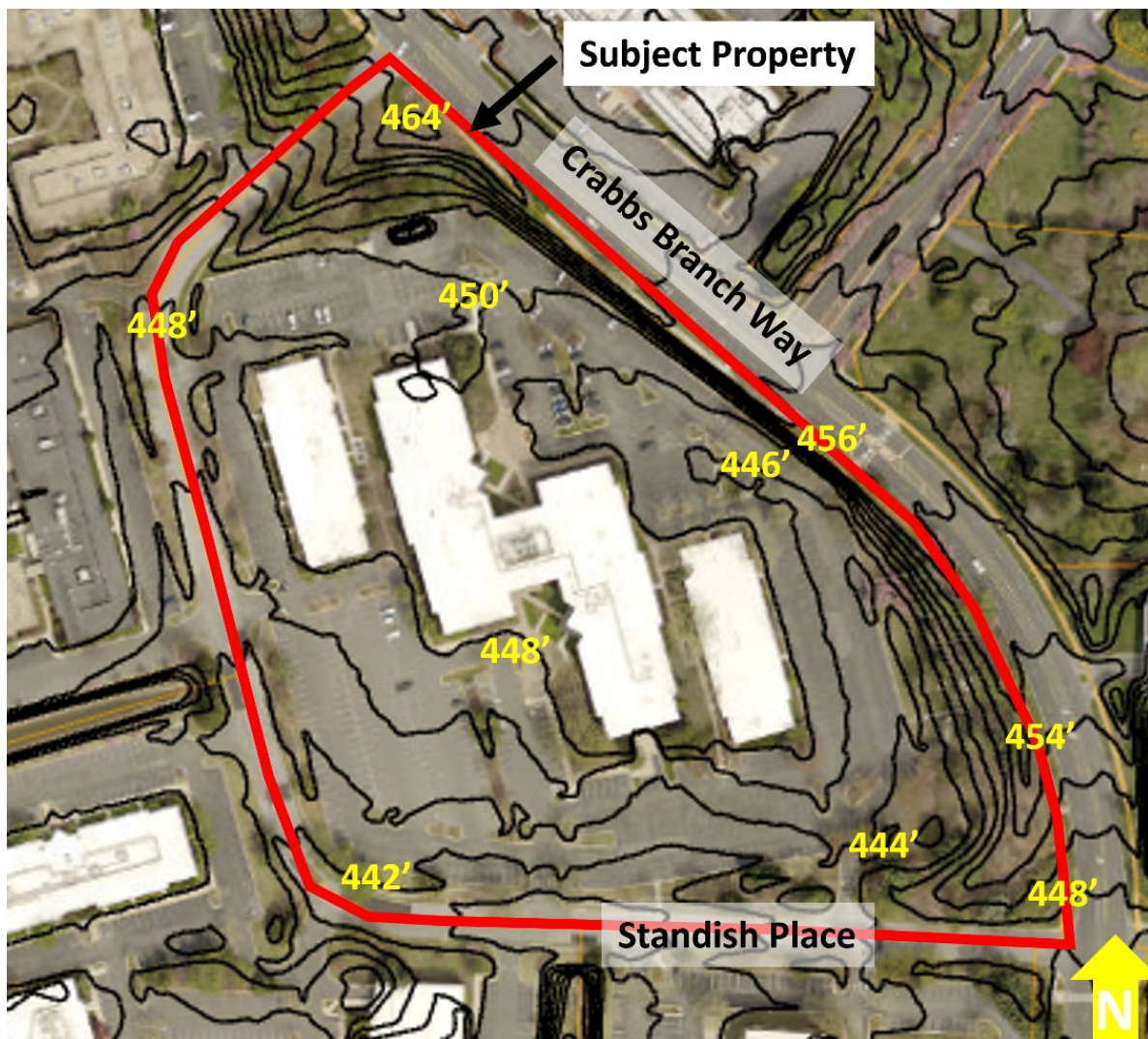


Figure 4: Topographic map with spot elevations of the Subject Property



Figure 5: Subject Property existing conditions, looking northeast towards Crabbs Branch Way

The Subject Property is located within the Middle Rock Creek Watershed, classified by the State of Maryland as Use Class IV waters. The Subject Property contains no forest, streams, wetlands, or their associated buffers. The site contains eight (8) specimen trees and numerous significant trees. There is an office building and associated surface parking lots. There are no wetlands, seeps, or springs on the Subject Property. No known rare, threatened, or endangered species or habitats exist on the Subject Property. There are no designated historic sites on or adjacent to the Property.

SECTION 3: PROJECT DESCRIPTION

PREVIOUS APPROVALS

LOCAL MAP AMENDMENT NO. H-156: 7501 STANDISH PLACE

On June 17, 2025, the Montgomery County Council approved Local Map Amendment (LMA) No. H-156, 7501 Metro Park Standish Place (Resolution No. 20-850), rezoning Lot 2, Lot 3, Parcel P393, Parcel P447 from R-90 and a sliver of Parcel P395 from IM-2.5, H-50' zone to the CRNF-1.25, C-0.0, R-1.25, H-60' zone (Attachment A). The following binding elements were established as part of approval and shown on the Approved Floating Zone Plan ("FZP") (Figure 6 and Attachment B):

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1. The development must provide a minimum of 15 percent (15%) Moderately Priced Dwelling Units (MPDUs) as approved by the Montgomery County Department of Housing and Community Affairs (MCDHCA), consistent with the requirements of Chapter 25A.
2. The development must provide at least 10 percent (10%) of the site area as open space, allocated proportionally by use (townhouse and multifamily) between Common and Public Open Space.
3. The Central Park open space, identified illustratively on the FZP, must be made accessible to the public. The final size, design, and programming of the Central Park will be determined at time of Site Plan.
4. Vehicular access to the Site will be limited to Standish Place.

The rezoning allows up to 210 dwelling units on the Property, including 15% MPDUs.

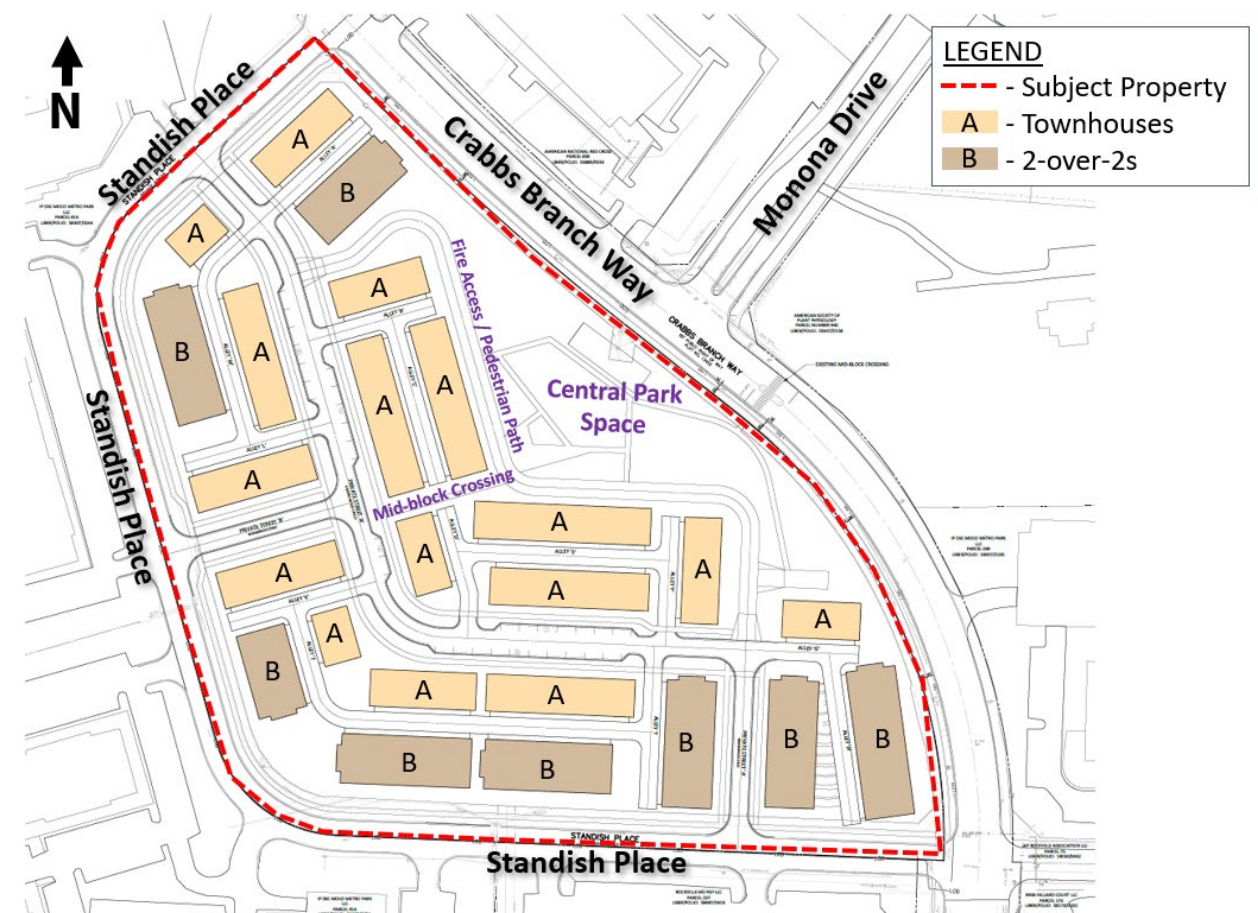


Figure 6: Floating Zone Plan

PRELIMINARY FOREST CONSERVATION PLAN (PFCP) NO. F20250010: 7501 STANDISH PLACE

The Planning Board via Resolution MCPB No. 24-121 on February 14, 2025, approved the PFCP. This application was reviewed under the CRNF-1.25, C-0.0, R-1.25, H-60' zone and it is assigned a Land Use Category of Mixed-Use Development Area ("MDP") as defined in Section 22A-3 of the Montgomery County Forest Conservation Law ("FCL") and in the Land Use Table of the Trees Technical Manual. This results in an afforestation requirement of 15% and a conservation threshold of 20% of the Net Tract Area.

The Net Tract Area for forest conservation purposes includes the Total Tract Area of 12.73 acres plus 0.46 acres of offsite disturbance associated with this Application. This results in a total Net Tract Area of 13.19 acres. There is no forest on the Subject Property, which results in a total afforestation requirement of 1.98 acres. The Applicant will address afforestation requirements by providing 1.98 acres of afforestation mitigation.

The PFCP also included the approval of a Variance Request for the removal of six (6) Protected Trees to be mitigated by planting 48-caliper inches.

PROPOSAL

Given the site's proximity to major roadways such as Frederick Road and E. Gude Drive, this development will create more housing opportunities by transforming the large, underutilized, auto-centric gray field office park site into a mixed housing development within the 2021 *Shady Grove Sector Plan Minor Master Plan* ("Master Plan" or "Sector Plan") area. This proposed development will further expand the residential neighborhood along the main roadway, adjacent to existing townhouses and single-family homes to the north.



Figure 7: Proposed Preliminary and Site Plan Rendering

A network of private neighborhood streets, alleys, and pedestrian connections, centered around a new central park and multiple publicly accessible open spaces, creates a well-designed, pedestrian-oriented community.

The proposed development has been designed as a thoughtfully integrated, walkable residential community that transitions from the industrial legacy of the Standish Place corridor to a vibrant neighborhood consistent with emerging planning goals.

This development features smaller, rear-loaded blocks of various housing types within the community, providing narrower residential streets to serve 200 residential units, organized into a mix of rear-loaded townhouses and rear-loaded two-over-two units. Additionally, it provides a much-needed pedestrian connection from Crabb Branch Way to Standish Place.

BUILDING/ARCHITECTURE

All residential units are designed as four-story dwelling units with a maximum building height of 60 feet, allowing for the architectural articulation of roof forms and the transition between the grade and the finished first-floor elevation. A portion of the ground floor is dedicated to an encapsulated parking garage located at the rear of the building.

The proposed architecture features a blend of contemporary residential design, characterized by varied elevations and a cohesive material palette. Both townhome units and two-over-two units (multi-family) are proposed, featuring a variety of façade treatments that break down massing and enhance visual interest. The two-over-two units introduce denser residential typology while preserving pedestrian-friendly street frontages. Building materials include a mix of high-quality masonry, fiber cement, and accent paneling in complementary earth tones. The massing and articulation of the units reinforce the human scale of the development, with pedestrian entries and porches enhancing ground-level activation. The architecture is calibrated to reflect the Property's transition from suburban office park to walkable neighborhood, consistent with the evolving design vision of the Master Plan area.

OPEN SPACE

This development offers three primary areas of Open Space, easily accessible for future residents and visitors:

- A Public Open Space, described as a "Central Park" space, adjacent to the intersection of Crabbs Branch Way and Monona Drive.
- Two Common Open Space areas along Standish Place to the south.
- A northern and southern Common Open Space fronting Crabbs Branch Way and framing the "central park."

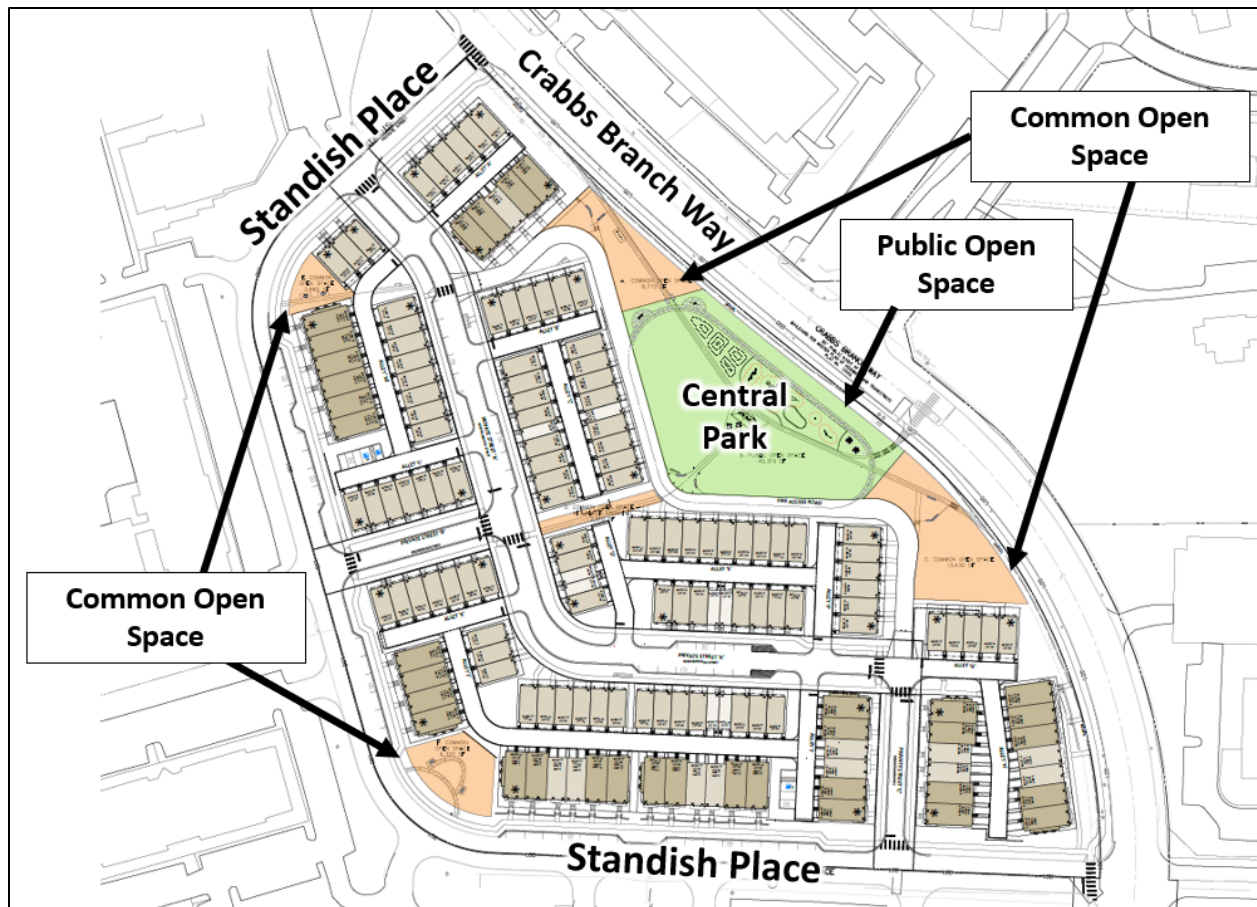


Figure 8: Open Space Exhibit





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Figure 9: Open Space Renderings (top to bottom) Central Park Entrance, Plaza, Amphitheater, Playground and Adult Fitness Stations

These public amenities include a playground, shade structures, and pedestrian seating, designed to enhance the development integration with the broader Derwood neighborhood. The entire open space network is connected via a continuous pedestrian pathway system linking every open space area throughout the site.

TRANSPORTATION

Vehicular access to the Site is provided primarily through Standish Place, a private street loop that is served by Crabbs Branch Way, a County-owned and maintained street. Internally, the Applicant is proposing vehicular access via a series of private streets and alleys. As part of this Application, three (3) private streets and twelve (12) private alleys will be constructed, following County Standards and the *Complete Streets Design Guide* (CSDG). The Private Streets and Alleys will consist of two (2) travel lanes. While Private Streets will have pedestrian facilities, the alleys do not include sidewalks.



Figure 10: Proposed Street Network, as conditioned

All private streets will follow the design parameters for a Neighborhood Street classification, including six (6)-foot-wide concrete sidewalks on both sides of the streets, separated from traffic by street buffers with trees. The internal pedestrian network will be connected to the external pedestrian network through an asphalt sidepath to be constructed as part of this Application. The proposed sidepath will also improve bicycle access into and out of the Proposed Development and will include a minimum width of eight (8) feet and be separated from traffic by a variable-width street buffer with a minimum width of six (6) feet.

The Site has access to public transit. There are six (6) bus stops along the boundary of the Site along the Crabbs Branch Way frontage. The existing bus service will provide future residents with transit

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access to nearby commercial and residential areas. The bus system will also allow access to the Shady Grove Metro Station, which is also within a mile distance from the Site.

ENVIRONMENT

The Site is currently developed as an office park with surface parking. The Subject Property is located in the Rock Creek Watershed, a Use Class IV Stream. The Site contains no streams, forests, stream buffers, wetlands, or other environmental features. Redevelopment of the Property will include open space and increased tree canopy cover, providing environmental benefits such as increased shade and increased rainwater absorption. Final Forest Conservation Plan (FFCP) No. F20250010 was filed concurrently with the Preliminary and Site Plan applications. Further details of the Project's Chapter 22A compliance can be found below in the Final Forest Conservation Plan Findings section. The Applicant has submitted a noise study due to proximity to Crabbs Branch Way and the rail line to the west. The Applicant proposes to address stormwater management by utilizing environmental site design features such as biofiltration swales and micro-bioretenment facilities.

SECTION 4: COMMUNITY OUTREACH

The Applicant has complied with all submittal and noticing requirements, and a pre-submittal public meeting was held on May 14, 2025.

Community questions focused on:

- Parking adequacy.
 - The Applicant noted that all homes will have internal garages.
- Affordable housing units.
 - The Applicant explained that the proposed affordable homes will be dispersed evenly throughout the community's townhomes and two-unit condos.
- Architectural features.
 - The Applicant discussed the design features.
- Potential traffic impacts.
 - The Applicant explained the new residential use will generate significantly less traffic than the prior office use. A traffic statement was submitted by the applicant's traffic engineers (Lenhart Traffic Consulting).

As of date of this Staff Report, no correspondence has been received.

SECTION 6: PRELIMINARY PLAN 120250180 FINDINGS AND ANALYSIS

Staff recommends approval of the Preliminary Plan to create eight (8) lots for up to 92 two-over-twos⁵ (multi-unit dwelling units) and one hundred and eight (108) lots for up to 108 attached (townhouse) dwelling units, including at least 15% MPDUs, and parcels for private roads, open space and stormwater management.

This Application has been reviewed for compliance with the Montgomery County Code, Chapter 50, the Subdivision Regulations and Chapter 59, the Zoning Ordinance. The Application has been reviewed by other applicable State, County and City agencies.

- 1. The layout of the subdivision, including size, width, shape, orientation and density of lots, and location and design of roads is appropriate for the subdivision given its location and the type of development or use contemplated and the applicable requirements of Chapter 59.***

The proposed size, width, shape, and orientation of the lots are appropriate for the location of the subdivision taking into account the recommendations included in the Master Plan, and for the uses and building types contemplated. The dimensions of the lots are sufficient to accommodate the proposed building type and infrastructure necessary to serve them, including parking, access, and infrastructure, taking into account the proposed townhouse and two-over-twos are rear loaded and served by alleys. Stormwater management, access, and infrastructure is accommodated in the private alleys and other common Homeowner Association (“HOA”) property.

⁵ Two-over-twos are categorized as “apartments,” or “multi-unit living” based on Section 59.4.1.3.D. of the Montgomery County Code.



Figure 11: Proposed Lot and Block Layout

a) The block design is appropriate for the development or use contemplated

As conditioned, the Subject Property is being divided into five new blocks, with each block being separated in a grid like form by new private street parcels, open space or pedestrian connections on HOA parcels.

b) The Preliminary Plan provides for required public sites and adequate open areas

While the Subject Property is not identified by the Master Plan as a master planned site or a site needed for a specific public use, the proposed subdivision does include ample room

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for the proposed common and public open spaces, which includes the recreational area needed to meet the needs of the new residents. As currently laid out, adequate space is provided to accommodate the pedestrian and vehicular transportation needs of the community, while accounting for the spaces necessary to provide utility services. Particular attention was given to the location of proposed electrical and telecommunication services, to limit conflicts between above ground transformers and service areas abutting the primary pedestrian connections between Standish Place and the proposed central park along Crabbs Branch Way.

c) The Lot(s) and Use comply with the basic requirements of Chapter 59

The lots were reviewed for compliance with the dimensional requirements for the CRNF zone as specified in the Zoning Ordinance and the Binding Elements stipulated in the Floating Zone Plan. With regard to the proposed use and maximum allowable density, the Hearing Examiner has already found (as part of LMA H-156) that in combination with the aforementioned binding elements, up to 210 dwelling units are permitted. The Applicant is proposing 108 Townhouses and 92 multi-family units, which are permitted use in the CRNF-1.25, C-0, R-1.25, H-60' zone. Section 5.3.5. of the Zoning Code stipulates that maximum height, and minimum setbacks from the site boundary are established by the FZP (i.e. binding elements), but all other setbacks are established by the Site Plan.

The lots as proposed will meet all the dimensional requirements for area, frontage, width, and setbacks in the CRNF zone. The proposed lot provides sufficient space to accommodate the proposed development and the infrastructure necessary to support it, such as parking, stormwater management, landscape screening and open space, while respecting the established setbacks. A summary of this review is included in Table 1.

Table 1: Preliminary Plan Data Table for the CRNF Zone, Standard Method, Section 59.5.3

	Approved per FZP	Proposed
Tract Area	N/A	13.86 ac (603,855 sf)
Previous ROW Dedications	N/A	1.13 ac (49,420 sf)
Proposed ROW Dedications	N/A	0.0 ac
Site Area	N/A	12.73 ac (554,435 sf)
Density (max.)		
Total FAR/Dwelling Units	1.25 FAR (754,819 sf) or 210 dus w/15% MPDUs	1.22 FAR (675,264 sf or 200 dus w/ 15% MPDUs
Commercial	0.0 FAR (0 sf)	0.0 FAR (0 sf)

	Approved per FZP	Proposed
Residential	1.25 FAR (754,819 sf)	1.22 FAR (675,264 sf)
Open Space	10% (55,444 sf) ⁶	78,879 sf ⁷
Lot Coverage (max.)	Determined at Site Plan	Determined at Site Plan
Building Height (max.)	60 feet (Per FZP)	60 feet ⁸
Townhouses and Multi-family		
Setback from Site Boundary		
- Crabbs Branch Way (min.)	10 ft.	10 ft.
- Standish Place (min.)	30 ft.	30 ft.
All other setbacks	Determined at Site Plan	Determined at Site Plan

2. *The Preliminary Plan substantially conforms to the Master Plan.*

a) *Land Use*

Overall, the Project advances the Master Plan’s broader vision for the Shady Grove area as a mixed-use and pedestrian-oriented environment with attractive streetscapes, distinctive architecture, and a sense of place that is complemented with amenities and mobility options, especially at properties around the Metro Station and other key properties, such as the Grove Shopping Center (p.1). The Project substantially conforms to the Master Plan’s vision by providing varying housing typologies, internal network of private streets and alleys creating a smaller and more compact development pattern, and the new central park area that will serve as an amenity for the future residents and surrounding community members.

Housing

The Sector Plan encourages residential development in proximity to the Metro Station and other key locations. As proposed, this development will advance key Sector Plan housing recommendations, including the following:

- Requiring 15 percent moderately priced dwelling units (MPDUs) as the highest priority public benefit for all new residential development.

⁶ Common and Public Open Space are required to be allocated proportionally by use (Townhouse and Multi-Family). See Site Plan Data Table.

⁷ Per Section 59.4.5.3.C., Open Space for townhouses is “Common Open Space” and for other building types (the proposed multifamily/two-over-twos) is “Public Open Space” but will be combined and appropriately allocated at time of Site Plan.

⁸ The project must take its height measurement from the average grade along the building facing the applicable abutting or confronting property, per Section 59.4.1.8.B.3. of the Zoning Ordinance.

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- Providing a range of unit types, including for families, seniors, and persons with physical challenges.

The proposed development will provide at least 15 percent of the residential development as Moderately Priced Dwelling Units (MPDUs), which is one of the County’s affordable housing programs. A binding element of the rezoning application requires the Applicant to provide “a minimum of 15 percent (15%) Moderately Priced Dwelling Units (MPDUs) as approved by the Montgomery County Department of Housing and Community Affairs (MCDHCA), consistent with the requirements of Chapter 25A”.

Residential townhouses and two-over-two residential units are proposed that advance the Sector Plan’s recommendations to “provide a range of unit types, including for families, seniors, and persons with physical challenges” (p. 69).

Public Open Space

The Sector Plan recommends several new public open spaces within the Metro Neighborhoods where new mixed-use development is recommended. A binding element of the rezoning case requires 10 percent of the site to be open space, including the central open space along Crabbs Branch Way. Approximately 13 percent of the Property will be common and public open spaces.

The proposed open space associated with this development advances open space design principles recommended in the Sector Plan, including organizing “public open space along existing public streets or extensions of them, or ensure these spaces are accessible from a public street” (p. 28).

Urban Design Guidance

The Sector Plan did not provide specific urban design guidance for the Property. However, the submitted project does illustrate a development “pattern of interconnected streets and public open spaces, with street-oriented buildings” and encourages “quality building and site design elements, such as building orientation that takes advantage of passive heating, lighting and ventilation” (p. 24).

b) Environment

The Sector Plan recommends a broad range of environmental measures, including restoring tree canopy in the Sector Plan area to “improve air quality, reduce storm runoff, contribute to keeping the area cooler in summer, and sequester carbon to ameliorate climate change” (p. 79)

A variety of open spaces are proposed throughout the Property, including the linear open space along Crabbs Branch Way and smaller spaces along Standish Place. In addition, all streets in the development will have new canopy trees.

Noise

The Property is impacted by mixed automotive transportation noise from Crabbs Branch Way and the B & O Railroad. The Montgomery Planning “Staff Guidelines for the Consideration of Transportation Noise Impacts in Land Use Planning and Development” is used to review traffic noise impact on residential developments. In this area of the County, the Guidelines recommend a maximum value of 65 dBA Ldn for exterior recreation areas and 45 dBA Ldn for indoor residential spaces. The Applicant submitted a noise study, analyzing existing noise impacts from current traffic and future traffic volumes. The proposed development will be primarily impacted on the eastern border, along Crabbs Branch Way (Figure 12). The noise reduces further in from the roadway. A Building Shell Analysis will be performed with the final architectural details and architectural noise mitigation measures included, as necessary.

The Central Park, located adjacent to Crabbs Branch Way, will exceed the maximum value of 65 dBA Ldn for exterior recreation areas. The Applicant has requested relief from this requirement because it is not possible to mitigate for the exterior noise while still creating an open space that is open and connected to Crabbs Branch Way and adjacent neighborhoods. Alternative configurations of sound barriers were evaluated; however, they would substantially obstruct physical connectivity to the public realm and conflict with established urban design goals for this space. While this space may be more impacted by transportation noise than is desirable, it is more important that the space be both physically and visually connected to Crabbs Branch Way, allowing the space to function as public open space. Accordingly, exterior noise attenuation is not feasible.



Figure 12: Year 2045 DNL, dB, at Facades of Residences on Loudest Floor

c) **Transportation**

The 2021 *Shady Grove Sector Plan Minor Master Plan Amendment* calls for new bikeways that link the Plan area to adjacent localities. Further, the Master Plan promotes the installation of new pedestrian walkways and bicycle facilities to connect existing residential communities and mixed-use developments to commercial centers and new development areas. In this sense, the Master Plan “prioritizes the provision of multimodal transportation connections as a high-priority public benefit for new development” (p. 18).

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The Subject Application conforms to the 2021 *Shady Grove Sector Plan Minor Master Plan Amendment*, as it is proposing the construction of a fully residential development with a well-connected internal pedestrian network via a system of six (6)-foot-wide concrete sidewalks separated by traffic with street buffers. The sidewalk system will connect to the proposed asphalt sidepath along Crabbs Branch Way, therefore, also providing the Proposed Development with improved bicycle access. Additionally, all six (6) bus stops that bound the area will provide the future residents with access to public transportation, including the Shady Grove Metro Station. The aforementioned multimodal facilities will enhance and diversify modes of transportation in the area and the neighboring sites.

3. *Public facilities will be adequate to support and service the area of the subdivision.*

a) *Roads and other Transportation Facilities*

i. *Existing Facilities*

The Property has frontage along Crabbs Branch Way, a County-owned and maintained public street classified as a Boulevard with a master planned right-of-way of 80 feet under the *Master Plan of Highways and Transitways*. Crabbs Branch Way is a four (4) lane street with existing sidewalk facilities on both sides of the street. There are six (6) existing bus stop facilities bounding the Property along Crabbs Branch Way that provide both WMATA and RideOn Bus service.

To the south, the Property is bounded by Standish Place, which is a privately-owned and maintained street. Standish Place has two (2) travel lanes and substandard sidewalk facilities along some segments.

ii. *Proposed public transportation infrastructure*

As part of this Application, the existing sidewalk along Crabbs Branch Way will be removed and replaced by an asphalt sidepath separated from traffic by a street buffer with trees. The proposed asphalt sidepath with a street buffer will vary in width due to space constraints with existing above-ground utilities and transformers. The asphalt sidepath facility with the street buffer will be constructed to the following dimensions, as shown in Figure 13 below:

- Blue segments: a ten-foot-wide (10 ft) asphalt sidepath separated from traffic by a six-foot-wide (6 ft) street buffer with trees.

- Red segments: an eleven-foot-wide (11 ft) asphalt sidepath separated from traffic by an eight-foot-wide (8 ft) street buffer with trees.
- Purple segments: an eight-foot-wide (8 ft) asphalt sidepath separated from traffic by a six-foot-wide (6 ft) street buffer with trees.
- Orange segments: transition zone with variable widths of a minimum of 8 feet and a maximum of 11 feet of asphalt sidepath separated from traffic by a minimum of six-foot-wide (6 ft) street buffer.

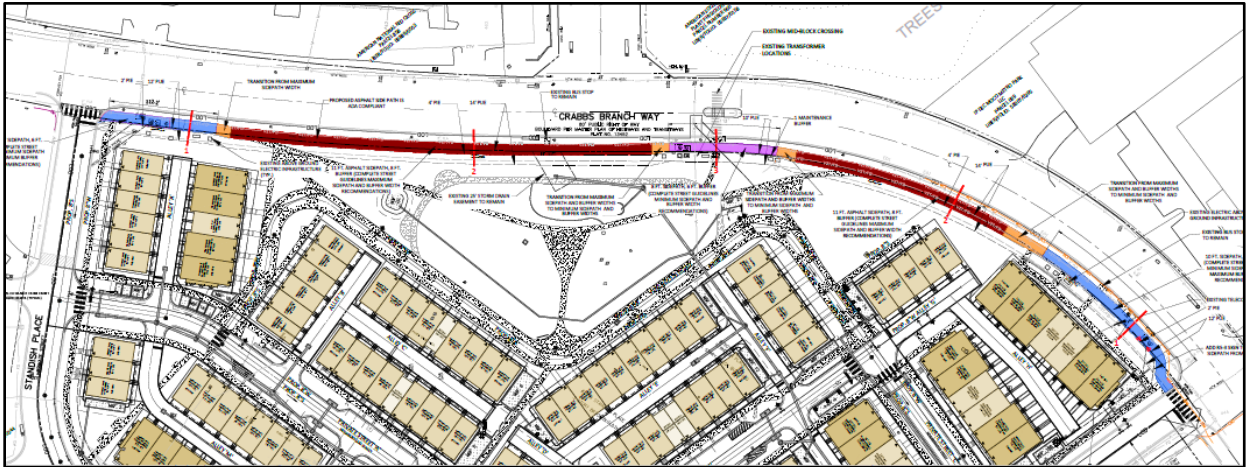


Figure 13: Proposed Asphalt Sidepath with Modified Dimensions, as approved by MCDOT.

The proposed asphalt sidepath and street buffer minimum and maximum widths were reviewed and approved by MCDOT and Planning Staff. In areas with insufficient right-of-way to accommodate the asphalt sidepath, the Applicant will be locating the facility within a Public Improvement Easement (PIE), as approved by MCDOT and DPS-ROW. These areas are illustrated in the approved cross-section for the sidepath on Sheet 07-SITE-820250090-SP-6, and the Applicant will comply with these requirements, as approved in consultation with MCDOT and Planning Staff.

The Applicant will also dedicate the required right-of-way for Crabbs Branch Way. As part of this Application, 40 feet from the centerline of the pavement to the Property line will be dedicated. The street configuration for Crabbs Branch Way will remain the same, as there is no anticipated removal of the curb and gutter. All access points will also remain and were approved under previous applications. As part of this Application, there will be no modifications to the existing six (6) bus stops bounding the Site along Crabbs Branch Way.

iii. ***Proposed private transportation infrastructure***

The Applicant is proposing to provide vehicular and pedestrian access to the Site through a series of private streets and alleys accessed via Standish Place. The Applicant opted to construct private streets and alleys because they are intended to accommodate local circulation and do not provide transportation capacity for the general public. Additionally, the proposed private streets do not comply with the Montgomery County standards MC-20002.01 and MC-2002.02, which comprise requirements for centerline radii and travel lane widths. All three (3) private streets and alleys will be constructed following County Standards and the design parameters for Neighborhood Streets under the CSDG.

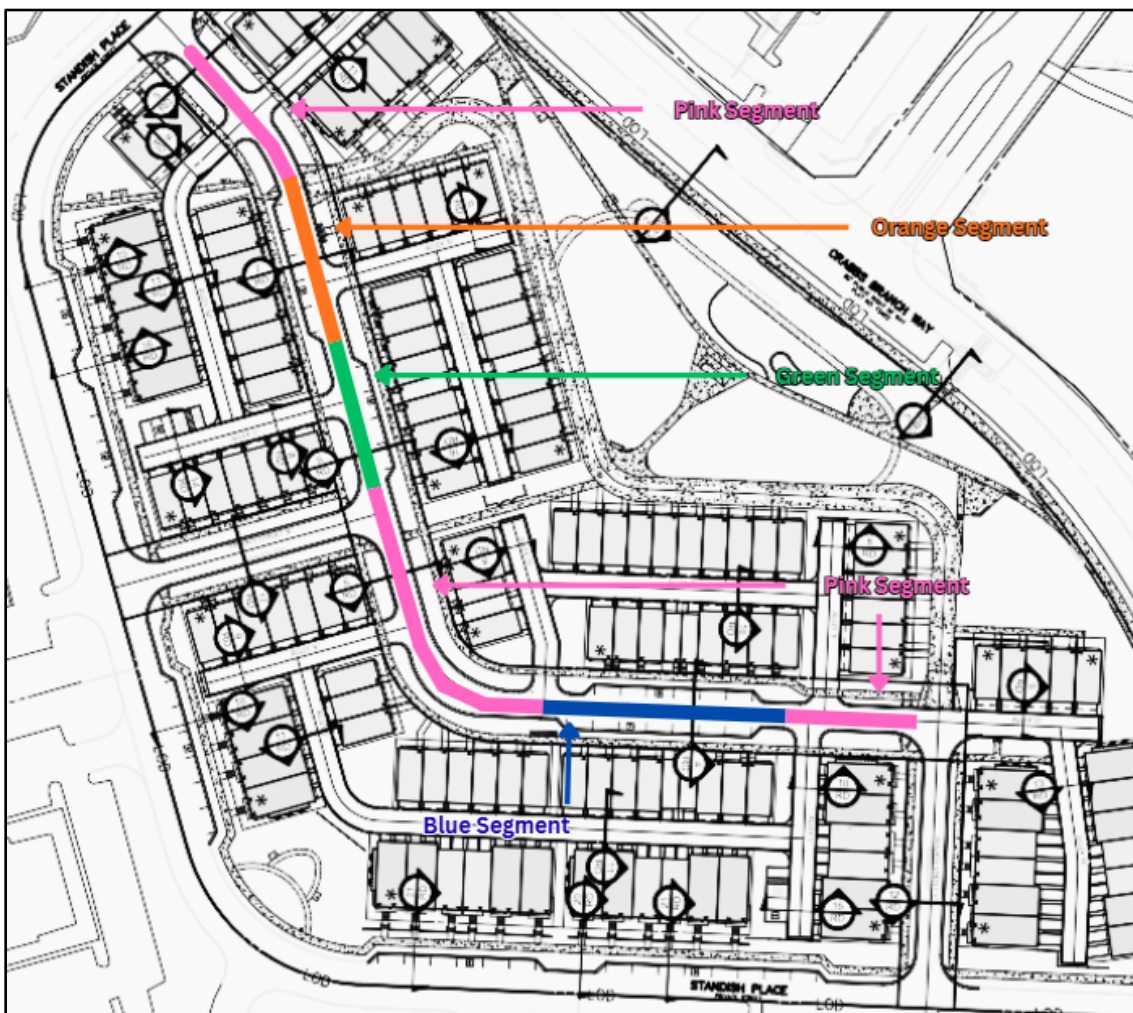


Figure 14: Private Street A extension through the Site, as conditioned.

Private Street A will connect the Site from west to east and will consist of two (2) travel lanes with six-foot-wide (6 ft) sidewalks separated from traffic by street buffers with trees (Figure 14 above). Due to the incorporation of on-street parallel parking spaces being provided within the Site, the dimensions for the street buffers will change. Therefore, the street cross-sections for Private Street A will be as follows:

- iv. Pink Segment: two (2) ten-and-a-half-foot-wide (10.5 ft) travel lanes and a six (6)-foot-wide concrete sidewalk separated from traffic by a fourteen-foot-wide (14 ft) street buffer with trees on both sides. The total width of the street section for this segment is 61 feet (Figure 15 below).

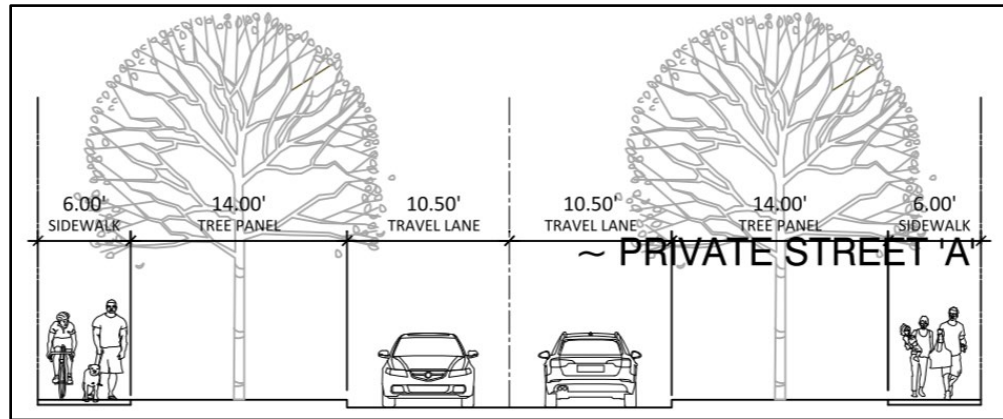


Figure 15: Proposed Cross-Section for Pink Segment of Street A, as conditioned.

- Orange Segment: two (2) ten and a half-foot-wide (10.5 ft) travel lanes, eight-foot-wide (8 ft) parallel street parking on one (1) side, a six-foot-wide (6 ft) concrete sidewalk separated from traffic by a six-foot-wide (6 ft) street buffer with trees on one (1) side, and a six-foot-wide (6 ft) concrete sidewalk separated from traffic by a fourteen-foot-wide (14 ft) street buffer with trees on the other side. The total width of the street section for this segment is 61 feet (Figure 16 below).

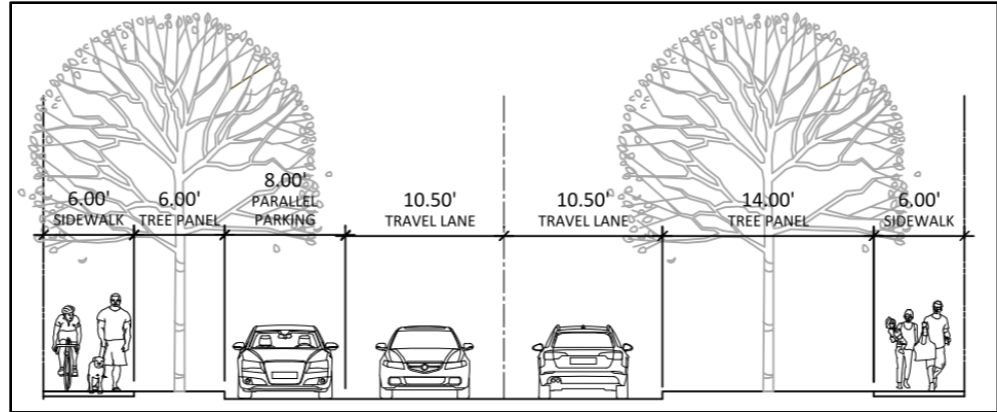


Figure 16: Proposed Cross-Section for Orange Segment of Street A, as conditioned.

- Green Segment two (2) ten and a half-foot-wide (10.5 ft) travel lanes, eight-foot-wide (8 ft) parallel street parking on one (1) side, a six-foot-wide (6 ft) concrete sidewalk separated from traffic by a six-foot-wide (6 ft) street buffer with trees on one (1) side, and a six-foot-wide (6 ft) concrete sidewalk separated from traffic by a fourteen-foot-wide (14 ft) street buffer with trees on the other side. The total width of the street section for this segment is 61 feet (Figure 17 below).

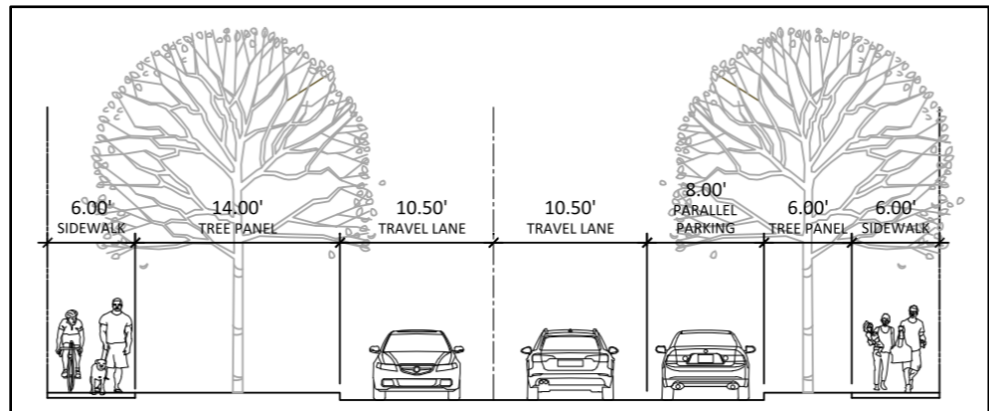


Figure 17: Proposed Cross-Section for Green Segment of Street A, as conditioned.

- Blue Segment: two (2) ten-and-a-half-foot-wide (10.5 ft) travel lanes, eight-foot-wide (8 ft) parallel street parking on both sides, and six-foot-wide (6 ft) concrete sidewalks separated from traffic by a six-foot-wide (6 ft) street buffers with trees on both sides. The total width of the street section for this segment is 67 feet (Figure 18 below).

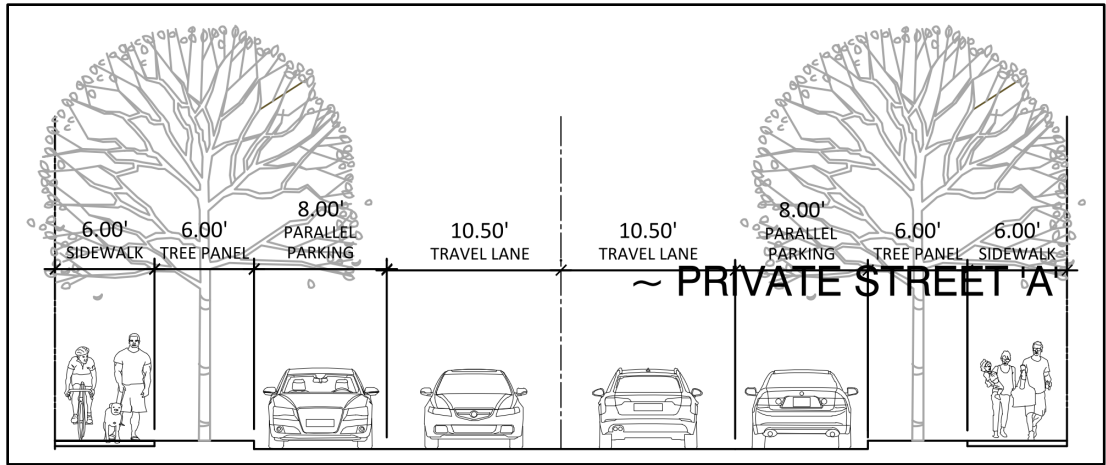


Figure 18: Proposed Cross-Section for Blue Segment of Street A, as conditioned.

Private Street B will connect Public Street A to Standish Place on the west side of the Proposed Development, and will consist of two (2) ten-and-a-half-foot-wide (10.5 ft) travel lanes with six-foot-wide (6 ft) concrete sidewalks separated from traffic by a ten-foot-wide (10 ft) street buffer with trees on both sides of the street. The total of the street section for Public Street B is 53 feet (Figure 19 below).

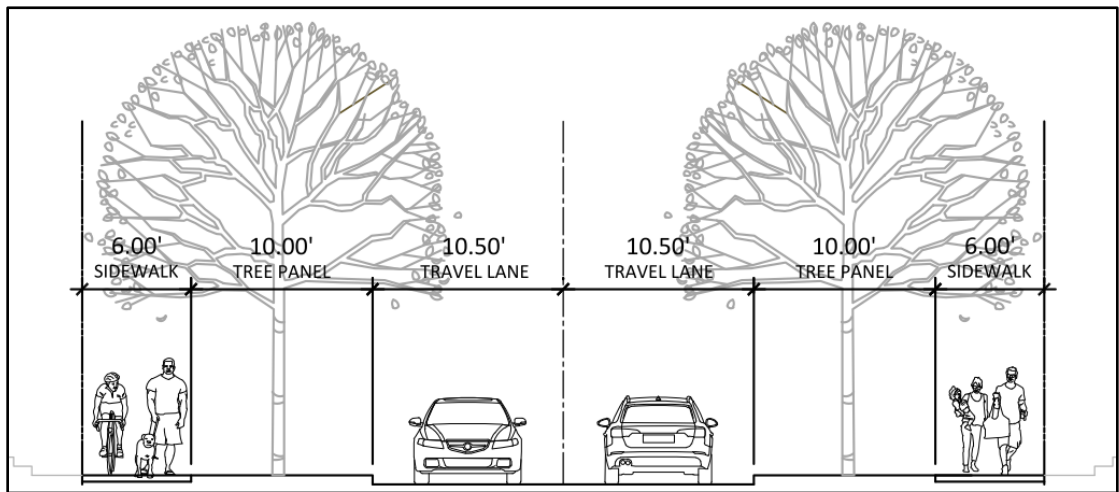


Figure 19: Proposed Cross-Section for Street B, as conditioned.

Public Street C will connect Public Street A to Standish Place to the east side of the Proposed Development, and will consist of two (2) ten and half -foot-wide (10.5 ft) travel lanes with six-foot-wide (6 ft) concrete sidewalks separated from traffic by a six-foot-wide (6 ft) street buffer with trees on both sides of the street. The total of the street section for Public Street B is 45 feet.

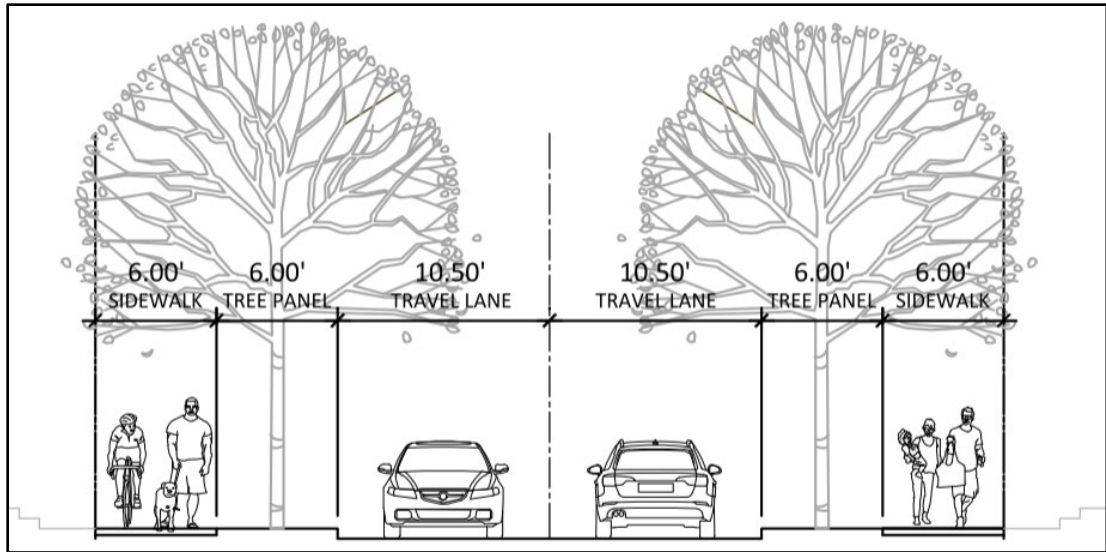


Figure 20: Proposed Cross-Section for Street C, as conditioned.

All private alleys A, B, C, D, E, F, G, H, I, J, K, L, and M will also be constructed following County standards and the CSGD, and will consist of two (2) ten-foot-wide (10 ft) travel lanes. All private alleys will provide exclusively motor vehicle access. All private alleys will also have a six-and-a-half-foot-wide (6.5 ft) separation from the driveways (Figure 21 below).

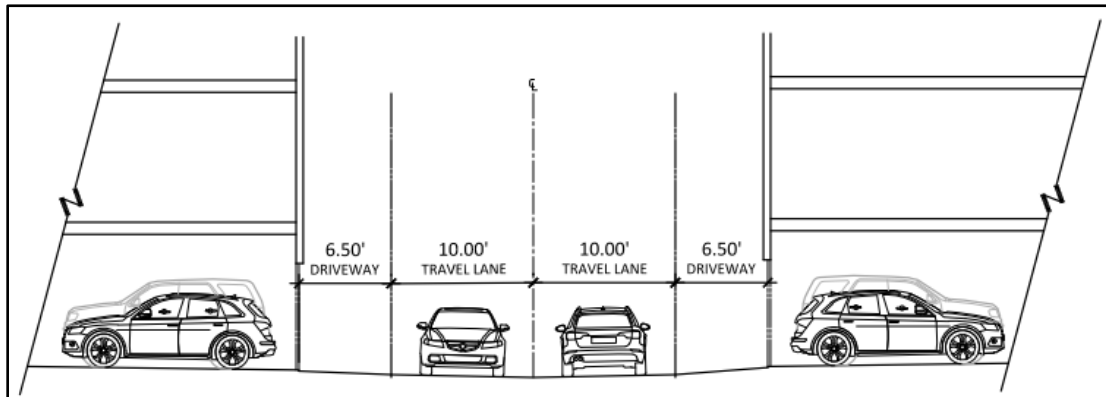


Figure 21: Typical Section for Private Alleys, as conditioned.

As conditioned, at all intersections, ADA-compliant curb ramps or at-grade-level pavement will be provided along with high-visibility continental crosswalks to ensure safe pedestrian crossing for all users, as shown in the Preliminary and Site plans.

Consistent with the CSGD and Section 49.32.f of the Montgomery County Code, at the intersections of Public Street A with Public Street B and Public Street C, mountable truck

aprons will be provided to reduce the turning radii for passenger vehicles to 15 feet, while allowing trucks and emergency vehicles to navigate entry and egress of the Private Streets (Figure 22 below). All the turning curb radii for all intersections with the private alleys will be designed to a maximum of 15 feet, as no emergency vehicles will be accessing the alleys.

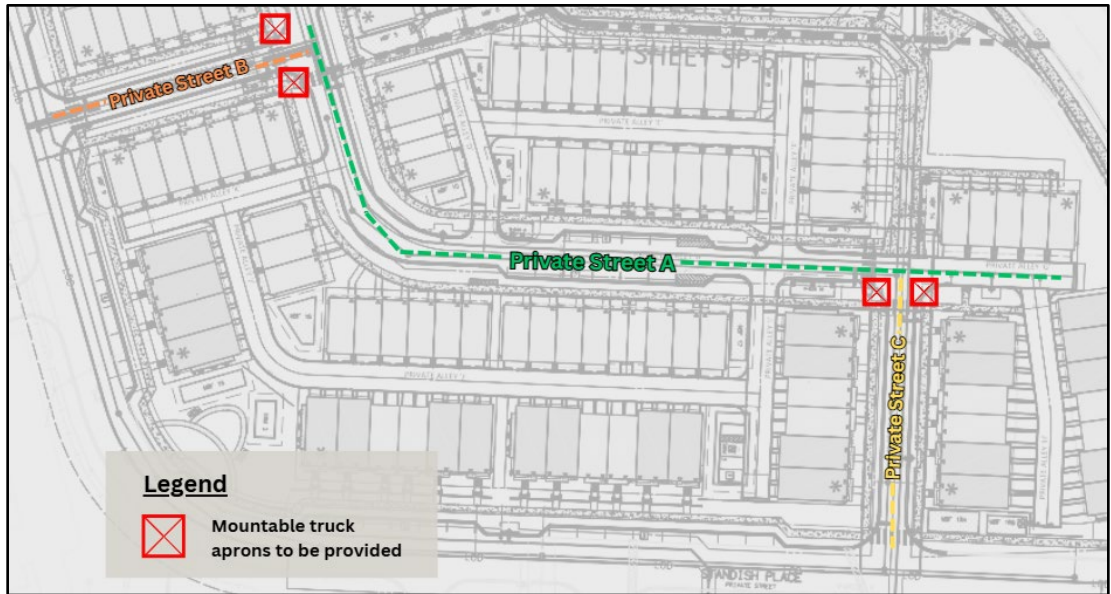


Figure 22: Mountable truck aprons at the intersections of Public Street A with Public Street B and Public Street C, as conditioned.

On the east side of Standish Place, the Applicant will install a high-visibility continental crosswalk at the exact same location where a high-visibility crosswalk is currently in place (Figure 23 below). The Applicant will also be responsible for providing an ADA-compliant curb ramp on the Subject Property, aligned with the existing receiving ramp to the opposite side of the Site, which will remain.

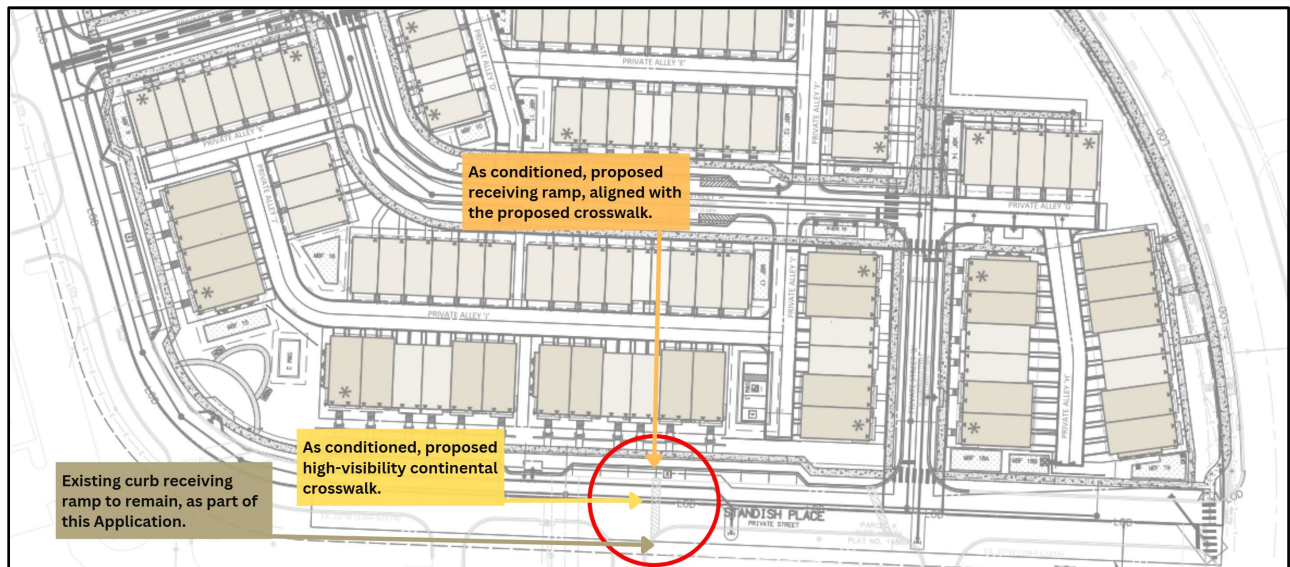


Figure 23: High-Continental Crosswalk Installation, as conditioned.

b) Local Area Transportation Review (LATR)

The previous use on the Site was a general office with 180,058 square feet of space. The proposed 200 single-family attached residential units are estimated to generate a net decrease of 169 vehicle trips in the morning peak hour and a net decrease of 146 vehicle trips in the evening peak hour. The 2024-2028 *Growth and Infrastructure Policy* (GIP) requires a transportation impact study for any project that is estimated to generate a net increase of 30 or more vehicle trips in either the morning or evening peak hours. As that is not the case for this Application, the LATR review is satisfied with a transportation exemption statement. A summary of the trip generation analysis is provided in Table 2 below.

Table 2: Trip Generation Analysis

Uses		ITE Trip Generation Vehicle Rates		Adjusted Vehicle Rates Derwood Policy Area	
		AM	PM	AM	PM
Existing	General Office (180,058 SF)	278	271	261	255

Proposed	200 Single-Family Attached Residential Units (ITE-215) ⁹	98	116	92	109
Net Change				-169	-146

Source: Transportation Exemption Statement from Lenhart Traffic Consulting, Inc., May 14, 2025.

c) Schools

The Subject Property is located within the Upper Rock Creek Policy Area, which is categorized as a Turnover Impact Area by the 2024-2028 Growth and Infrastructure Policy.

Annual School Test Results

This Application is scheduled to be considered by the Planning Board on December 11, 2025. Therefore, the results of the FY2026 Annual School Test, approved by the Planning Board on June 26, 2025 and effective since July 1, 2025, are applicable.

The Property is served by College Gardens Elementary School, Julius West Middle School, and Richard Montgomery High School. The enrollment and capacity projections of these schools reflected in the FY2026 Annual School Test, which evaluates for the 2029-2030 school year, and the default Utilization Premium Payment (UPP) tier placements are shown in Table 3.

Table 3: FY2026 Annual School Test Projections (2029-2030 School Year) & UPP Tier Placements

	Program Capacity	Enrollment	Utilization Rate	Seat Surplus or Deficit	UPP Tier Placement
College Gardens ES	702	527	75.1%	+175	No UPP
Julius West MS	1,432	1,459	101.9%	-27	No UPP
Richard Montgomery HS ⁱ	2,236	2,082	93.1%	+154	No UPP

⁹ The Project was analyzed using land use code (LUC) ITE-215 (Single-Family Attached Residential Units) to be more conservative with the trip generation. This LUC provides a higher trip generation for the Project and, therefore, better assesses the impact on the Site.

Based on the FY2026 Annual School Test results above, the property is not subject to any UPP by default. However, if the project's enrollment impact estimate exceeds an adequacy ceiling of a school to subsequent UPP tiers shown in Table 3, a partial payment will be required.

Table 4. FY2026 Annual School Test Adequacy Ceilings

	Tier 1 Adequacy Ceiling	Tier 2 Adequacy Ceiling	Tier 3 Adequacy Ceiling
College Gardens ES	249	316	421
Julius West MS	93	260	475
Richard Montgomery HS	314	602	937

Enrollment Impact Estimate

Table 5 shows the project's enrollment impact estimate calculation based on the School Impact Area classification and net residential units proposed, using the FY2026-2027 Student Generation Rates.

Table 5. Student Enrollment Impact Estimate (reflects FY2026-2027 Student Generation Rates)

Type of Unit	Net Number of Units	Turnover ES Student Generation Rate	ES Student Estimate	Turnover MS Student Generation Rate	MS Student Estimate	Turnover HS Student Generation Rate	HS Student Estimate
SF Attached	100	x 0.207	= 20.700	x 0.113	= 11.300	x 0.166	= 16.600
MF Low-rise	100	x 0.123	= 12.300	x 0.064	= 6.400	x 0.083	= 8.300
TOTAL (rounded down)			33		17		24

The enrollment impact of the proposed units during an average year throughout the life of this project is estimated to be 33 elementary school students, 17 middle school students, and 24 high school students. This does not exceed the adequacy ceilings identified in Table 4. Therefore, a Utilization Premium Payment is not required for this Application.

d) Other Public Facilities and Services

The Subject Property is in water category W-1 and sewer category S-1, respectively. The use of public water and sewer service is consistent with the current categories. The existing water and sewer lines serving the office building will be abandoned. New water and sewer lines will be extended from the existing mains along Standish Place and Crabbs Branch to serve the individual units and supply a new fire hydrant along Standish Place. Similarly, underground electrical, telecommunications services and natural gas will be extended to serve the Property. Services will be provided within the private road parcels or along the front of each stick of dwelling units. Utility easements are provided for each service as necessary.

The Application was reviewed by the MCDPS, Fire Department Access and Water Supply Section, and a Fire Access Plan was approved on October 20, 2025 (Attachment C). The Fire Department Access Plan provides fire code compliant access from Standish Place to the proposed residential structures.

Other public facilities and services, such as police stations, firehouses and health services are currently operating within the standards set by Growth and Infrastructure Policy currently in effect.

4. All Forest Conservation Law, Chapter 22A requirements are satisfied.

The Application satisfies all of the applicable requirements of the Forest Conservation Law, Montgomery County Code, Chapter 22A and is in compliance with the Montgomery County Planning Department's Environmental Guidelines as further discussed in the findings for Forest Conservation Plan F20250010.

5. All stormwater management, water quality plan, and floodplain requirements of Chapter 19 are satisfied.

The Preliminary Plan Application meets the stormwater management requirements of Chapter 19 of the County Code. MCDPS Water Resources Section approved a Combined Stormwater Management Concept/ Site Development Stormwater Management Plan on October 30, 2025 (Attachment C). The plan proposes to meet required stormwater management goals using micro-bioretenion facilities and underground structural filtration. A partial waiver from stormwater management has been granted by DPS due to site constraints.

6. Any burial site of which the applicant has actual notice or constructive notice or that is included in the Montgomery County Cemetery Inventory and located within the subdivision

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boundary is approved under Subsection 50-4.3.M.

Not applicable to this Property.

7. *Any other applicable provisions specific to the property and necessary for approval of the subdivision is satisfied.*

The Applicant meets the justification requirements for private streets under Section 4.3.D.4.b and conforms to the standards for private streets as outlined in Section 4.3.D.4.c. The Applicant opted to construct private streets and alleys because the proposed street grid does not comply with the Montgomery County standards MC-2002.01 and MC-2002.02, which comprise requirements for centerline radii and travel lane widths. However, to meet compliance with the aforementioned Sections of the Montgomery County Code, the three (3) private streets and alleys will be constructed following modified County Standards and the design parameters for Neighborhood Streets under the CSDG.

SECTION 7: SITE PLAN 820250090 FINDINGS AND ANALYSIS

The CRNF zone development standards for the Project are governed by the floating zone requirements set forth in Section 59-5.3.5 (Commercial/Residential Floating, General Requirements). More specifically, Section 59-5.3.5.B.2 (Commercial/Residential Floating, Setback and Height) provides that “setbacks from the site boundary and maximum height are established by the floating zone plan. All other setbacks are established by the site plan approval process under Section 7.3.4 (Administration and Procedures, Site Plan).” Therefore, an approved site plan is required for the Project.

1. *When reviewing an application, the approval findings apply only to the site covered by the application.*

The findings herein apply only to the Subject Property.

2. *To approve a Site Plan, the Planning Board must find that the proposed development:*

a) satisfies any previous approval that applies to the site;

This Application is under the Standard Method of Development of the current CRNF Zone. The residential development will comply with all applicable conditions of approval and binding elements related to the LMA (including the Floating Zone Plan), which rezoned the

Property to the CRNF Zone, and the concurrently reviewed Preliminary Plan.

b) satisfies under Section 7.7.1.B.5 the binding elements of any development plan or schematic development plan in effect on October 29, 2014;

This section is not applicable as there are no binding elements of an associated development plan or schematic development plan in effect on October 29, 2014.

c) satisfies under Section 7.7.1.B.5 any green area requirement in effect on October 29, 2014 for a property where the zoning classification on October 29, 2014 was the result of a Local Map Amendment;

This section is not applicable as the Subject Property's zoning classification on October 29, 2014 was not the result of a Local Map Amendment.

d) satisfies applicable use standards, development standards, and general requirements under this Chapter;

i. Division 4.5 Commercial/Residential Zones

Table 6: Centro Square Site Plan Data Table for CRNF Zone, Standard Method, Section 59.4.5

PROJECT DATA TABLE		
Address	7501 STANDISH PLACE	
Property Description	Parcel D, Plat No. 13905	
Zoning:	CRNF-1.25 C-0.0 R-1.25 H-60	
GROSS TRACT AREA	SF	Acres
Tract Area	603,855 SF	13.86 AC
Previous R.O.W. Dedications	49,420 SF	1.13 AC
Proposed R.O.W. Dedications		0.00 AC
Proposed Site Area	554,435 SF	12.73 AC
PROPOSED USES		
Townhouse - Residential Market Rate	100 du	
Townhouse - Residential MPDU (27%)	8 du	
Two-over-twos (multi-family) - Market Rate	70 du	
Two-over-twos (multi-family) MPDU (73%)	22 du	
Total Units	200 du	
Total MPDU (15%)	30 du	
DEVELOPMENT STANDARD	Permitted / Required	Provided

Maximum Density				
CRNF-1.25 Zone (CRNF-1.25 C-0.00 R-1.25 H-60)	FAR	SF	FAR (up to)	SF (up to)
Commercial	0	0	0.00	.0 SF
Residential	1.25	693,044 SF	1.22	675,264 SF
Principal Building Setbacks				
From Site Boundary - Crabbs Branch Way	10'		10'	
From Site Boundary - Private Standish Place Centerline	30'		30'	
Front Setback	Est. by Site Plan		3'	
Side Street Setback	Est. by Site Plan		2'	
Side Setback	Est. by Site Plan		2'	
Rear Setback, Alley	Est. by Site Plan		3'	
Accessory Structure Setbacks	Est. by Site Plan		0'	
Maximum Building Height	60'		up to 60'	
Minimum Lot Size	Est. by Site Plan		780 SF	
Maximum Lot Coverage	Est. by Site Plan		75%	
Minimum Public Open Space, see Open Space Plan for Breakdown by Type	25,504 SF		45,319 SF	
Minimum Common Open Space, see Open Space Plan for Breakdown by Type	29,940 SF		33,560 SF	
PARKING	Required		Provided	
Townhouse (1 per unit min) (1 Stall Per DU)	108 min		Off-Street Resident Parking	208 spaces
Two-over-twos (multi-family) (1 per unit min)	92 min			184 spaces
On-Street Visitor Parking (For Townhomes and Multi-family)				49 spaces
Total Parking (1 per unit min)	200 min		441 spaces	
Open Space Calculations				
		%	Required	Proposed
Site Area (Combined Common and Public Open Space)	554,435 SF	10%	55,444 SF	55,444 SF
Multifamily Public Open Space (46%)		46%	25,504 SF	25,504 SF
Townhouse Common Open Space (54%)		54%	29,940 SF	29,940 SF

ii. **Division 59-6 General Development Standards**

Centro Square

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(1) Division 6.1 Site Access

As previously discussed, the Site will have vehicular access through three (3) curb cuts along Standish Place, which is a privately-owned and maintained street. Standish Place is bounded and served by Crabbs Branch Way, a County-owned and maintained street. Internally, the Applicant is proposing vehicular access via a series of private streets and alleys. As part of this Application, three (3) private streets and twelve (12) private alleys will be constructed, following County Standards and the Complete Streets Design Guide (CSDG). The Private Streets and Alleys will consist of two (2) travel lanes.

All private streets will follow the design parameters for a Neighborhood Street classification, including six (6)-foot-wide concrete sidewalks on both sides of the streets, separated from traffic by street buffers with trees, as described in the findings section of the Preliminary Plan. This will allow safe and efficient pedestrian circulation into the Site.

The internal pedestrian network will be connected to the external pedestrian network through the asphalt sidepath to be constructed as part of this Application. The asphalt sidepath will have a variable pavement width and be separated from traffic by a variable-width street buffer, as described in the findings section of the Preliminary Plan.

The Site has access to public transit. There are six (6) bus stops that bound the Site and will provide future residents with bus service to nearby commercial and residential areas. The bus system will also allow access to the Shady Grove Metro Station. Similarly, this Metro Station is within a mile walking distance from the Site.

(2) Division 6.2 Parking, Queuing and Loading

Under Section 6.2.4. of the Montgomery County Code, residential developments with single and townhouse living are required to provide a minimum of one (1) and a maximum of two (2) parking spaces per unit. As part of this Project, the Applicant is proposing a total number of 441 vehicle parking spaces. The provided number of vehicle parking spaces exceeds the 200 minimum requirement for single and townhouse living developments. As such, no additional parking is required. Detailed numbers are discussed in *Table 6* above.

(3) Division 6.3 Open Space and Recreation

The proposed open space design provides Common Open Space (townhouses) and Public Open Space (two-over-twos/apartment building) to meet both the quantitative and qualitative requirements of the Zoning Ordinance.

Three primary areas of Open Space are proposed: (1) a Public Open Space described as a “central park” space will be located adjacent to the intersection of Crabbs Branch Way and Monona Drive, (2) two Common Open Space areas located along Standish Place to the south, and (3) a northern and southern Common Open Space fronting Crabbs Branch Way and framing the “central park.” Each Common Open Space area exceeds the minimum 2,000-square-foot requirement and the required width of 50 feet, providing residents and visitors with recreational lawns, walkways, and seating areas.

The Public Open Space provided near the central portion of the Site, directly abutting the Crabbs Branch Way, will be accessible from the public sidewalk. This public amenity will include a playground, shade structures, and pedestrian seating, designed to enhance the development's integration with the broader Derwood neighborhood. The entire open space network is connected via a continuous pedestrian pathway system linking every open space area throughout the site.

Table 7: Proposed Onsite Recreation Facilities and their Supply Points

Recreation Facility	Quantity	% Bonus Points	Tots	Children	Teens	Young Adults	Adults	Seniors
Pedestrian Connection - Trail System	1	0%	2.98	8.98	5.08	18.38	18.45	1.92
Open Grass Area Lawn - Small (5,000 sf)	2	0%	6	8	18	18	18	6
Open Grass Area Lawn - Large (10,000 sf)	1	10%	6.6	9.9	16.5	22	16.5	6.6
Playground (Age 2-12) [Multi-age Play]	1	10%	9.9	12.1	3.3	2.2	4.4	2.2
Urban Plaza	1	10%	4.4	6.6	6.6	19.8	16.5	6.6
Picnic/Seating	12	0%	12	12	18	36	36	36
Outdoor Game Station	2	0%	0	0	6	6	4	4
Outdoor Fitness Station	2	10%	0	0	6.6	6.6	4.4	4.4
Total Onsite Supply Points=			41.88	57.58	80.08	128.98	118.25	67.72

Table 8: Results: Demand, Supply & Adequacy

Age Group	Total Demand Points	Offsite Supply Points	Onsite Supply Points	Total Supply Points	Adequacy
Tots	29.84	1.57	41.88	43.45	Adequate
Children	44.92	4.72	57.58	62.3	Adequate
Teens	33.84	3.55	80.08	83.63	Adequate
Young Adults	122.56	12.87	128.98	141.85	Adequate
Adults	123	12.92	118.25	131.17	Adequate
Seniors	19.24	1.35	67.72	69.07	Adequate

(4) Division 6.4 General Landscaping and Outdoor lighting

The Site Plan meets the standards for the provision of landscaping and outdoor lighting as required by Division 6.4. The Application includes a variety of new landscaping and lighting throughout the Property. Street trees are provided throughout the Site to enhance the pedestrian environment, provide shade, and create an aesthetically pleasing presence on the roads.

The Applicant will plant a variety of native shade trees, such as yellowwoods, sweet gums, and oaks. These shade trees will be planted throughout the site and along roadways. Shrubs and ground cover will also be planted in planting beds throughout the Site.

Lighting on-site consists of private streetlight fixtures and poles, path lights, and decorative pole lights. Light poles will be installed throughout the site to illuminate the pedestrian and vehicular circulation environment. The streetlights are decorative full-cutoff fixtures mounted on a 12-foot-tall pole. The proposed fixtures will provide illumination and be visually appealing. The photometric plan submitted by the Applicant shows that the proposed lighting package will adequately illuminate the site, creating a safe environment, without creating light spillage or excessive glare on adjacent properties or the rights-of-way. All site lighting provides adequate, safe, and efficient illumination.

(5) Division 6.5 Screening

The Applicant is not required to provide screening for the townhouses or apartment buildings because the Property does not directly abut a property in the Agricultural, Rural Residential, or Residential zone that is vacant or improved with a residential use.

e) Satisfies the applicable requirements of:

i. Chapter 19, Erosion, Sediment Control, and Stormwater Management; and

MCDPS Water Resources Section approved a Combined Stormwater Management Concept/ Site Development Stormwater Management Plan on October 30, 2025 (Attachment C) as part of Preliminary Plan No. 120250180. The plan proposes to meet required stormwater management goals using micro-bioretenion facilities and underground structural filtration. A partial waiver from stormwater management has been granted by DPS due to site constraints.

ii. Chapter 22A, Forest Conservation

The Application satisfies all of the applicable requirements of the Forest Conservation Law, Montgomery County Code, Chapter 22A, and is in compliance with the Montgomery County Planning Department's Environmental Guidelines, as further discussed in the findings for Forest Conservation Plan F20250010.

f) Provides safe, well-integrated parking, circulation patterns, building massing, and, where required, open spaces and site amenities;

This development features a mix of rear-loaded townhomes and rear-loaded two-over-two units, where the parking and driveways are hidden in the alleys away from the public realm.

All residential units in this development are designed as four-story dwelling units with a maximum building height of 60 feet, allowing for the architectural articulation of roof forms and the transition between the grade and the finished first-floor elevation.

g) Substantially conforms with the recommendations of the applicable master plan and any guidelines approved by the Planning Board that implement the applicable plan;

The Property is in substantial conformance with the goals and recommendations

contained in the Master Plan and associated Design Guidelines.

- h) will be served by adequate public services and facilities, including schools, police and fire protection, water, sanitary sewer, public roads, storm drainage, and other public facilities. If an approved adequate public facilities test is currently valid and the impact of the development is equal to or less than what was approved, a new adequate public facilities test is not required. If an adequate public facilities test is required the Planning Board must find that the proposed development will be served by adequate public services and facilities, including schools, police and fire protection, water, sanitary sewer, public roads, and storm drainage;***

As discussed in the accompanying Preliminary Plan No. 120150180 findings, the proposed development will be served by adequate public facilities, including schools, police and fire protection, water, sanitary sewer, public roads, storm drainage, and other public facilities.

- i) on a property in a Rural Residential or Residential zone, is compatible with the character of the residential neighborhood; and***

The Property is zoned CRNF. Thus, this provision is not applicable.

- j) on a property in all other zones, is compatible with existing, approved, or pending adjacent development.***

Given the site's proximity to major roadways such as Frederick Road and E. Gude Drive, this development will create more housing opportunities by transforming the large, underutilized, auto-centric gray field office park site into a mixed housing development within the 2021 Shady Grove Sector Plan Minor Master Plan ("Master Plan" or "Sector Plan") area. This development will further expand the residential neighborhood along the main roadway, adjacent to existing townhouses and single-family homes to the north.

A network of private neighborhood streets, alleys, and pedestrian connections, centered around a new central park and multiple publicly accessible open spaces, creates a well-designed, pedestrian-oriented community.

The proposed development has been designed as a thoughtfully integrated, walkable residential community that transitions from the industrial legacy of the Standish Place corridor to a vibrant neighborhood consistent with emerging planning goals.

This development features smaller, rear-loaded blocks of various housing types within the community, providing narrower residential streets to serve 200 residential units, organized into a mix of rear-loaded townhouses and rear-loaded two-over-two units. Additionally, it provides a much-needed pedestrian connection from Crabb Branch Way to Standish Place.

3. ***To approve a Site Plan for a Restaurant with a Drive-Thru, the Planning Board must also find that a need exists for the proposed use due to an insufficient number of similar uses presently serving existing population concentrations in the County, and the uses at the location proposed will not result in a multiplicity or saturation of similar uses in the same general neighborhood.***

Not applicable, this Site Plan does not include a restaurant with a drive-thru.

4. ***For a property zoned C-1 or C-2 on October 29, 2014 that has not been rezoned by Sectional Map Amendment or Local Map Amendment after October 30, 2014, if the proposed development includes less gross floor area for Retail/Service Establishment uses than the existing development, the Planning Board must consider if the decrease in gross floor area will have an adverse impact on the surrounding area.***

Not applicable.

SECTION 8: FOREST CONSERVATION PLAN F20250010 FINDINGS AND ANALYSIS

All Forest Conservation Law, Chapter 22A requirements are satisfied.

Final Forest Conservation Plan No. F20250010 has been submitted concurrently with Preliminary Plan No. 120250180 and Site Plan No. 820250090. The Application satisfies all the applicable requirements of the Forest Conservation Law, Montgomery County Code, Chapter 22A and is in compliance with the Montgomery County Planning Department's Environmental Guidelines. Final Forest Conservation Plan No. F20250010 is consistent with the Preliminary Forest Conservation Plan (PFCP No. F20250010) and meets all requirements. The forest conservation requirements will be met by planting 1.98 acres in an M-NCPPC-approved off-site forest bank within the Middle Rock Creek watershed or Priority Area to satisfy the afforestation requirement of 1.98 acres of mitigation credit. If no off-site forest banks exist within the Middle Rock Creek watershed or Priority Area, then the off-site requirement may be met by purchasing 1.98 acres of mitigation credits from a mitigation bank within Montgomery County.

outside of the Middle Rock Creek watershed or Priority Area. If forest mitigation bank credits are not available for purchase, a fee-in-lieu payment must be made to M-NCPPC for the appropriate mitigation credits outside of the same watershed or Priority Area. The FFCP includes 48 inches of variance mitigation plantings as required by the approved PFCP.

SECTION 9: CONCLUSION

As conditioned, the Preliminary Plan, Site Plan, and Forest Conservation Plan applications each satisfy the applicable standards of the Zoning Ordinance, Subdivision Regulations, Forest Conservation Law and substantially conform to the recommendations of the Master Plan. Therefore, Staff recommends approval of the Preliminary Plan, Site Plan, and Forest Conservation Plan with the conditions specified at the beginning of this report.

ATTACHMENTS

Attachment A: Preliminary Plan, Site Plan and Forest Conservation Plan

Attachment B: Prior Approvals

Attachment C: Agency Letters

Attachment D: Affidavit Regarding Community Meeting and Meeting Minutes
